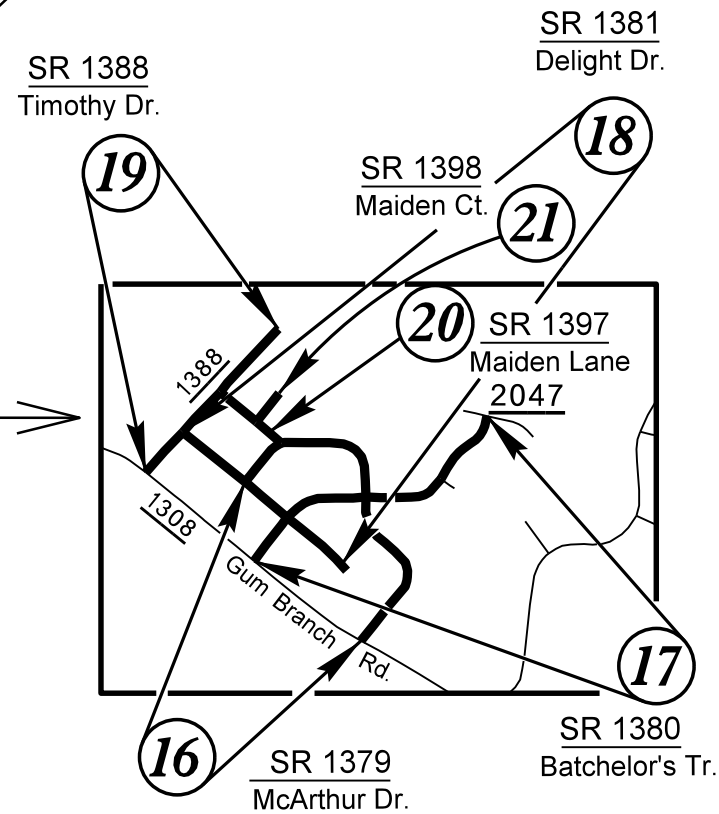
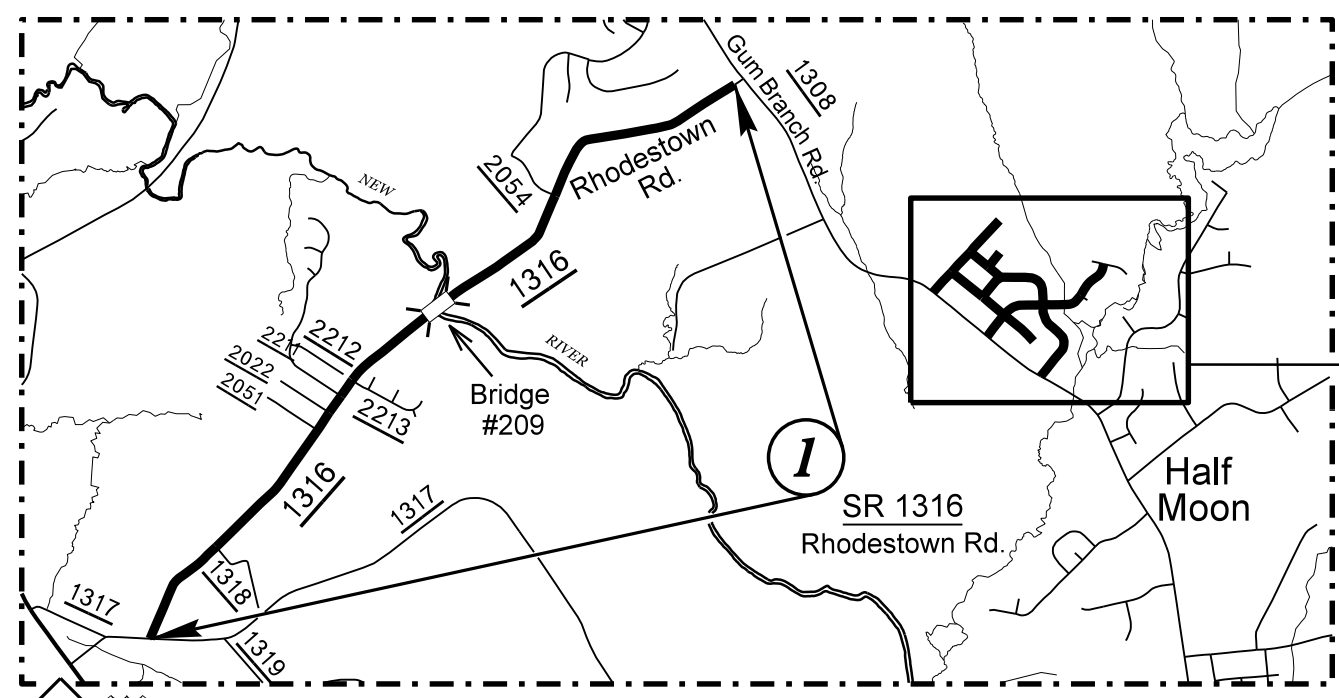
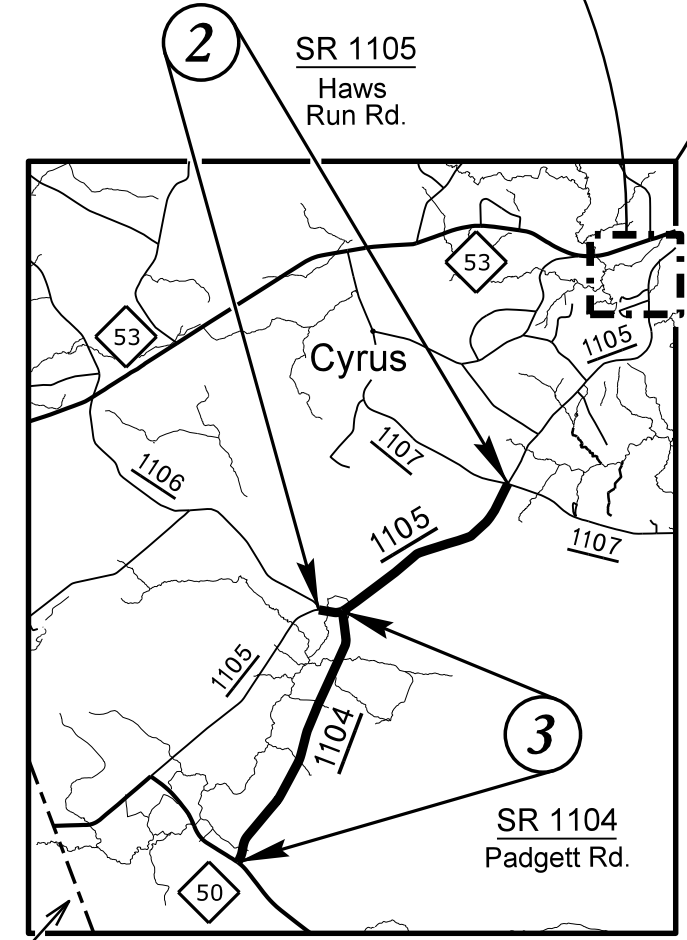
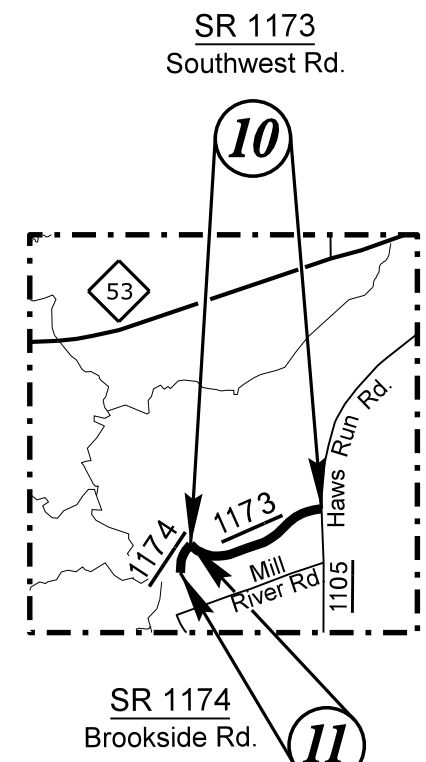
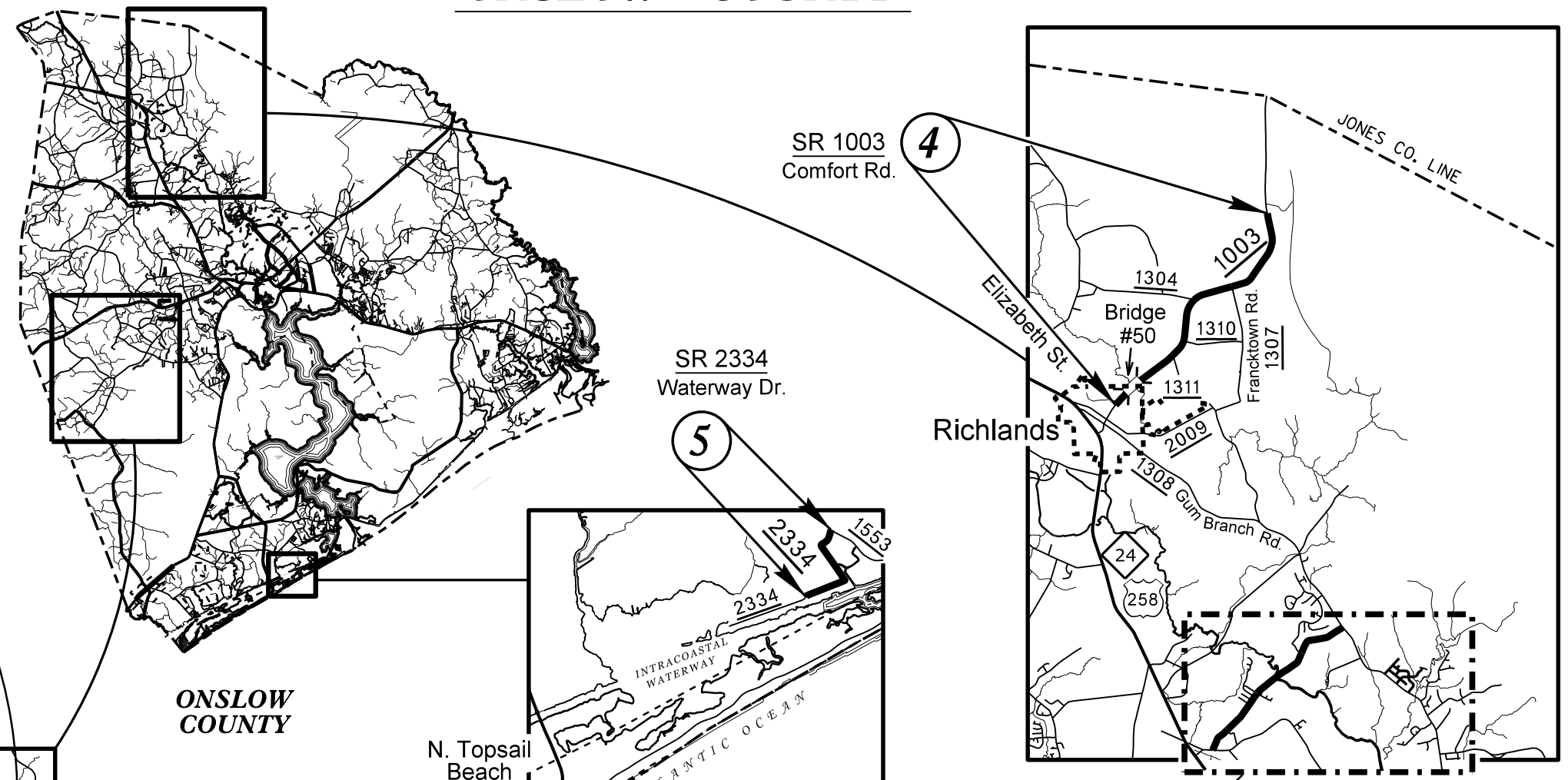


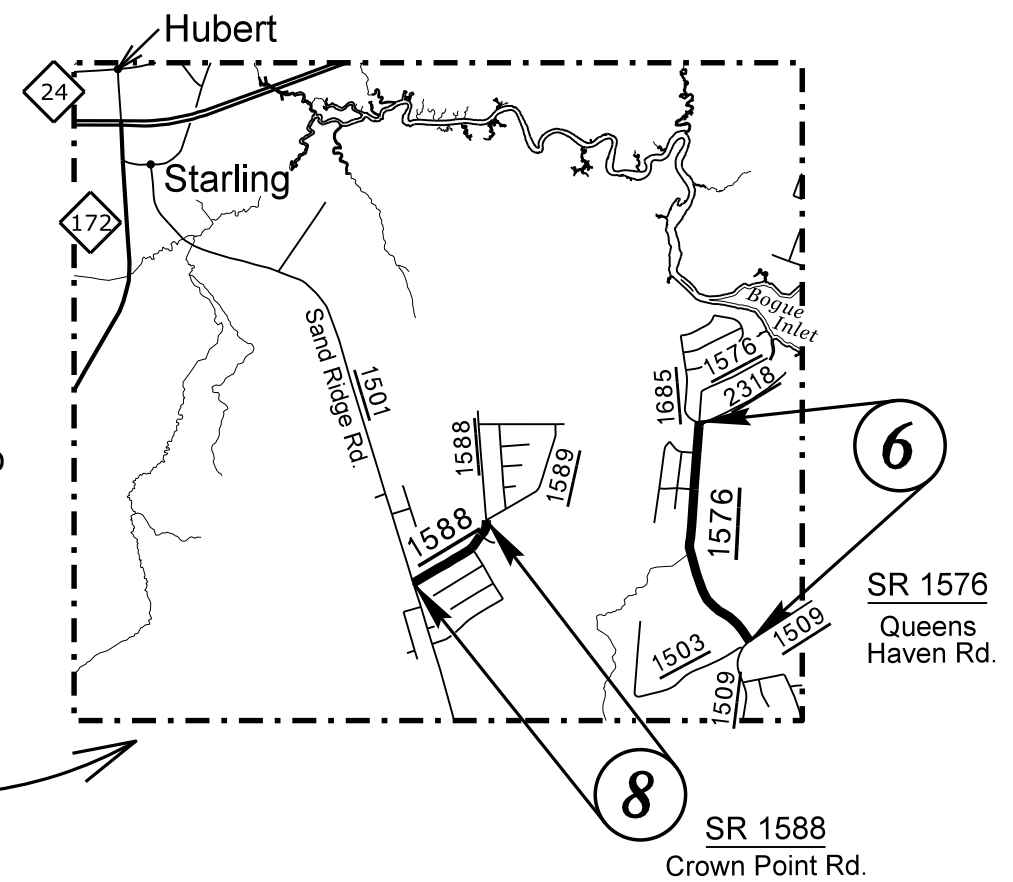
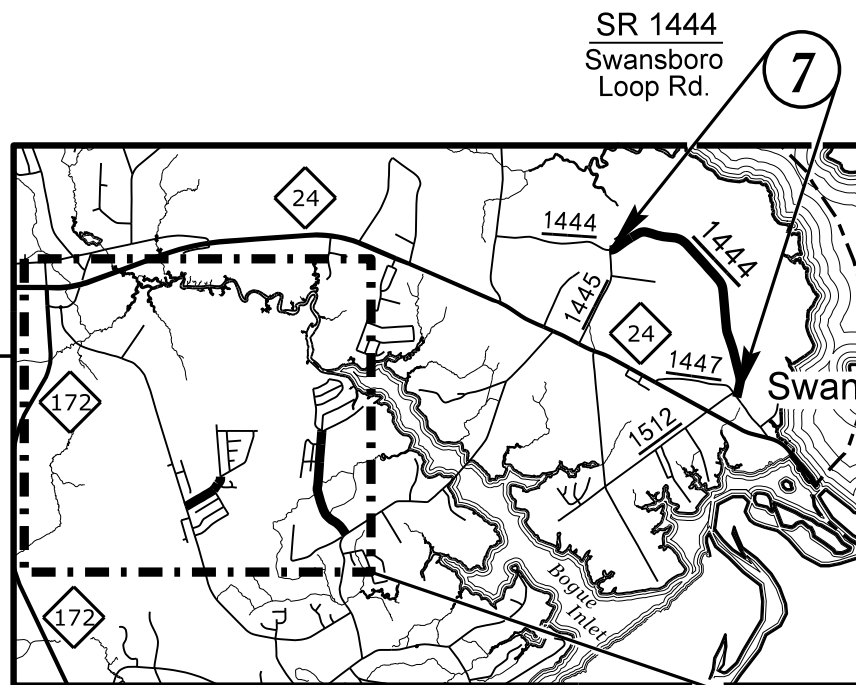
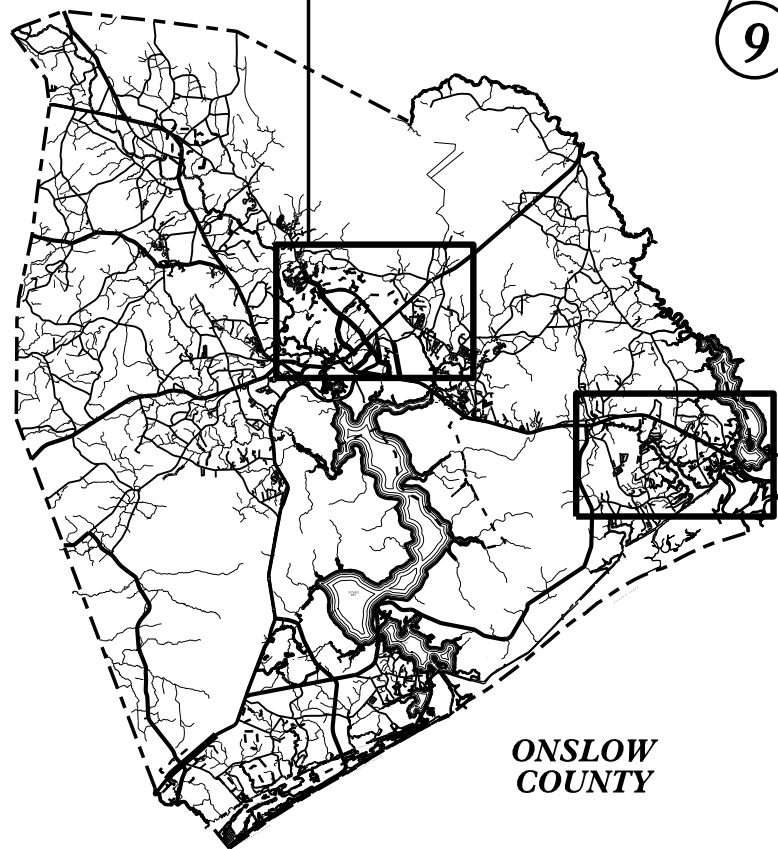
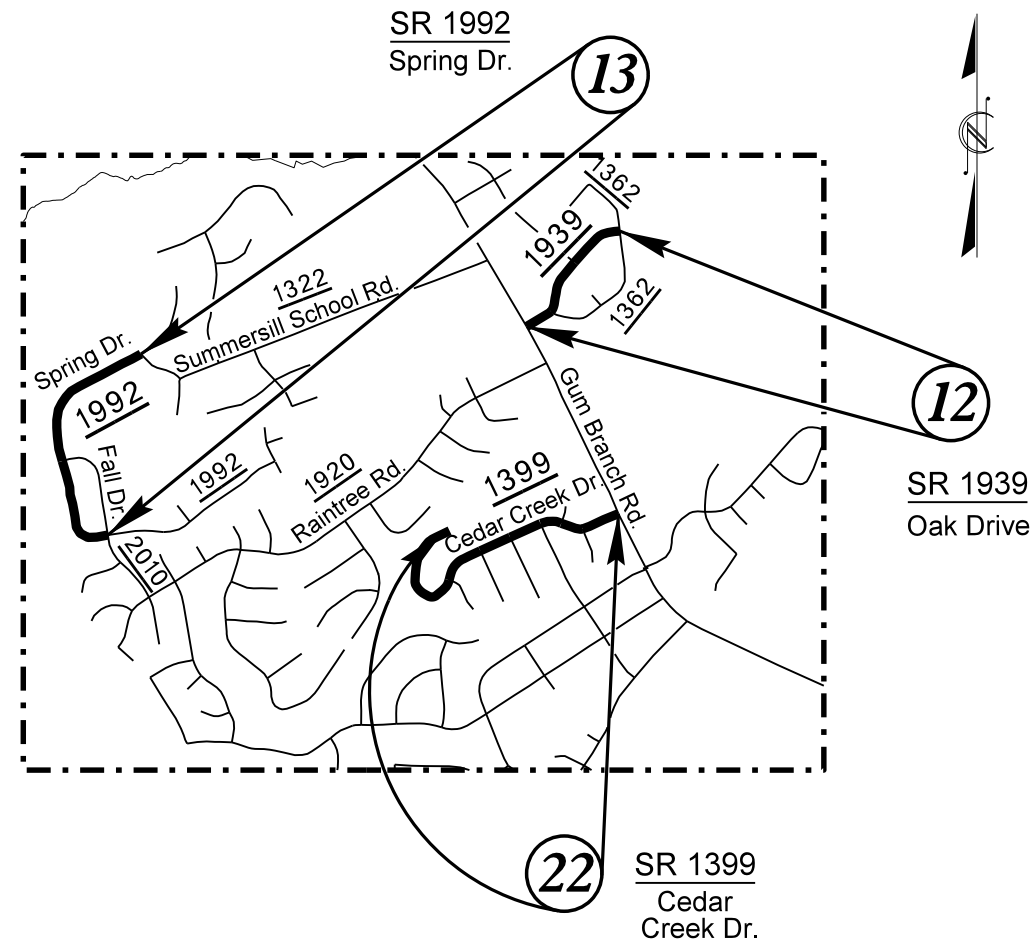
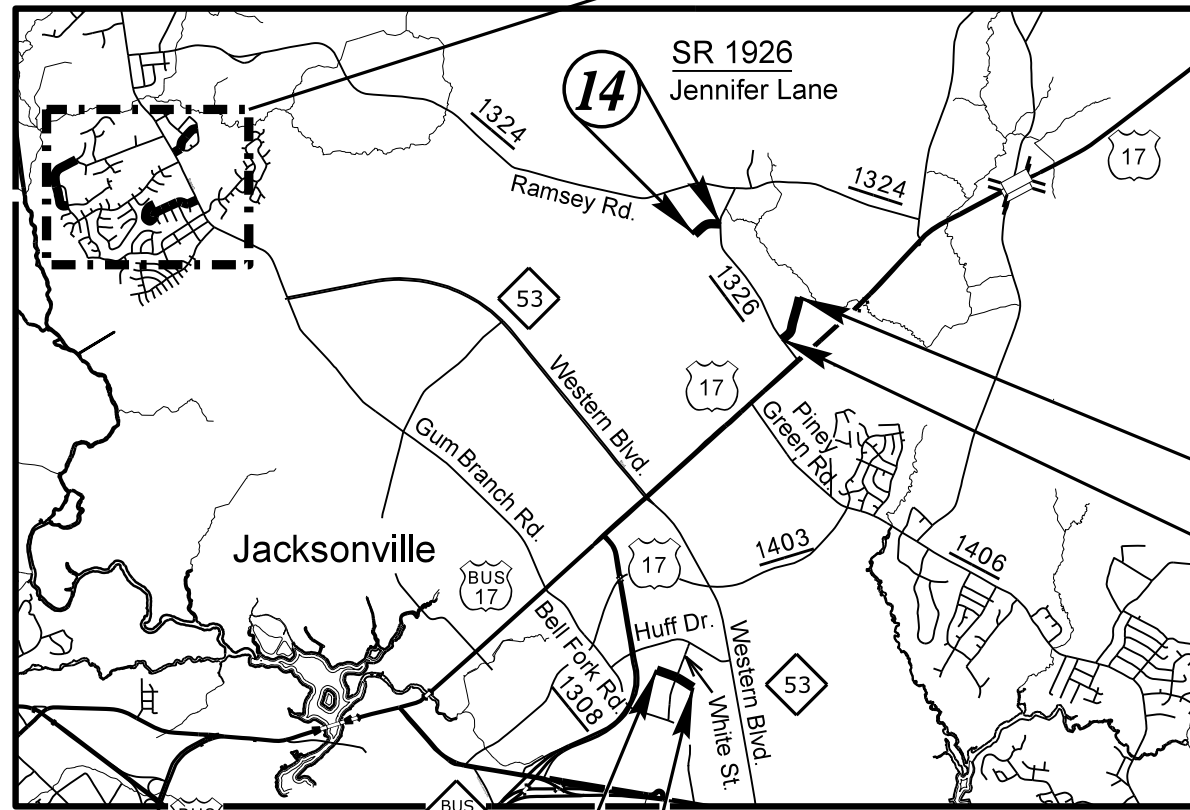
ONSLOW COUNTY



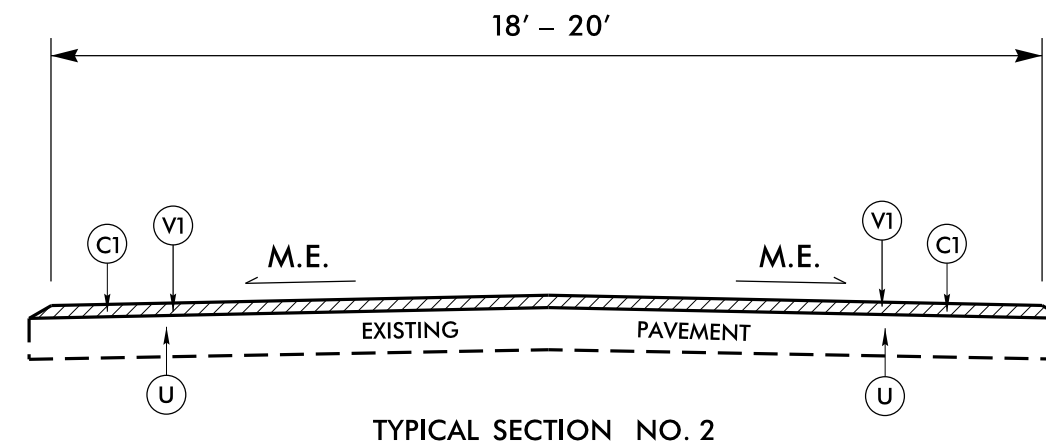
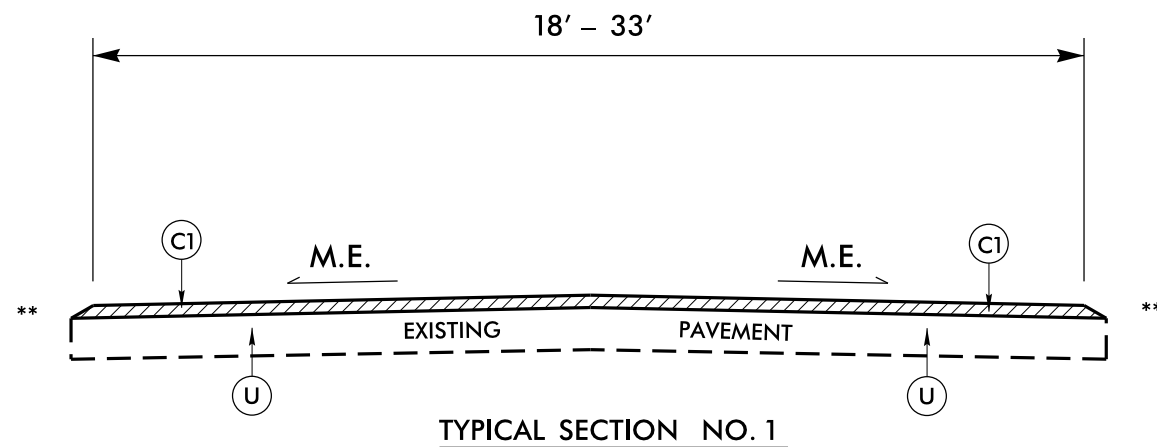
REVISIONS

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ONSLOW COUNTY - CONT.



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MAP NO. 1
SR 1316 (RHODESTOWN RD.)
MP 0.090 - MP 2.788
NO PAVING: MP 0.411 - MP 0.791
NO WORK BRIDGE #209:
MP 1.323 - MP 1.343

MAP NO. 2
SR 1105 (HAWS RUN RD.)
MP 3.049 - MP 5.529
MAP NO. 7
SR 1444 (SWANSBORO LOOP RD.)
MP 1.045 - MP 3.310

MAP NO. 9
SR 1793 (CENTER DR.)
MP 0.144 - MP 0.301

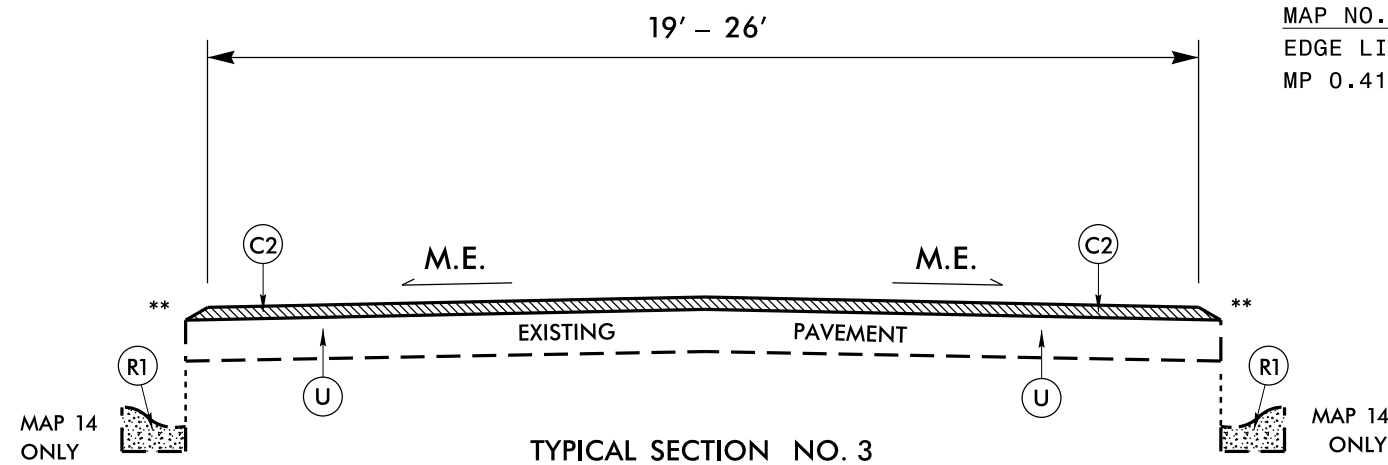
MAP NO. 2
SR 1105 (HAWS RUN RD.)
MP 2.770 - MP 3.049

MAP NO. 4
SR 1003 (COMFORT RD.)
MP 5.330 - MP 5.610
NO WORK BRIDGE #50:
MP 5.434 - MP 5.448

NOTES: MILL A SINGLE LANE AND PAVE BACK BY THE END OF EACH WORK DAY (MAP NO. 2 & 4)

** SHOULDER WORK TO BE DONE BY STATE FORCES

MAP NO.1: NO THERMOPLASTIC PROFILED EDGE LINES WILL BE PLACED FROM MP 0.411 - MP 0.791.



MAP NO. 3
SR 1104 (PADGETT RD.)
MP 0.000 - MP 2.410
MP 2.900 - MP 3.213

MAP NO. 5
SR 2334 (WATERWAY DR.)
MP 0.000 - MP 0.799

MAP NO. 6
SR 1576 (QUEENS HAVEN RD.)
MP 0.000 - MP 1.030

MAP NO. 8
SR 1588 (CROWN POINT RD.)
MP 0.000 - MP 0.429

MAP NO. 14
SR 1926 (JENNIFER LANE)
MP 0.000 - MP 0.222

MAP NO. 15
SR 1918 (ELWOOD RD.)
MP 0.000 - MP 0.350

MAP NO. 16
SR 1379 (MCARTHUR DR.)
MP 0.000 - MP 0.584

MAP NO. 17
SR 1380 (BATCHELORS TRAIL)
MP 0.000 - MP 0.170
MP 0.240 - MP 0.495

MAP NO. 18
SR 1381 (DELIGHT DR.)
MP 0.000 - MP 0.359

MAP NO. 19
SR 1388 (TIMOTHY DR.)
MP 0.000 - MP 0.320

MAP NO. 20
SR 1397 (MAIDEN LANE)
MP 0.000 - MP 0.131

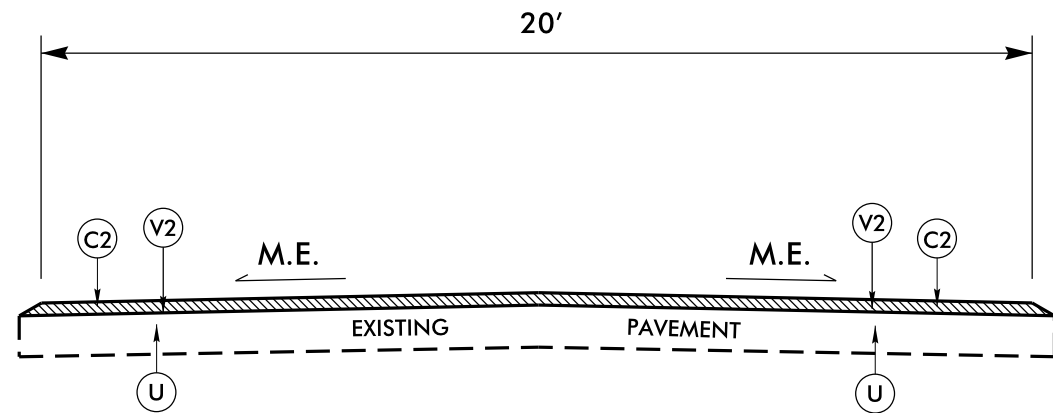
MAP NO. 21
SR 1398 (MAIDEN COURT)
MP 0.000 - MP 0.057

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ.YD.
C2	PROP. APPROX. 1¼" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 138 LBS. PER SQ.YD.
D1	PROP. APPROX. 2½" DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ.YD.
F1	ASPHALT SURFACE TREATMENT - MAT COAT #6 STONE (GRANITE)
R1	EXISTING ROLLED ASPHALT CURB
R2	EXISTING CONCRETE 2'-6" CURB & GUTTER
R3	EXISTING CONCRETE VALLEY CURB
U	EXISTING PAVEMENT
V1	MILLING ASPHALT PAVEMENT, 1½" DEPTH
V2	MILLING ASPHALT PAVEMENT, 1¼" DEPTH
V3	MILLING ASPHALT PAVEMENT, 1" DEPTH

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

8/17/99

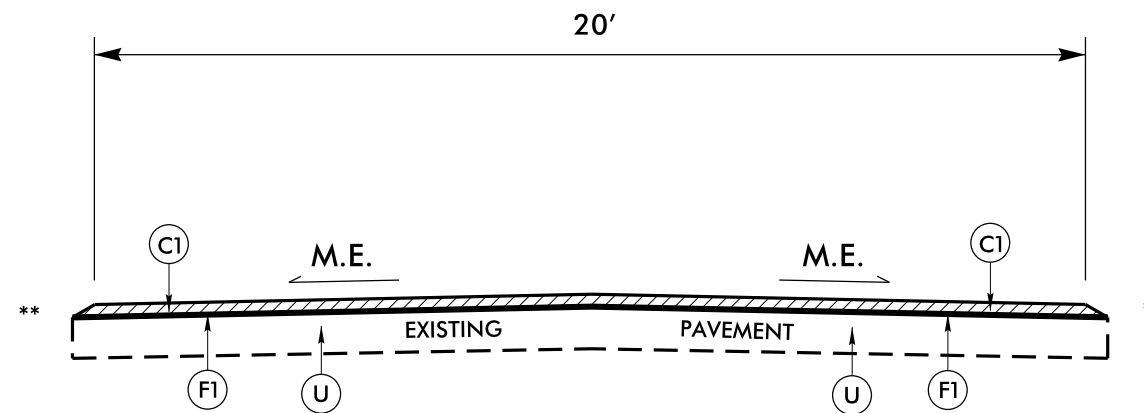
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TYPICAL SECTION NO. 4

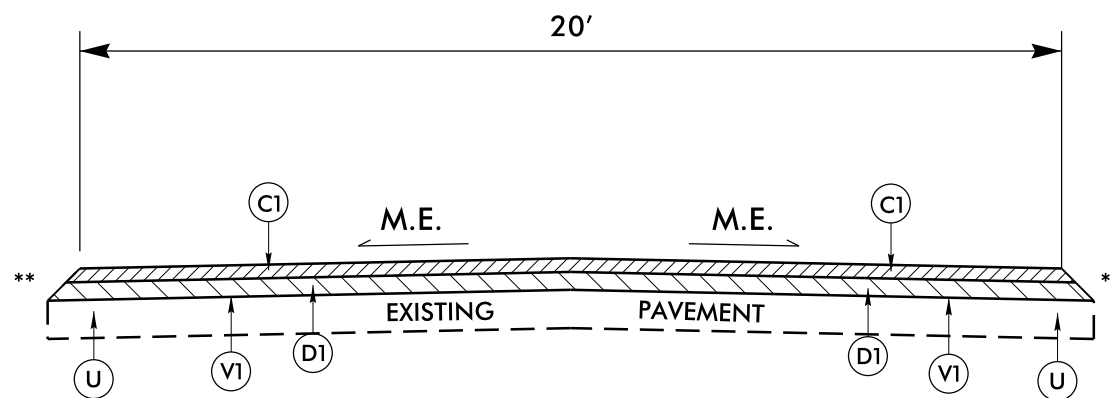
MAP NO. 3
SR 1104 (PADGETT RD.)
MP 2.410 – MP 2.900

MAP NO. 17
SR 1380 (BATCHELORS TRAIL)
MP 0.170 – MP 0.240



TYPICAL SECTION NO. 5

MAP NO. 4
SR 1003 (COMFORT RD.)
MP 1.500 – MP 5.330
MP 5.610 – MP 5.820



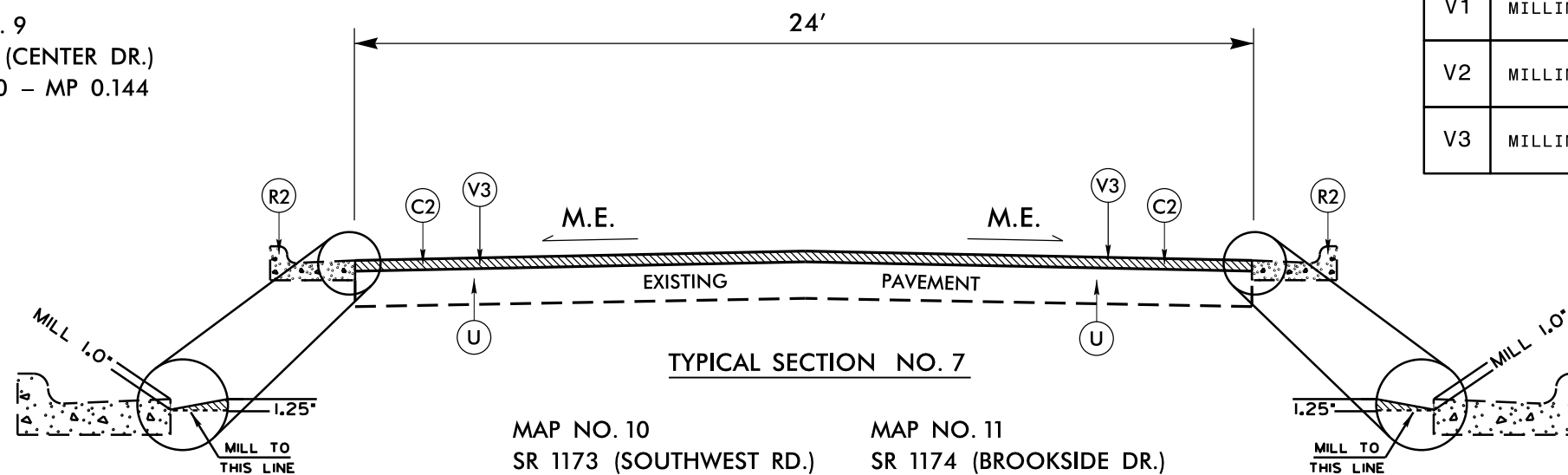
TYPICAL SECTION NO. 6

MAP NO. 9
SR 1793 (CENTER DR.)
MP 0.000 – MP 0.144

NOTE: MILL A SINGLE LANE AND PAVE BACK BY THE END OF EACH WORK DAY.
(MAP NO. 3, 9-11 & 17)

** SHOULDER WORK TO BE DONE BY STATE FORCES

PAVEMENT SCHEDULE	
C1	1½" S9.5B
C2	1¼" SF9.5A
D1	2½" I19.0B
F1	AST MAT COAT #6 STONE (GRANITE)
R2	EXISTING 2'-6" CURB & GUTTER
U	EXISTING PAVEMENT
V1	MILLING 1½" DEPTH
V2	MILLING 1¼" DEPTH
V3	MILLING 1" DEPTH

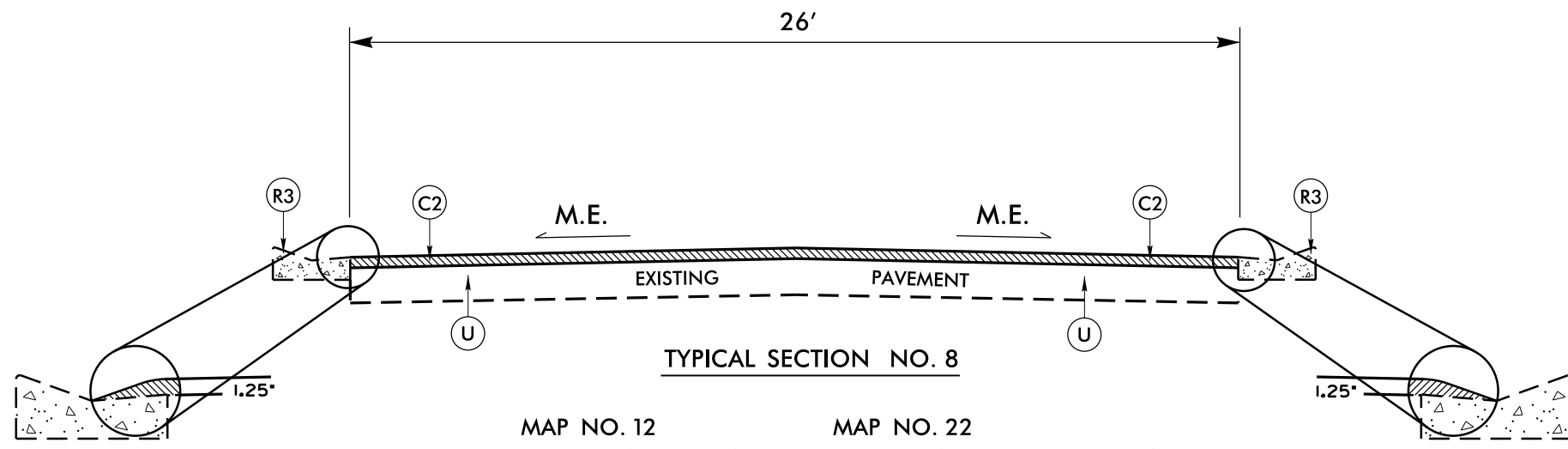


TYPICAL SECTION NO. 7

MAP NO. 10
SR 1173 (SOUTHWEST RD.)
MP 0.000 – MP 0.339

MAP NO. 11
SR 1174 (BROOKSIDE DR.)
MP 0.000 – MP 0.073

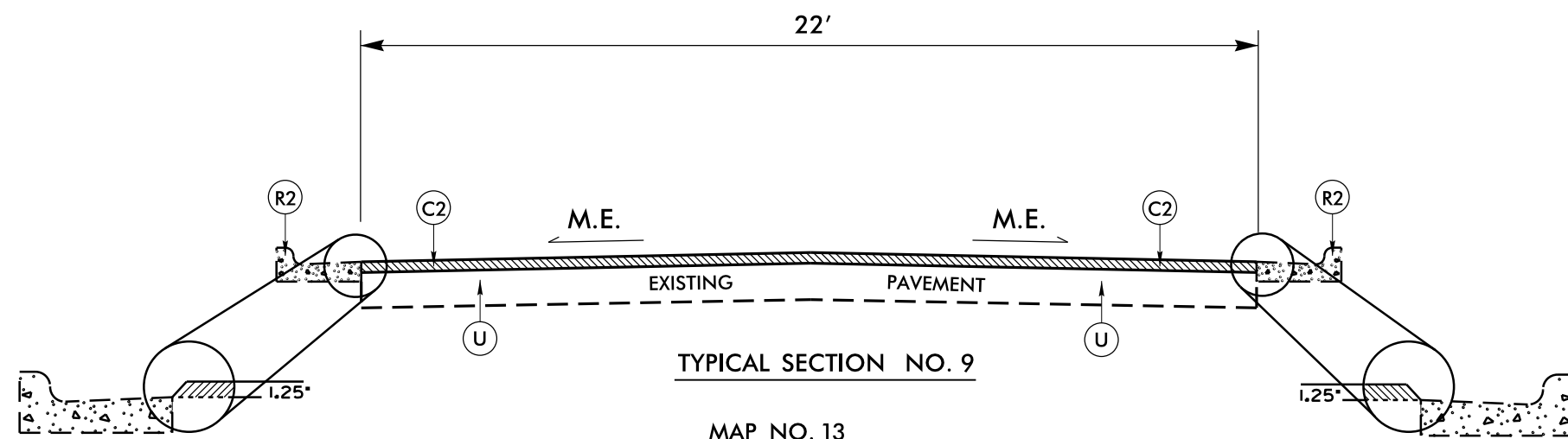
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MAP NO. 12
SR 1939 (OAK DR.)
MP 0.000 – MP 0.268

MAP NO. 22
SR 1399 (CEDAR CREEK DR.)
MP 0.000 – MP 0.577

PAVEMENT SCHEDULE	
C2	1¼" SF9.5A
R2	EXISTING 2'-6" CURB & GUTTER
R3	EXISTING VALLEY CURB
U	EXISTING PAVEMENT



MAP NO. 13
SR 1992 (SPRING DR.)
MP 0.155 – MP 0.638

EFF.1-01-2018

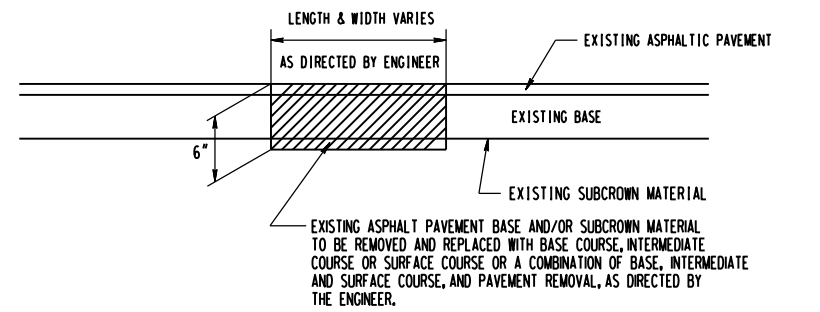
2018 ROADWAY ENGLISH STANDARD DRAWINGS

The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch - N.C. Department of Transportation - Raleigh, N.C., Dated January, 2018 are applicable to this project and by reference hereby are considered a part of these plans:

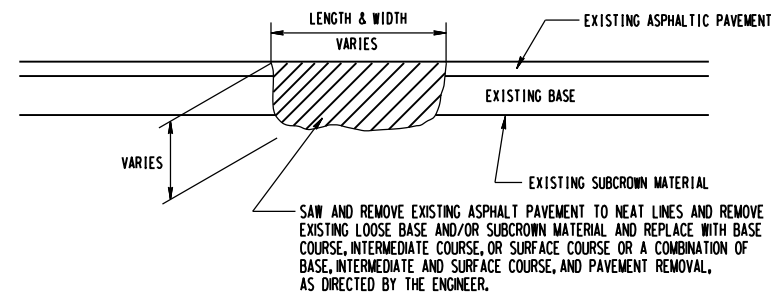
STD.NO.	TITLE
DIVISION 8 - INCIDENTALS	
846.01	Concrete Curb, Gutter and Curb & Gutter

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 8/17/99

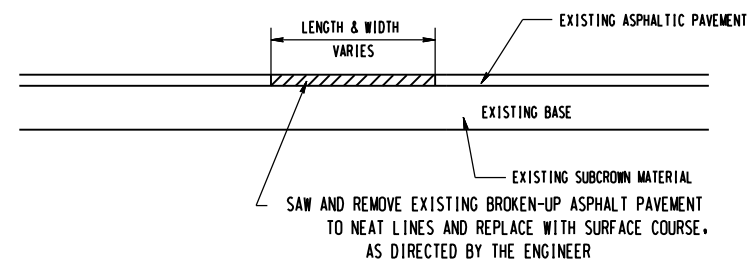
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



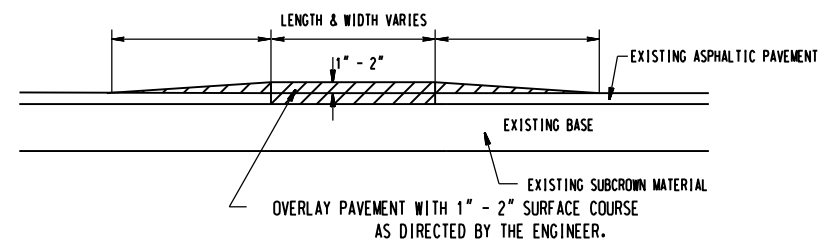
DETAIL NO. 1



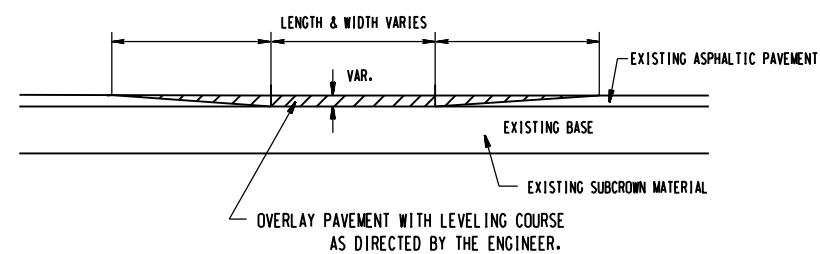
DETAIL NO. 2



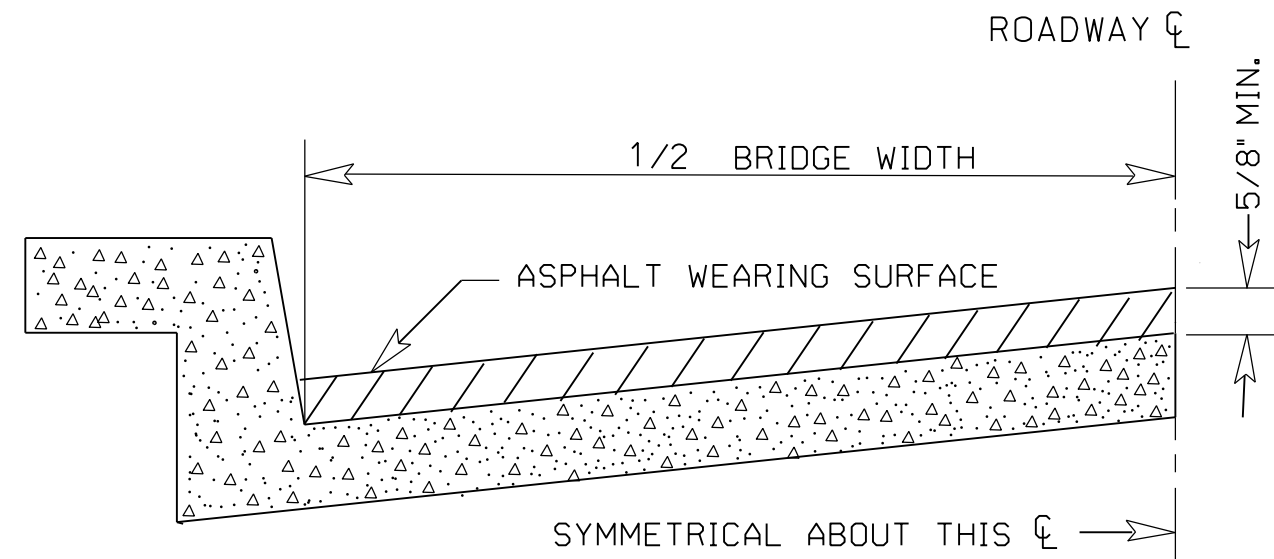
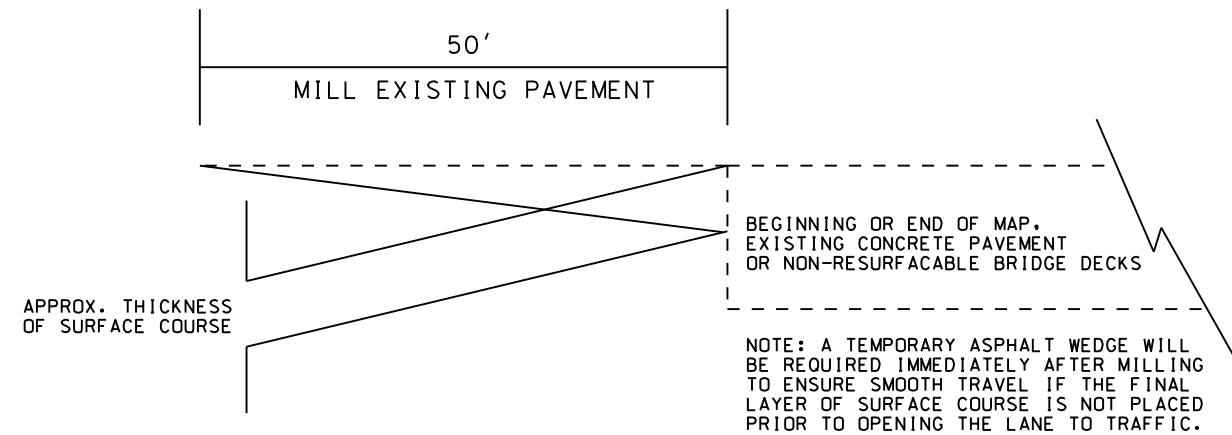
DETAIL NO. 3



DETAIL NO. 4



DETAIL NO. 5



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN.

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

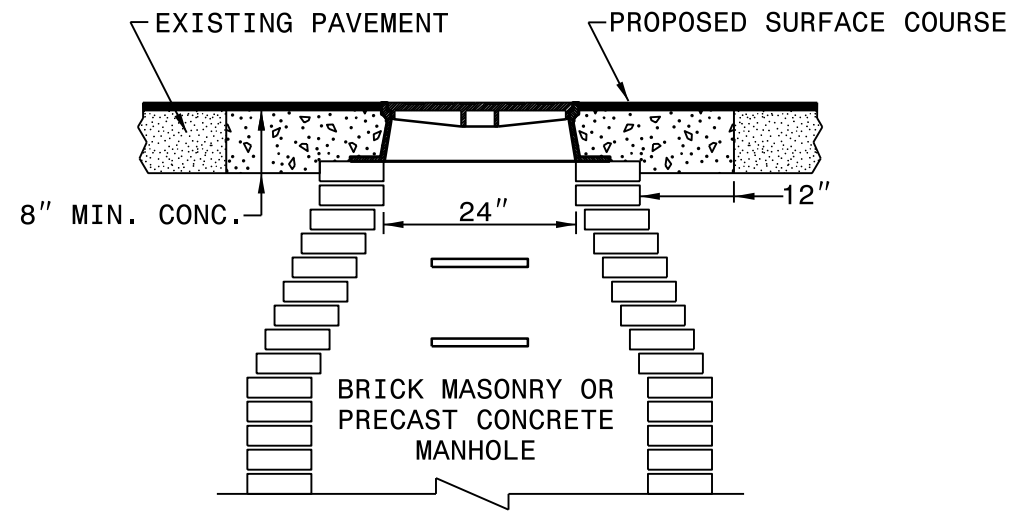
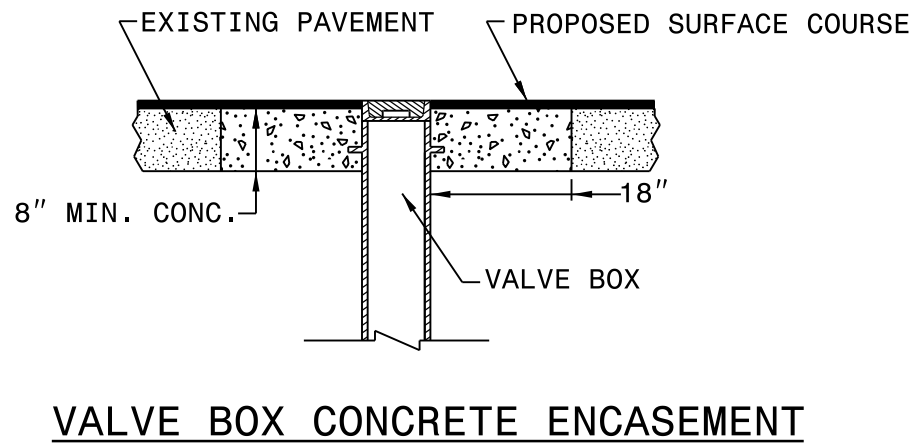
STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

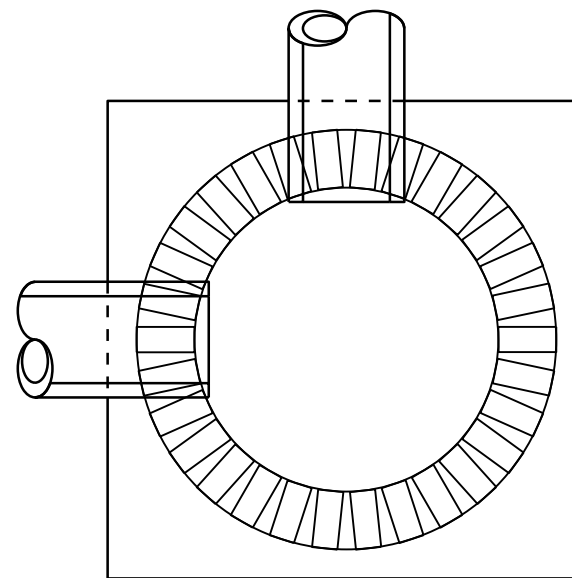
SHEET 1 OF 1
840D55

GENERAL NOTES:

1. USE RAPID SET GROUT, MORTAR, OR CONCRETE THAT WILL TAKE FULL SET AND BECOME LOAD BEARING WITHIN SIXTY MINUTES OF PLACEMENT WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS $\frac{1}{2}$ " +/- $\frac{1}{8}$ "
7. CONSTRUCT AN ASPHALT RAMP IN ACCORDANCE WITH SECTION 858-3 OF THE 2012 STANDARD SPECIFICATIONS.



MANHOLE CONCRETE ENCASEMENT



ELEVATION VIEW

PLACE BRICK ACCORDING TO ELEVATION VIEW

ENGLISH DETAIL DRAWING FOR

MANHOLE AND VALVE BOX ADJUSTMENTS

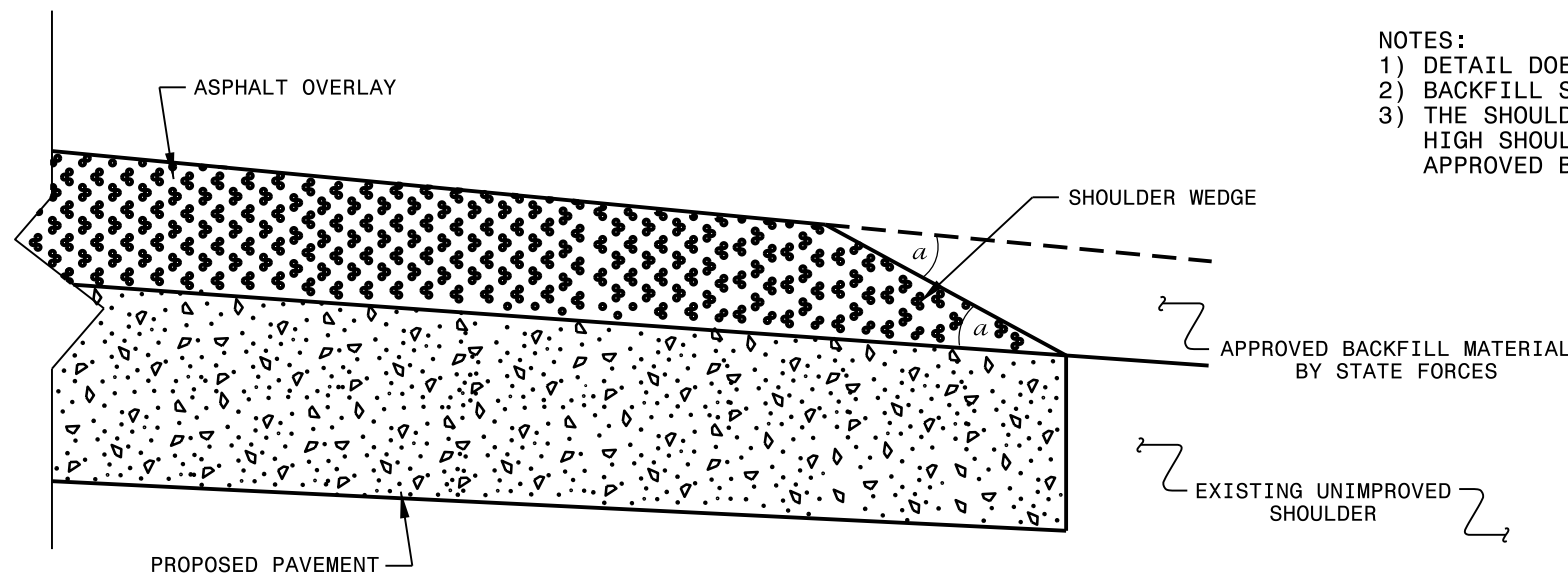
SHEET 1 OF 1
840D55

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

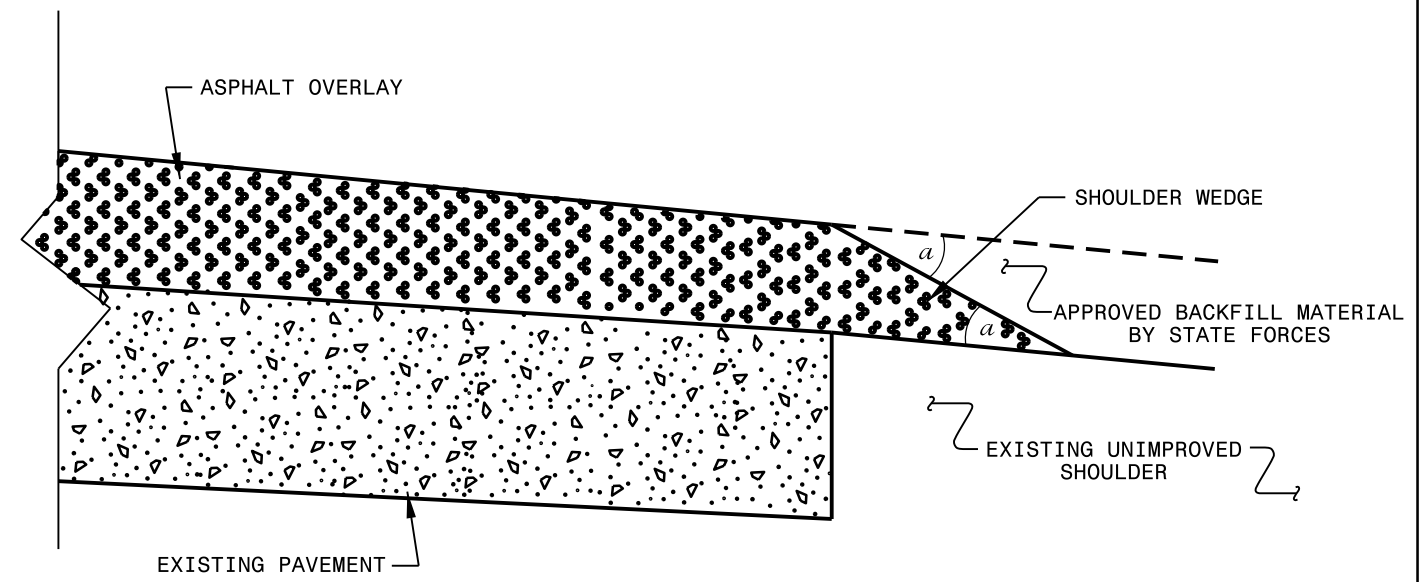
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Resurfacing\Duplin_Co\2018CPT.03.06.20671.Etc.US.117 & NC.11.24.50\2018CPT.03.06.20671.Etc.RdJ_tjy.dgn

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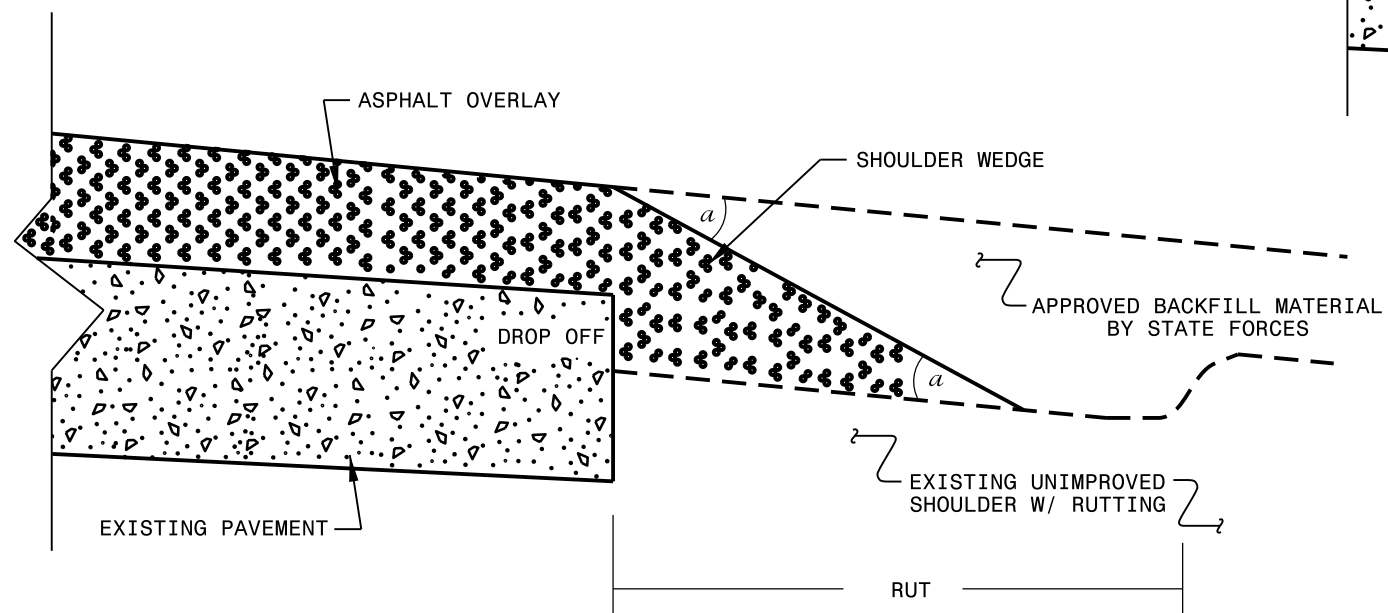
- NOTES:
 1) DETAIL DOES NOT APPLY TO OGAFc AND ULTRA-THIN BONDED WEARING COURSE.
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC.: stusr/details/stand/shoulderwedgedetail.dgn	

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SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	INCIDENTAL STONE BASE	1.5" MILLING	1.25" MILLING	1" MILLING	INCIDENTAL MILLING	INTER-MEDIATE COURSE, I19.0B	SURFACE COURSE, S9.5B	SURFACE COURSE, SF9.5A	LEVELING COURSE, SF9.5A	ASPHALT BINDER FOR PLANT MIX	ASPHALT SURFACE TREATMENT #6 STONE (GRANITE)	EMULSION FOR ASPHALT SURFACE TREATMENT	PATCHING EXISTING PAVEMENT (MILL)	REMOVE & REPLACE 2'-6" CURB & GUTTER	ADJ. OF MAN-HOLE S	ADJ. OF METER OR VALVE BOX	
										MI	FT	TON	SY	SY	SY	SY	TON	TON	TON	TON	TON	SY	GAL	TON	LF	EA	EA	
		1	SR 1316 (RHODESTOWN RD)	FROM SR 1308 TO SR 1317 (MP 0.090 TO MP 2.788) NO PAVING: MP 0.411 - MP 0.791; NO WORK BRIDGE #209: MP 1.323 - MP 1.343	1	2	2WU	NO	NO	2.698	22	140				1,100		3,605				216		700				
		2	SR 1105 (HAWES RUN RD)	FROM SR 1106 TO SR 1107 (MP 2.77 TO MP 5.529)	1 & 2	2	2WU	NO	NO	2.759	18	150	3,000			785		2,715				163		400				
		3	SR 1104 (PADGETT RD)	FROM NC 50 TO SR 1105 (MP 0.00 TO MP 3.213)	3 & 4	2	2WU	NO	NO	3.213	20	200		5,842					2,605			175						
		4	SR 1003 (COMFORT RD)	FROM MP 1.500 TO ELIZABETH ST (MP 1.500 TO MP 5.820) NO WORK BRIDGE #50: MP 5.434 - MP 5.448	2 & 5	2	2WU	NO	NO	4.32	20	180	3,284					4,338				260	51,322	17,965				
		5	SR 2334 (WATERWAY DR)	FROM SR 1553 TO PAVEMENT CHANGE (MP 0.000 TO MP 0.799)	3	2	2WU	NO	NO	0.799	20	25				120			648	50		47			125		5	
		6	SR 1576 (QUEENS HAVEN RD)	FROM SR 1509 TO SR 1685 (MP 0.000 TO MP 1.030)	3	2	2WU	NO	NO	1.03	20	80				155			1,015			68						
		7	SR 1444 (SWANSBORO LOOP RD)	FROM SR 1445 TO SR 1447 (MP 1.045 TO MP 3.310)	1	2	2WU	NO	NO	2.265	19	160						2,020				121		135				
		8	SR 1588 (CROWN POINT RD)	FROM SR 1501 TO SR 1589 (MP 0.00 TO MP 0.429)	3	2	2WU	NO	NO	0.436	20	20							355	175		36						
		9	SR 1793 (CENTER DRIVE)	FROM END MAINTENANCE TO END MAINTENANCE (MP 0.00 TO MP 0.301)	1 & 6	2	2WU	NO	NO	0.301	20	10	1,780				240	299				30		45			1	
		10	SR 1173 (SOUTHWEST RD)	FROM SR 1105 TO SR 1174 (MP 0.00 TO MP 0.339)	7	2	2WU	NO	NO	0.335	24	10							325			22						5
		11	SR 1174 (BROOKSIDE DR)	FROM SR 1173 TO CUL DE SAC (MP 0.00 TO MP 0.073)	7	2	2WU	NO	NO	0.073	24								62			4						2
		12	SR 1939 (OAK DRIVE)	FROM SR 1308 TO SR 1362 (MP 0.00 TO MP 0.268)	8	2	2WU	NO	NO	0.27	26								284			19		20				
		13	SR 1992 (SPRING DRIVE)	FROM SR 1991 TO SR 2010 (MP 0.155 TO MP 0.638)	9	2	2WU	NO	NO	0.48	22								428	50		32		45	50	11	11	
		14	SR 1926 (JENNIFER LN)	FROM SR 1326 TO CUL DE SAC (MP 0.00 TO MP 0.222)	3	2	2WU	NO	NO	0.222	26	40							245			16						
		15	SR 1918 (ELWOOD RD)	FROM SR 1326 TO DEAD END (MP 0.00 TO MP 0.35)	3	2	2WU	NO	NO	0.35	20	40							284	100		26		25				
		16	SR 1379 (MCARTHUR DR)	FROM SR 1308 TO SR 1381 (MP 0.00 TO MP 0.584)	3	2	2WU	NO	NO	0.584	20	20							474	250		49						1
		17	SR 1380 (BATCHELORS TRAIL)	FROM SR 1308 TO SR 2047 (MP 0.00 TO MP 0.495)	3 & 4	2	2WU	NO	NO	0.495	20	30		727					401			27		30				
		18	SR 1381 (DELIGHT DR)	FROM SR 1388 TO CUL DE SAC (MP 0.00 TO MP 0.359)	3	2	2WU	NO	NO	0.359	20	10							306			21						1
		19	SR 1388 (TIMOTHY DR)	FROM SR 1308 TO END MAINTENANCE (MP 0.00 TO MP 0.32)	3	2	2WU	NO	NO	0.32	20	15							259	130		26		25				
		20	SR 1397 (MAIDEN LANE)	FROM SR 1388 TO SR 1379 (MP 0.00 TO MP 0.131)	3	2	2WU	NO	NO	0.131	20	20							106			7		20				
		21	SR 1398 (MAIDEN COURT)	FROM SR 1397 TO CUL DE SAC (MP 0.00 TO MP 0.057)	3	2	2WU	NO	NO	0.057	19								44	25		5		15				
		22	SR 1399 (CEDAR CREEK DR)	FROM SR 1308 TO DEAD END (MP 0.00 TO MP 0.577)	8	2	2WU	NO	NO	0.58	26	20							641	25		45		35			9	
GRAND TOTAL FOR PROJ NO. 2018CPT.03.06.20671										22.077		1,170	8,064	6,569	5,745	2,160	240	12,977	8,482	805	1,415	51,322	17,965	1,620	50	26	20	

2018CPT.03.06.20671

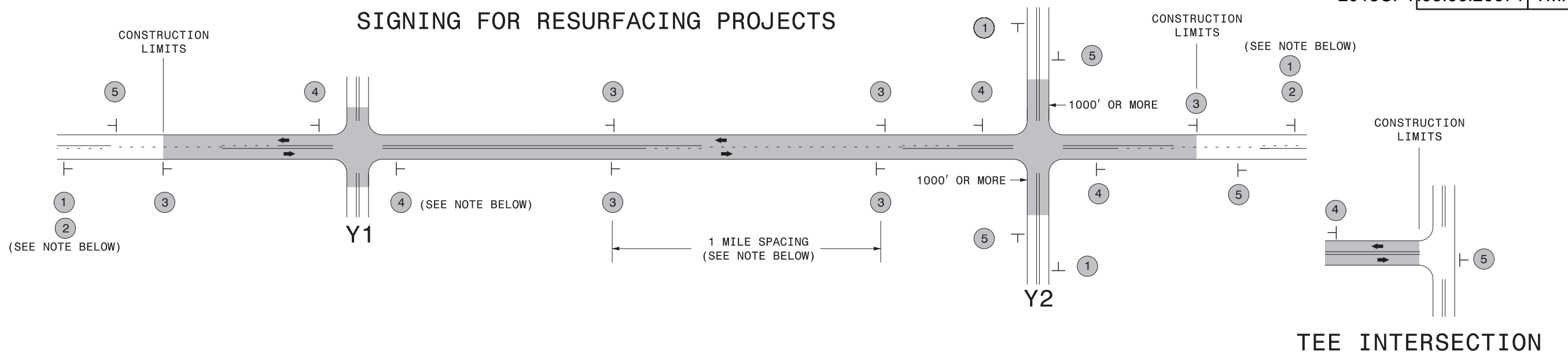
Onslow

PROJECT NO.	SHEET NO.
2018CPT.03.06.20671	10

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	4413000000-E		44570000	46850000	4686000000-E			47000000	47250000	489000000	4900000000-N			
								WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMP. TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 120 M YELLOW THERMO	4" X 120 M WHITE THERMO	12" X 90 M YELLOW THERMO	THERMO LT ARROW 90 M	4" WHITE THERMO PROFILED EDGE LINE	YELLOW & YELLOW MARKERS	CRYSTAL & RED MARKERS				
								MI	FT	SF	LS	LF	LF	LF	LF	EA	LF	EA	EA		
2018CPT.03.06.20671	Onslow	1	SR 1316 (RHODESTOWN RD)	FROM SR 1308 TO SR 1317 (MP 0.090 TO MP 2.788) NO PAVING: MP 0.411 - MP 0.791; NO WORK BRIDGE #209: MP 1.323 - MP 1.343	1	2	2WU	2.698	22	320		4,012	18,401	130	180	1	25,999	263	6		
		2	SR 1105 (HAWS RUN RD)	FROM SR 1106 TO SR 1107 (MP 2.77 TO MP 5.529)	1 & 2	2	2WU	2.759	18	288											
		3	SR 1104 (PADGETT RD)	FROM NC 50 TO SR 1105 (MP 0.00 TO MP 3.213)	3 & 4	2	2WU	3.213	20	368											
		4	SR 1003 (COMFORT RD)	FROM MP 1.500 TO ELIZABETH ST (MP 1.500 TO MP 5.820) NO WORK BRIDGE #50: MP 5.434 - MP 5.448	2 & 5	2	2WU	4.32	20	496		47,064	28,868	175					289		
		5	SR 2334 (WATERWAY DR)	FROM SR 1553 TO PAVEMENT CHANGE (MP 0.000 TO MP 0.799)	3	2	2WU	0.799	20	96											
		6	SR 1576 (QUEENS HAVEN RD)	FROM SR 1509 TO SR 1685 (MP 0.000 TO MP 1.030)	3	2	2WU	1.03	20	128		11,083	6,798	75					68	4	
		7	SR 1444 (SWANSBORO LOOP RD)	FROM SR 1445 TO SR 1447 (MP 1.045 TO MP 3.310)	1	2	2WU	2.265	19	256											
		8	SR 1588 (CROWN POINT RD)	FROM SR 1501 TO SR 1589 (MP 0.00 TO MP 0.429)	3	2	2WU	0.436	20	64											
		9	SR 1793 (CENTER DRIVE)	FROM END MAINTENANCE TO END MAINTENANCE (MP 0.00 TO MP 0.301)	1 & 6	2	2WU	0.301	20	128											
		10	SR 1173 (SOUTHWEST RD)	FROM SR 1105 TO SR 1174 (MP 0.00 TO MP 0.339)	7	2	2WU	0.335	24	48	*										
		11	SR 1174 (BROOKSIDE DR)	FROM SR 1173 TO CUL DE SAC (MP 0.00 TO MP 0.073)	7	2	2WU	0.073	24												
		12	SR 1939 (OAK DRIVE)	FROM SR 1308 TO SR 1362 (MP 0.00 TO MP 0.268)	8	2	2WU	0.27	26	32											
		13	SR 1992 (SPRING DRIVE)	FROM SR 1991 TO SR 2010 (MP 0.155 TO MP 0.638)	9	2	2WU	0.48	22	64											
		14	SR 1926 (JENNIFER LN)	FROM SR 1326 TO CUL DE SAC (MP 0.00 TO MP 0.222)	3	2	2WU	0.222	26	32											
		15	SR 1918 (ELWOOD RD)	FROM SR 1326 TO DEAD END (MP 0.00 TO MP 0.35)	3	2	2WU	0.35	20	48											
		16	SR 1379 (MCARTHUR DR)	FROM SR 1308 TO SR 1381 (MP 0.00 TO MP 0.584)	3	2	2WU	0.584	20	80											
		17	SR 1380 (BATCHELORS TRAIL)	FROM SR 1308 TO SR 2047 (MP 0.00 TO MP 0.495)	3 & 4	2	2WU	0.495	20	64											
		18	SR 1381 (DELIGHT DR)	FROM SR 1388 TO CUL DE SAC (MP 0.00 TO MP 0.359)	3	2	2WU	0.359	20	48											
		19	SR 1388 (TIMOTHY DR)	FROM SR 1308 TO END MAINTENANCE (MP 0.00 TO MP 0.32)	3	2	2WU	0.32	20	48											
		20	SR 1397 (MAIDEN LANE)	FROM SR 1388 TO SR 1379 (MP 0.00 TO MP 0.131)	3	2	2WU	0.131	20												
		21	SR 1398 (MAIDEN COURT)	FROM SR 1397 TO CUL DE SAC (MP 0.00 TO MP 0.057)	3	2	2WU	0.057	19												
		22	SR 1399 (CEDAR CREEK DR)	FROM SR 1308 TO DEAD END (MP 0.00 TO MP 0.577)	8	2	2WU	0.58	26	80											
GRAND TOTAL FOR PROJ NO. 2018CPT.03.06.20671							22.077			2,688	1	62,159	54,067	380	180	1	25,999	620	10		
													54,447				630				

SIGNING FOR RESURFACING PROJECTS



LEGEND	
T	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

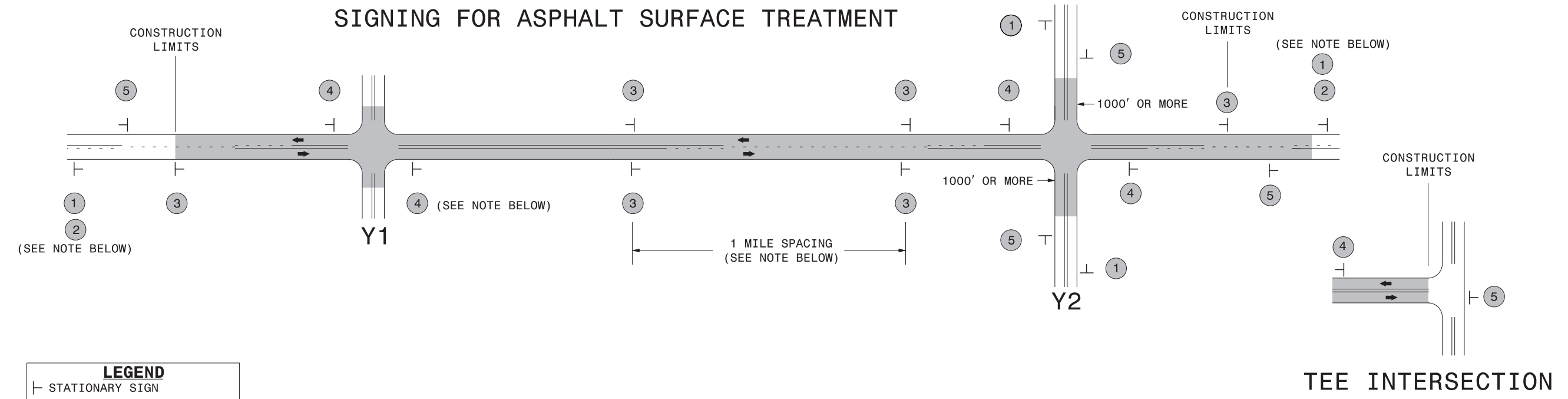
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	<p>W20-1 48" X 48"</p>	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. #2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.
	2	<p>W7-3aP 24" X 18"</p>		
	3	<p>SP 13107 48" X 48"</p>	- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4	<p>SP 13106 48" X 48"</p>	- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5	<p>G20-2 A 48" X 24"</p>	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	
		<p>W20-1 48" X 48"</p>	<p>W20-7 A 48" X 48"</p>	PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.

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**RESURFACING
 ADVANCE WARNING SIGNS
 FOR
 RURAL AND SUBURBAN
 2 LANE ROADWAYS**



LEGEND
 ┆ STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.
	2		#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)
	3	 	<ul style="list-style-type: none"> - ALTERNATE THE FOLLOWING TWO SIGNS: - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
	4		<ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

W20-1
 48" X 48"

W20-7 A
 48" X 48"

PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.

ADVANCE WARNING SIGNS FOR ASPHALT SURFACE TREATMENTS 2 LANE ROADWAYS

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