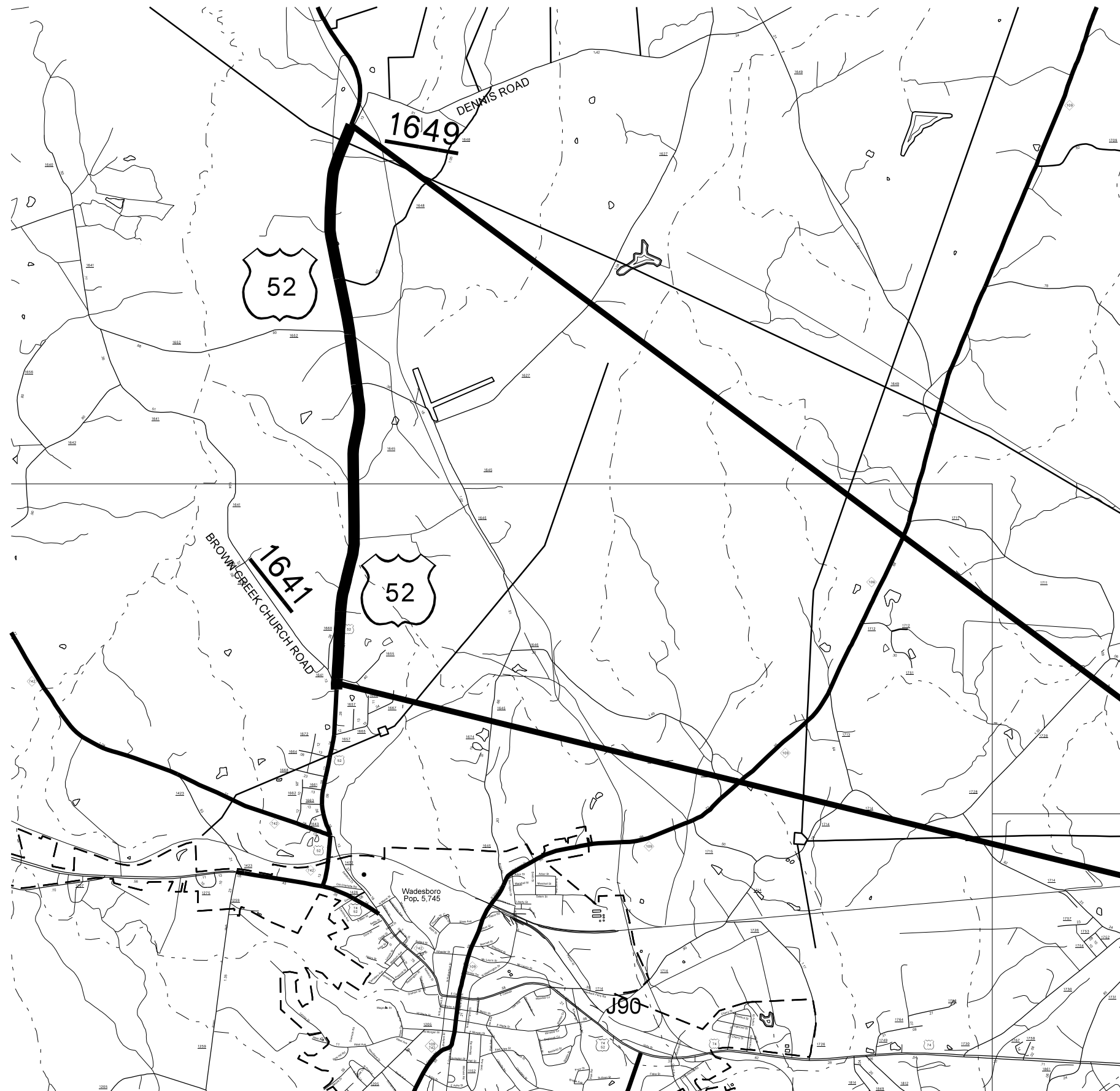


| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------|---|-----------|--------------|
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | | |
| F.A. PROJECT NO. | | | |



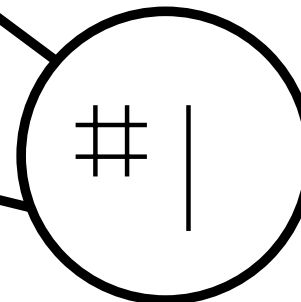
ENLARGED MUNICIPAL AND SUBURBAN AREAS

ANSON COUNTY

NORTH CAROLINA

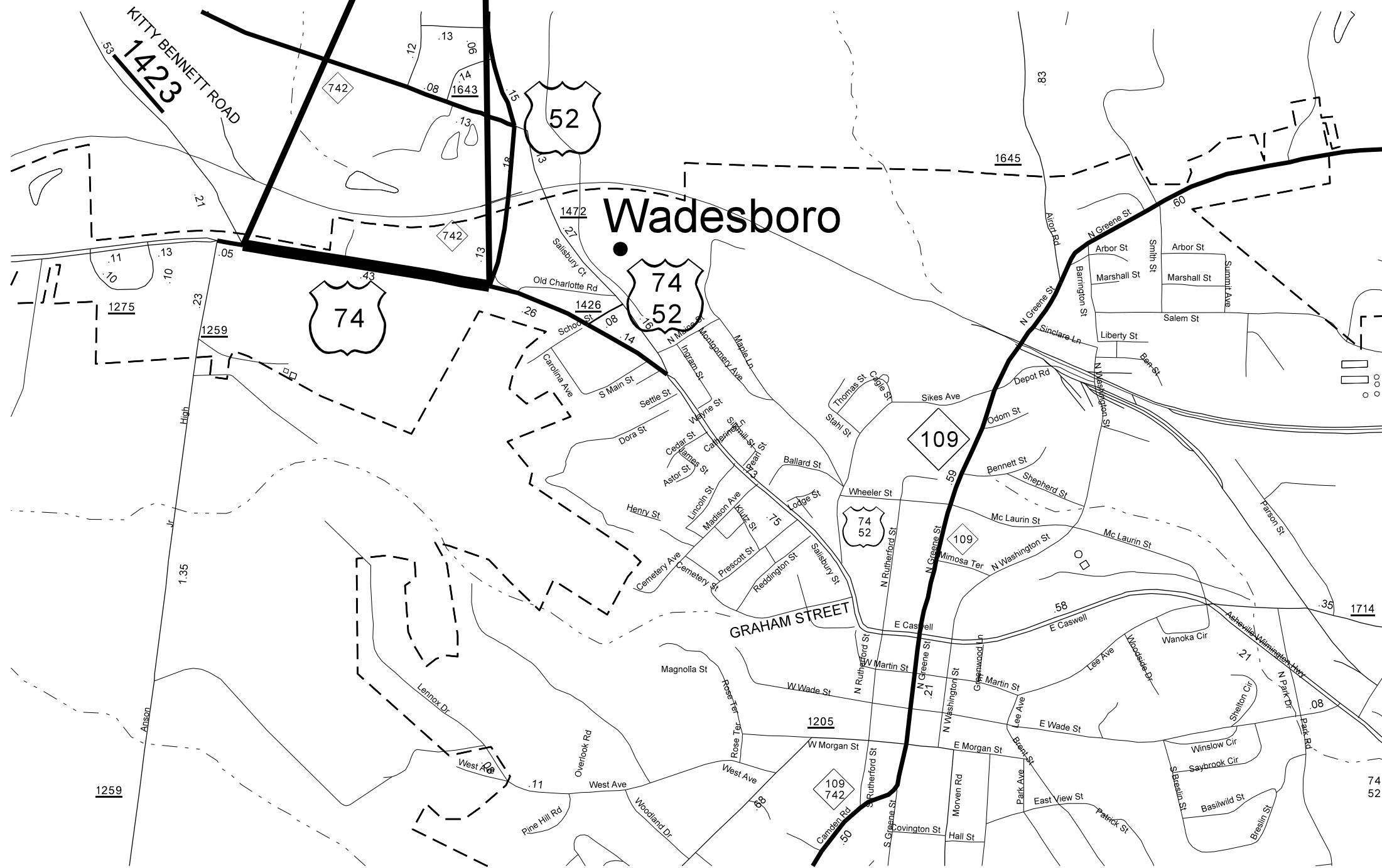
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

Map #1 US 52 NORTH
3.4 MILES
FROM PVMT JT AT SR 1641
BROWN CREEK CHURCH ROAD
TO PVMT JT 2050 FT SOUTH OF
SR 1649 DENNIS ROAD



| | | | |
|------------------|---|-----------|--------------|
| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 2 | |
| F.A. PROJECT NO. | | | |

2



ENLARGED MUNICIPAL AND SUBURBAN AREAS
ANSON COUNTY
 NORTH CAROLINA
PREPARED BY THE
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP # 2 US74
0.53 MILES
FROM US 52 NORTH TO
SR 1423 KITTY BENNETT ROAD

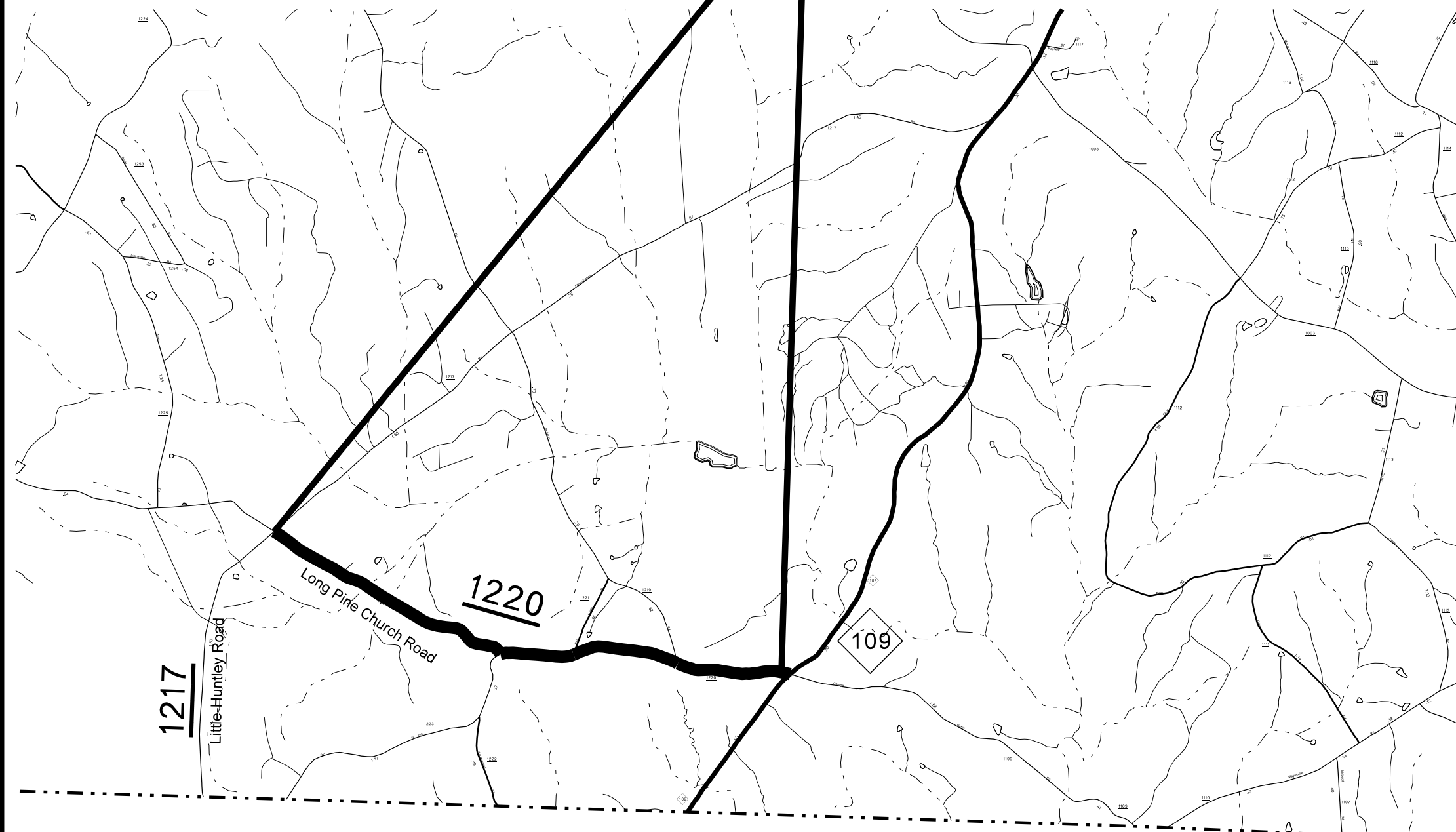
| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------|---|-----------|--------------|
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 3 | |
| F.A. PROJECT NO. | | | |

#3



ENLARGED MUNICIPAL AND SUBURBAN AREAS
ANSON COUNTY
 NORTH CAROLINA
PREPARED BY THE
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

Map #3 SR 1220 LONG PINE CHURCH ROAD
 2.9 MILES
 FROM SR 1217 LITTLE HUNTLEY ROAD
 TO NC 109 SOUTH



SOUTH CAROLINA

| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------|---|-----------|--------------|
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 4 | |
| F.A. PROJECT NO. | | | |

4



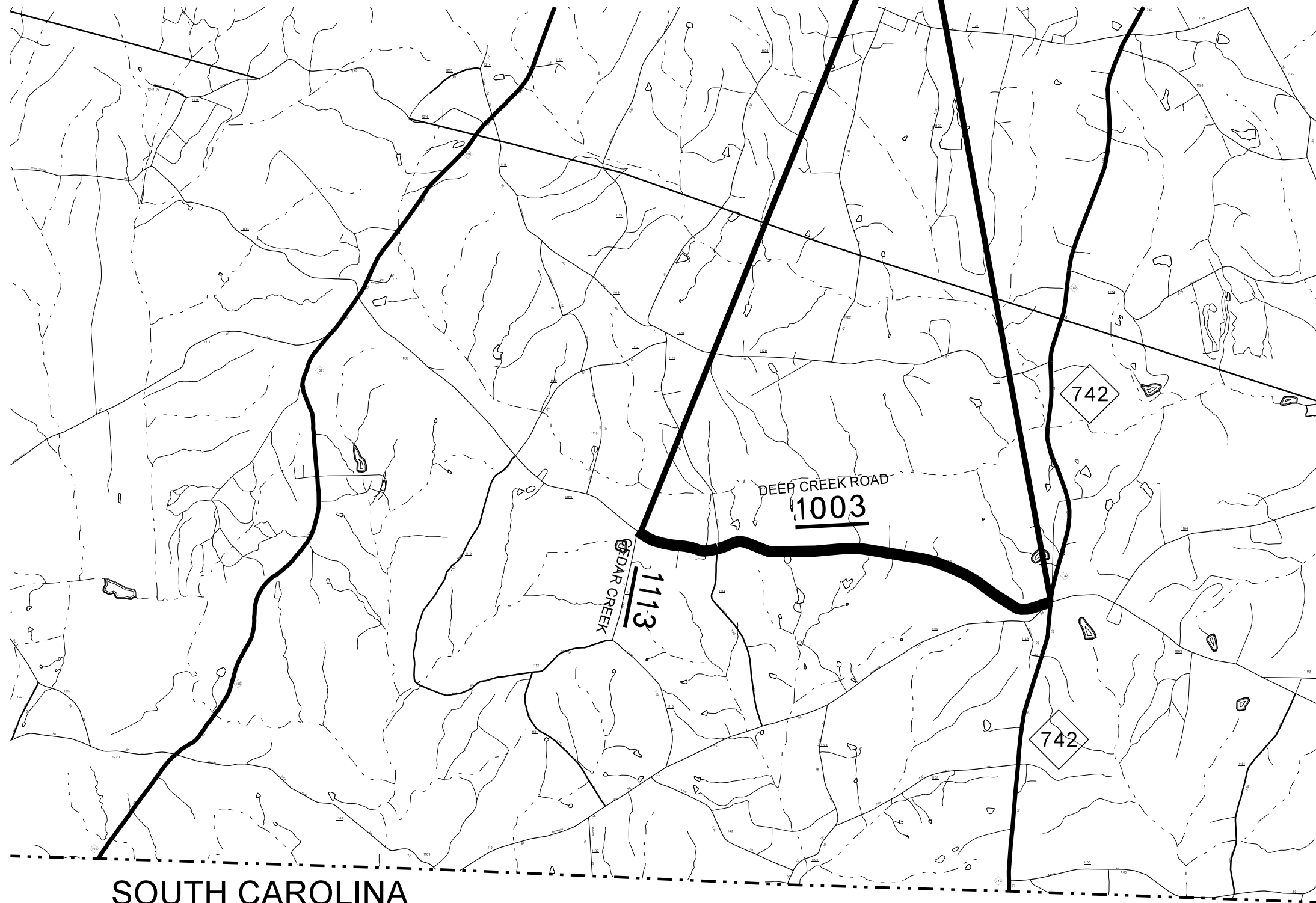
ENLARGED MUNICIPAL AND SUBURBAN AREAS

ANSON COUNTY

NORTH CAROLINA

PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #4 SR 1003 DEEP CREEK ROAD
3.0 MILES
FROM SR 1113 CEDAR CREEK ROAD
TO NC 742 SOUTH



SOUTH CAROLINA

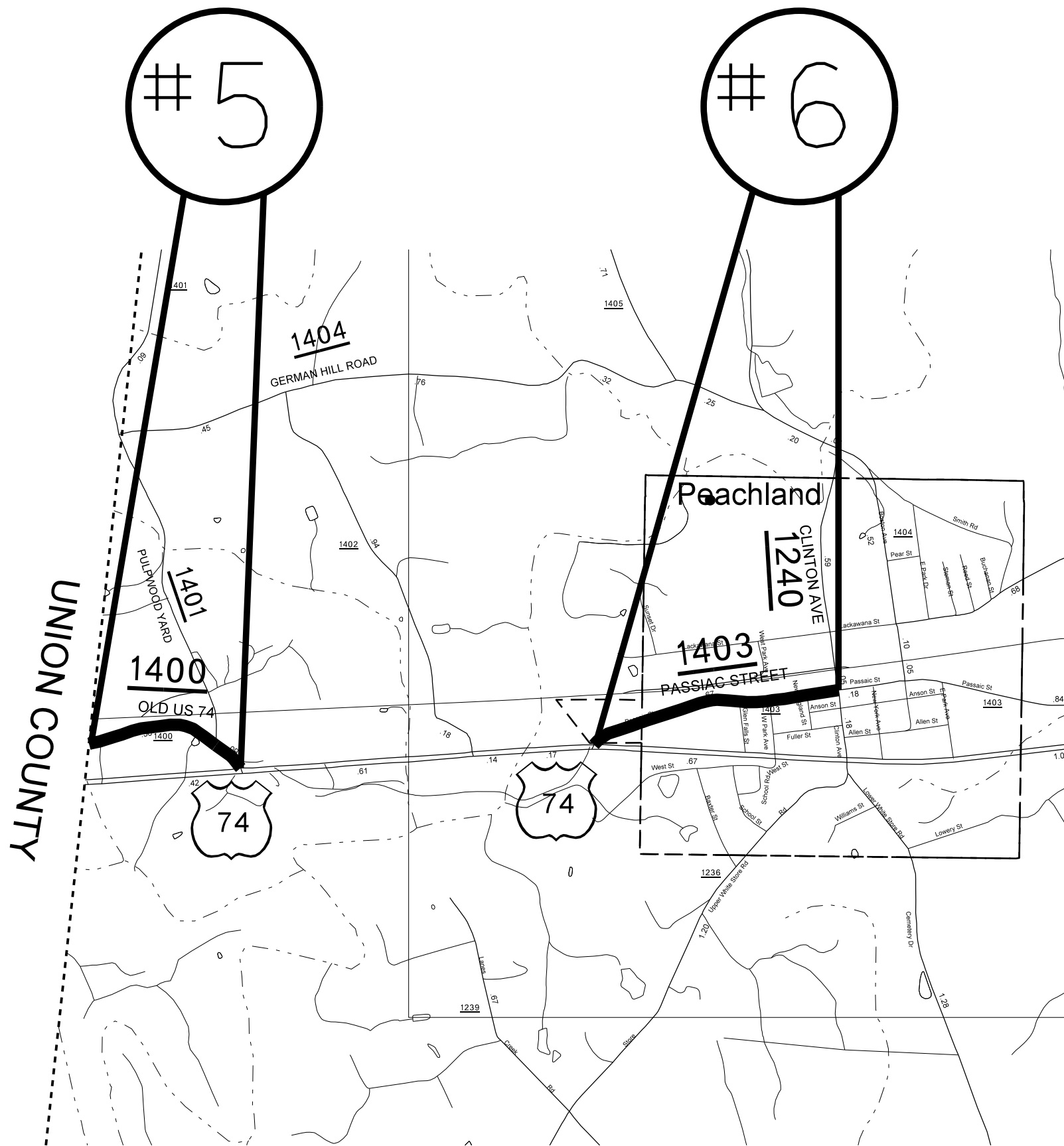
| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------|---|-----------|--------------|
| | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 5 | |
| F.A. PROJECT NO. | | | |



ENLARGED MUNICIPAL AND SUBURBAN AREAS
ANSON COUNTY
 NORTH CAROLINA
PREPARED BY THE
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #5 SR 1400 OLD US 74
0.44 MILES
FROM US 74 TO UNION COUNTY LINE

MAP #6 SR 1403 PASSIAC STREET
0.65 MILES
FROM US 74 TO SR 1240 CLINTON AVENUE



| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------|---|-----------|--------------|
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 6 | |
| F.A. PROJECT NO. | | | |



ENLARGED MUNICIPAL AND SUBURBAN AREAS

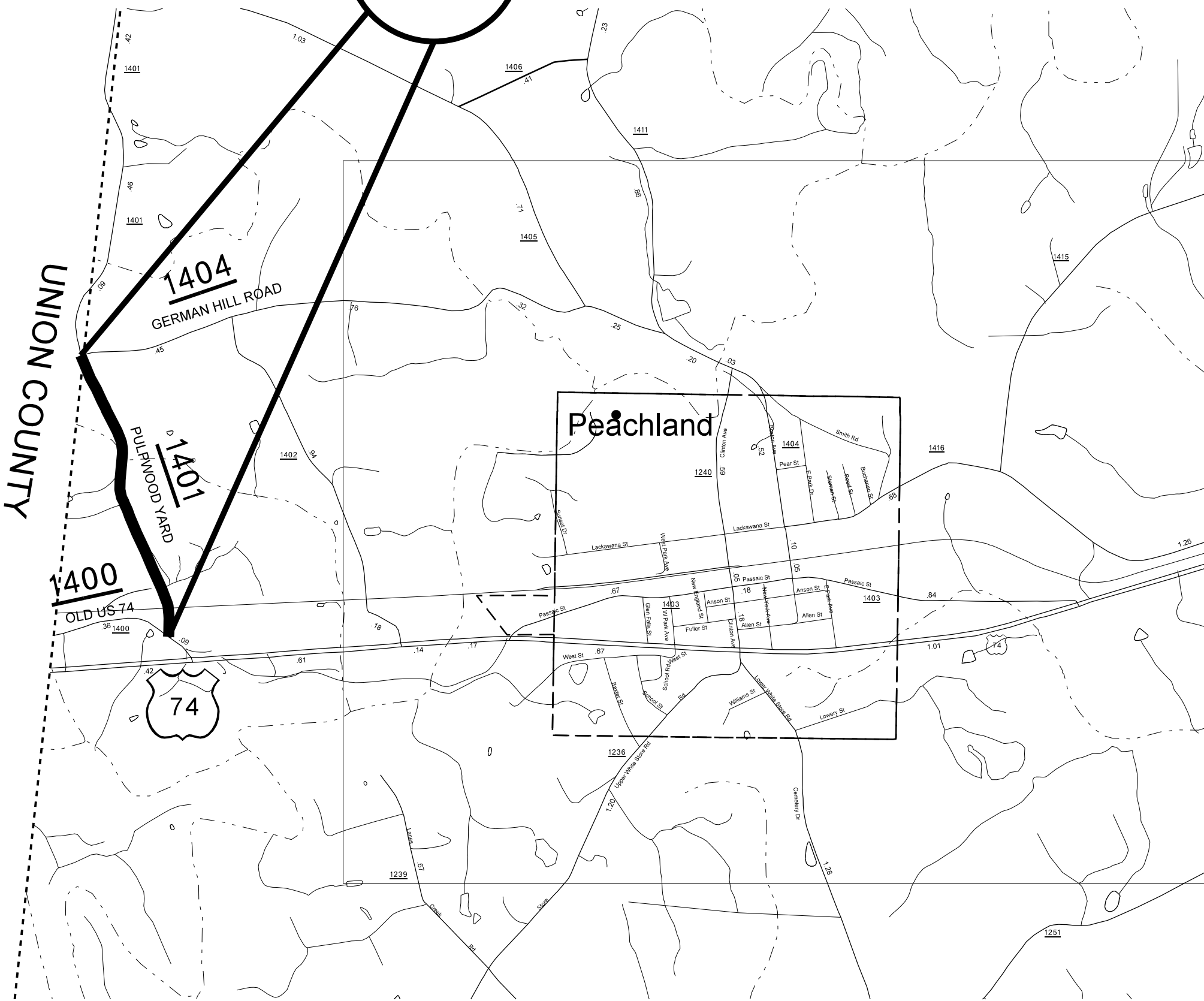
ANSON COUNTY

NORTH CAROLINA

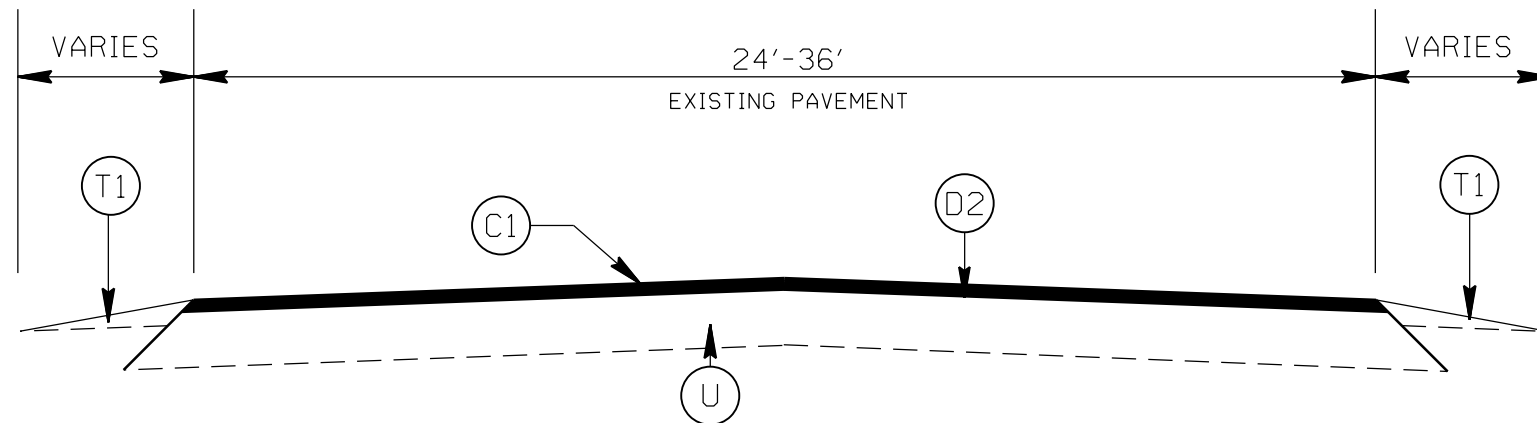
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #7 SR 1401 PULPWOOD YARD ROAD
0.9 MILES
FROM SR 1404 GERMAN HILL ROAD
TO SR 1400 OLD US 74

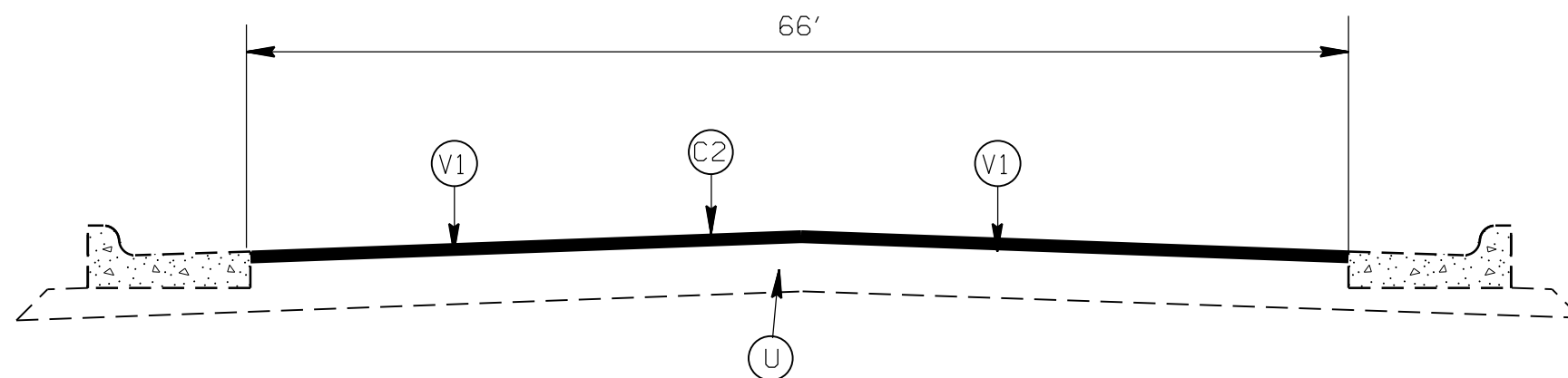
7



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| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 7 | |
| F.A. PROJECT NO. | | | |



TYPICAL SECTION NO. 1
US 52 NORTH (MAP 1)



TYPICAL SECTION NO. 2
US 74 (MAP 2)

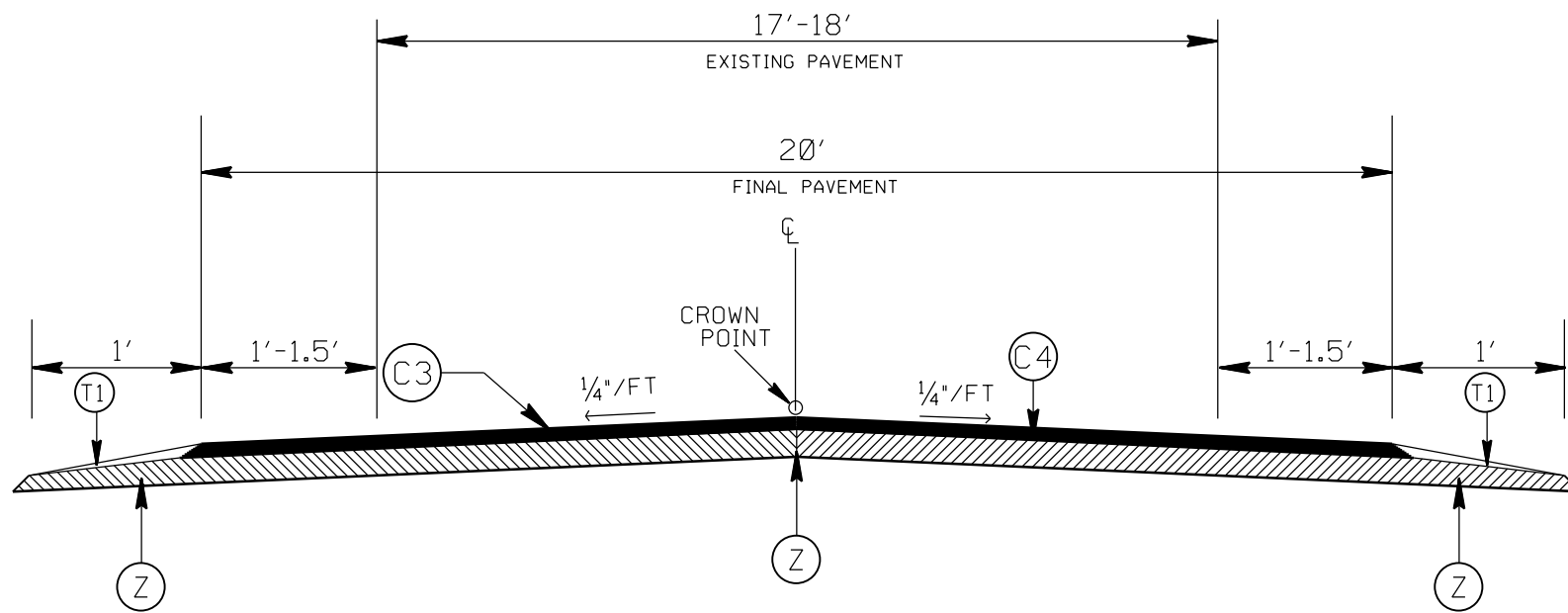
PAVEMENT SCHEDULE

| | |
|------|--|
| (C1) | PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| (C2) | PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| (C3) | PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. |
| (C4) | PROP. ASPHALT SURFACE TREATMENT, DOUBLE SEAL, (SEE S. P. FOR AGGREGATE TYPE.) |
| (D1) | PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| (D2) | AST MAT COAT # 67 |
| (E1) | PROP. APPROX. 5" ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS PER SQYD. |
| (T1) | SHOULDER RECONSTRUCTION |
| (T2) | SHOULDER CONSTRUCTION |
| (V1) | MILLING EXISTING ASPHALT, 1.5" DEPTH |
| (V2) | PROFILE MILLING EXISTING PAVEMENT, 0-1.5" |
| (V3) | MILLING EXISTING PAVEMENT, 5" IN DEPTH. (SEE S.P. FOR "TRENCHING FOR BASE COURSE BY MILLING.") |
| (U) | EXISTING PAVEMENT |
| (Z) | FULL DEPTH RECLAMATION |

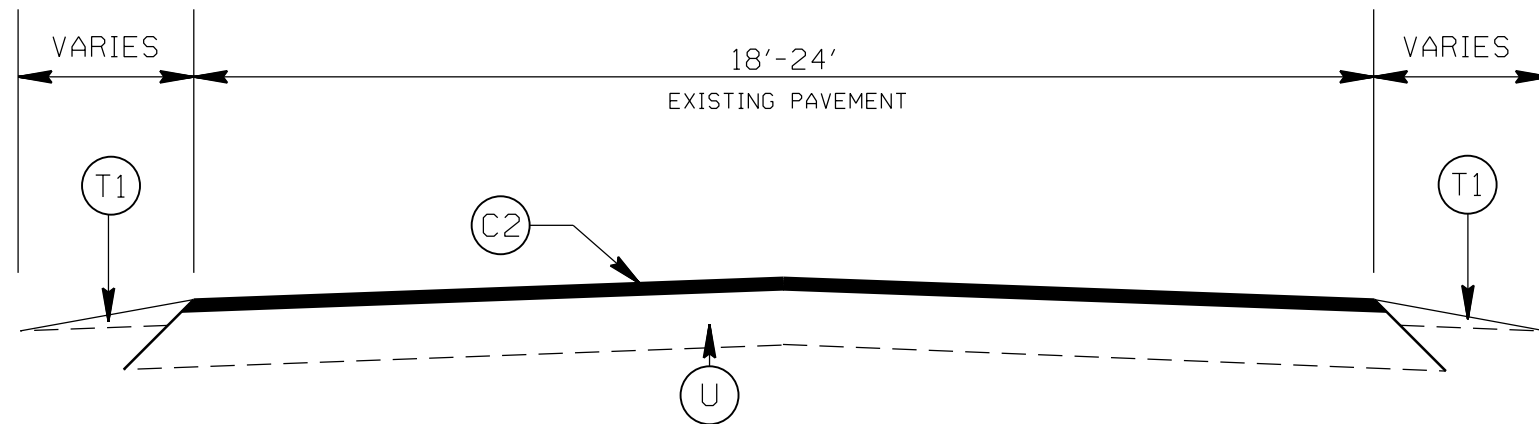
2019-2020
ANSON COUNTY RESURFACING

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| SCALE | -NA- | | REVISIONS | |
| DATE | 08/18 | | | |
| DWG. BY | AMO | | | |
| DESIGN BY | AMO | | | |
| APPROVED | CLA | | | |

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| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 8 | |
| F.A. PROJECT NO. | | | |



TYPICAL SECTION NO. 3
SR 1220 LONG PINE CHURCH ROAD (MAP 3)



TYPICAL SECTION NO. 4
SR 1403 W. PASSIAC STREET (MAP 6)
APPROX. STA. 10+00 TO STA. 38+40

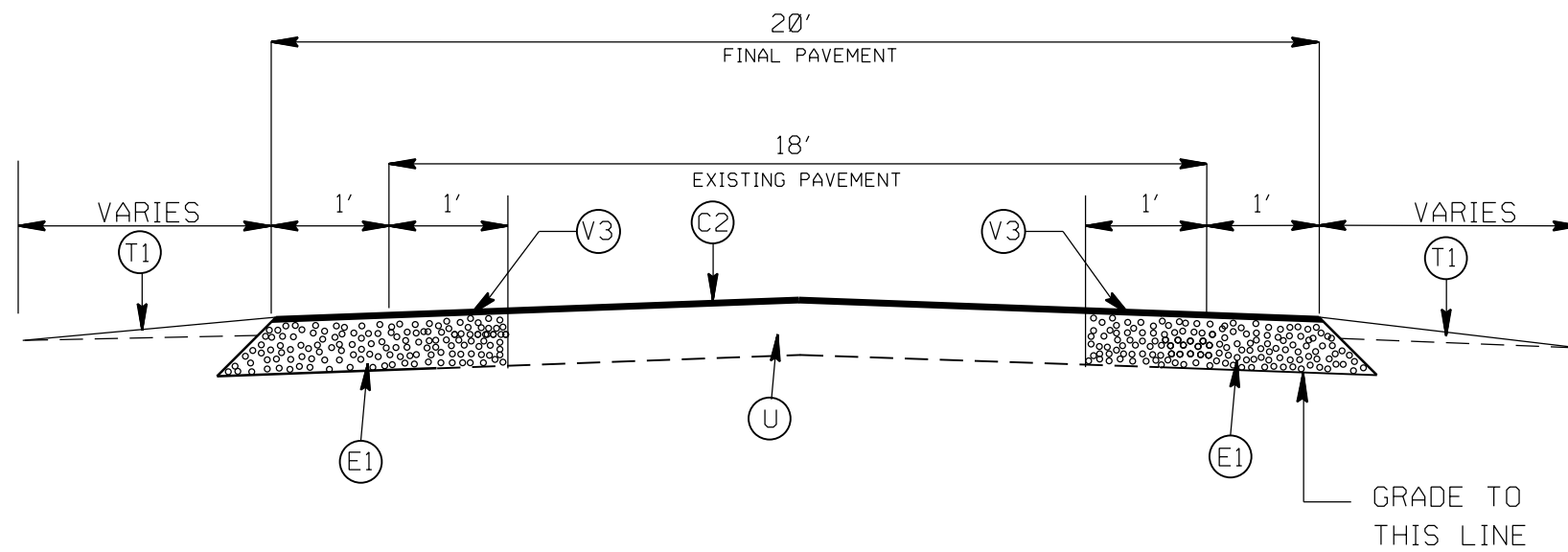
PAVEMENT SCHEDULE

| | |
|------|--|
| (C1) | PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| (C2) | PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| (C3) | PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. |
| (C4) | PROP. ASPHALT SURFACE TREATMENT, DOUBLE SEAL, (SEE S. P. FOR AGGREGATE TYPE.) |
| (D1) | PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| (D2) | AST MAT COAT # 67 |
| (E1) | PROP. APPROX. 5" ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS PER SQ. YD. |
| (T1) | SHOULDER RECONSTRUCTION |
| (T2) | SHOULDER CONSTRUCTION |
| (V1) | MILLING EXISTING ASPHALT, 1.5" DEPTH |
| (V2) | PROFILE MILLING EXISTING PAVEMENT, 0-1.5" |
| (V3) | MILLING EXISTING PAVEMENT, 5" IN DEPTH. (SEE S.P. FOR "TRENCHING FOR BASE COURSE BY MILLING.") |
| (U) | EXISTING PAVEMENT |
| (Z) | FULL DEPTH RECLAMATION |

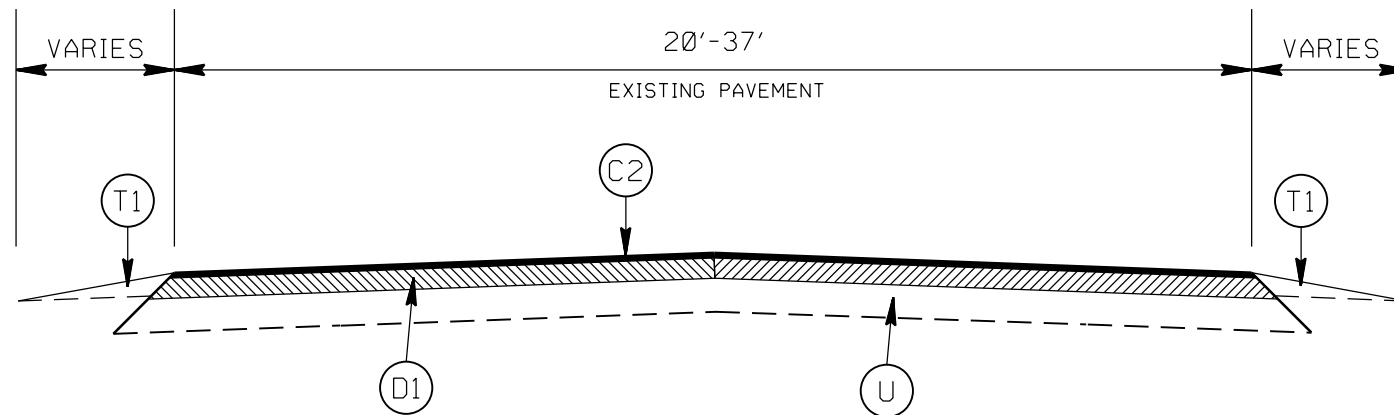
2019-2020
ANSON COUNTY RESURFACING

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|-----------|-------|---|-----------|
| SCALE | -NA- |  | REVISIONS |
| DATE | 08/18 | | |
| DWG. BY | AMO | | |
| DESIGN BY | AMO | | |
| APPROVED | CLA | | |

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| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.J0.09.10041 2020CPT.J0.09.20041 2020CPT.J0.09.20042 | 9 | |
| F.A. PROJECT NO. | | | |



TYPICAL SECTION NO. 5
SR 1003 DEEP CREEK ROAD (MAP 4)



TYPICAL SECTION NO. 6
SR 1400 OLD US 74 (MAP 5)
APPROX. STA. 10+00 TO STA. 11+90
SR 1401 PULPWOOD YARD ROAD (MAP 7)

PAVEMENT SCHEDULE

| | |
|------|--|
| (C1) | PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| (C2) | PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| (C3) | PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. |
| (C4) | PROP. ASPHALT SURFACE TREATMENT, DOUBLE SEAL, (SEE S. P. FOR AGGREGATE TYPE.) |
| (D1) | PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| (D2) | AST MAT COAT # 67 |
| (E1) | PROP. APPROX. 5" ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS PER SQYD. |
| (T1) | SHOULDER RECONSTRUCTION |
| (T2) | SHOULDER CONSTRUCTION |
| (V1) | MILLING EXISTING ASPHALT, 1.5" DEPTH |
| (V2) | PROFILE MILLING EXISTING PAVEMENT, 0-1.5" |
| (V3) | MILLING EXISTING PAVEMENT, 5" IN DEPTH. (SEE S.P. FOR "TRENCHING FOR BASE COURSE BY MILLING.") |
| (U) | EXISTING PAVEMENT |
| (Z) | FULL DEPTH RECLAMATION |

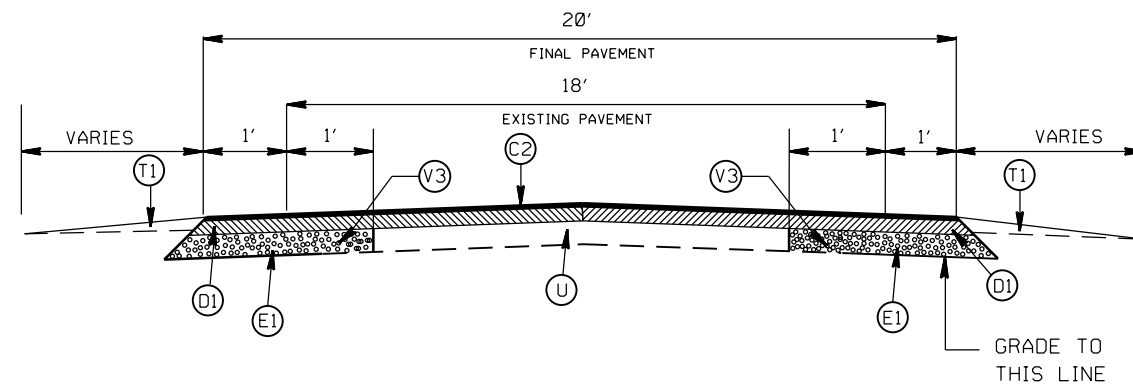
2019-2020
ANSON COUNTY RESURFACING

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| SCALE | -NA- |
| DATE | 08/18 |
| DWG. BY | AMO |
| DESIGN BY | AMO |
| APPROVED | CLA |

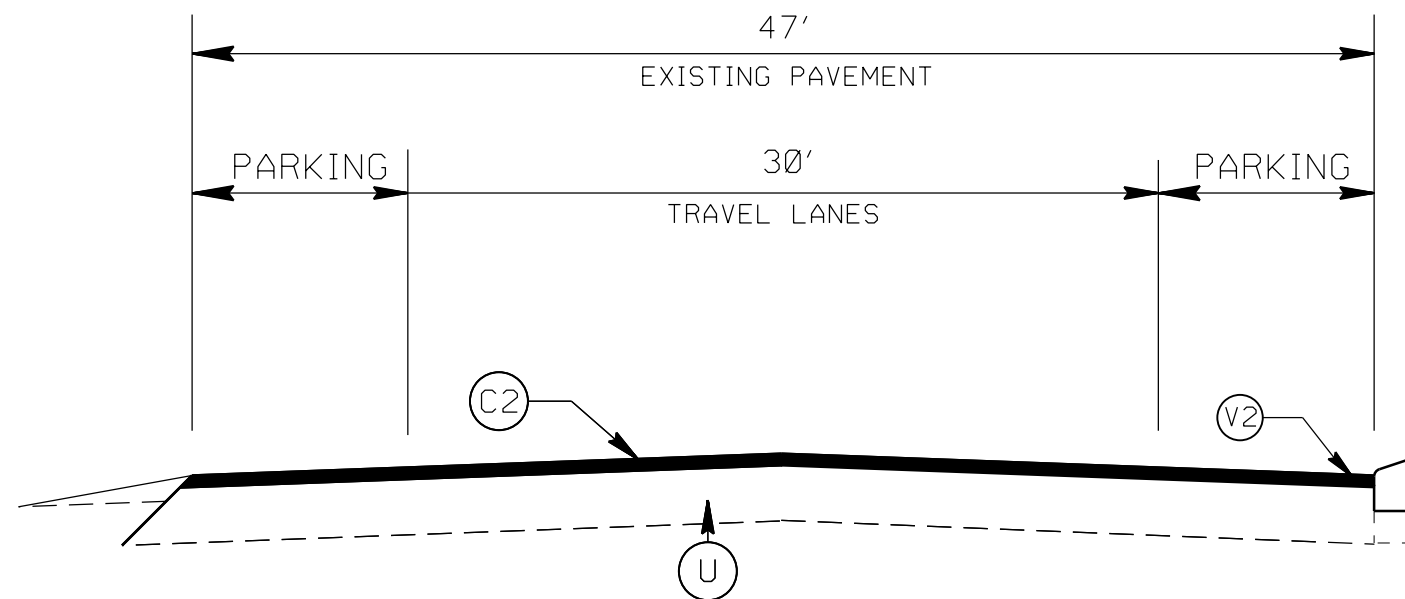


| REVISIONS | |
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| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 10 | |
| F.A. PROJECT NO. | | | |



TYPICAL SECTION NO. 7
SR 1400 OLD US 74 (MAP 5)
APPROX. STA. 11+90 TO STA. 31+15



TYPICAL SECTION NO. 8
SR 1403 W. PASSIAC STREET (MAP 6)
APPROX. STA. 38+40 TO STA. 40+05

PAVEMENT SCHEDULE

| | |
|------|--|
| (C1) | PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| (C2) | PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| (C3) | PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. |
| (C4) | PROP. ASPHALT SURFACE TREATMENT, DOUBLE SEAL, (SEE S. P. FOR AGGREGATE TYPE.) |
| (D1) | PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| (D2) | AST MAT COAT # 67 |
| (E1) | PROP. APPROX. 5" ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS PER SQ. YD. |
| (T1) | SHOULDER RECONSTRUCTION |
| (T2) | SHOULDER CONSTRUCTION |
| (V1) | MILLING EXISTING ASPHALT, 1.5" DEPTH |
| (V2) | PROFILE MILLING EXISTING PAVEMENT, 0-1.5" |
| (V3) | MILLING EXISTING PAVEMENT, 5" IN DEPTH. (SEE S.P. FOR "TRENCHING FOR BASE COURSE BY MILLING.") |
| (U) | EXISTING PAVEMENT |
| (Z) | FULL DEPTH RECLAMATION |

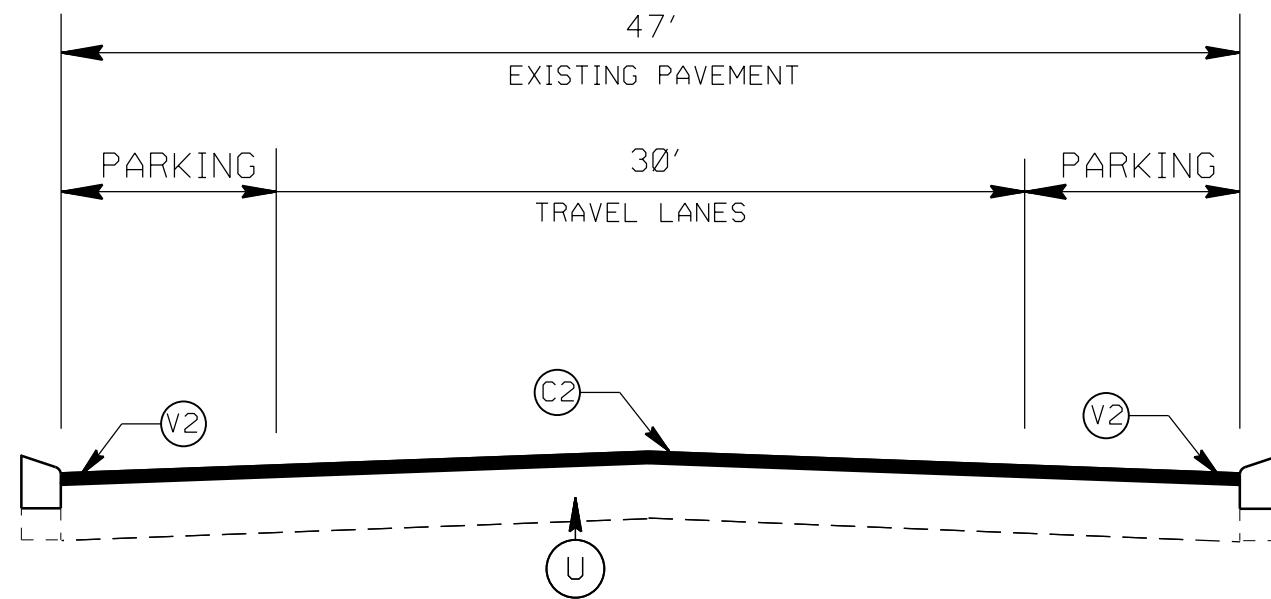
2019-2020
ANSON COUNTY RESURFACING

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|-----------|-------|--|-----------|--|
| SCALE | -NA- | | REVISIONS | |
| DATE | 08/18 | | | |
| DWG. BY | AMO | | | |
| DESIGN BY | AMO | | | |
| APPROVED | CLA | | | |

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|------------------|---|-----------|--------------|
| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | | |
| F.A. PROJECT NO. | | | |

PAVEMENT SCHEDULE

| | |
|------|--|
| (C1) | PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| (C2) | PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| (C3) | PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. |
| (C4) | PROP. ASPHALT SURFACE TREATMENT, DOUBLE SEAL, (SEE S. P. FOR AGGREGATE TYPE.) |
| (D1) | PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| (D2) | AST MAT COAT # 67 |
| (E1) | PROP. APPROX. 5" ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS PER SQYD. |
| (T1) | SHOULDER RECONSTRUCTION |
| (T2) | SHOULDER CONSTRUCTION |
| (V1) | MILLING EXISTING ASPHALT, 1.5" DEPTH |
| (V2) | PROFILE MILLING EXISTING PAVEMENT, 0-1.5" |
| (V3) | MILLING EXISTING PAVEMENT, 5" IN DEPTH. (SEE S.P. FOR "TRENCHING FOR BASE COURSE BY MILLING.") |
| (U) | EXISTING PAVEMENT |
| (Z) | FULL DEPTH RECLAMATION |



TYPICAL SECTION NO. 9
SR 1403 W. PASSIAC STREET (MAP 6)
APPROX. STA. 40+05 TO STA. 45+07

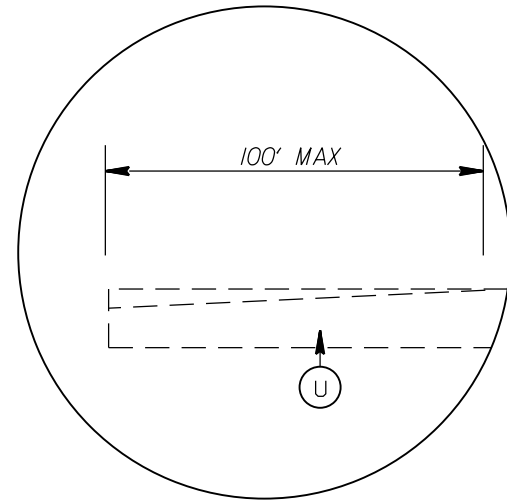
2019-2020
ANSON COUNTY RESURFACING

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| SCALE | -NA- | | REVISIONS | |
| DATE | 08/18 | | | |
| DWG. BY | AMO | | | |
| DESIGN BY | AMO | | | |
| APPROVED | CLA | | | |

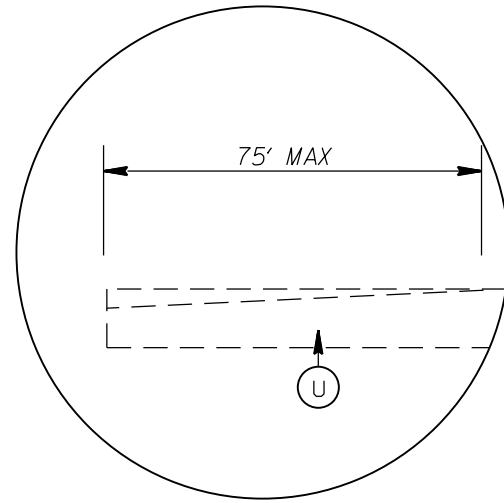
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| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | 2020CPT.I0.09.I0041 2020CPT.I0.09.20041 2020CPT.I0.09.20042 | 12 | |
| F.A. PROJECT NO. | | | |

PAVEMENT SCHEDULE

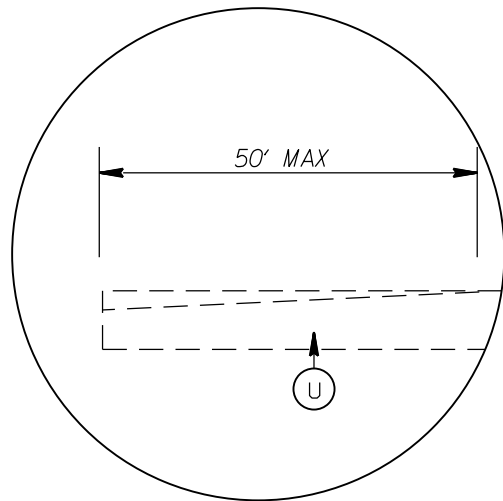
| | |
|------|--|
| (C1) | PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| (C2) | PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| (C3) | PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. |
| (C4) | PROP. ASPHALT SURFACE TREATMENT, DOUBLE SEAL, (SEE S. P. FOR AGGREGATE TYPE.) |
| (D1) | PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| (D2) | AST MAT COAT # 67 |
| (E1) | PROP. APPROX. 5" ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS PER SQYD. |
| (T1) | SHOULDER RECONSTRUCTION |
| (T2) | SHOULDER CONSTRUCTION |
| (V1) | MILLING EXISTING ASPHALT, 1.5" DEPTH |
| (V2) | PROFILE MILLING EXISTING PAVEMENT, 0-1.5" |
| (V3) | MILLING EXISTING PAVEMENT, 5" IN DEPTH. (SEE S.P. FOR "TRENCHING FOR BASE COURSE BY MILLING.") |
| (U) | EXISTING PAVEMENT |
| (Z) | FULL DEPTH RECLAMATION |



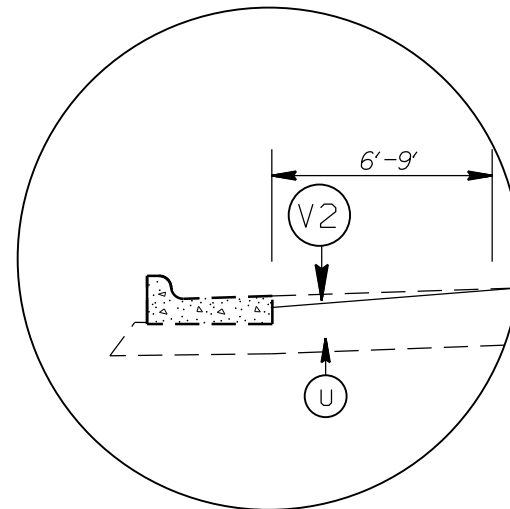
DETAIL FOR INCIDENTAL MILLING (0" TO 2.0")



DETAIL FOR INCIDENTAL MILLING (0" TO 1.5")

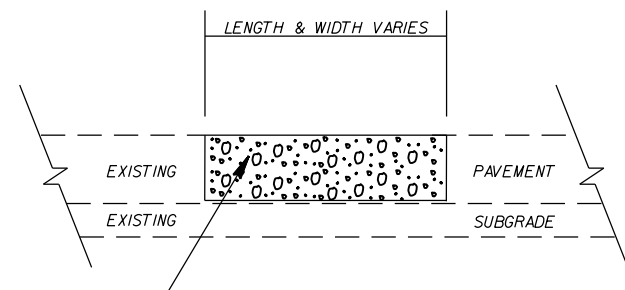


DETAIL FOR INCIDENTAL MILLING (0" TO 1.0")



DETAIL FOR PROFILE MILLING

PATCHING DETAIL



RATE IS VARIABLE AND SHALL BE AS DIRECTED BY THE ENGINEER. ASPHALT TYPE I19.0C SHALL BE PLACED.


2019-2020
ANSON COUNTY RESURFACING

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|-----------|-------|--|-----------|
| SCALE | -NA- | | REVISIONS |
| DATE | 08/18 | | |
| DWG. BY | AMO | | |
| DESIGN BY | AMO | | |
| APPROVED | CLA | | |

| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------|---|-----------|--------------|
| N.C. | 2020CPT.10.09.10041 2020CPT.10.09.20041 2020CPT.10.09.20042 | 13 | |
| F.A. PROJECT NO. | | | |

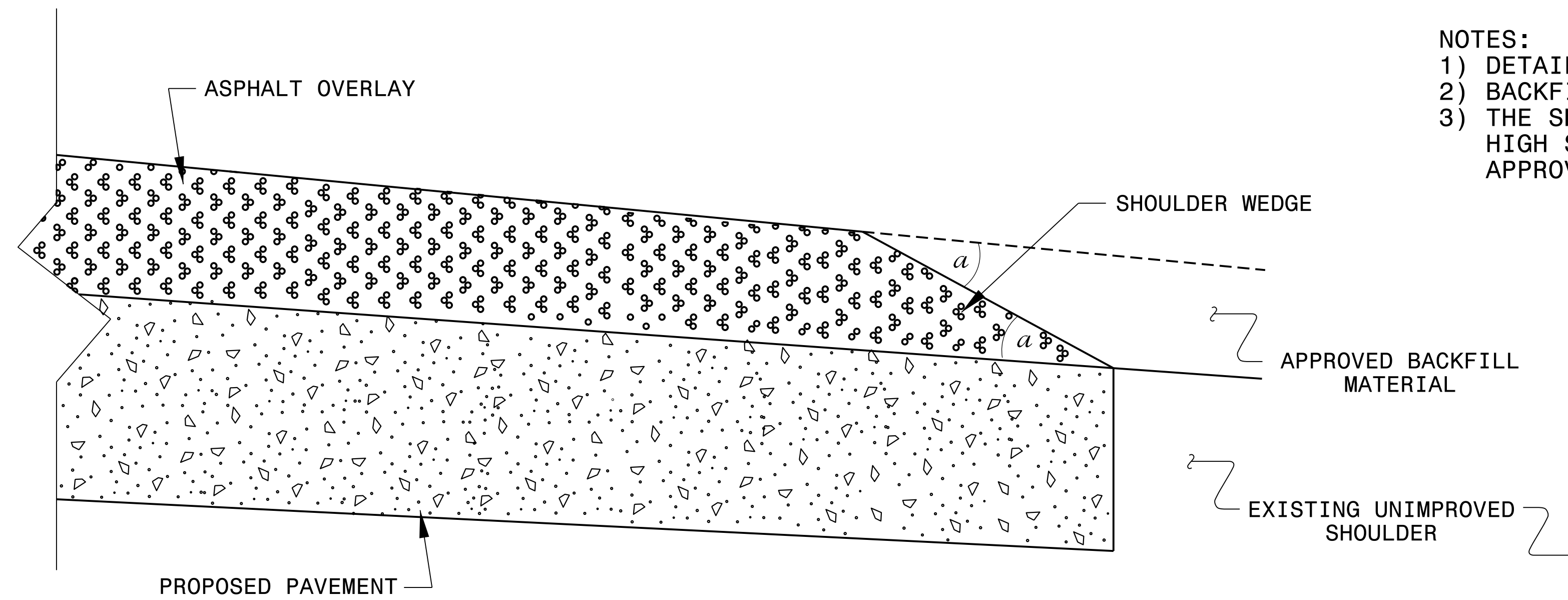
NOTES:

- 1: SHOULDER CONSTRUCTION/RECONSTRUCTION WILL BE AS DIRECTED BY THE ENGINEER.*
- 2: FULL DEPTH RECLAMATION AND AST SHALL BE COMPLETED, ON MAP 3, PRIOR TO THE ASPHALT OVERLAY.*
- 3: WIDENING ON FULL DEPTH RECLAMATION MAP TO BE DONE BY STATE FORCES.*

| 2019-2020 ANSON COUNTY RESURFACING | | | |
|---------------------------------------|-------|---|-----------|
| SCALE | -NA- |  | REVISIONS |
| DATE | 08/18 | | |
| DWG. BY | AMO | | |
| DESIGN BY | AMO | | |
| APPROVED | CLA | | |

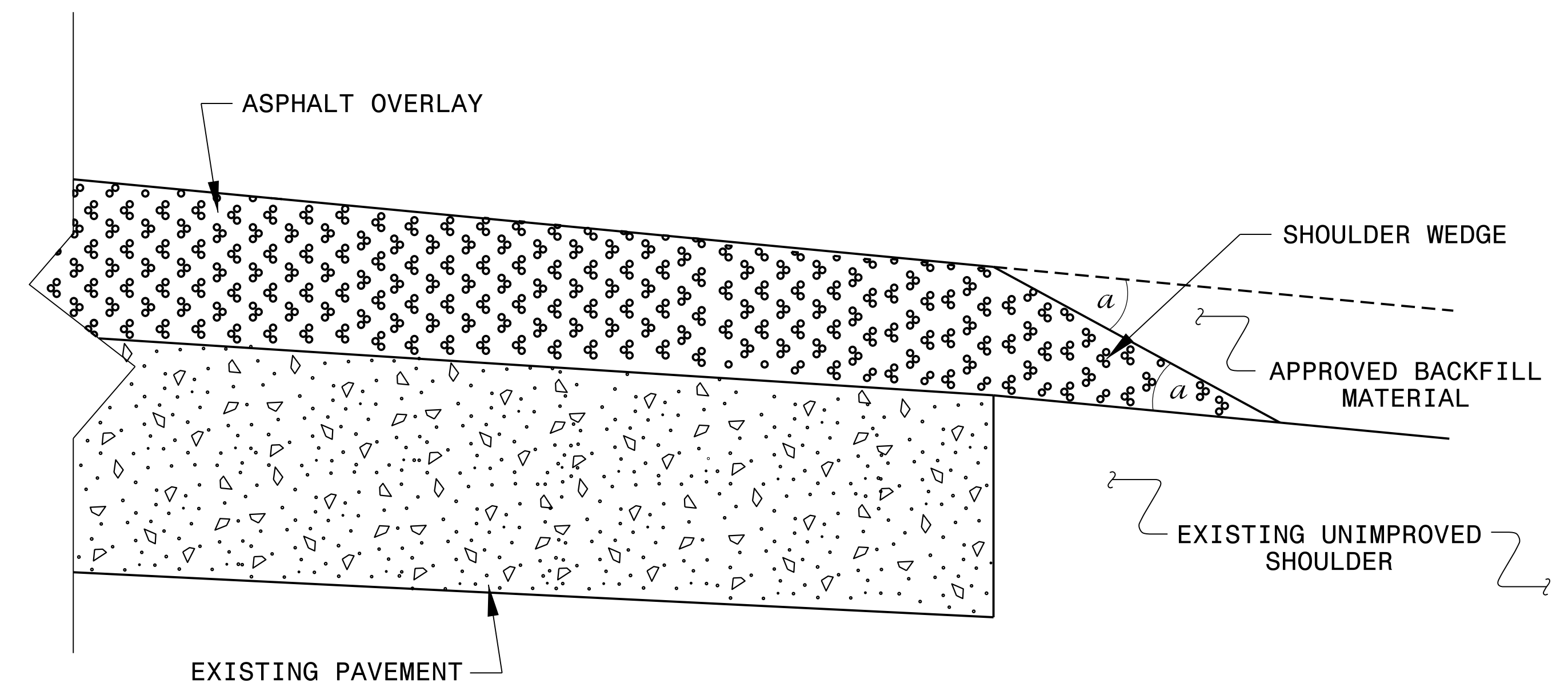
NOTES:

- 1) DETAIL DOES NOT APPLY TO OGAFc AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



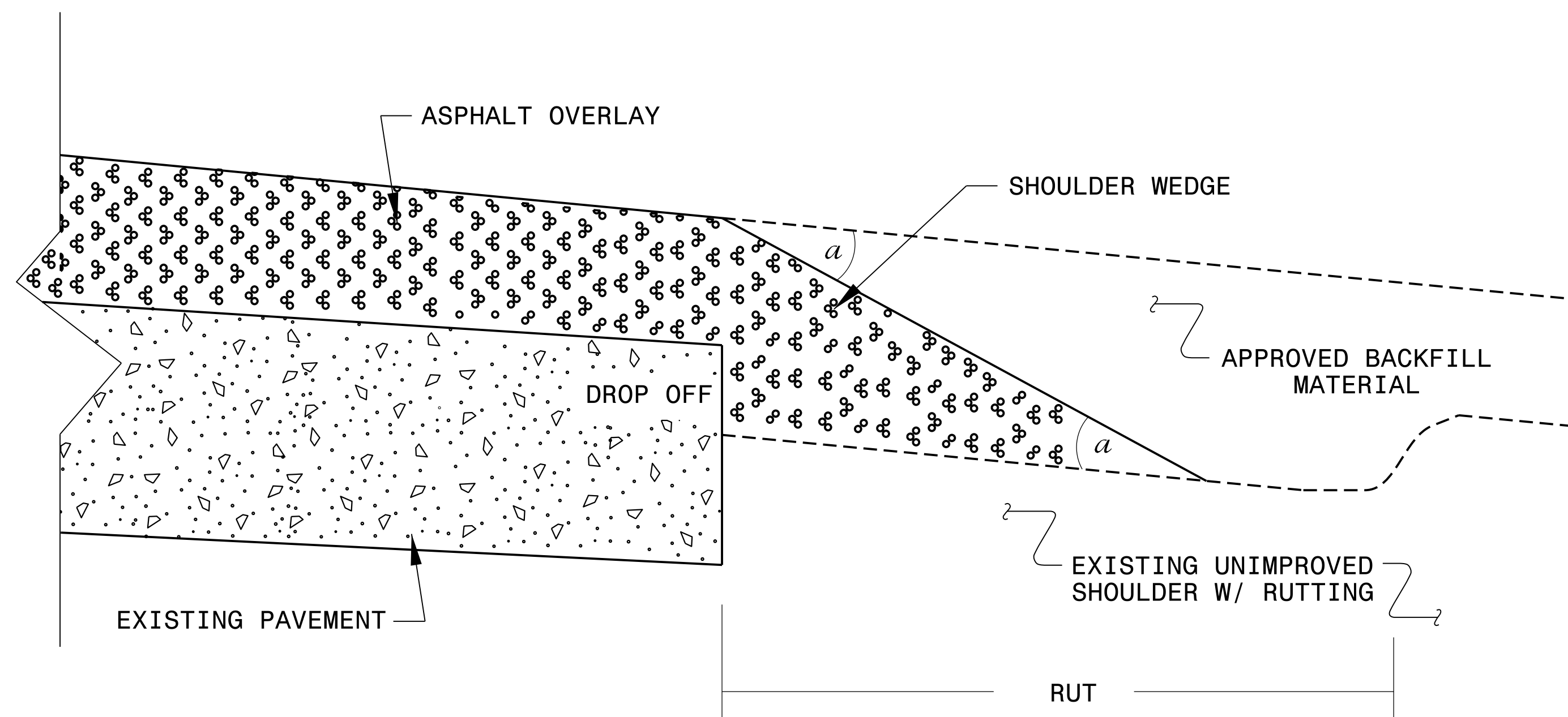
SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°



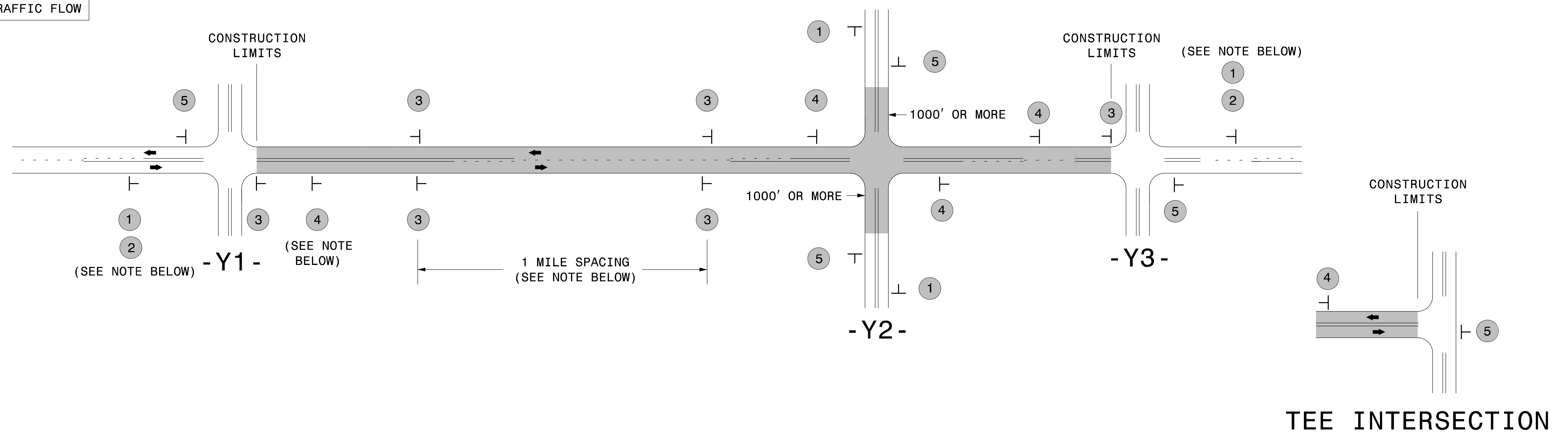
| | |
|--|---------------|
| CONTRACT STANDARDS AND DEVELOPMENT UNIT | |
| Office 919-707-6950 FAX 919-250-4119 | |
| SHOULDER WEDGE DETAILS | |
| ORIGINAL BY: T. SPELL | DATE: 7-19-11 |
| MODIFIED BY: | DATE: 2/2/16 |
| CHECKED BY: | DATE: |
| FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn | |

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

I2-SEP-2018 10:10 S:\Contracts\Resurfacing Projects\Shoulder Wedge Details\Revised Shoulder Wedge Detail.dgn J:\over-ton AT_CSD-232595

SIGNING FOR RESURFACING PROJECTS

LEGEND
 ┆ STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

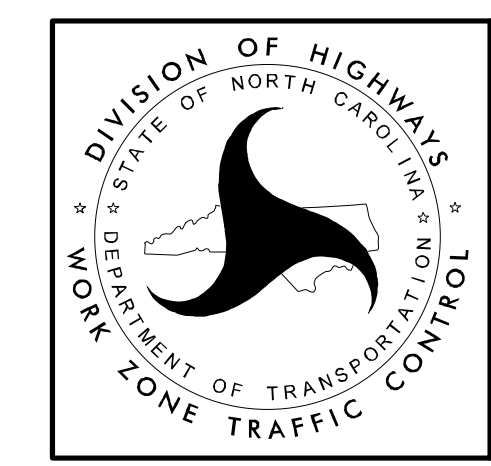
-Y- LINE SIGNING

| | | | | |
|---|---|--|---|---|
| SIGNING NOTES AND PLACEMENT PER DIRECTION | 1 | | PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. | <p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER. </div> </div> |
| | 2 | | #2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS) | |
| | 3 | | - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER. | |
| | 4 | | - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE. | |
| | 5 | | PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION. | |

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



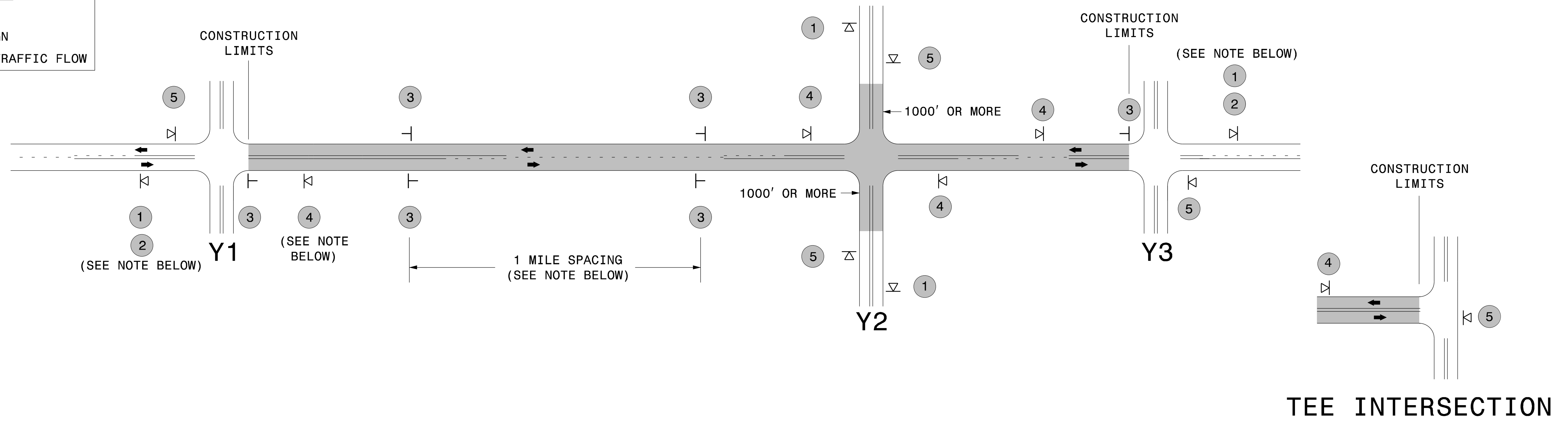
ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

5/15/2017 S:\TMU\WZTC\Resurfacing\2L2W & AST Resurfacing Details\Resurfacing_AdvWarn_2Ln.dgn User:kadai

SIGNING FOR ASPHALT SURFACE TREATMENT

LEGEND

- ▷ PORTABLE SIGN
- └ STATIONARY SIGN
- ← DIRECTION OF TRAFFIC FLOW



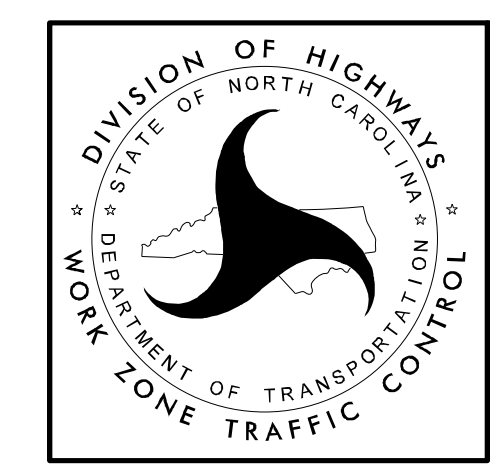
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

| | | | | |
|---|---|------|--|--|
| SIGNING NOTES AND PLACEMENT PER DIRECTION | 1 | | <ul style="list-style-type: none"> - PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. - SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS. | <p>STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER. </div> </div> |
| | 3 | | <ul style="list-style-type: none"> - ALTERNATE THE FOLLOWING TWO SIGNS: - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER. | |
| | 4 | | <ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. | |
| | 5 | | <ul style="list-style-type: none"> - PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION. | |
| | <p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p> | | | |

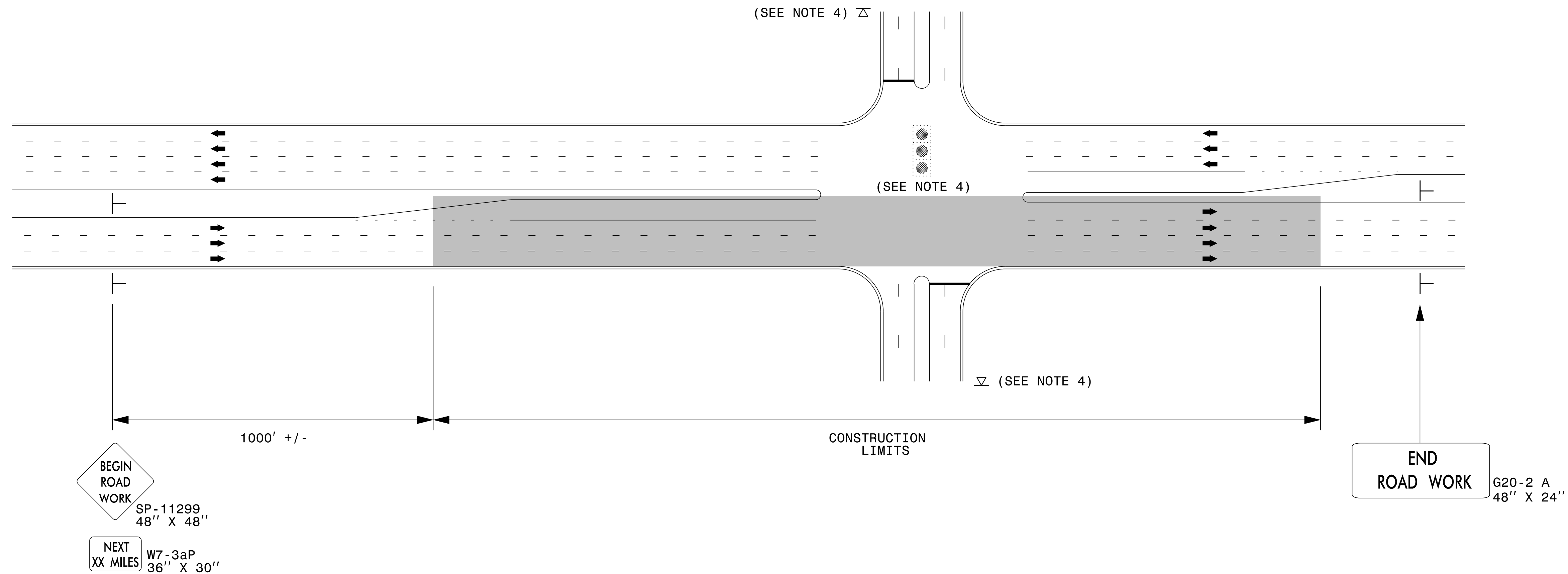
MAPS LESS THAN 2 MILES

FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.



ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT

URBAN / SUBURBAN WORKZONES

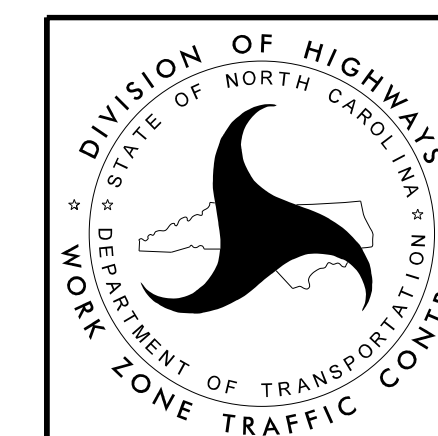


NOTES:

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

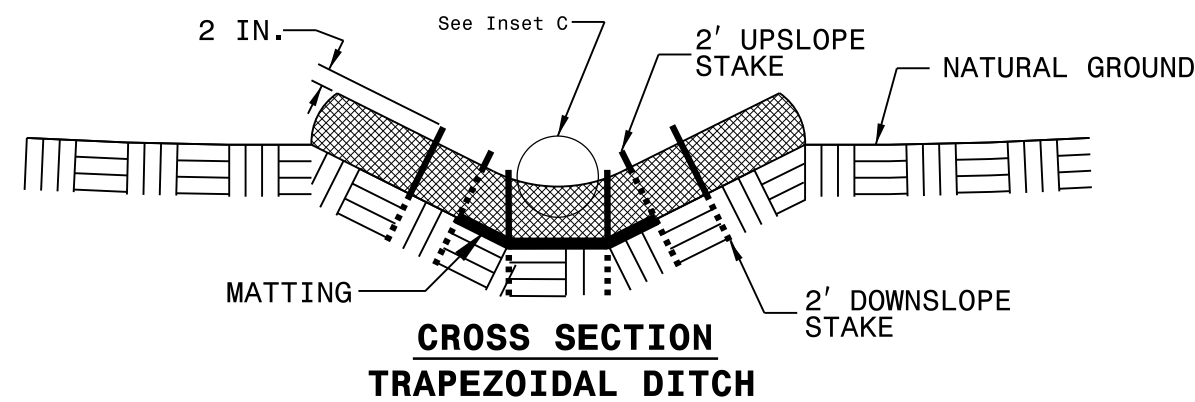
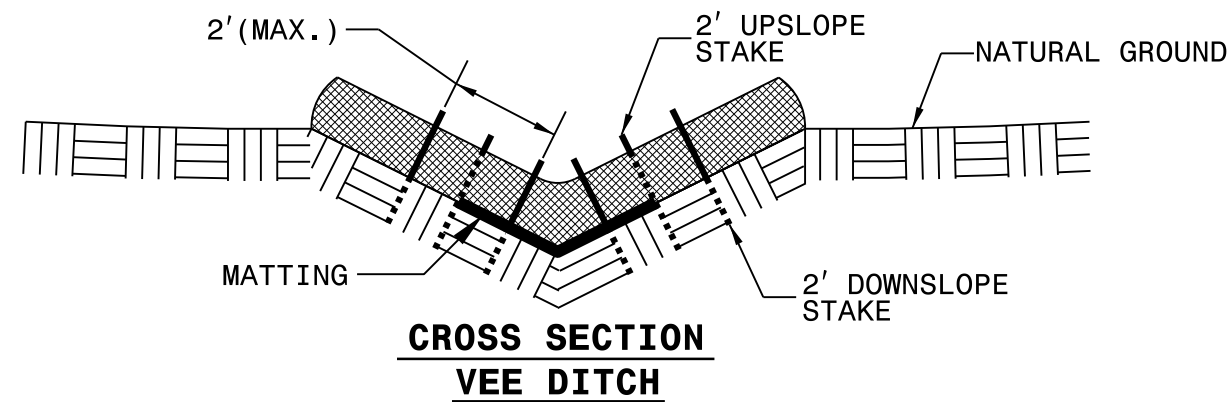
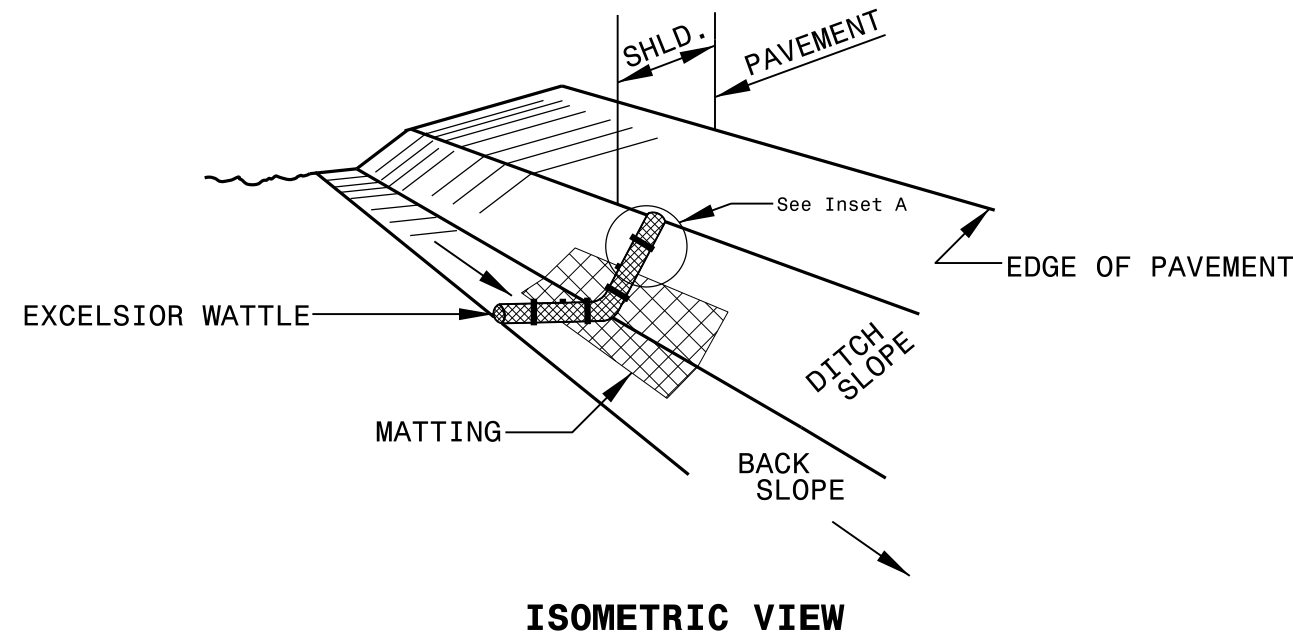
LEGEND

- ┆ STATIONARY SIGN
- ➔ DIRECTION OF TRAFFIC FLOW



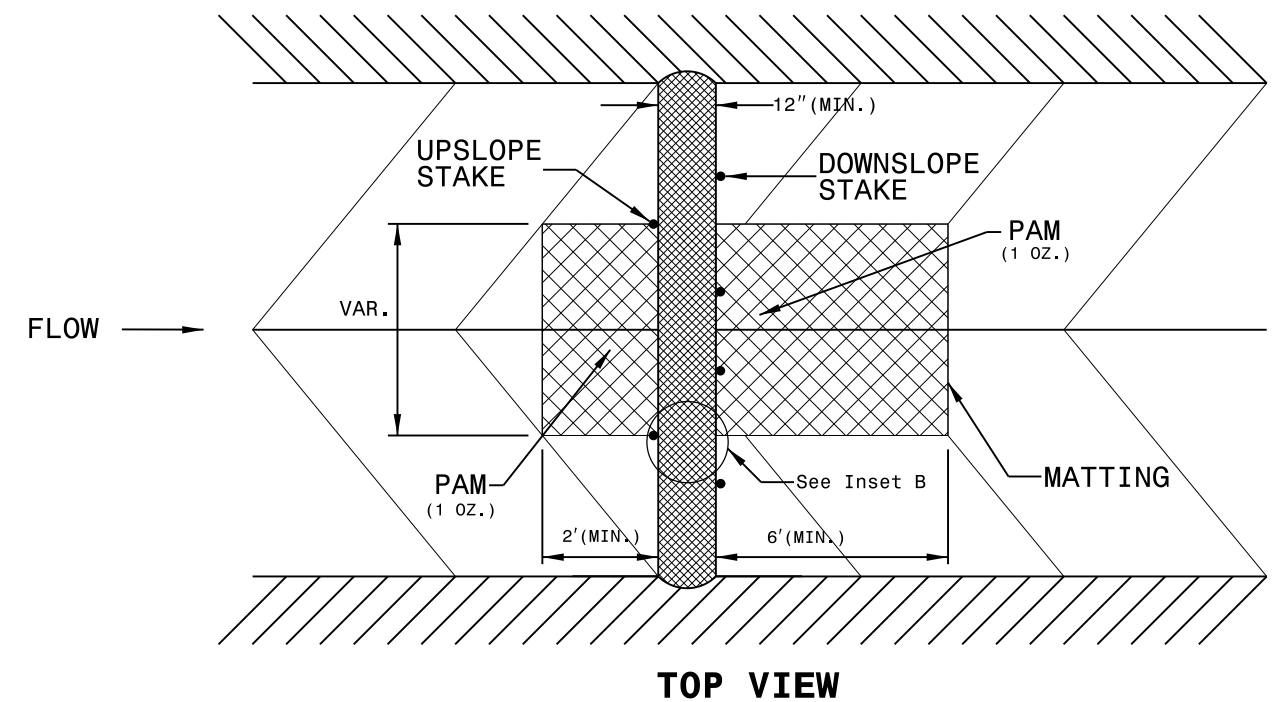
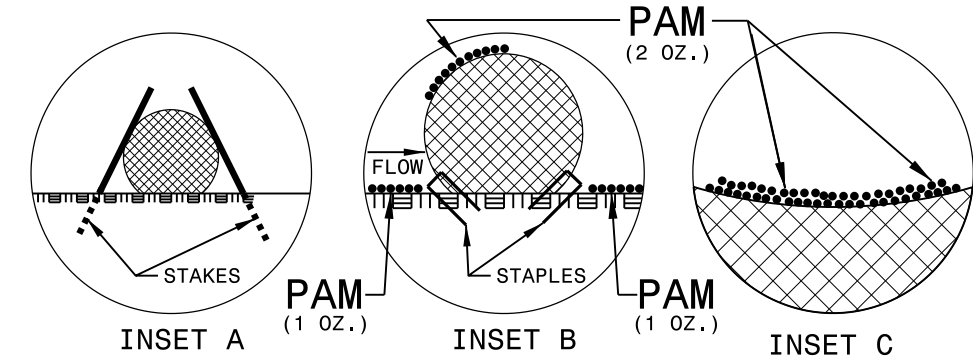
**RESURFACING ADVANCE
WARNING SIGNS FOR
URBAN / SUBURBAN
FACILITIES**

WATTLE WITH POLYACRYLAMIDE (PAM) DETAIL



NOTES:

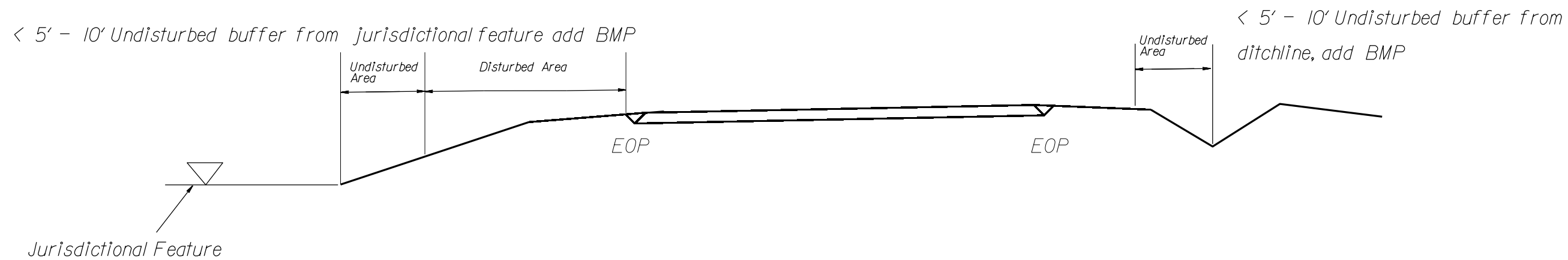
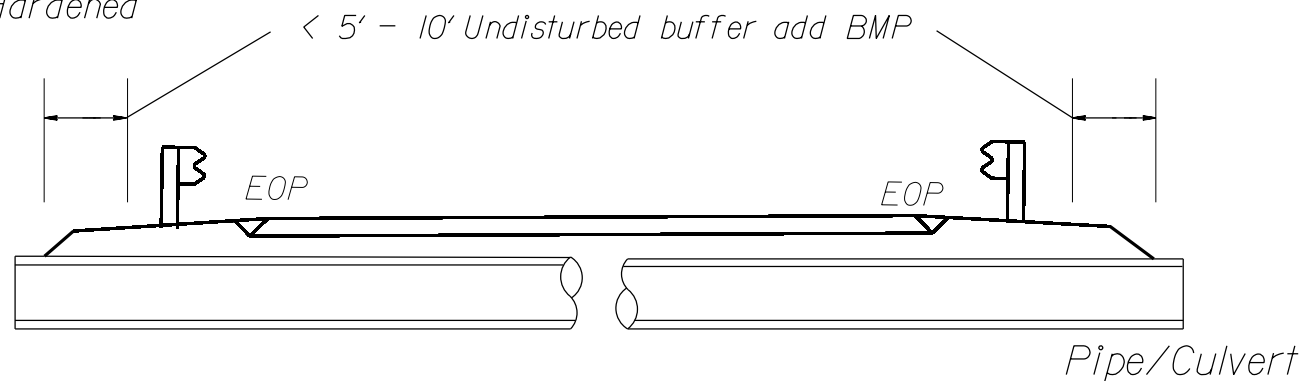
- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
- USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
- ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
- INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
- PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
- INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
- INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.
- PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH WATTLE.
- INITIALLY APPLY 2 OUNCES OF ANIONIC OR NEUTRALLY CHARGED PAM OVER WATTLE WHERE WATER WILL FLOW AND 1 OUNCE OF PAM ON MATTING ON EACH SIDE OF WATTLE. REAPPLY PAM AFTER EVERY RAINFALL EVENT THAT IS EQUAL TO OR EXCEEDS 0.50 IN.



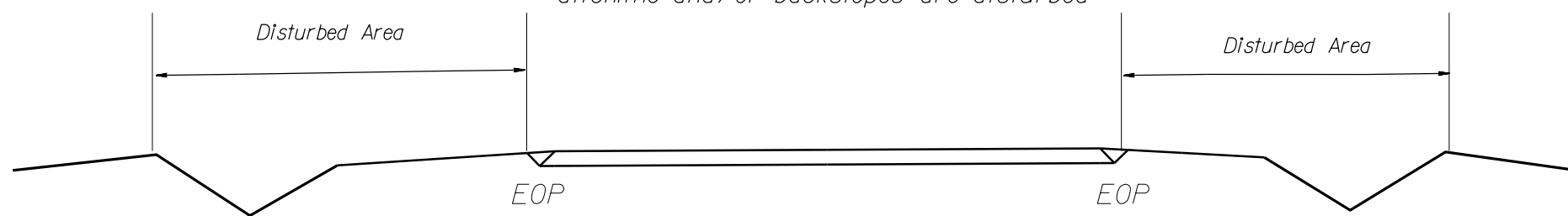
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

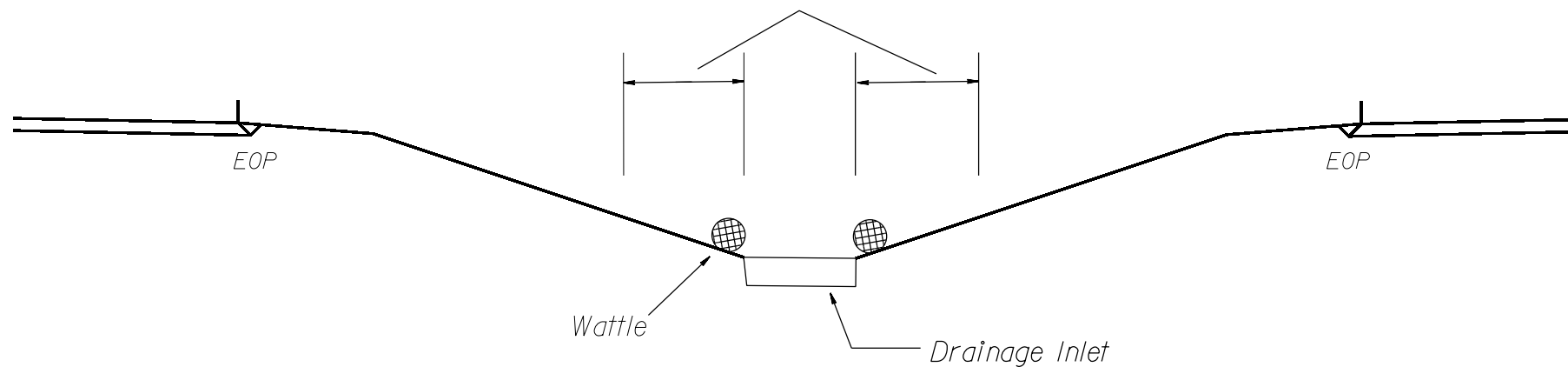
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed



< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE