

09/28/09

MAPS N.T.S.

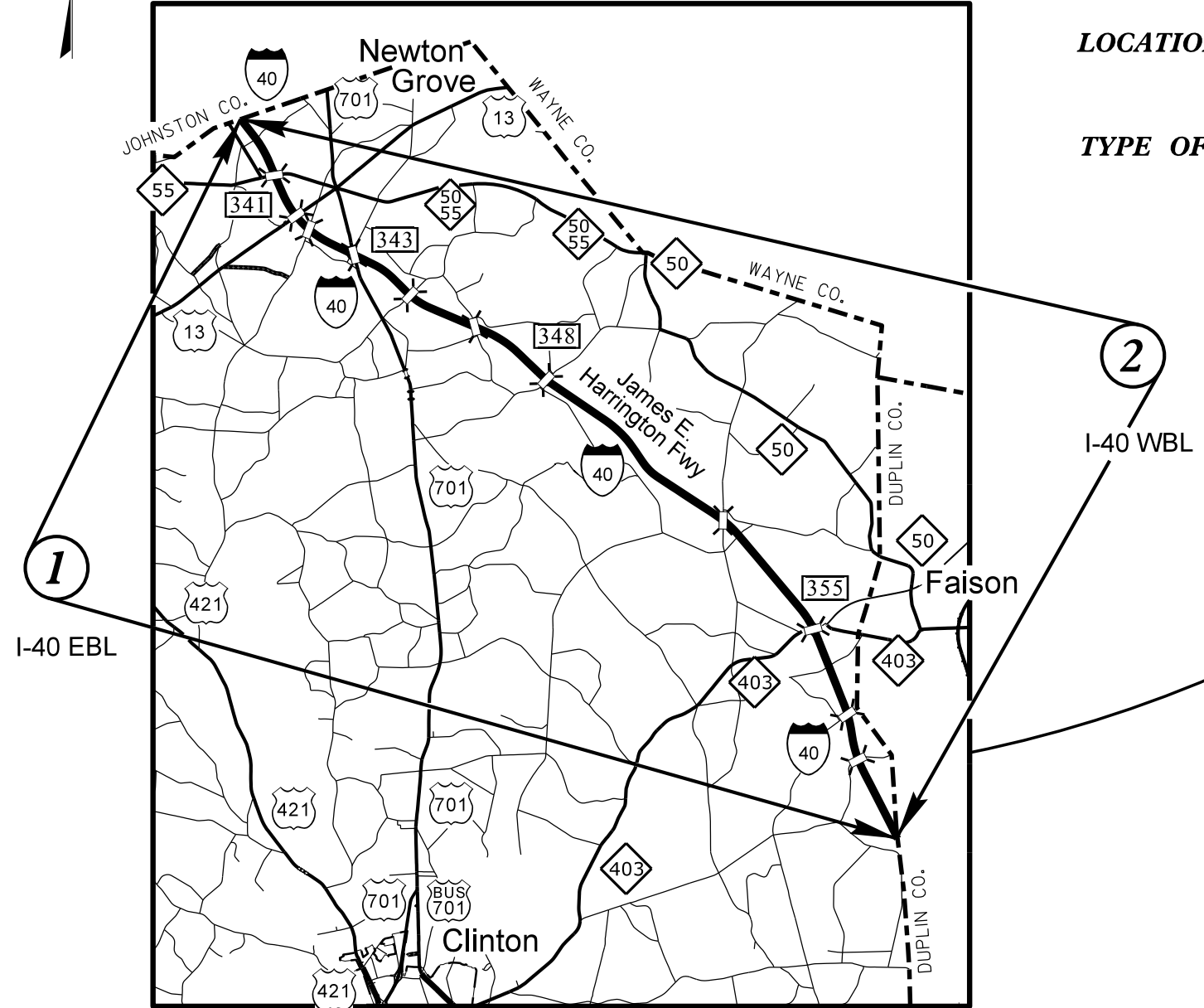
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

SAMPSON COUNTY

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5780	1	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
53021.1.1		P.E.	
53021.3.1	NHFP-0040(072)	CONST.	

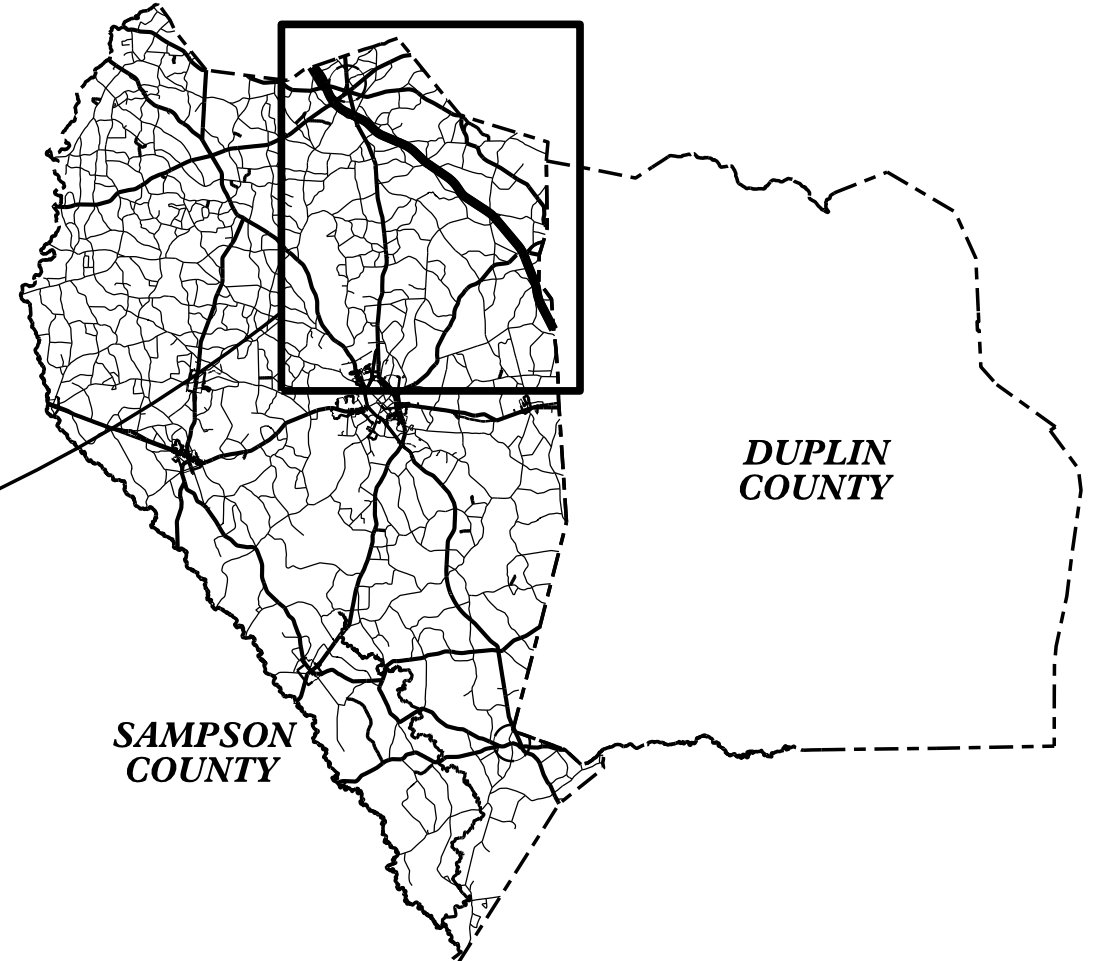
TIP PROJECT: I-5780

CONTRACT: C204153



LOCATION: I-40 EBL MM 339.76 (JOHNSTON CO.) TO MM 359.89 (DUPLIN CO.)
I-40 WBL MM 359.89 (DUPLIN CO.) TO MM 339.76 (JOHNSTON CO.)

TYPE OF WORK: PAVEMENT REHABILITATION, DRAINAGE & GUARDRAIL REPLACEMENT



04-DEC-2018 14:45 S:\Division_Resurfacing\Interstate_Rehab\2019_I-40_Resurfacing_I-5780\RALEIGH_REVISIONS\I-5780_53021.1.1-I-40_Rdy_tsh_REV.dgn \$\$\$USERNAME\$\$\$

PROJECT LENGTH

MAP 1: I-40 (EB) &
MAP 2: I-40 (WB) = 20.13 MI.

TOTAL LENGTH OF PROJECT = 20.13 MI.

Prepared In the Office of:
DIVISION OF HIGHWAYS
5501 Barbados Blvd., Castle Hayne, NC, 28429

2018 STANDARD SPECIFICATIONS

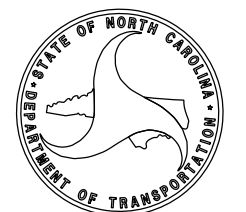
RIGHT OF WAY DATE:

LYDIA MCKEEL, PE
PROJECT ENGINEER

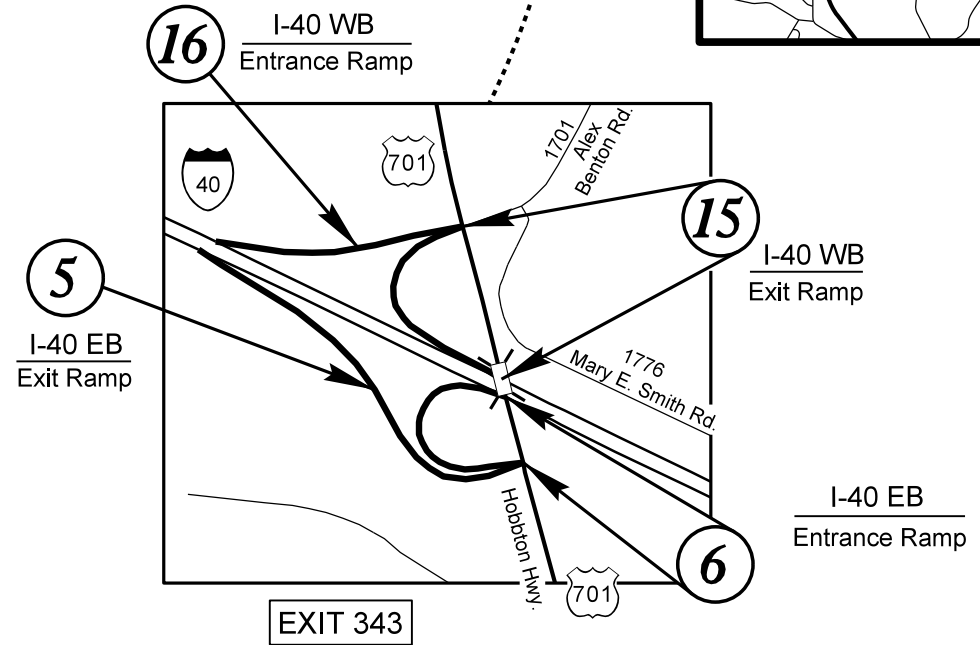
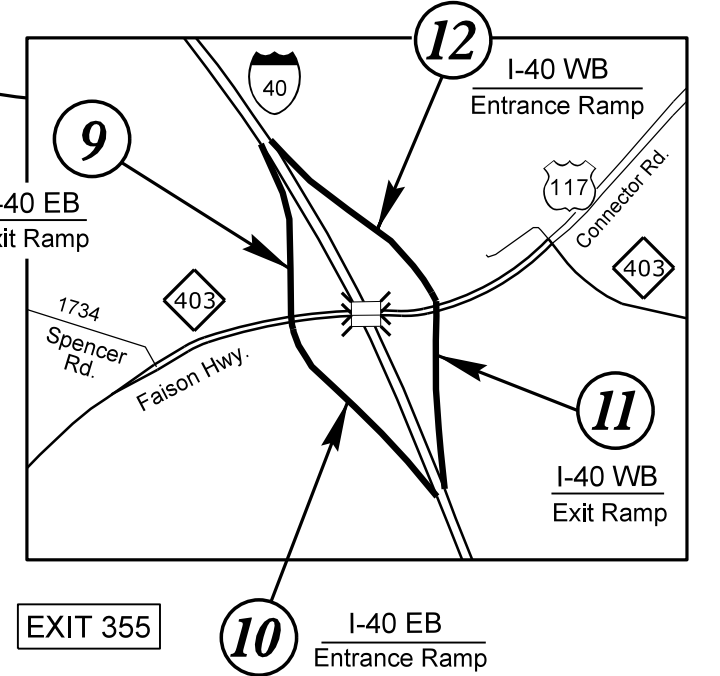
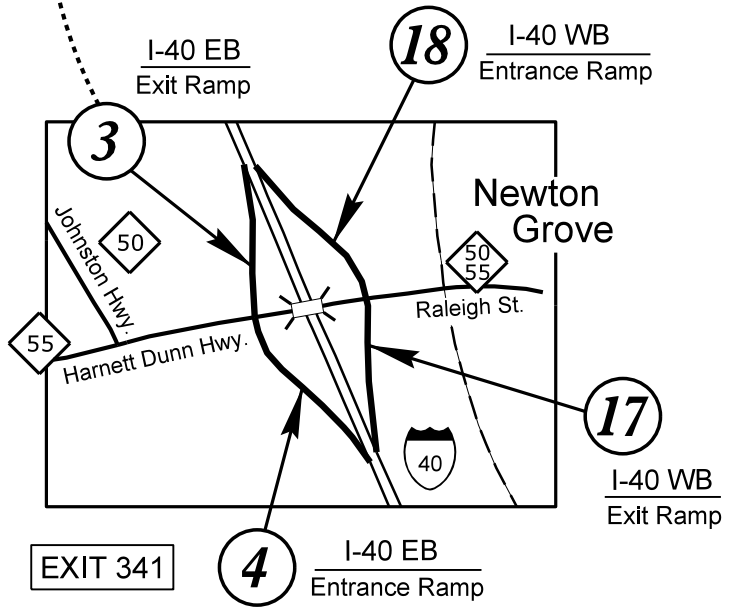
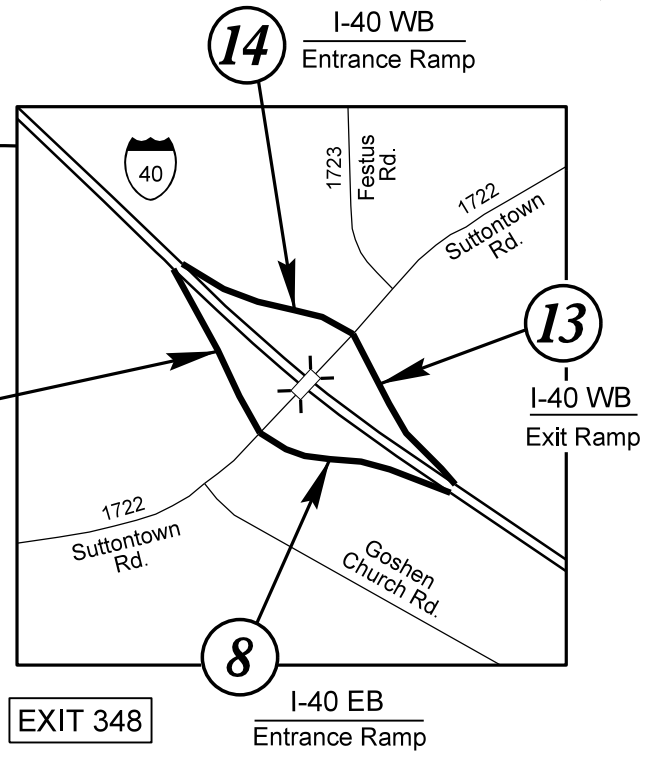
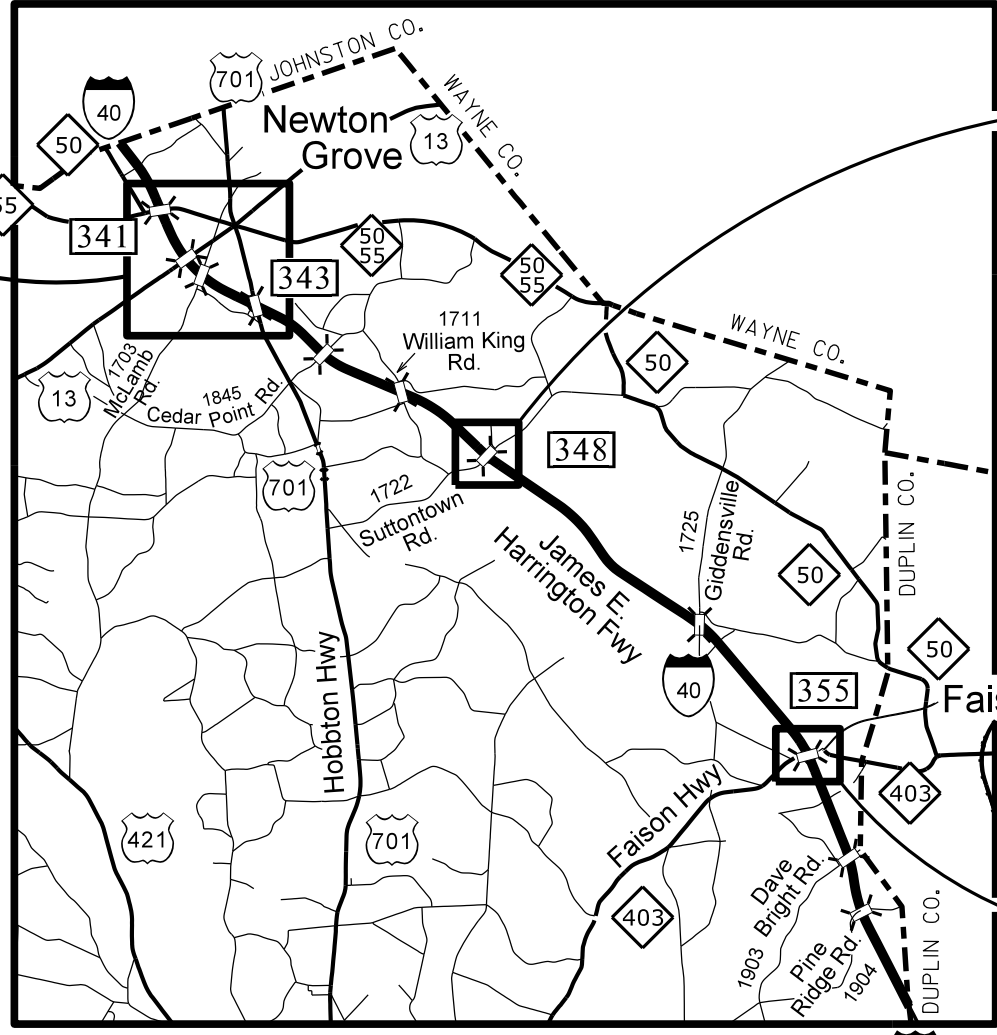
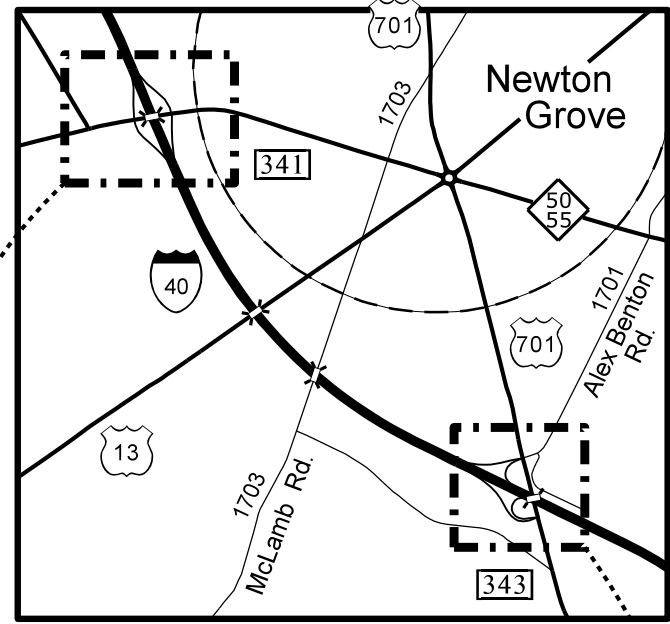
LETTING DATE:
FEBRUARY 19, 2019

MARY KIMMEL
PROJECT DESIGN TECHNICIAN

KEVIN BRADSHAW
PROJECT DESIGN TECHNICIAN

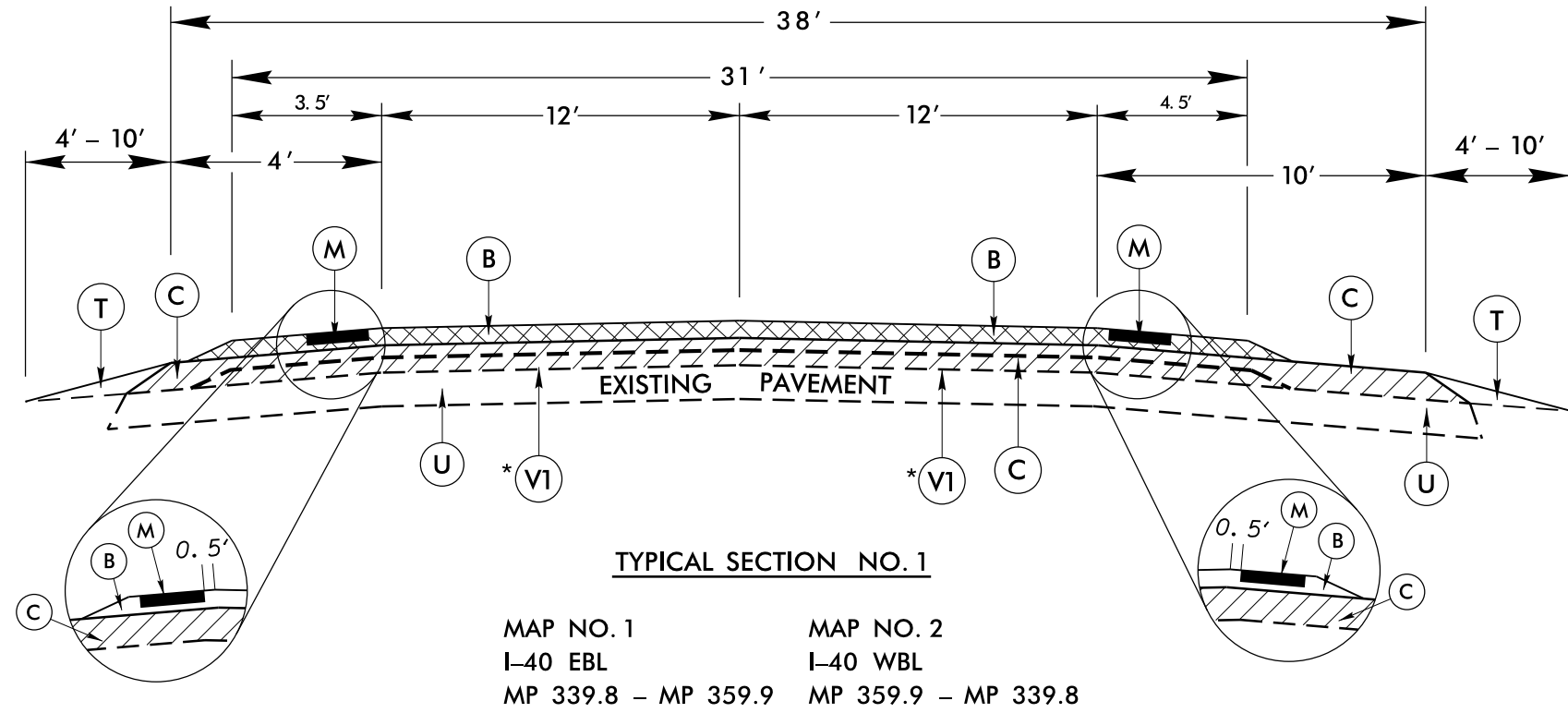


I-40 RAMPS - SAMPSON COUNTY



MAPS N.T.S.

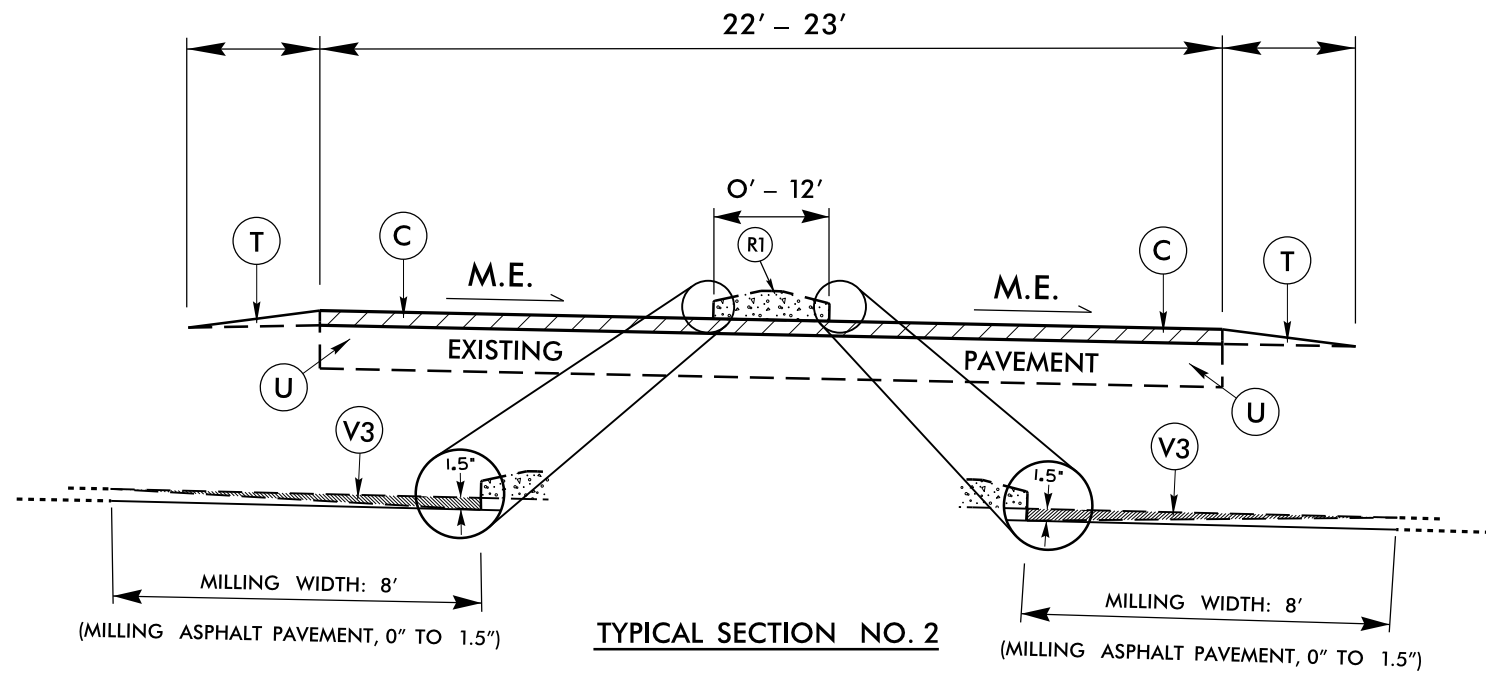
07-NOV-2018 11:20 S:\Division Resurfacing\Winterstate Rehab\2019\I-40 Resurfacing_I-5780\1-5780_53021.dwg I-40 Resurfacing_I-5780_53021.dwg



NOTES: ALL MILLED SURFACES MUST BE RESURFACED BY THE END OF EACH WORK DAY.

* **V1** MILL 7/8" TO 1 1/2" TO REMOVE EXISTING OGAF C

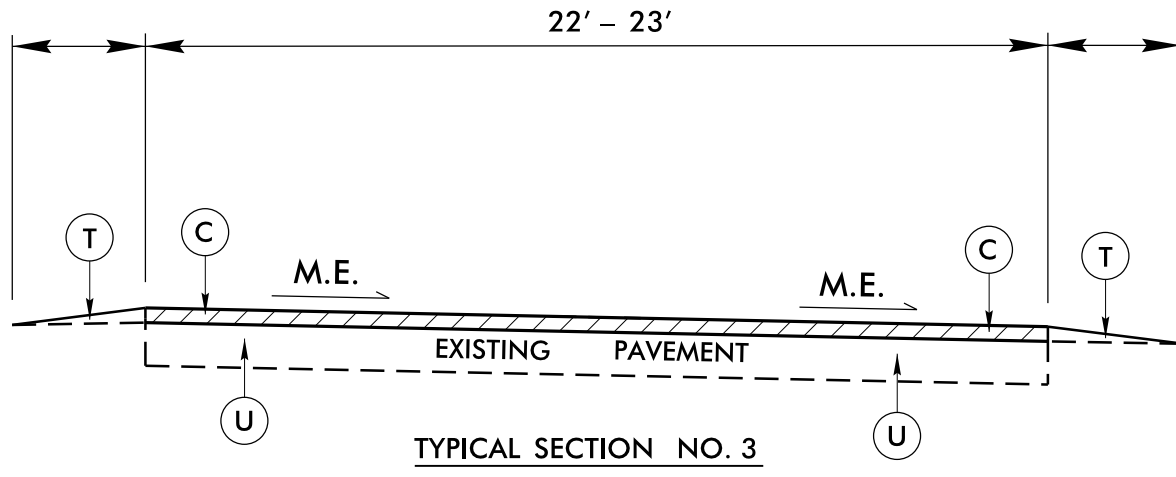
V2 MILL 2" TO 3" IN BRIDGE OVERPASS AREAS (SEE MILLING FOR VERTICAL CLEARANCE DETAIL)



PAVEMENT SCHEDULE	
B	PROP. APPROX. 3/4" OPEN-GRADED ASPHALT FRICTION COURSE, TYPE FC-1 MODIFIED, AT AN AVERAGE RATE OF 90 LBS. PER SQ.YD.
C	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ.YD.
M	MILLED RUMBLE STRIP
R1	EXISTING CONCRETE MONOLITHIC ISLAND
R2	EXISTING CONCRETE 2'-0" CURB & GUTTER
T	AGGREGATE SHOULDER BORROW (ASB) SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING ASPHALT PAVEMENT 7/8" TO 1 1/2" DEPTH
V2	MILLING ASPHALT PAVEMENT 2" TO 3" DEPTH
V3	MILLING ASPHALT PAVEMENT 0" TO 1 1/2" DEPTH

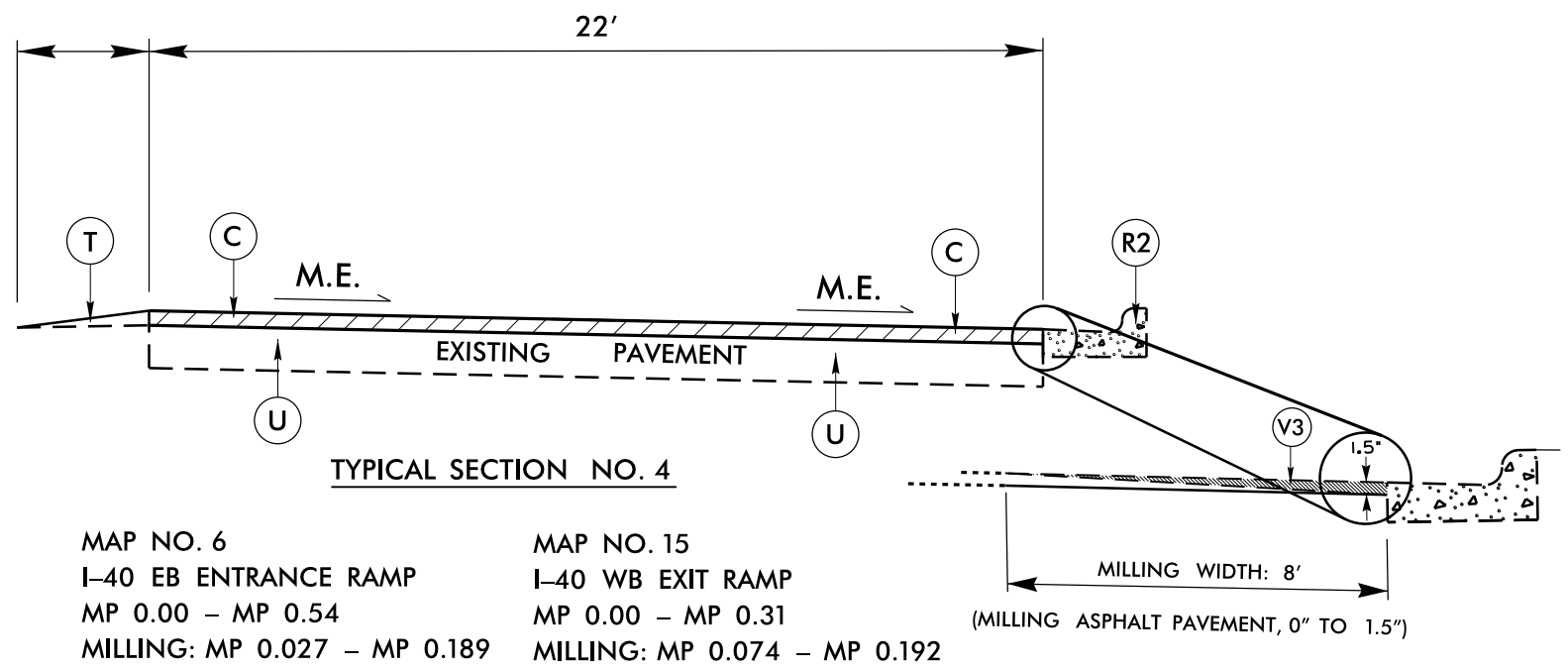
PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

REVISIONS
 07-DEC-2018 10:55 S:\Division\Resurfacing\Interstate Rehab\2019\I-40 Resurfacing\I-5780\RALEIGH REVISIONS\I-5780_53021.i.l.I-40_Rdy_tup_REV.dgn
 \$\$\$\$\$\$\$\$\$\$



TYPICAL SECTION NO. 3

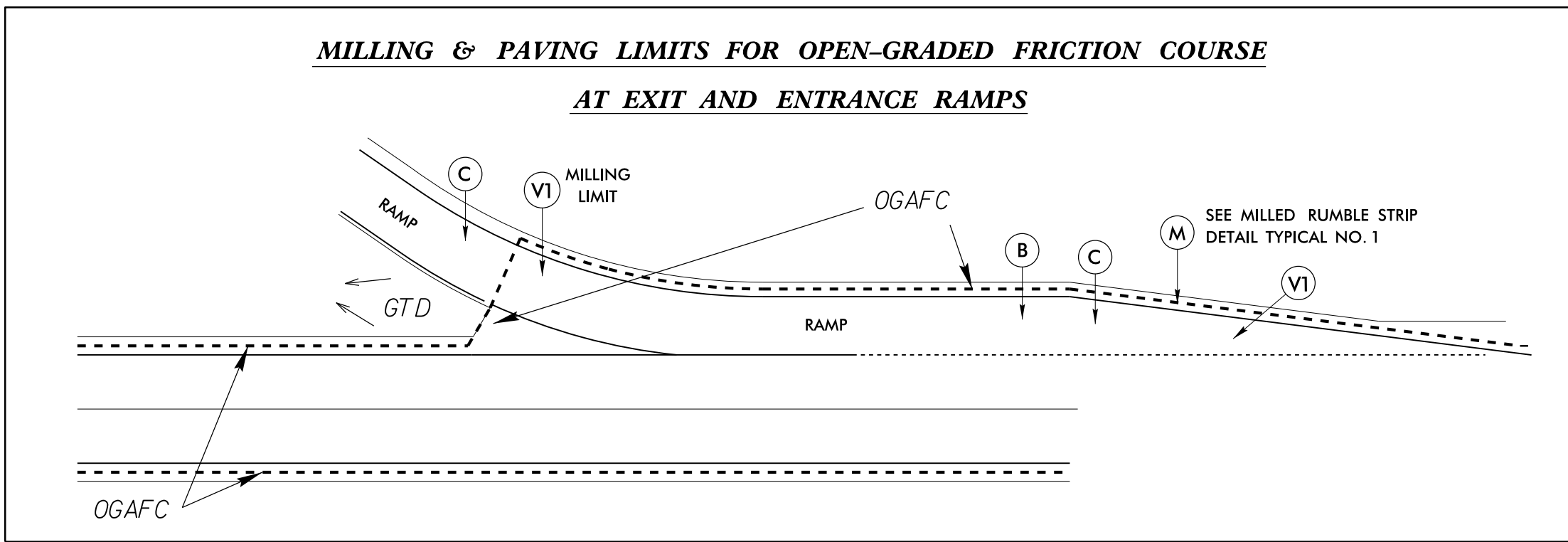
- | | | |
|---|--|--|
| MAP NO. 4
I-40 EB ENTRANCE RAMP
MP 0.00 - MP 0.43 | MAP NO. 8
I-40 EB ENTRANCE RAMP
MP 0.00 - MP 0.49 | MAP NO. 13
I-40 WB EXIT RAMP
MP 0.00 - MP 0.38 |
| MAP NO. 5
I-40 EB EXIT RAMP
MP 0.00 - MP 0.45 | MAP NO. 10
I-40 EB ENTRANCE RAMP
MP 0.00 - MP 0.51 | MAP NO. 14
I-40 WB ENTRANCE RAMP
MP 0.00 - MP 0.44 |
| MAP NO. 7
I-40 EB EXIT RAMP
MP 0.00 - MP 0.40 | MAP NO. 12
I-40 WB ENTRANCE RAMP
MP 0.00 - MP 0.50 | MAP NO. 16
I-40 WB ENTRANCE RAMP
MP 0.00 - MP 0.48 |
| | | MAP NO. 18
I-40 WB ENTRANCE RAMP
MP 0.00 - MP 0.43 |



TYPICAL SECTION NO. 4

- | | |
|---|--|
| MAP NO. 6
I-40 EB ENTRANCE RAMP
MP 0.00 - MP 0.54
MILLING: MP 0.027 - MP 0.189 | MAP NO. 15
I-40 WB EXIT RAMP
MP 0.00 - MP 0.31
MILLING: MP 0.074 - MP 0.192 |
|---|--|

**MILLING & PAVING LIMITS FOR OPEN-GRADED FRICTION COURSE
AT EXIT AND ENTRANCE RAMPS**



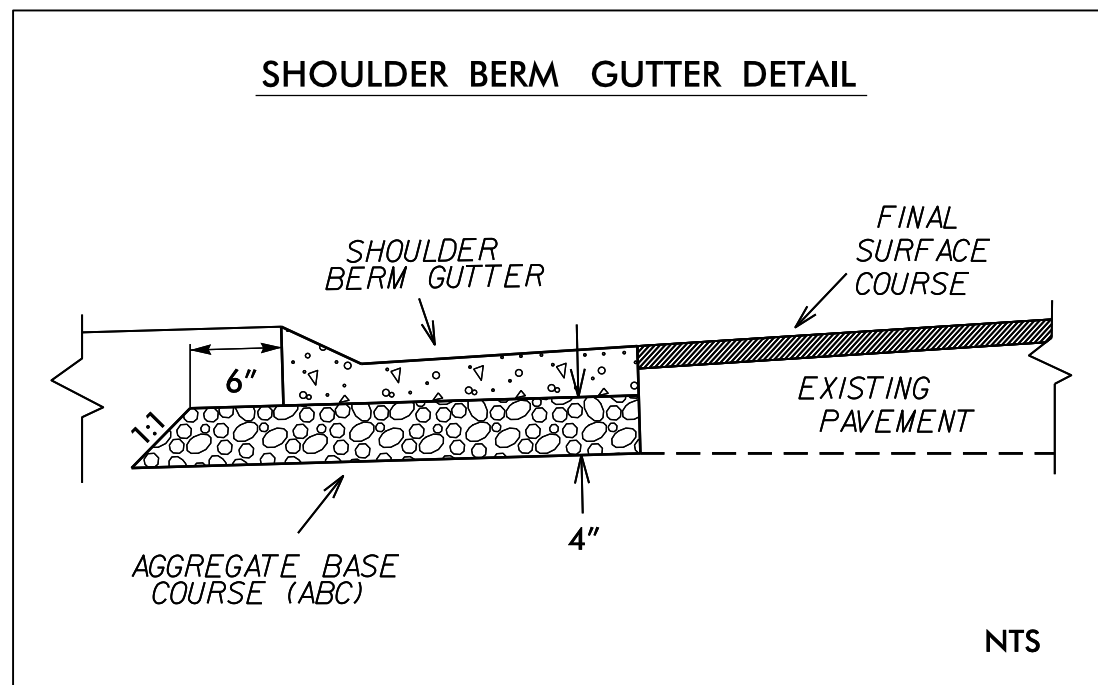
PAVEMENT SCHEDULE	
B	¾" OGAFC, TYPE FC-1
C	1½" S9.5D
M	MILLED RUMBLE STRIP
R2	EXIST. CONCRETE 2'-0" CURB & GUTTER
T	AGGREGATE SHOULDER BORROW - SH RECONST.
U	EXISTING PAVEMENT
V1	MILLING ⅞" TO 1½" DEPTH
V3	MILLING 0" TO 1½" DEPTH

FOR USE WITH TYPICALS 2, 3 & 4

V1 MILL ⅞" TO 1½" TO REMOVE EXISTING OGAFC

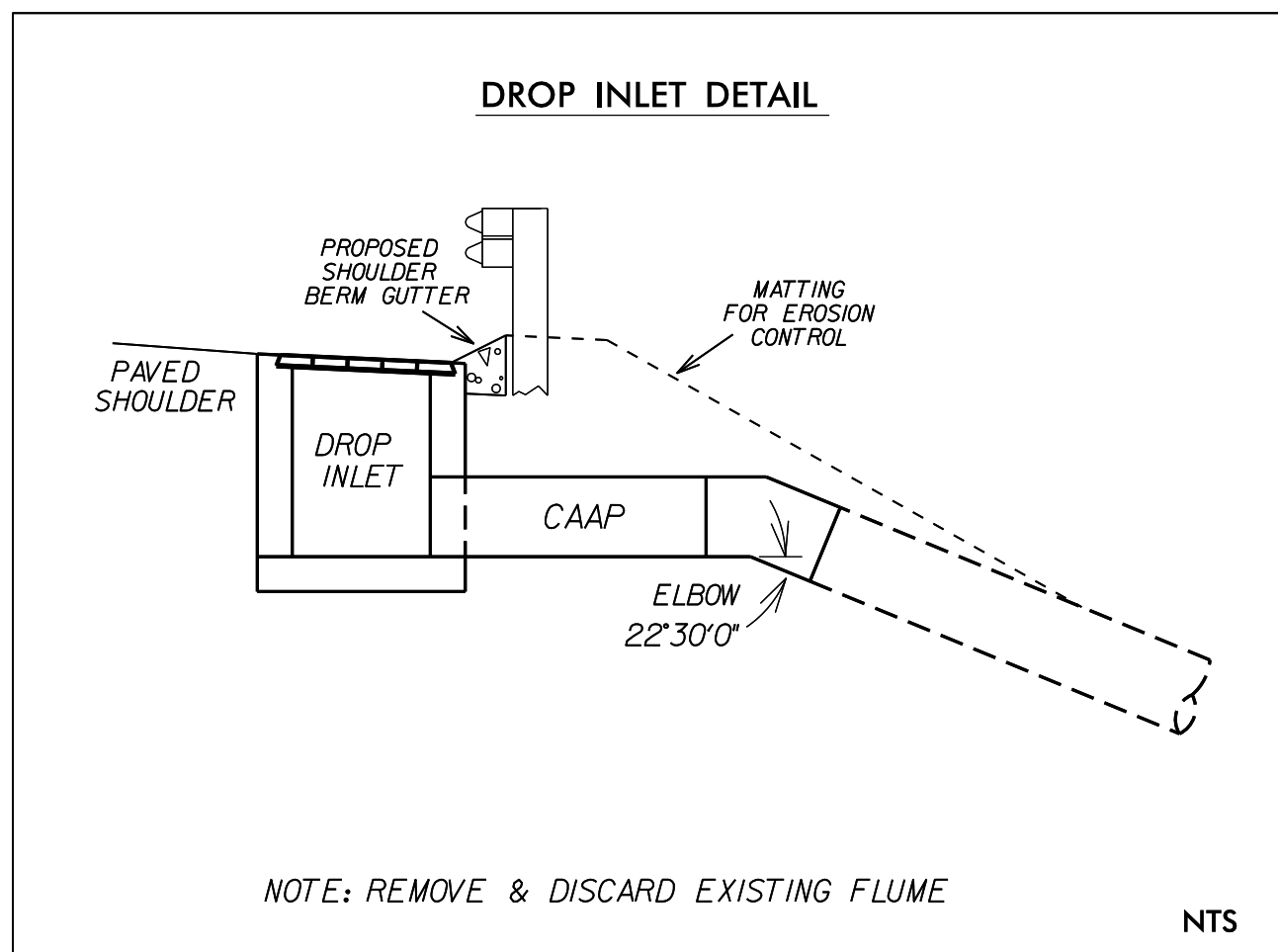
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 \$\$\$I5780RALEIGH\$\$\$

DRAINAGE, SHOULDER BERM GUTTER & MISC. DETAILS



SHOULDER BERM GUTTER SUMMARY

EBL /WBL	BEGIN MILEMARKER	END MILEMARKER	LENGTH
MAP 1 - EBL	344.08	344.31	1200
MAP 2 - WBL	345.22	345.14	430
WBL	342.86	342.70	720
		TOTAL:	2,350
		SAY:	2,350



EFF.01-16-2018 REV.

2018 ROADWAY ENGLISH STANDARD DRAWINGS

The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch - N.C. Department of Transportation - Raleigh, N.C., Dated January, 2018 are applicable to this project and by reference hereby are considered a part of these plans:

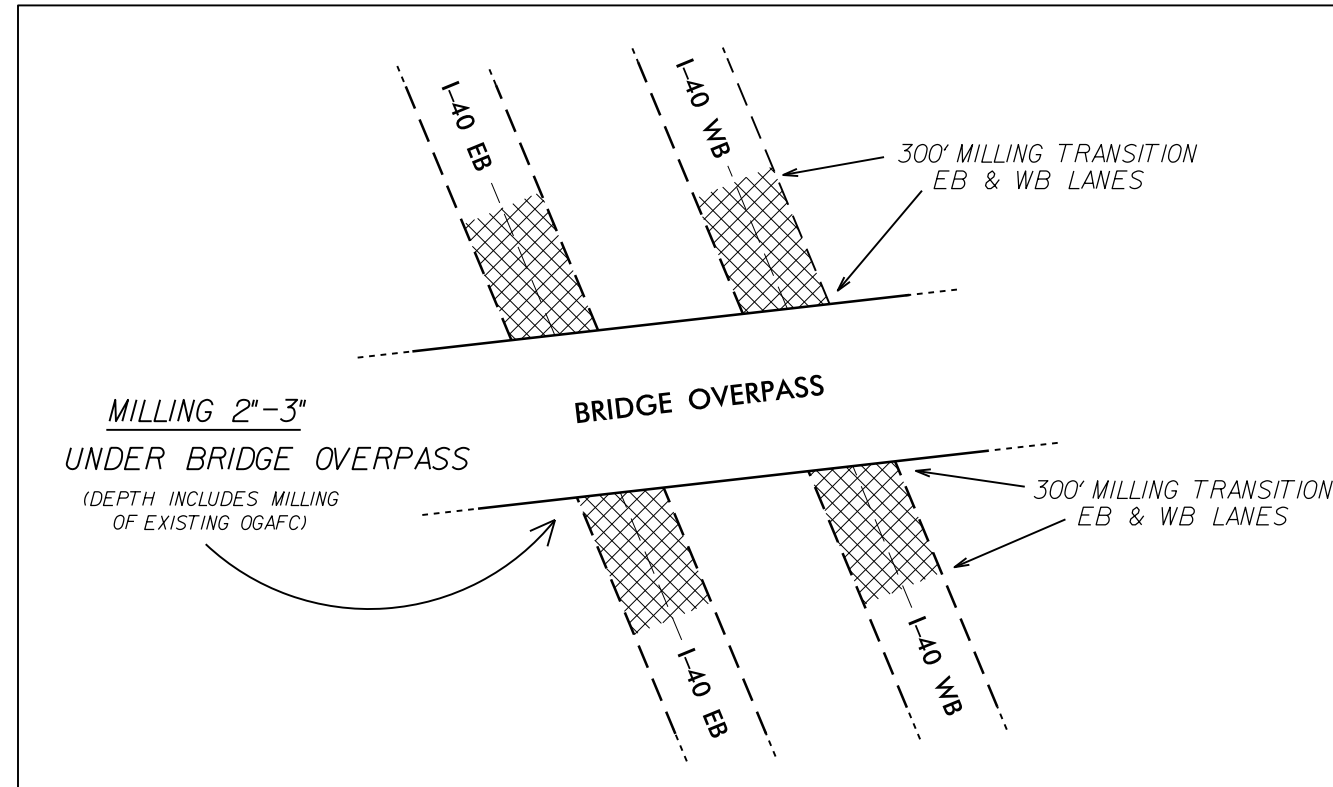
STD.NO.	TITLE
DIVISION 3 - PIPE CULVERTS	
300.01	Method of Pipe Installation
310.03	Cross Pipe End Section
DIVISION 6 - ASPHALT BASES AND PAVEMENTS	
665.01	Asphalt Shoulders - Milled Rumble Strips
DIVISION 8 - INCIDENTALS	
816.04	Markers for Drainage Structures & Concrete Pad
840.18	Concrete Grated Drop Inlet Type B - 12" thru 36" Pipe
840.20	Frames and Wide Slot Flat Grates
840.45	Precast Drainage Structure
846.01	Concrete Curb, Gutter and Curb & Gutter
846.04	Drop Inlet Installation in Shoulder Berm Gutter
862.01	Guardrail Placement
862.02	Guardrail Installation
876.02	Guide for Rip Rap at Pipe Outlets
DIVISION 11 - WORK ZONE TRAFFIC CONTROL	
1101.02	Temporary Lane Closures (Sheet 9 & 10 of 15)

REVISIONS

8/17/99

07-NOV-2018 14:07 S:\Division\Rehab\2019\1-40_Resturfacing\1-5780\1-5780_53021.dwg I-40_Rdy.txd

MILLING FOR VERTICAL CLEARANCE DETAIL



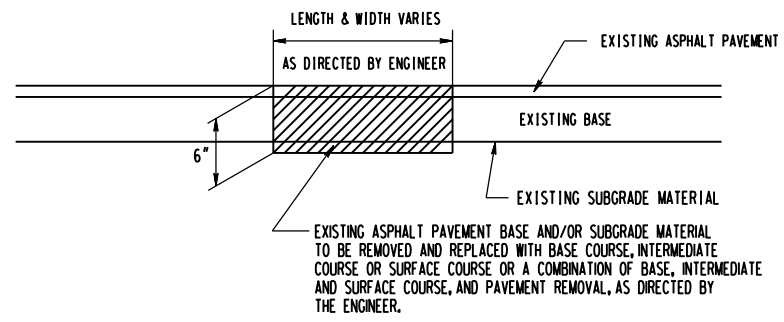
I-40 Milling Locations Under Bridge Overpasses

Location	Mile Post from Duplin Co.	Bridge Location Mile Marker	Begin Milling MP	End Milling MP	Begin Milling Mile Marker	End Milling Mile Marker	Distance
SR-1904 (Pine Ridge Rd)	1.72	358.18	1.66	1.79	358.24	358.11	0.13
SR-1903 (Dave Bright Rd)	2.61	357.29	2.55	2.68	357.35	357.22	0.13
<u>EXIT 355</u> - NC 403 (Faison Hwy)	4.46	355.44	4.38	4.53	355.52	355.37	0.15
SR-1725 (Giddensville Rd)	7.30	352.60	7.23	7.37	352.67	352.53	0.14
<u>EXIT 348</u> - SR-1722 (Suttontown Rd)	11.85	348.05	11.78	11.92	348.12	347.98	0.14
SR-1711 (William R. King Rd)	13.67	346.23	13.60	13.74	346.30	346.16	0.14
SR-1845 (Cedar Point Rd)	15.11	344.79	15.04	15.18	344.86	344.72	0.14
<u>EXIT 343</u> - US 701 (Hobpton Hwy)	16.51	343.39	16.43	16.59	343.47	343.31	0.16
SR-1703 (McLamb Rd)	17.55	342.35	17.48	17.61	342.42	342.29	0.13
US-13 (Newton Grove Hwy)	17.91	341.99	17.84	17.97	342.06	341.93	0.13
<u>EXIT 341</u> - NC-50 (Harnett Dunn Hwy)	18.83	341.07	18.76	18.89	341.14	341.01	0.13

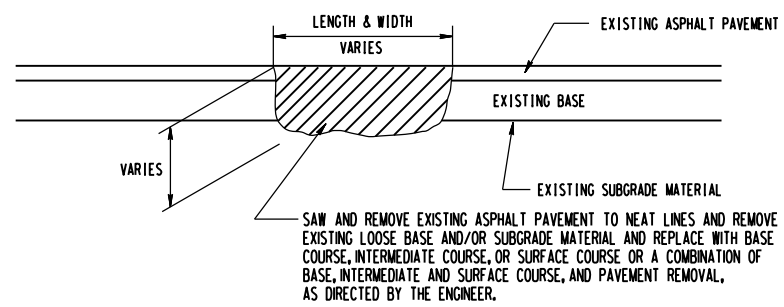
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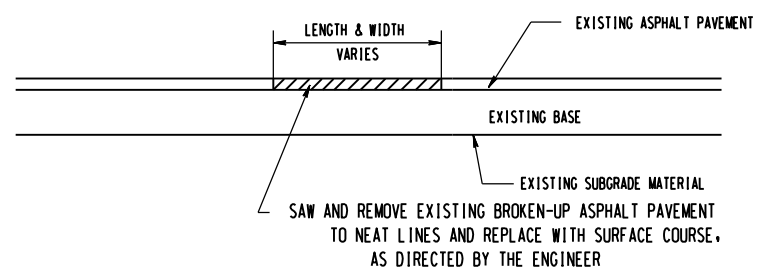
DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



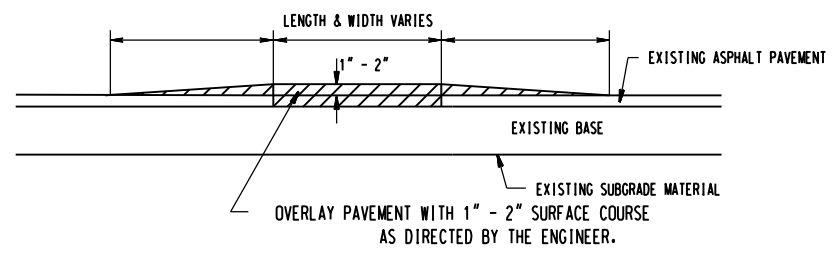
DETAIL NO. 1



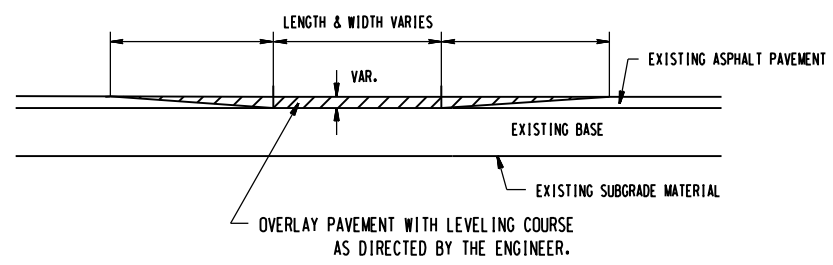
DETAIL NO. 2



DETAIL NO. 3



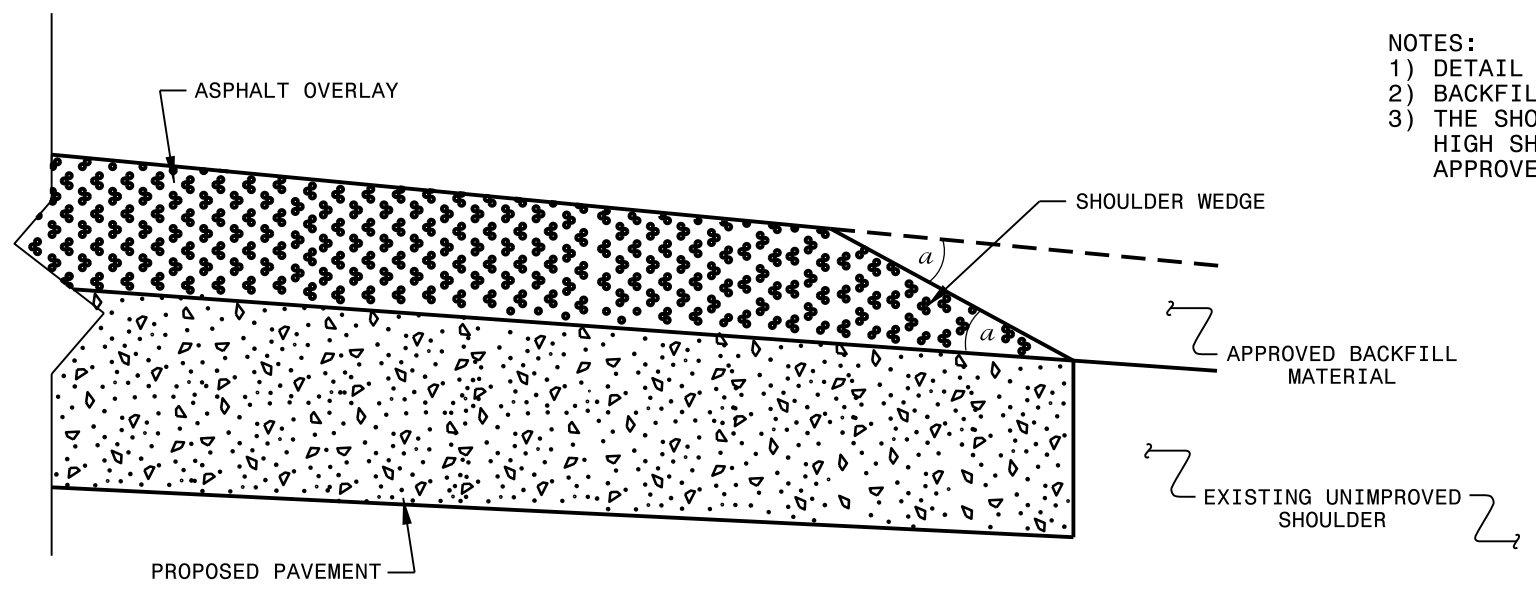
DETAIL NO. 4



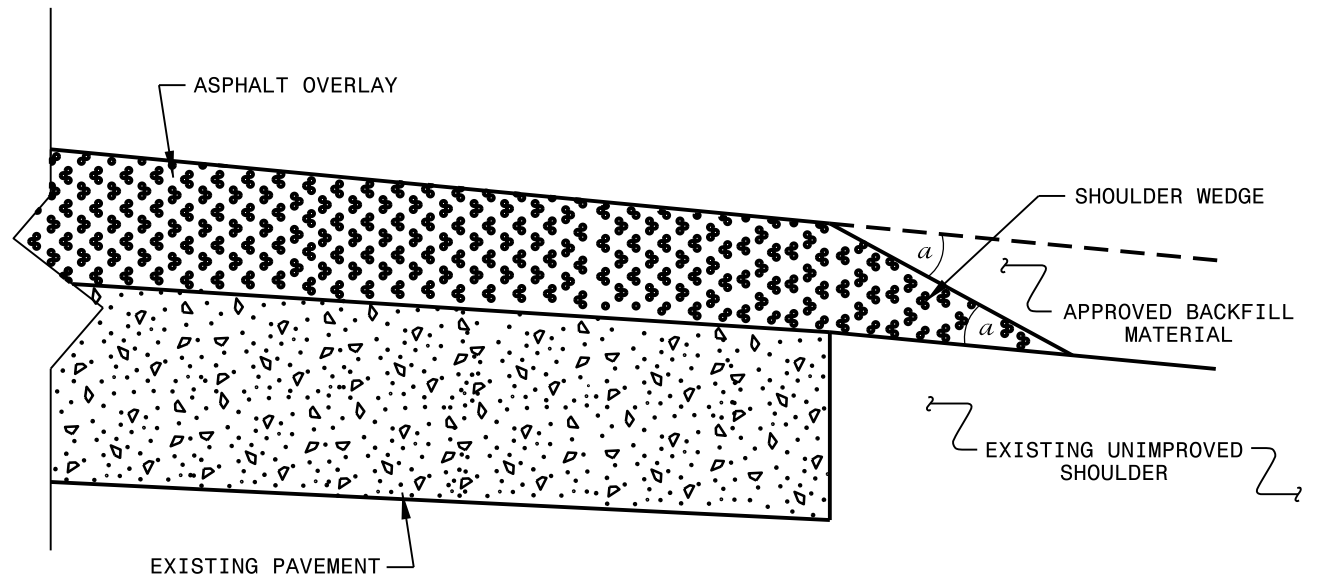
DETAIL NO. 5

09-JUN-2016 12:55
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 I-5780.dwg
 5/14/99

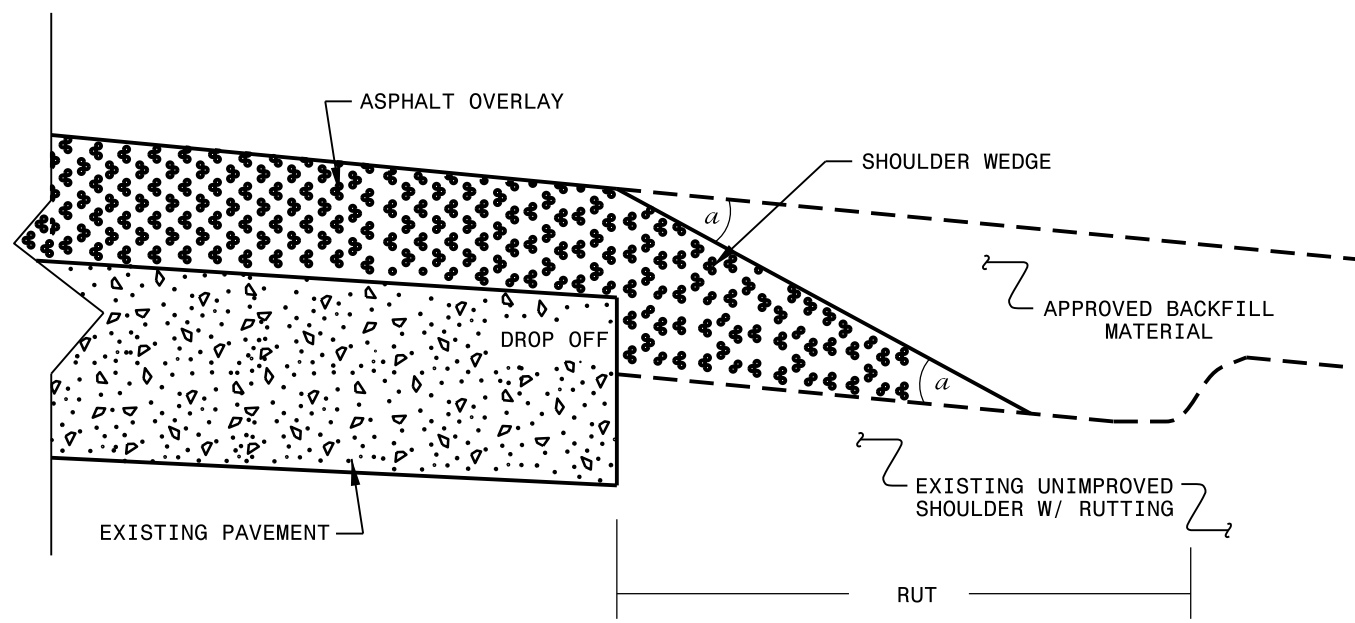
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn			

27 JUN 2018 13:22
 s:\usr\details\stand\shoulderwedgedetail.dgn
 3\Sampson August 2018 Revised Shoulder Wedge Detail.dgn
 P:\porter_41_CSD_20152542

COMPUTED BY: M. KIMMEL DATE: 11/13/2018

CHECKED BY: DATE:

PROJECT NO. SHEET NO.

I-5780 9

NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Note: Invert Elevations indicated are for Bid Purposes only and shall not be used for project construction stakeout. See "Standard Specifications For Roads and Structures, Section 300-5".

LIST OF PIPES, ENDWALLS, ETC. (FOR PIPES 48 INCHES & UNDER)

Table with columns for Station, Location, Structure No., Top Elevation, Invert Elevation, Slope Critical, Slope Drain Pipe (RCP, CSP, CAAP, HDPE, or PVC), C.S. Pipe, R.C. Pipe Class III, R.C. Pipe Class IV, Endwalls, Quantities for Drainage Structures, Frame, Grates, and Hood Standard, Concrete Transitional Section, Conc. & Brick Pipe Plug, Conc. Collars, Pipe Removal Lin., and Abbreviations. Includes a 'SHEET TOTALS' row at the bottom.

I-40 SAMPSON COUNTY STORM DRAINAGE													
<u>STR. #</u>	<u>COORDINATES</u>		<u>MILE MARKER</u>	<u>DI REPAIR EA</u>	<u>REMOVE & REPLACE APRON EA</u>	<u>FRAME & GRATE STD. 840.22</u>	<u>FLOWABLE FILL CY</u>	<u>PIPE DIAMETER IN</u>	<u>FES REPAIR EA</u>	<u>REMOVE & REPLACE PIPE END SECT. EA</u>	<u>FOUNDATION COND. MATERIAL TON</u>	<u>BORROW EXC. CY</u>	<u>LOCATION</u>
1	35.2643460	-78.3868730	339.8		1								M
2	35.2629840	-78.3857450	339.9	1	1		1.60						M
3	35.2622370	-78.3851730	340.0		1								M
4	35.2594120	-78.3823510	340.2	1	1		2.00						M
5	35.2547770	-78.3791740	340.6		1								M
6	35.2532320	-78.3783000	340.7	1	1		1.00						M
7	35.2532360	-78.3782830	340.7	1	1		2.00						M
8	35.2515810	-78.3774110	340.8		1								M
9	35.2515830	-78.3771640	340.8	1	1		4.00						WB GORE
10	35.2516900	-78.3770060	340.8	1	1		2.00						WB OS
11	35.2504900	-78.3771110	340.9	1	1		4.00						EB OS
12	35.2498150	-78.3765170	341.0	1	1		5.00						M
13	35.2498850	-78.3762770	341.0		1								WB OS
14	35.2496960	-78.3766960	341.0	1	1		2.00						EB OS
15	35.2469390	-78.3752990	341.0	1	1		1.50						EB GORE
16	35.3471270	-78.3747960	341.1	1	1		2.00						WB GORE
17	35.2485330	-78.3755680	341.2		1								WB OS
18	35.2483600	-78.7599600	341.2	1									EB OS
19	35.2447940	-78.3742240	341.3	1	1		5.00						WB OS
20	35.2443520	-78.3736560	341.4		1								M
21	35.2437630	-78.3733330	341.4		1								M
22	35.2439047	-78.3731483	341.4	1	1		2.00						WB OS
23	35.2436676	-78.3735704	341.4	1	1		2.00						EB OS
24	35.2424669	-78.3726501	341.5		1								M
25	35.2423864	-78.3728640	341.6	1	1		5.00						EB OS
26	35.2411566	-78.3718213	341.7	1	1		4.00						M
27	35.2411848	-78.3715745	341.7	1	1		2.00						WB OS
28	35.2396409	-78.3707933	341.7	1	1		2.00						M
29	35.2399356	-78.3706793	341.8	1	1		3.00						WB OS
30	35.2394060	-78.3707665	341.8	1	1		2.00						EB OS
31	35.2389930	-78.3702059	341.8		1								M
32	35.2391678	-78.3700467	341.8	1	1		1.50						WB OS

<u>STR. #</u>	<u>COORDINATES</u>		<u>MILE MARKER</u>	<u>DI REPAIR EA</u>	<u>REMOVE & REPLACE APRON EA</u>	<u>FRAME & GRATE STD. 840.22</u>	<u>FLOWABLE FILL CY</u>	<u>PIPE DIAMETER IN</u>	<u>FES REPAIR EA</u>	<u>REMOVE & REPLACE PIPE END SECT. EA</u>	<u>FOUNDATION COND. MATERIAL TON</u>	<u>BORROW EXC. CY</u>	<u>LOCATION</u>
33	35.2389418	-78.3704124	341.8	1	1		4.00						EB OS
34	35.2624504	-78.3848860	341.8	1	1		5.00						WB OS
35	35.2632769	-78.3856001	339.9	1	1		2.00						WB OS
36	35.2644532	-78.3868031	339.8	1	1		2.00						WB OS
37	35.2641948	-78.3871682	339.8		1								EB OS
38	35.2532070	-78.3753511	340.0	1	1		2.00						EB OS
39	35.2461284	-78.3748297	341.9	1									EB OS
40	35.2365156	-78.3678908	342.0		1								M
41	35.2364277	-78.3679985	342.0	1	1		3.00						EB OS
42	35.2366651	-78.3676706	342.0		1		1.50						WB OS
43	35.2355325	-78.3667673	342.1	1	1								M
44	35.2356678	-78.3665608	342.1		1								WB OS
45	35.2339885	-78.3644710	342.3		1								WB OS
46	35.2309281	-78.3599676	342.6		1								M
47	35.2310744	-78.3598355	342.6		1								WB OS
48	35.2311565	-78.3601248	342.6	1									WB OS
49	35.2304825	-78.3591093	342.7		1								M
50	35.2293950	-78.3567231	342.8		1								M
51	35.2289524	-78.3555667	342.9	1	1		4.00						M
52	35.2287884	-78.3557159	342.9		1								EB OS
53	35.2280487	-78.3539846	343.0		1								EB OS
54	35.2290264	-78.3552466	343.0	1	1		4.00						WB OS
55	35.2272399	-78.3520054	343.0		1								EB GORE
56	35.2271569	-78.3521952	343.0		1								EB OS
57	35.2280039	-78.3529285	343.1		1								WB GORE
58	35.2282634	-78.3528269	343.1	1									WB OS
59	35.2252784	-78.3502761	343.1		1								EB OS
60	35.2254569	-78.3499938	343.1	1	1		2.00						EB OS
61	35.2260061	-78.3493480	343.1		1								EB OS
62	35.2261134	-78.3494768	343.1	1	1								EB OS
63	35.2268020	-78.3508253	343.2		1								EBOS
64	35.2265574	-78.3495445	343.2		1								M
65	35.2252439	-78.3458618	343.5	1	1		1.50						WB OS
66	35.2248736	-78.3461656	343.5	1	1		2.00						EB OS
67	35.2241686	-78.3444775	343.5	1	1	1	4.00						EB OS

<u>STR. #</u>	<u>COORDINATES</u>		<u>MILE MARKER</u>	<u>DI REPAIR EA</u>	<u>REMOVE & REPLACE APRON EA</u>	<u>FRAME & GRATE STD. 840.22</u>	<u>FLOWABLE FILL CY</u>	<u>PIPE DIAMETER IN</u>	<u>FES REPAIR EA</u>	<u>REMOVE & REPLACE PIPE END SECT. EA</u>	<u>FOUNDATION COND. MATERIAL TON</u>	<u>BORROW EXC. CY</u>	<u>LOCATION</u>
68	35.2244052	-78.3443353	343.6		1								M
69	35.2233510	-78.3423552	343.6	1	1		3.00						EB OS
70	35.2235422	-78.3422446	343.7	1	1		2.00						M
71	35.2226006	-78.3401269	343.8		1								M
72	35.2215885	-78.3381280	344.0		1								M
73	35.2201826	-78.3364838	344.0		1								EB OS
74	35.2203954	-78.3362723	344.1	1	1		5.00						M
75	35.2205480	-78.3361429	344.1	1	1		5.00						WB OS
76	35.2198523	-78.3358478	344.1	1	1		3.00						EB OS
77	35.2199643	-78.3356651	344.2	1	1		2.00						M
78	35.2201169	-78.3355370	344.2	1	1		1.50						WB OS
79	35.2185972	-78.3342509	344.2		1								EB OS
80	35.2186876	-78.3343579	344.2	1									EB OS
81	35.2187304	-78.3341118	344.3	1	1		1.50						M
82	35.2188449	-78.3339089	344.3		1								WB OS
83	35.2158935	-78.3306755	344.5	1	1	1	3.00						M
84	35.2157587	-78.3308619	344.5		1								EB OS
85	35.2143146	-78.3289284	344.7		1								M
86	35.2145129	-78.3286293	344.7	1			2.00						WB OS
87	35.2132606	-78.3272664	344.8	1	1								M
88	35.2131176	-78.3273968	344.8	1	1		2.00						EB OS
89	35.2134172	-78.3271404	344.8		1								WB OS
90	35.2123092	-78.3256068	344.9	1	1		1.50						M
91	35.2121495	-78.3257751	344.9	1	1		2.00						EB OS
92	35.2124922	-78.3255149	344.9		1								WB OS
93	35.2118613	-78.3248464	345.0		1								M
94	35.2120024	-78.3247069	345.0		1								WB OS
95	35.2109278	-78.3226433	345.1	1	1		1.50						M
96	35.2104733	-78.3215359	345.2	1	1		1.50						M
97	35.2092609	-78.3181057	345.4	1	1		1.50						M
98	35.2084043	-78.3156565	345.6		1								M
99	35.2065834	-78.3105080	345.9	1			1.50						M
100	35.2064113	-78.3107048	345.9	1									EB OS
101	35.2051136	-78.3063328	346.1		1								M
102	35.2093690	-78.3063308	346.2	1			1.50						EB OS

<u>STR. #</u>	<u>COORDINATES</u>		<u>MILE MARKER</u>	<u>DI REPAIR EA</u>	<u>REMOVE & REPLACE APRON EA</u>	<u>FRAME & GRATE STD. 840.22</u>	<u>FLOWABLE FILL CY</u>	<u>PIPE DIAMETER IN</u>	<u>FES REPAIR EA</u>	<u>REMOVE & REPLACE PIPE END SECT. EA</u>	<u>FOUNDATION COND. MATERIAL TON</u>	<u>BORROW EXC. CY</u>	<u>LOCATION</u>
103	35.2034581	-78.3017412	346.4		1								M
104	35.2027330	-78.2999166	346.5		1								M
105	35.2025489	-78.3000135	346.5					15"	1		8	4	EB OS
106	35.2018100	-78.2982731	346.6					24"				8	EB OS
107	35.2015226	-78.2972666	346.7		1								M
108	35.2010415	-78.2912135	346.8	1	1		1.50						M
109	35.1997059	-78.2945314	346.9					18"		1	12	6	EB OS
110	35.1985465	-78.2926750	347.0					18"		1	12	6	EB OS
111	35.1974185	-78.2907686	347.2	1									M
112	35.1958744	-78.2888304	347.3				0.75						M
113	35.1957546	-78.2890185	347.3					15"		1	8	4	EB OS
114	35.1927503	-78.2848734	347.6	1	1		1.50						M
115	35.1929651	-78.2847407	347.6					15"		1	8	4	WB OS
116	35.1912217	-78.2830398	347.8		1								M
117	35.1896783	-78.2809738	347.9	1	1		2.00						M
118	35.1883345	-78.2789957	348.1	1	1		2.00						M
119	35.1873700	-78.2773022	348.2	1	1		5.00						M
120	35.1852933	-78.2737627	348.4				0.75						M
121	35.1852437	-78.2740480	348.4				1.00						EB OS
122	35.1842084	-78.2717668	348.6	1			2.00						M
123	35.1830285	-78.2702993	348.7				2.00						EB OS
124	35.1823875	-78.2686400	348.8	1	1		2.00						M
125	35.1822889	-78.2689508	348.8					15"			20	11	EB OS
126	35.1825201	-78.2682099	348.8					36"				8	WB OS
127	35.1815923	-78.2671243	348.9	1			1.50						M
128	35.1817416	-78.2669824	348.9					15"		1	8	4	WB OS
129	35.1807252	-78.2656393	348.9	1	1		1.50						M
130	35.1790117	-78.2625860	349.2		1								M
131	35.1781591	-78.2610933	349.3	1	1		3.00						M
132	35.1772984	-78.2595936	349.3		1								M
133	35.1764995	-78.2583172	349.3	1	1		1.50						M
134	35.1757547	-78.2572410	349.3	1									M
135	35.1743704	-78.2553591	349.70		1								M
136	35.1741898	-78.2555358	349.7					15"		1	8	4	EB OS
137	35.1728039	-78.2534155	349.9	1	1		1.50						M

<u>STR. #</u>	<u>COORDINATES</u>		<u>MILE MARKER</u>	<u>DI REPAIR EA</u>	<u>REMOVE & REPLACE APRON EA</u>	<u>FRAME & GRATE STD. 840.22</u>	<u>FLOWABLE FILL CY</u>	<u>PIPE DIAMETER IN</u>	<u>FES REPAIR EA</u>	<u>REMOVE & REPLACE PIPE END SECT. EA</u>	<u>FOUNDATION COND. MATERIAL TON</u>	<u>BORROW EXC. CY</u>	<u>LOCATION</u>
173	35.1150744	-78.1861397	355.5				0.75						M
174	35.1140662	-78.1856448	355.5		1								M
175	35.1130400	-78.1851603	355.6	1									M
176	35.1119766	-78.1846577	355.7		1								M
177	35.1109646	-78.1842054	355.8				0.75						M
178	35.1089059	-78.1832204	355.9	1	1		0.40						M
179	35.1067709	-78.1821992	356.1	1			1.50						M
180	35.1047497	-78.1812389	352.2				0.50						M
181	35.1006106	-78.1792678	356.5				0.75						M
182	35.0984504	-78.1785155	356.7					15"		1	8	4	EB OS
183	35.0965617	-78.1773393	356.8	1									M
184	35.0964591	-78.1775727	356.8					15"		1	8	4	EB OS
185	35.0949965	-78.1766289	357.0	1			2.00						M
186	35.0910036	-78.1749126	357.2		1								M
187	35.0849850	-78.1735896	357.7		1								M
188	35.0831533	-78.1731195	357.8		1								M
189	35.0811842	-78.1726498	357.9		1								M
190	35.0768085	-78.1706844	358.3	1	1		1.50						M
191	35.0769193	-78.1704129	358.3					15"	1		8	4	WB OS
192	35.0752014	-78.1698151	358.4				0.75						M
193	35.0732090	-78.1686376	358.5	1	1		1.00						M
194	35.0700581	-78.1666537	358.8		1								M
195	35.0626951	-78.1621832	359.3	1	1		4.00						M
196	35.0606939	-78.1612608	359.4					18"		1	12	6	EB OS
197	35.0588338	-78.1598017	359.6		1		1.50						M
198	35.0587128	-78.1600454	359.6					18"		1	12	6	EB OS
199	35.0569170	-78.1585652	359.9		1								M
200	CONTINGENCY FOR ADDITIONAL WORK		-	10			22.50						-
			TOTALS:	102	139	2	202.00		3	19	232	188	

PROJECT NO.	SHEET NO.
I-5780	17

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH		4400000000-E	4405000000-E	4420000000-N	4423000000-N	4465000000-N	4470000000-N	4485000000-E	4500000000-E	4510000000-N	4686000000-E	4695000000-E	4710000000-E				
								MI	FT	WORK ZONE SIGNS (STATIONARY)	WORK ZONE SIGNS (PORTABLE)	PORTABLE CHANGEABLE MESSAGE SIGN	DIGITAL SPEED LIMIT SIGNS	TEMPORARY CRASH CUSHIONS	REMOVE & RESET TEMPORARY CRASH CUSHIONS	PORT. CONC BARRIER	REM & RESET PORT. CONC. BARRIER	LAW ENFORCEMENT	4" X 120 M YELLOW THERMO	8" X 90 M YELLOW THERMO	8" X 90 M WHITE THERMO	24" X 120 M WHITE THERMO			
I-5780	Sampson	1	I-40 EB (JAMES HARRINGTON FWY.)	FROM JOHNSTON CO. TO DUPLIN CO. (MM 339.76 - MM 359.89)	1	2	2WD	20.13	38	328	39	3	2	1	10	1,440	22,290	202							
		2	I-40 WB (JAMES HARRINGTON FWY.)	FROM DUPLIN CO. TO JOHNSTON CO. (MM 359.89 - MM 339.76)	1	2	2WD	20.13	38	328	39	3	1	1	10			201							
		3	EXIT 341 - EB EXIT RAMP AT NC 50/55	FROM I-40 TO NC 50/55 (MP 0.00 - MP 0.33)	2	1		0.33	22	16												57			
		4	EXIT 341 - EB ENTRANCE RAMP AT NC 50/55	FROM NC 50/55 TO I-40 (MP 0.00 - MP 0.43)	3	1		0.43	23	32															
		5	EXIT 343 - EB EXIT RAMP AT US 701	FROM I-40 TO US 701 (MP 0.00 - MP 0.45)	3	1		0.45	22	16												210	58	28	
		6	EXIT 343 - EB ENTRANCE RAMP AT US 701	FROM US 701 TO I-40 (MP 0.00 - MP 0.54)	4	1		0.54	22	32												210	58	256	
		7	EXIT 348 - EB EXIT RAMP AT SR 1722	FROM I-40 TO SR 1722 (SUTTONTOWN RD.) [MP 0.00 - 0.40]	3	1		0.4	22	16														48	
		8	EXIT 348 - EB ENTRANCE RAMP AT SR 1722	FROM SR 1722 (SUTTONTOWN RD.) TO I-40 [MP 0.00 - MP 0.49]	3	1		0.49	22	32															
		9	EXIT 355 - EB EXIT RAMP AT NC 403	FROM I-40 TO NC 403 (MP 0.00 - MP 0.40)	2	1		0.4	22	16															54
		10	EXIT 355 - EB ENTRANCE RAMP AT NC 403	FROM NC 403 TO I-40 (MP 0.00 - MP 0.51)	3	1		0.51	22	32															
		11	EXIT 355 - WB EXIT RAMP AT NC 403	FROM I-40 TO NC 403 (MP 0.00 - MP 0.40)	2	1		0.4	22	16															26
		12	EXIT 355 - WB ENTRANCE RAMP AT NC 403	FROM NC 403 TO I-40 (MP 0.00 - MP 0.50)	3	1		0.5	22	32															
		13	EXIT 348 - WB EXIT RAMP AT SR 1722	FROM I-40 TO SR 1722 (SUTTONTOWN RD.) [MP 0.00 - MP 0.38]	3	1		0.38	22	16															44
		14	EXIT 348 - WB ENTRANCE RAMP AT SR 1722	FROM SR 1722 (SUTTONTOWN RD) TO I-40 [MP 0.00 - MP 0.44]	3	1		0.44	22	32															
		15	EXIT 343 - WB EXIT RAMP AT US 701	FROM I-40 TO US 701 (MP 0.00 - MP 0.31)	4	1		0.31	22	16													172	42	50
		16	EXIT 343 - WB ENTRANCE RAMP AT US 701	FROM US 701 TO I-40 (MP 0.00 - MP 0.48)	3	1		0.48	22	32													172	42	190
		17	EXIT 341 - WB EXIT RAMP AT NC 50/55	FROM I-40 TO NC 50/55 (MP 0.00 - MP 0.33)	2	1		0.33	23	16															58
		18	EXIT 341 - WB ENTRANCE RAMP AT NC 50/55	FROM NC 50/55 TO I-40 (MP 0.00 - MP 0.43)	3	1		0.43	22	32															
GRAND TOTAL									47.08	1,040	78	6	3	2	20	1,440	22,290	403	764	200	446	365			
																			646						

PROJECT NO.	SHEET NO.
I-5780	18

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	4725000000-E				4815000000-E		4825000000-E	4835000000-E	4845000000-N	4847040000-E	4847080000-E	4895000000-N	4900000000-N	4905000000-N		4600000000-N	
								WRONG-WAY RAMP ARROW 90 M	MERGE ARROW 90 M	24" YIELD LINE TRIANGLE (90 MIL)	ELONGATE D STR ARROW	6" WHITE PAINT	6" YELLOW PAINT	12" WHITE PAINT	24" WHITE PAINT	MERGE ARROW	POLYUREA PVMT MARKING LINES (6", 30 MILS)	POLYUREA PVMT MARKING LINES (12", 30 MILS)	MARKERS FOR DR. STR & CONC PAD	YELLOW & YELLOW MARKERS	SNOW PLOW MARKERS (C/R)	SNOW PLOW MARKERS (Y/Y)	LANE CLOSURE	RAMP / LOOP CLOSURE
								EA	EA	EA	EA	LF	LF	LF	LF	EA	LF	LF	EA	EA	EA	EA	EA	EA
			I-40 EB (JAMES HARRINGTON FWY.)	FROM JOHNSTON CO. TO DUPLIN CO. (MM 339.76 - MM 359.89)	1	2	2WD					133,000	106,286	2,797			240,000	2,797	100		1,475		200	16
			I-40 WB (JAMES HARRINGTON FWY.)	FROM DUPLIN CO. TO JOHNSTON CO. (MM 359.89 - MM 339.76)	1	2	2WD					133,000	106,286	3,322			240,000	3,322	100		1,505		200	16
			EXIT 341 - EB EXIT RAMP AT NC 50/55	FROM I-40 TO NC 50/55 (MP 0.00 - MP 0.33)	2	1		1						328	57		2,740	454		10	38			
			EXIT 341 - EB ENTRANCE RAMP AT NC 50/55	FROM NC 50/55 TO I-40 (MP 0.00 - MP 0.43)	3	1			3					195		3	3,398	195			12			
			EXIT 343 - EB EXIT RAMP AT US 701	FROM I-40 TO US 701 (MP 0.00 - MP 0.45)	3	1		1						355	28		4,148	355			28	6		
			EXIT 343 - EB ENTRANCE RAMP AT US 701	FROM US 701 TO I-40 (MP 0.00 - MP 0.54)	4	1			3	6	1			148		3	3,936	148			18	6		
			EXIT 348 - EB EXIT RAMP AT SR 1722	FROM I-40 TO SR 1722 (SUTTONTOWN RD.) [MP 0.00 - 0.40]	3	1		1						458	48		3,475	458			38			
			EXIT 348 - EB ENTRANCE RAMP AT SR 1722	FROM SR 1722 (SUTTONTOWN RD.) TO I-40 [MP 0.00 - MP 0.49]	3	1			3							3	3,812	222			12			
			EXIT 355 - EB EXIT RAMP AT NC 403	FROM I-40 TO NC 403 (MP 0.00 - MP 0.40)	2	1		1						460	54		3,388	637		12	46			
			EXIT 355 - EB ENTRANCE RAMP AT NC 403	FROM NC 403 TO I-40 (MP 0.00 - MP 0.51)	3	1			3					230		3	4,140	230			12			
			EXIT 355 - WB EXIT RAMP AT NC 403	FROM I-40 TO NC 403 (MP 0.00 - MP 0.40)	2	1		1						454	26		3,487	454		13	40			
			EXIT 355 - WB ENTRANCE RAMP AT NC 403	FROM NC 403 TO I-40 (MP 0.00 - MP 0.50)	3	1			3					248		3	4,030	248			13			
			EXIT 348 - WB EXIT RAMP AT SR 1722	FROM I-40 TO SR 1722 (SUTTONTOWN RD.) [MP 0.00 - MP 0.38]	3	1		1						492	44		3,204	492			40			
			EXIT 348 - WB ENTRANCE RAMP AT SR 1722	FROM SR 1722 (SUTTONTOWN RD.) TO I-40 [MP 0.00 - MP 0.44]	3	1			3					246		3	3,423	246			13			
			EXIT 343 - WB EXIT RAMP AT US 701	FROM I-40 TO US 701 (MP 0.00 - MP 0.31)	4	1		1						210	50		1,598	210			25	5		
			EXIT 343 - WB ENTRANCE RAMP AT US 701	FROM US 701 TO I-40 (MP 0.00 - MP 0.48)	3	1			3	6	1			255		3	3,740	415			13	5		
			EXIT 341 - WB EXIT RAMP AT NC 50/55	FROM I-40 TO NC 50/55 (MP 0.00 - MP 0.33)	2	1		1						350	58		2,760	350		11	32			
			EXIT 341 - WB ENTRANCE RAMP AT NC 50/55	FROM NC 50/55 TO I-40 (MP 0.00 - MP 0.43)	3	1			3					215		3	3,432	215			12			
GRAND TOTAL								8	24	12	2	266,000	212,572	10,763	365	24	534,711	11,448	200	46	3,372	22	400	32
								46				478,572									3,394			

RESURFACING LIMITS, PAVEMENT MARKING IMPROVEMENTS & PAVEMENT MARKERS

I-40 SIDE BY SIDE /ADJACENT ON/OFF RAMP

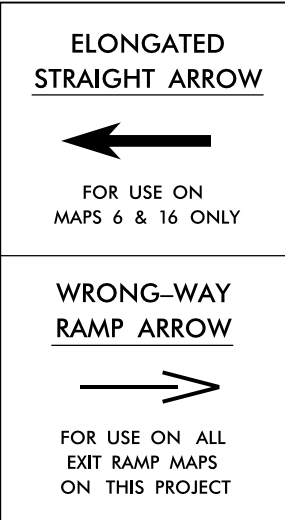
THERMOPLASTIC PAVEMENT MARKINGS FOR EXIT 343:

MAP 5: (EB EXIT RAMP)
MAP 6: (EB ENTRANCE RAMP)

MAP 15: (WB EXIT RAMP)
MAP 16: (WB ENTRANCE RAMP)

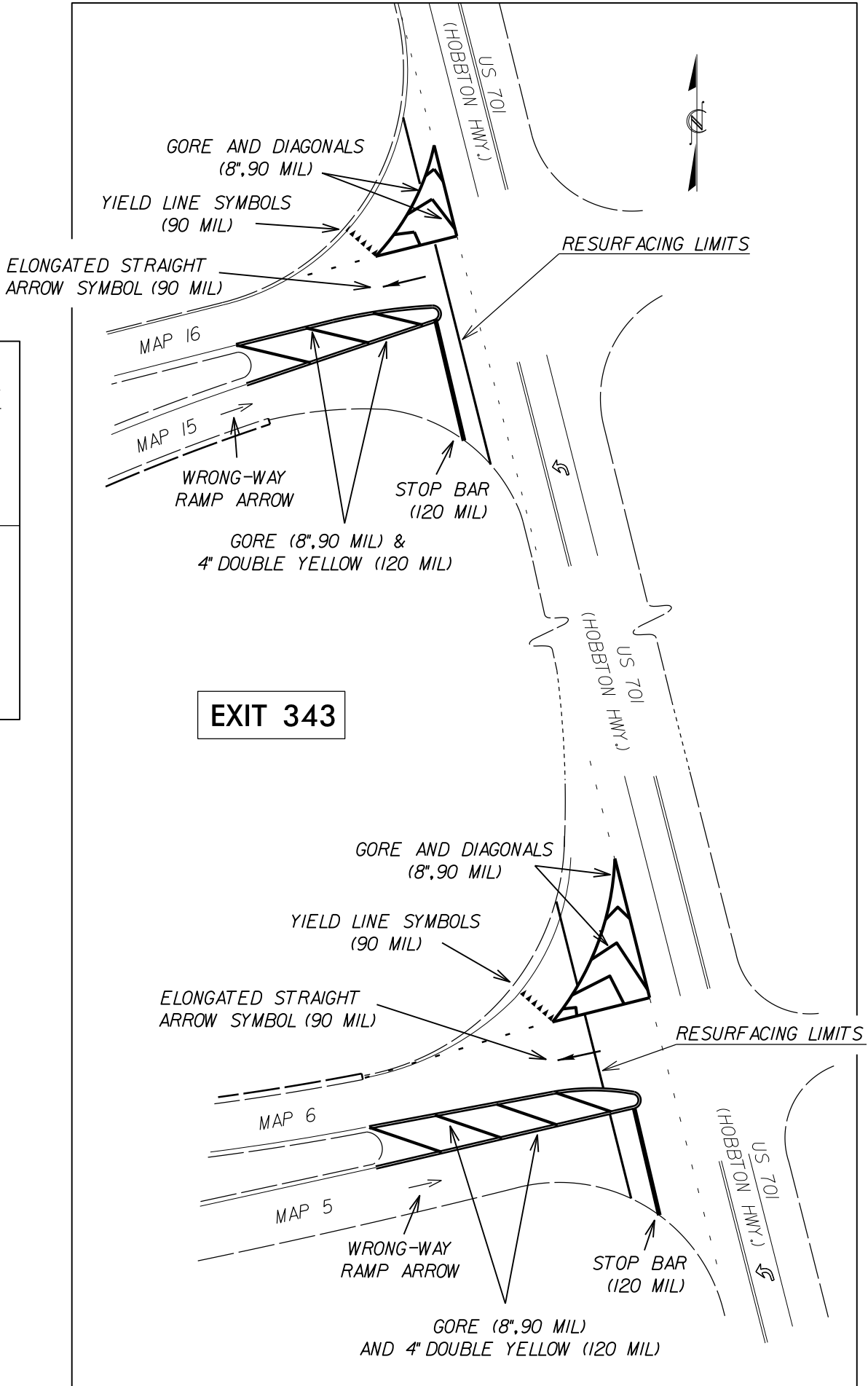
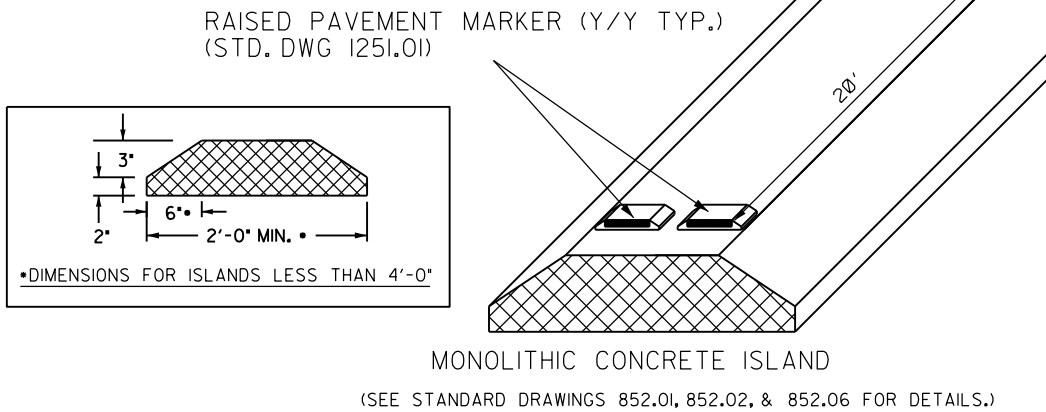
NOTES:

- 1) REFER TO THE LATEST NCDOT ROADWAY STANDARD DRAWINGS
- 2) ELONGATED STRAIGHT ARROW SHALL BE APPROXIMATELY 12' LONG AND SHALL BE PAID AS A SYMBOL. REFER TO STD. DWG. 1205.08 (SHEET 1 OF 9) FOR ADDITIONAL ARROW INFORMATION.



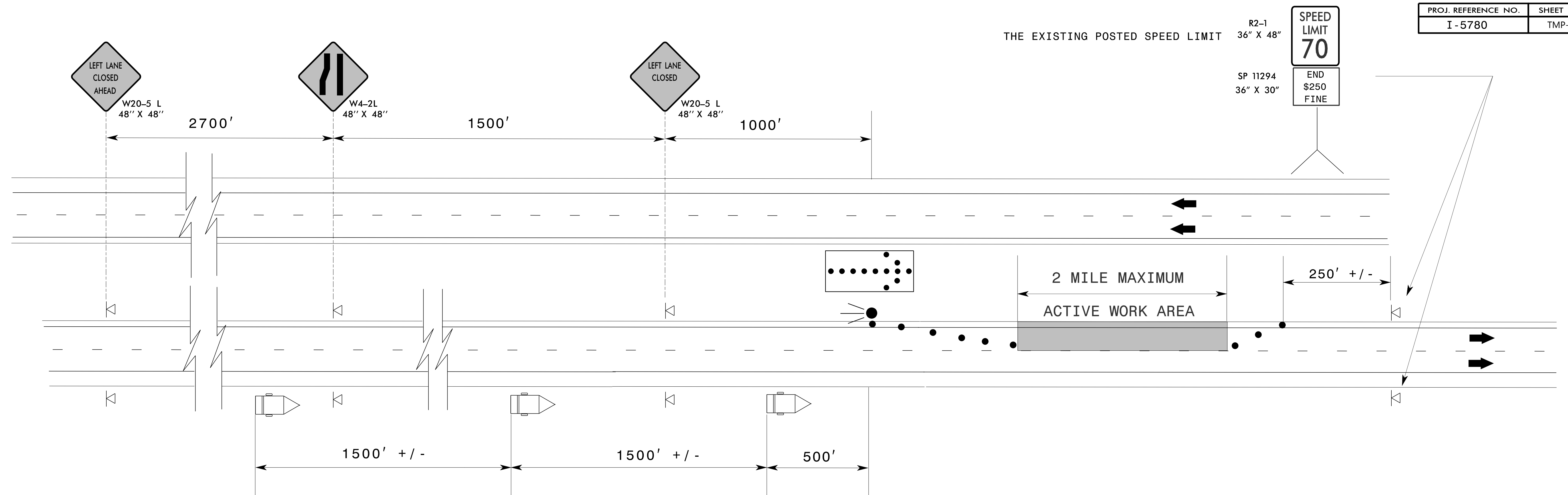
FOR USE WITH MAPS 3, 9, 11 & 17

PAVEMENT MARKER DETAIL FOR CONCRETE ISLANDS

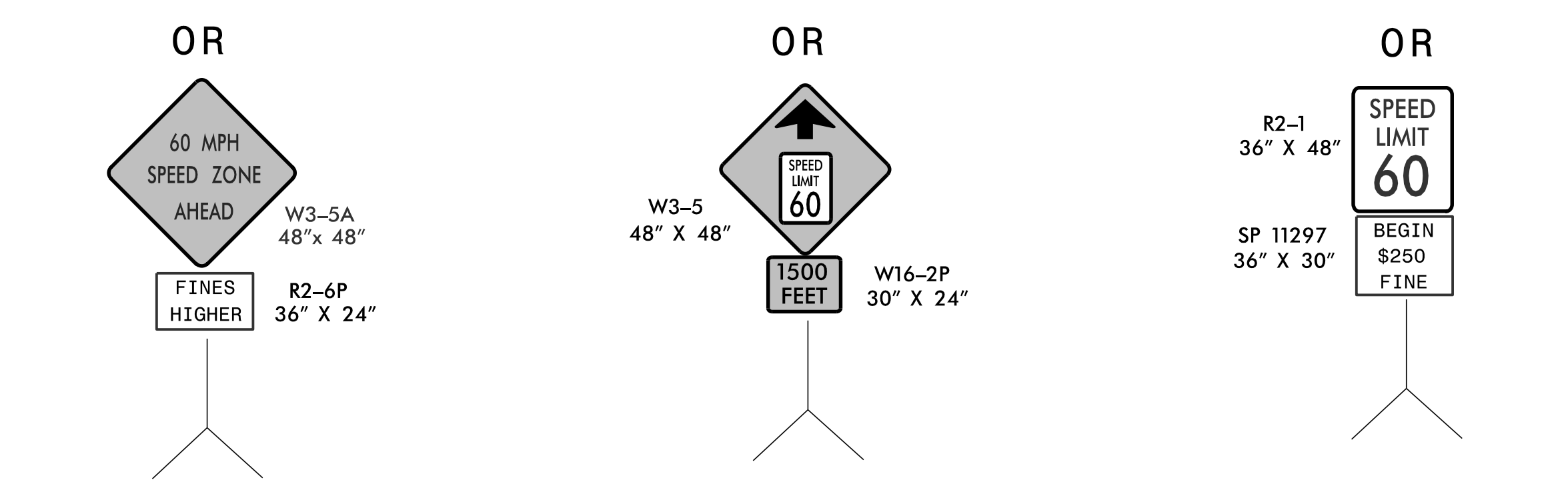


REVISIONS

05-NOV-2018 14403 S:\Division Resurfacing\Winterstate_Rehab\2019\I-40_Resurfacing_I-5780\I-5780_53021.dwg I-40_Rdy_Painted_Is_Dwg.dgn 8/17/99

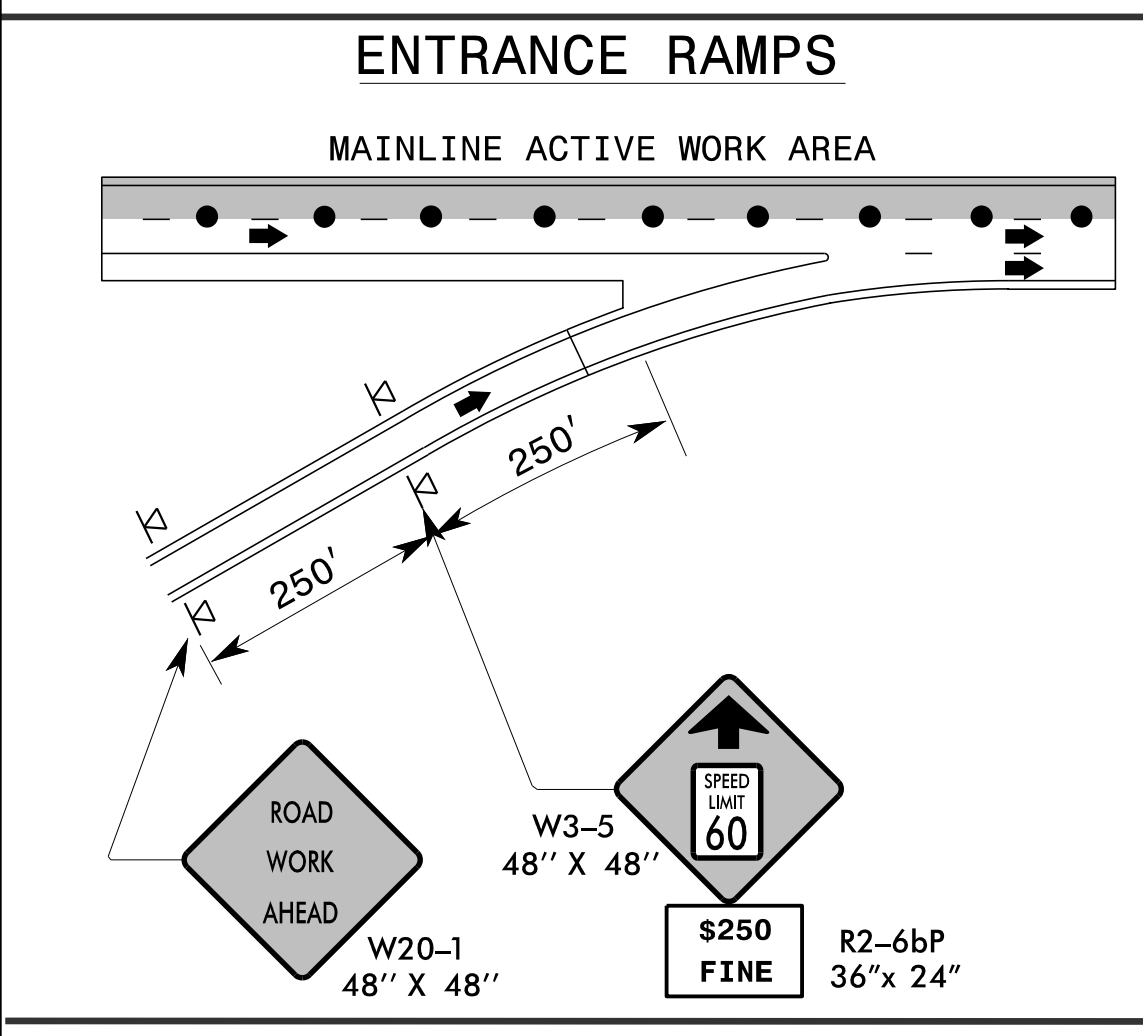


MESSAGE NO. 1	MESSAGE NO. 2	MESSAGE NO. 1	MESSAGE NO. 2	MESSAGE NO. 1	MESSAGE NO. 2
60 MPH ZONE AHEAD	\$250 FINE AHEAD	BEGIN 60 MPH 1500 FT	WORKERS IN ROADWAY	SPEED LIMIT 60	BEGIN \$250 FINE



GUIDELINES

- THIS DRAWING IS FOR USE ONLY AFTER AN ENGINEERING INVESTIGATION HAS BEEN PERFORMED BY THE REGIONAL TRAFFIC ENGINEER AND WORK ZONE TRAFFIC CONTROL SECTION. THE WORK ZONE "VARIABLE" SPEED LIMIT IS INTENDED FOR USE ON FREEWAYS WITH SPEED LIMITS 65 MPH OR GREATER. SEE WORK ZONE "VARIABLE" SPEED LIMIT GUIDELINES FOR CRITERIA.
- THE STATE TRAFFIC ENGINEER HAS TO ORDINANCE THE SPEED LIMIT REDUCTION IN ORDER FOR THE REDUCTION AND/OR \$250 SPEEDING FINE TO BE VALID AND ENFORCEABLE. NO SPEED LIMIT MESSAGE/SIGNS SHALL BE INSTALLED PRIOR TO RECEIVING A SIGNED ORDINANCE. IN ADDITION, THE \$250 SPEEDING FINE ALSO REQUIRES A SEPARATE SIGNED ORDINANCE BY THE STATE TRAFFIC ENGINEER.
- EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.
- THE MAXIMUM LANE CLOSURE LENGTH IS 2 MILES UNLESS OTHERWISE SHOWN IN THE PLANS. IN ADDITION, FOR ACTIVE WORK AREAS THAT EXCEED 1 MILE IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF ADDITIONAL CMS'S/SIGNS ARE NEEDED TO SUPPLEMENT THE INITIAL ONES. PORTABLE MOUNTED W3-5 SIGNS WITH SPEEDING PENALTY SIGNS ARE TO BE PLACED ALONG ENTRANCE RAMP LOCATED WITHIN THE ACTIVE WORK AREA.
- THE \$250 SPEEDING FINE PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION.
- THIS APPLICATION IS FOR SHORT-TERM ACTIVITIES SUCH AS LANE CLOSURES AND ROAD CLOSURES. THE WORK ZONE "VARIABLE" SPEED LIMIT SHALL NOT BE IN OPERATION CONTINUOUSLY (24/7) FOR A PERIOD EXCEEDING 30 CALENDAR DAYS. THE WORK ZONE "VARIABLE" SPEED LIMIT MESSAGING/SIGNAGE SHALL BE REMOVED AT THE COMPLETION OF THE ACTIVITY. THE REGIONAL TRAFFIC ENGINEER WILL BE NOTIFIED BY THE RESIDENT ENGINEER TO RESCIND THE ORDINANCE.
- WHEN WORK ZONE "VARIABLE" SPEED LIMIT REDUCTIONS ARE IN EFFECT, THE CONTRACTOR IS TO COVER ANY EXISTING SPEED LIMIT SIGNS LOCATED WITHIN THE ACTIVE WORK AREA THAT CONFLICT WITH THE WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION.



NOTES

- THE WORK ZONE "VARIABLE" SPEED LIMIT WILL BE ESTABLISHED IN COLLABORATION BETWEEN THE REGIONAL TRAFFIC ENGINEER, THE DIVISION, AND THE WORK ZONE TRAFFIC CONTROL SECTION. THIS DRAWING SHOWS THE TYPICAL APPLICATION OF REDUCING THE SPEED LIMIT TO 60 MPH.
- TYPICALLY THE WORK ZONE "VARIABLE" SPEED LIMIT APPLIES TO A SPECIFIC PORTION AND NOT THE ENTIRE PROJECT. THE WORK AREA AFFECTED BY THE SPEED REDUCTION SHOULD NOT EXCEED 2 MILES UNLESS THE MAXIMUM LENGTH OF LANE CLOSURE ALLOWED BY THE PLANS EXCEEDS THE 2 MILE MAXIMUM.
- THE WORK ZONE "VARIABLE" SPEED LIMIT AND THE \$250 SPEEDING FINE IS TO BEGIN 500' IN ADVANCE OF THE FLASHING ARROW PANEL AT THE MERGE TAPER. THE EXISTING SPEED LIMIT SIGNS AND THE "END \$250 FINE" SIGNS ARE TO BE INSTALLED AT THE LOCATION WHERE THE EXISTING SPEED LIMIT IS TO RESUME.
- THE NEED AND LOCATION OF ADDITIONAL POSTED WORK ZONE "VARIABLE" SPEED LIMIT SIGNS WITHIN THE LANE CLOSURE IS TO BE DETERMINED BY THE REGIONAL TRAFFIC ENGINEER.

APPROVED: <i>Shawn Kirt</i> DATE: 2/4/2018	SEAL			<h2 style="margin: 0;">WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION</h2>
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DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

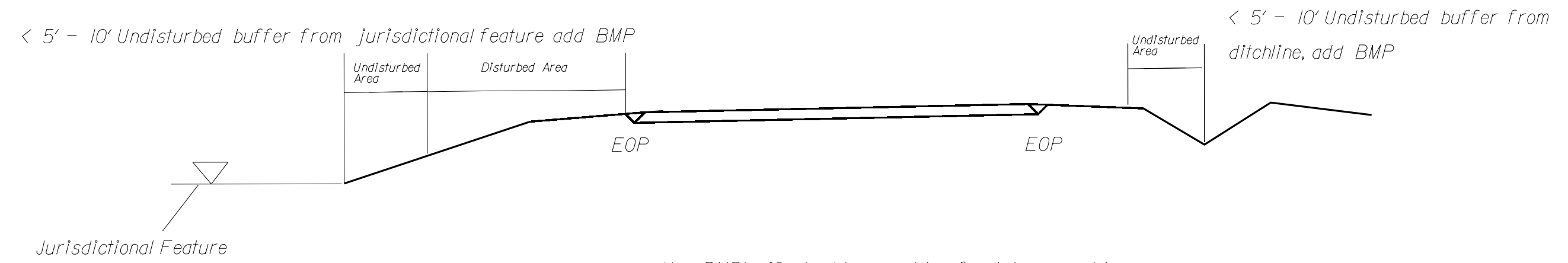
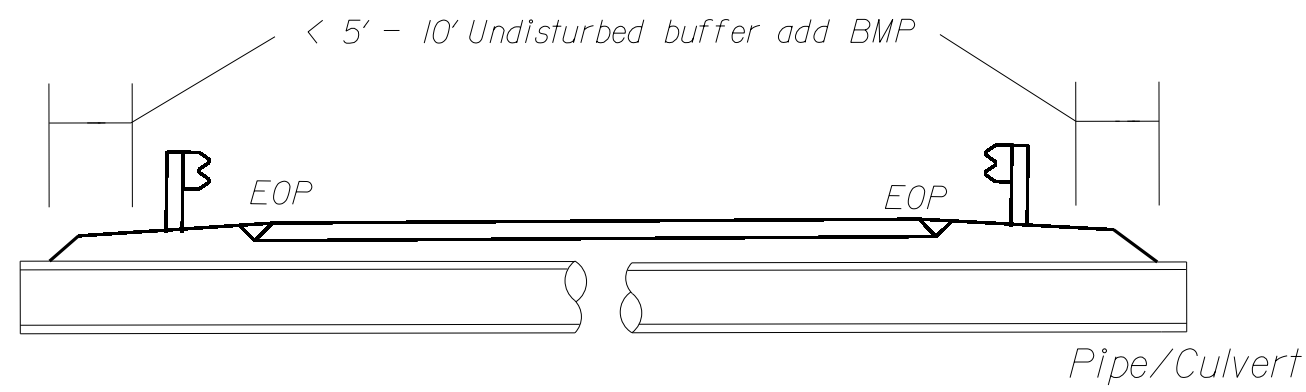
SOIL STABILIZATION TIMEFRAMES

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

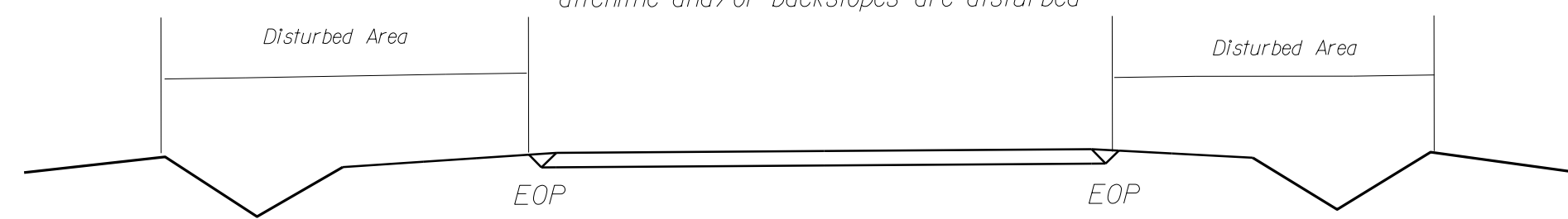
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

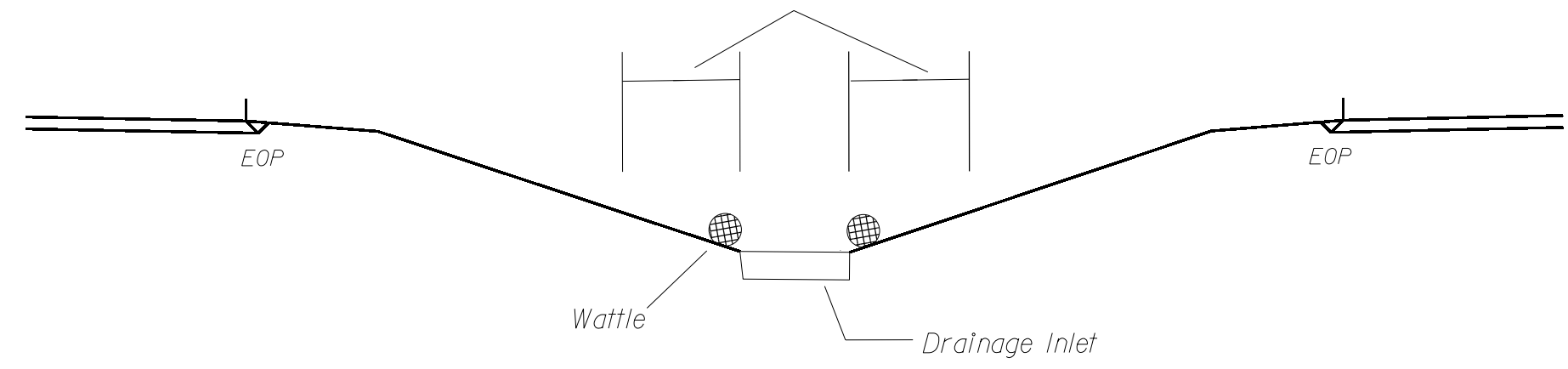
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

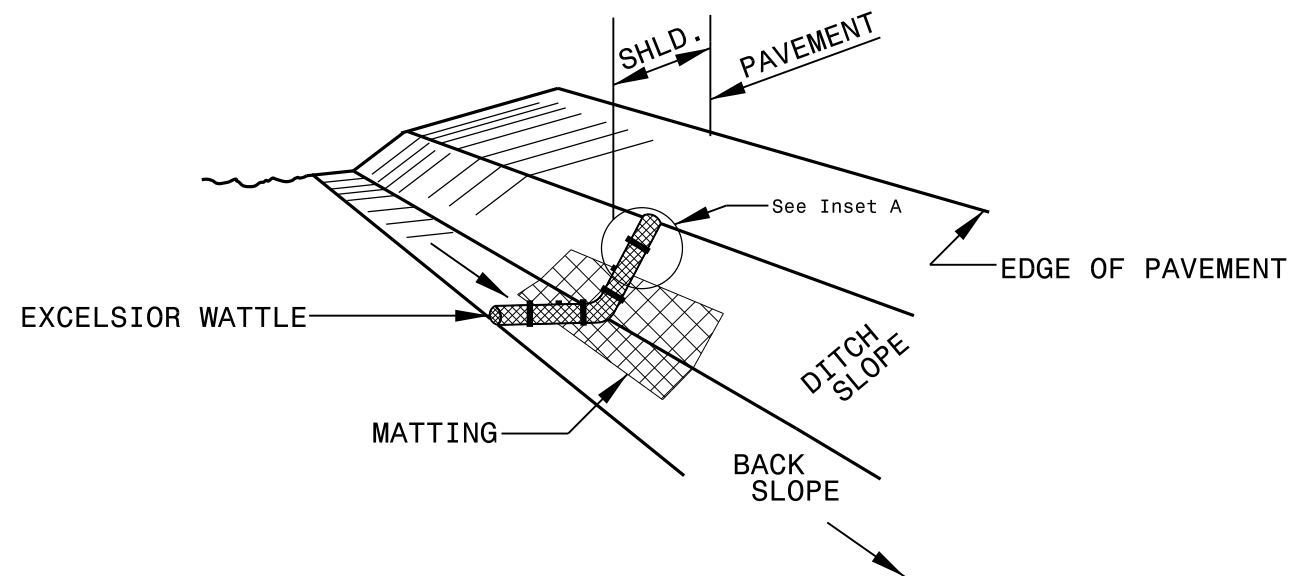


< 5' - 10' Undisturbed buffer from inlet, add wattle

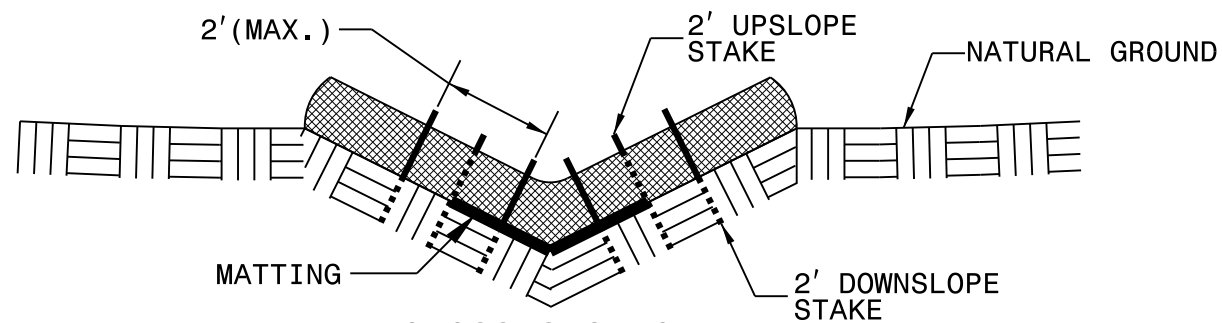


NOT TO SCALE

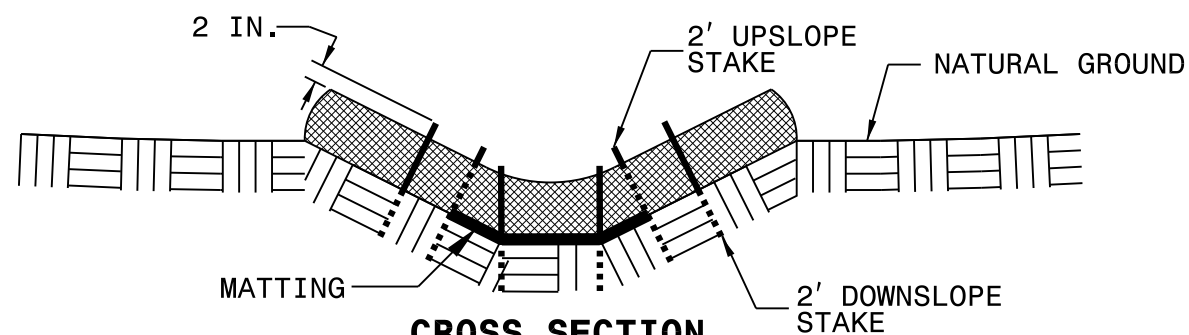
WATTLE DETAIL



ISOMETRIC VIEW



**CROSS SECTION
VEE DITCH**



**CROSS SECTION
TRAPEZOIDAL DITCH**

NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

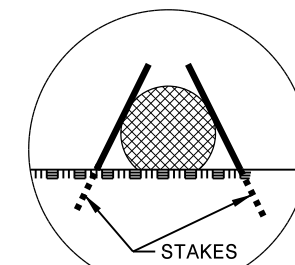
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

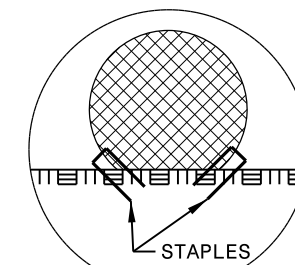
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

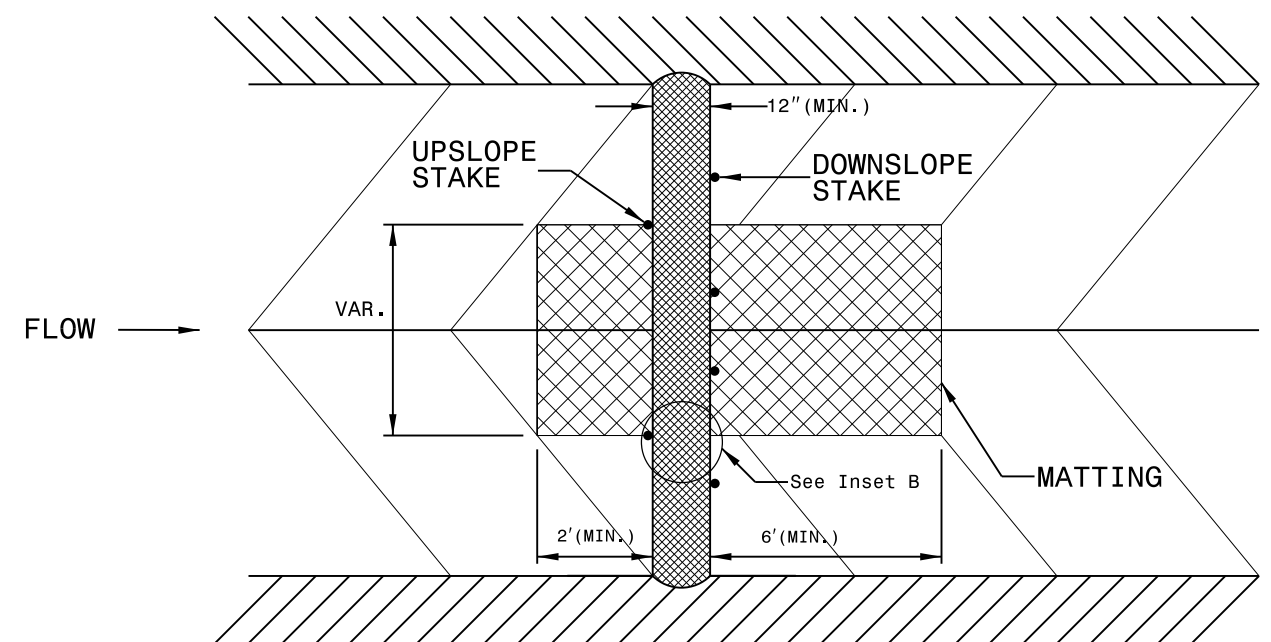
INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



INSET A



INSET B



TOP VIEW