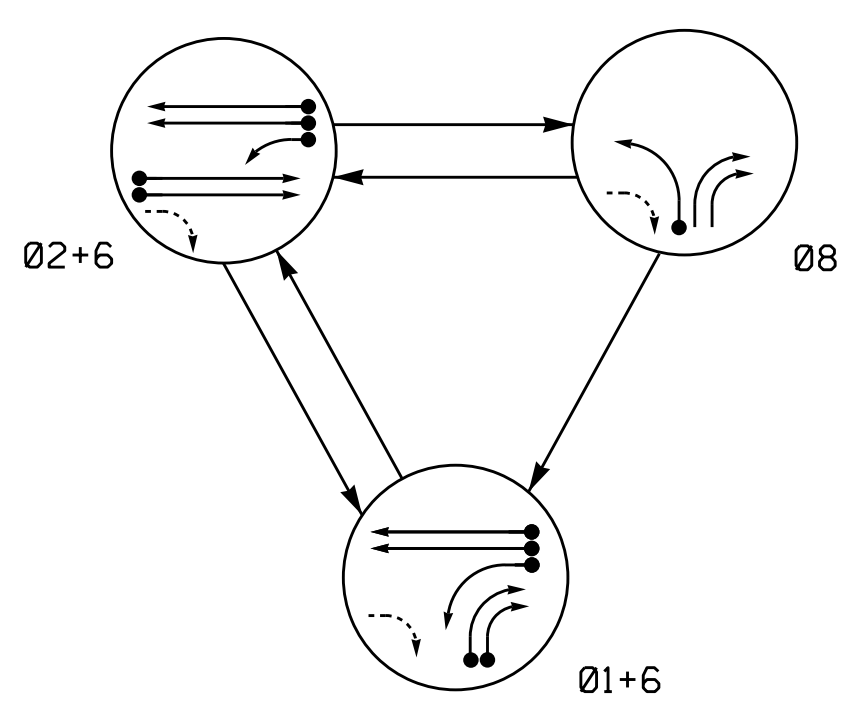
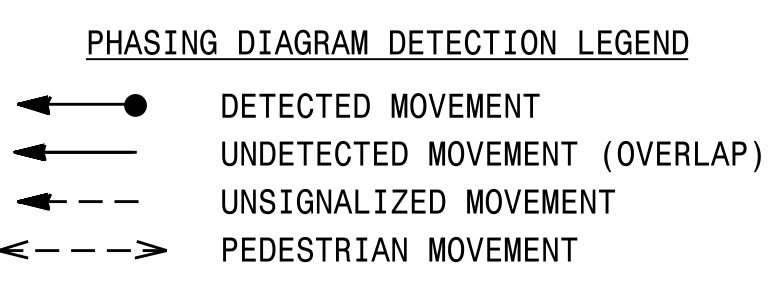
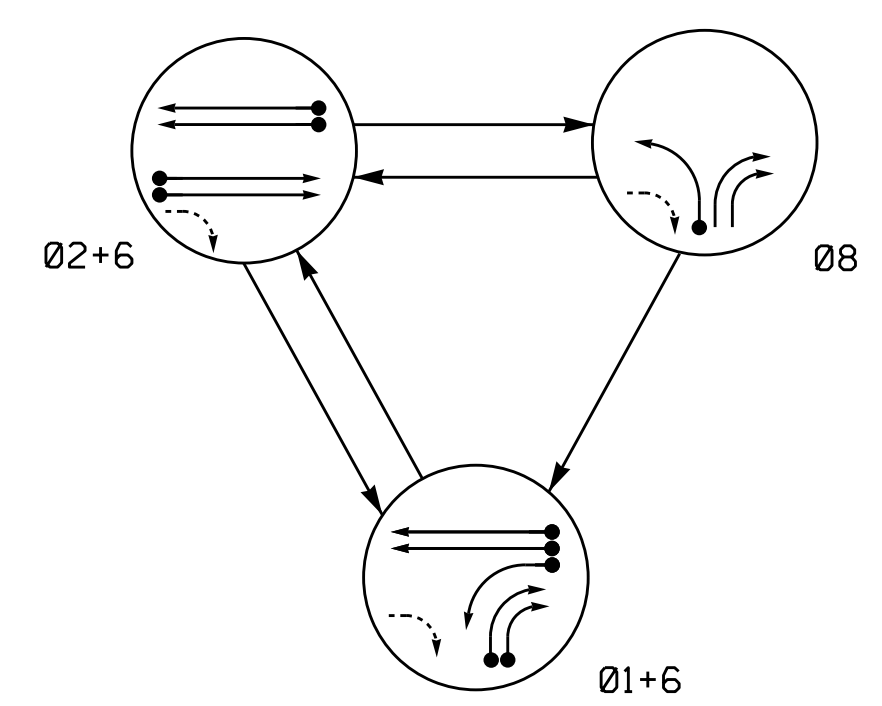


DEFAULT PHASING DIAGRAM



ALTERNATE PHASING DIAGRAM



DEFAULT PHASING TABLE OF OPERATION

SIGNAL FACE	PHASE			
	01+6	02+6	08	FLHSAH
11	←	←	←	←
12, 13	←	←	←	←
21, 22	←	←	←	←
61, 62	←	←	←	←
81	←	←	←	←

ALTERNATE PHASING TABLE OF OPERATION

SIGNAL FACE	PHASE			
	01+6	02+6	08	FLHSAH
11	←	←	←	←
12, 13	←	←	←	←
21, 22	←	←	←	←
61, 62	←	←	←	←
81	←	←	←	←

OASIS 2070 LOOP & DETECTOR INSTALLATION CHART

LOOP	SIZE (FT)	DISTANCE FROM STOPBAR (FT)	TURNS	NEW LOOP	DETECTOR PROGRAMMING				STRETCH TIME	DELAY TIME	SYSTEM LOOP	NEW CARD
					PHASE	CALLING	EXTENSION	FULL TIME DELAY				
1A	6X40	0	2-4-2	Y	1	Y	Y	-	-	15 *	-	Y
1B	6X40	0	2-4-2	-	1	Y	Y	-	-	15	-	-
1C	6X40	0	2-4-2	-	1	Y	Y	-	-	15	-	-
1D	6X15	0	4	-	1	Y	Y	-	-	20	-	-
2A	6X6	420	5	-	2	Y	Y	-	-	-	-	-
2B	6X6	420	5	-	2	Y	Y	-	-	-	-	-
6A/S3	6X6	420	5	-	6	Y	Y	-	-	-	-	Y
6B/S4	6X6	420	5	-	6	Y	Y	-	-	-	-	Y
8A	6X40	0	2-4-2	-	8	Y	Y	-	-	-	-	-

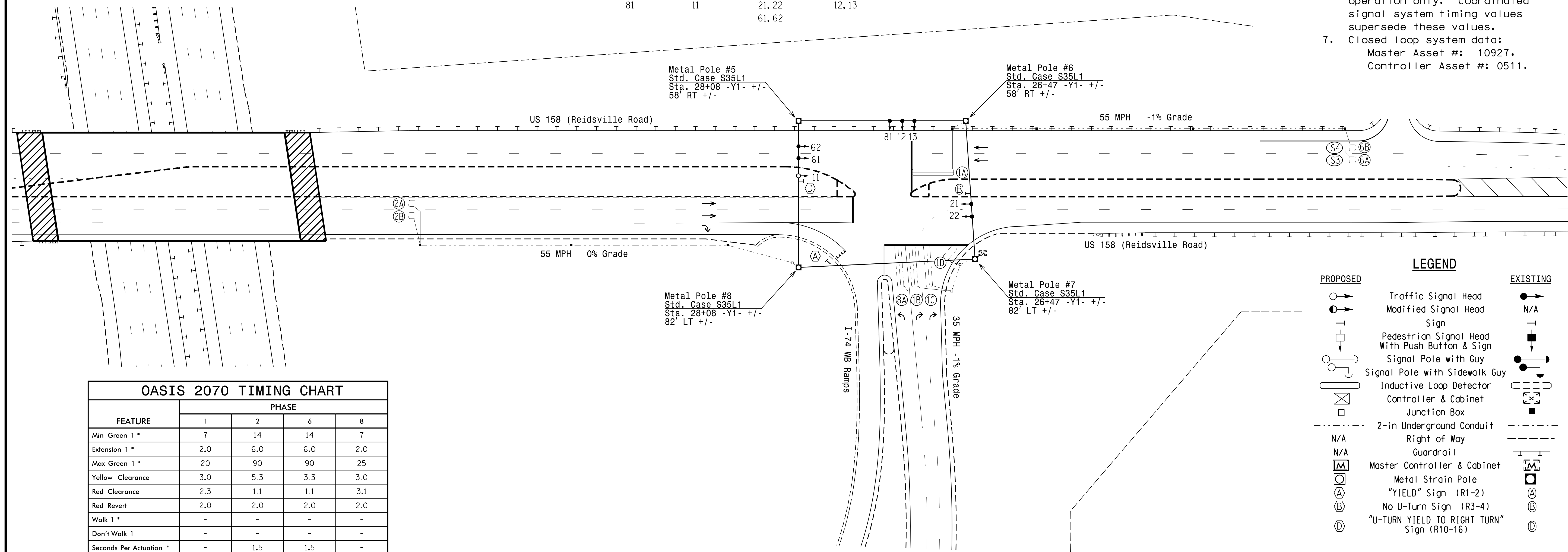
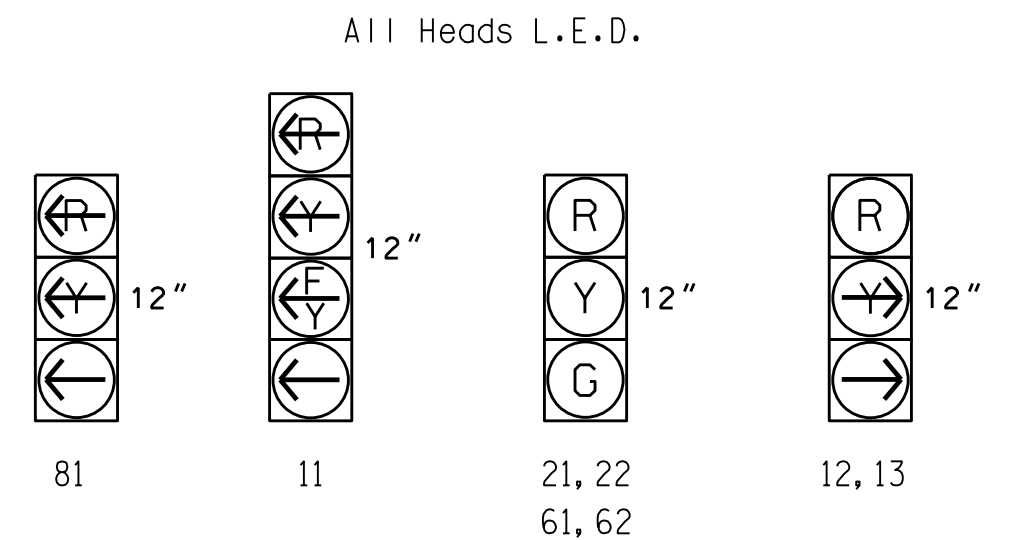
* Disable Delay During Alternate Phasing Operation.
Disable Phase 6 Call For Loop 1A During Alternate Phasing Operation.

3 Phase Fully Actuated US 158 (Reidsville Road) CLS

NOTES

- Refer to "Roadway Standard Drawings NCDOT" dated January 2012 and "Standard Specifications for Roads and Structures" dated January 2012.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Phase 1 may be lagged.
- Set all detector units to presence mode.
- The Division Traffic Engineer will determine the hours of use for each phasing plan.
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values supersede these values.
- Closed loop system data:
Master Asset #: 10927,
Controller Asset #: 0511.

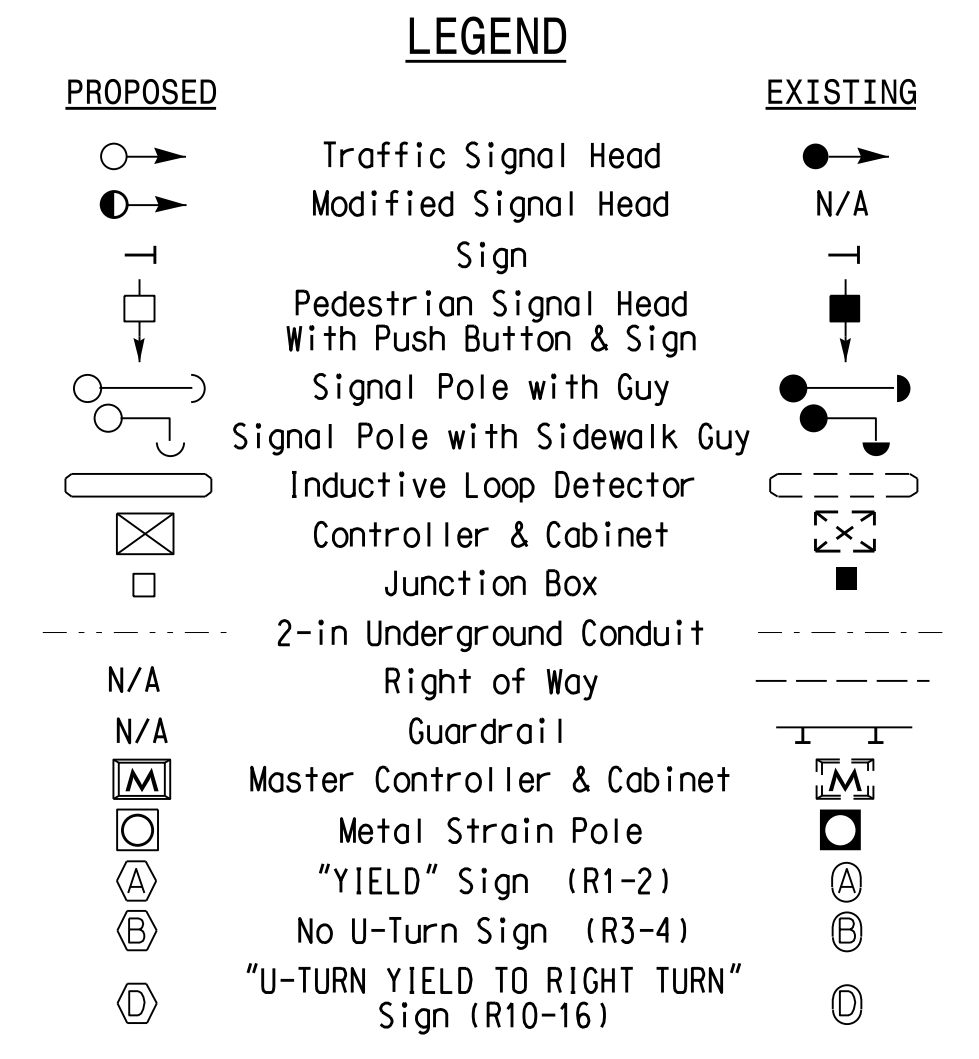
SIGNAL FACE I.D.



OASIS 2070 TIMING CHART

FEATURE	PHASE			
	1	2	6	8
Min Green 1 *	7	14	14	7
Extension 1 *	2.0	6.0	6.0	2.0
Max Green 1 *	20	90	90	25
Yellow Clearance	3.0	5.3	3.3	3.0
Red Clearance	2.3	1.1	1.1	3.1
Red Revert	2.0	2.0	2.0	2.0
Walk 1 *	-	-	-	-
Don't Walk 1	-	-	-	-
Seconds Per Actuation *	-	1.5	1.5	-
Max Variable Initial *	-	46	46	-
Time Before Reduction *	-	15	15	-
Time To Reduce *	-	30	30	-
Minimum Gap	-	3.4	3.4	-
Recall Mode	-	MIN RECALL	MIN RECALL	-
Vehicle Call Memory	-	YELLOW	YELLOW	-
Dual Entry	-	-	-	-
Simultaneous Gap	ON	ON	ON	ON

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.



Signal Upgrade

Prepared In the Offices of:
TRANSPORTATION MOBILITY AND SAFETY DIVISION
DEPARTMENT OF TRANSPORTATION
SIGNAL DESIGN SECTION
750 N. Greenfield Pkwy, Garner, NC 27529

US 158 (Reidsville Road) at I-74 WB Ramps

Division 9 Forsyth County Winston-Salem

PLAN DATE: April 2017 REVIEWED BY:

PREPARED BY: I. O. Umozurike REVIEWED BY:

REVISIONS: INIT. DATE

SCALE: 0 50
1"=50'

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SEAL
NORTH CAROLINA PROFESSIONAL ENGINEER
SEAL 026486
ROBERT J. ZILMERA

5/16/2017
DATE

SIG. INVENTORY NO. 09-0511

16-MAY-2017 13:56
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