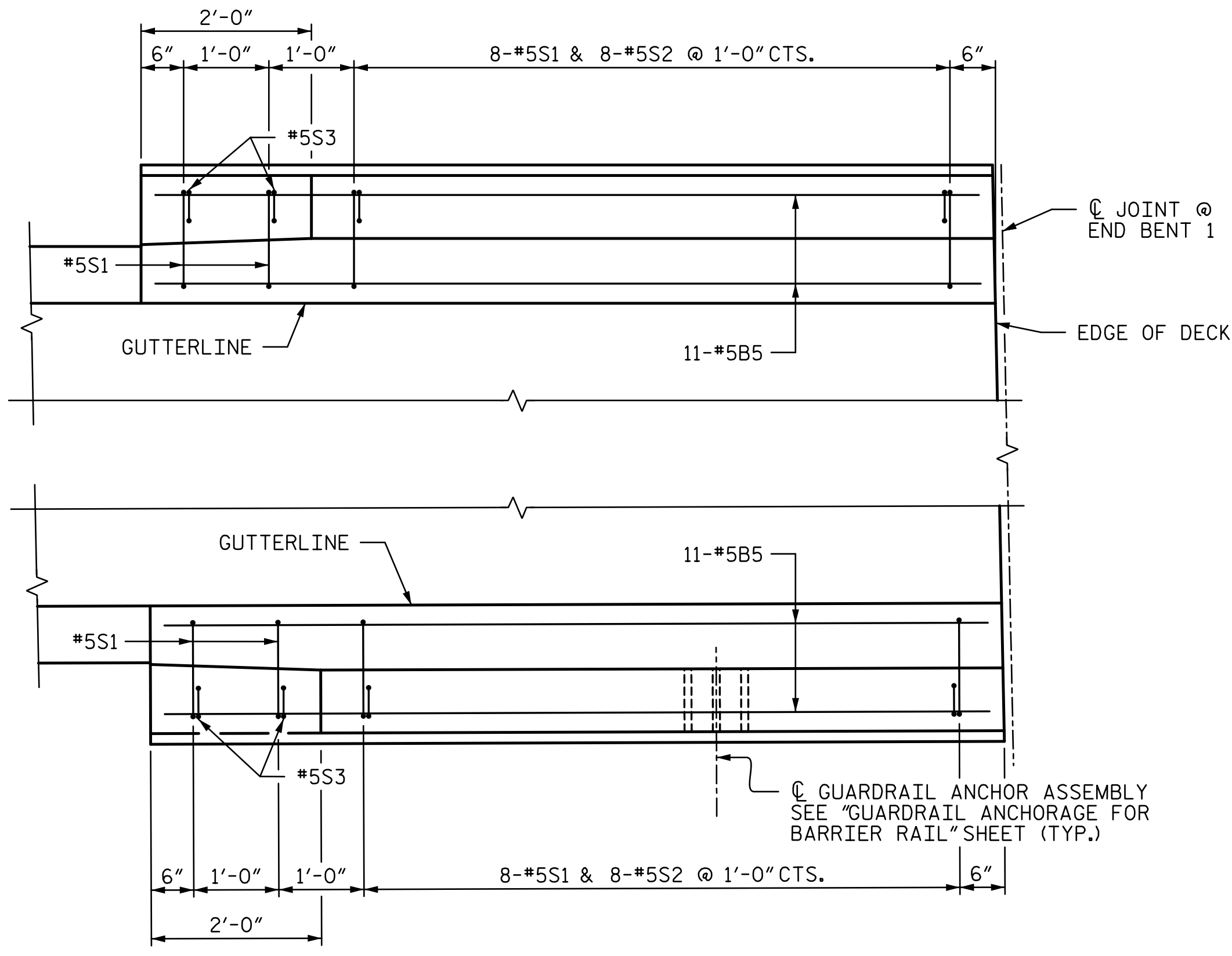
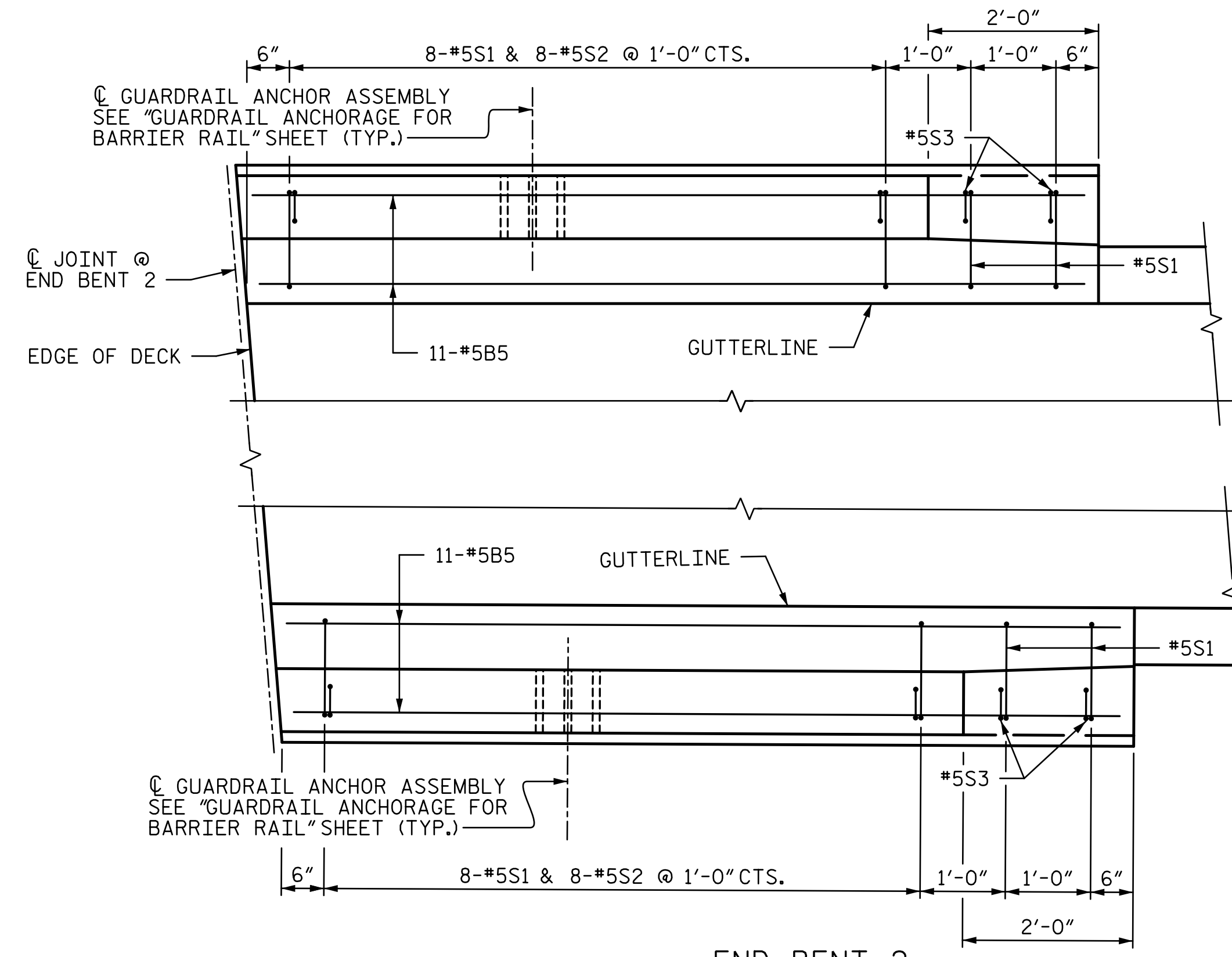


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12:15:43
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END BENT 1



END BENT 2

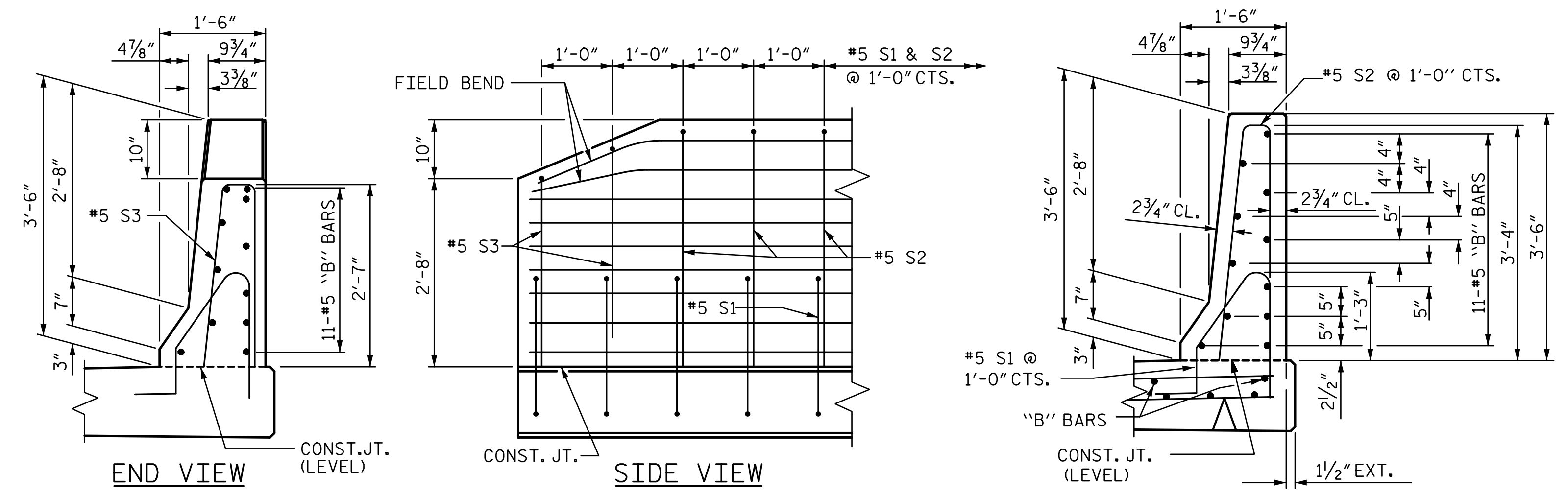
NOTES

THE COST OF THE BARRIER RAIL ON THE APPROACH SLAB SHALL BE INCLUDED IN THE LINEAR FOOT CONTRACT PRICE BID FOR "CONCRETE BARRIER RAIL".

THE BARRIER RAIL ON EACH APPROACH SLAB SHALL NOT BE CAST UNTIL ALL APPROACH SLAB CONCRETE HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

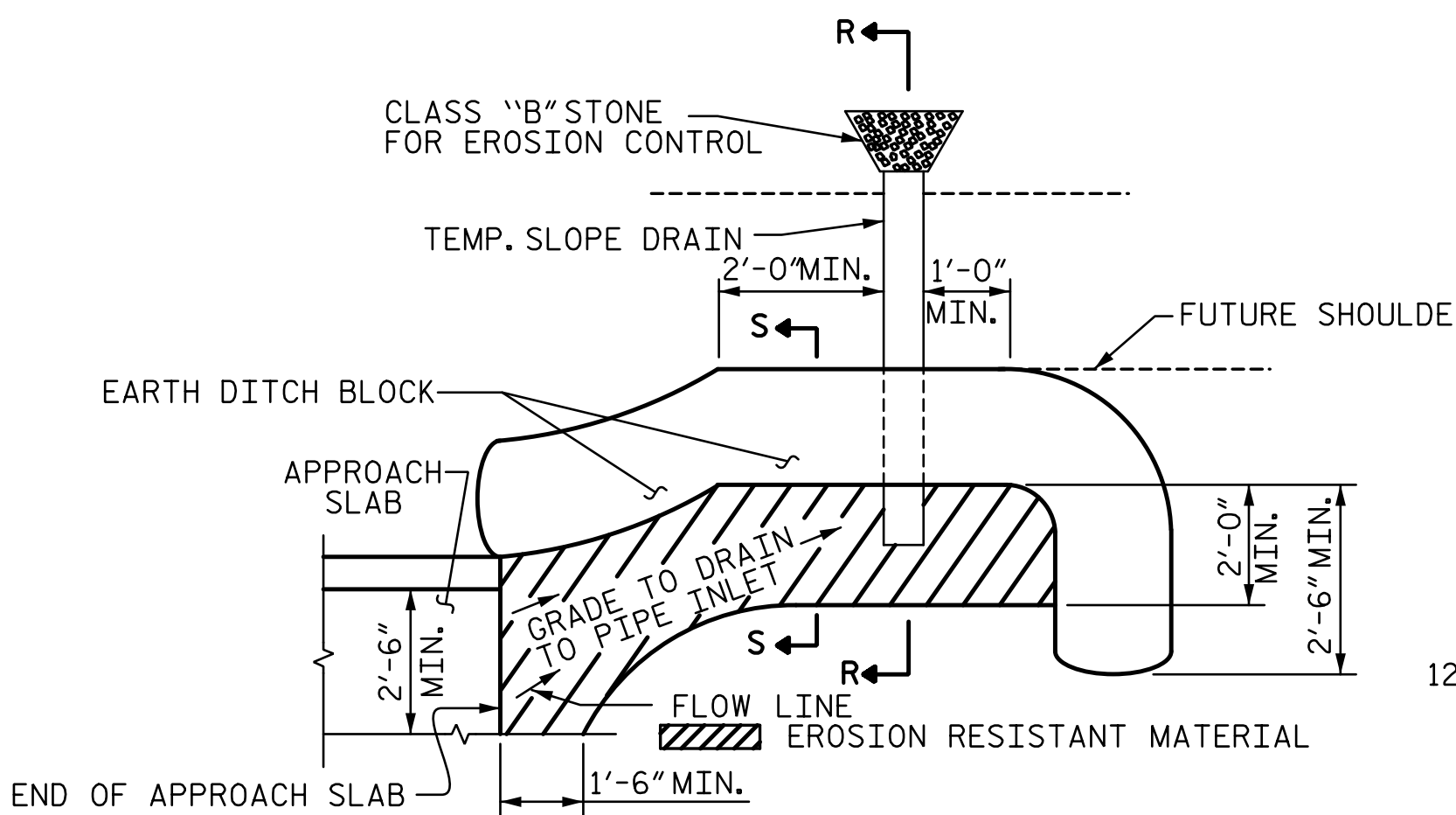
ALL REINFORCING STEEL IN BARRIER RAILS SHALL BE EPOXY COATED.

PLAN OF BARRIER RAIL



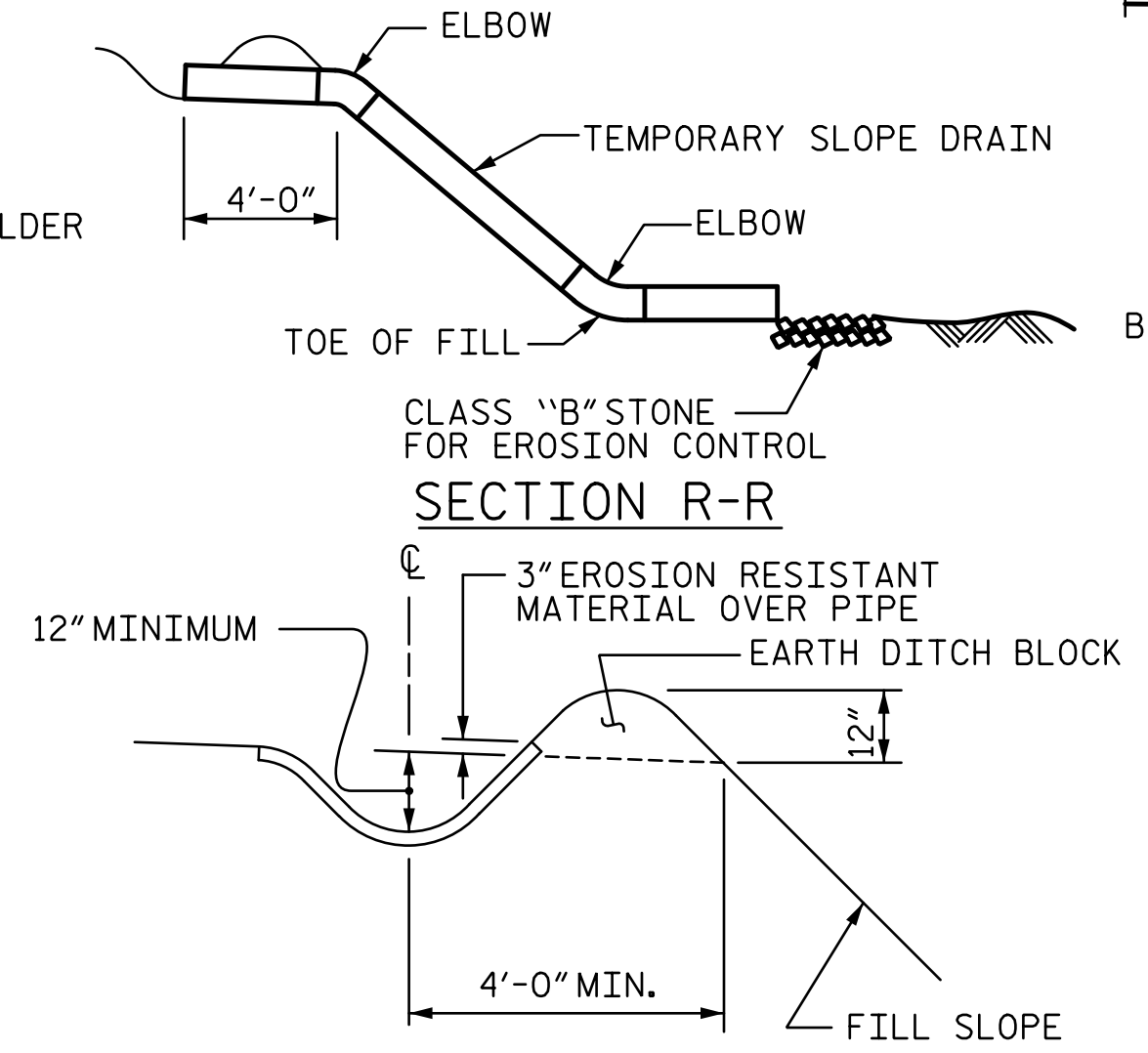
END OF RAIL DETAILS

SECTION THRU RAIL

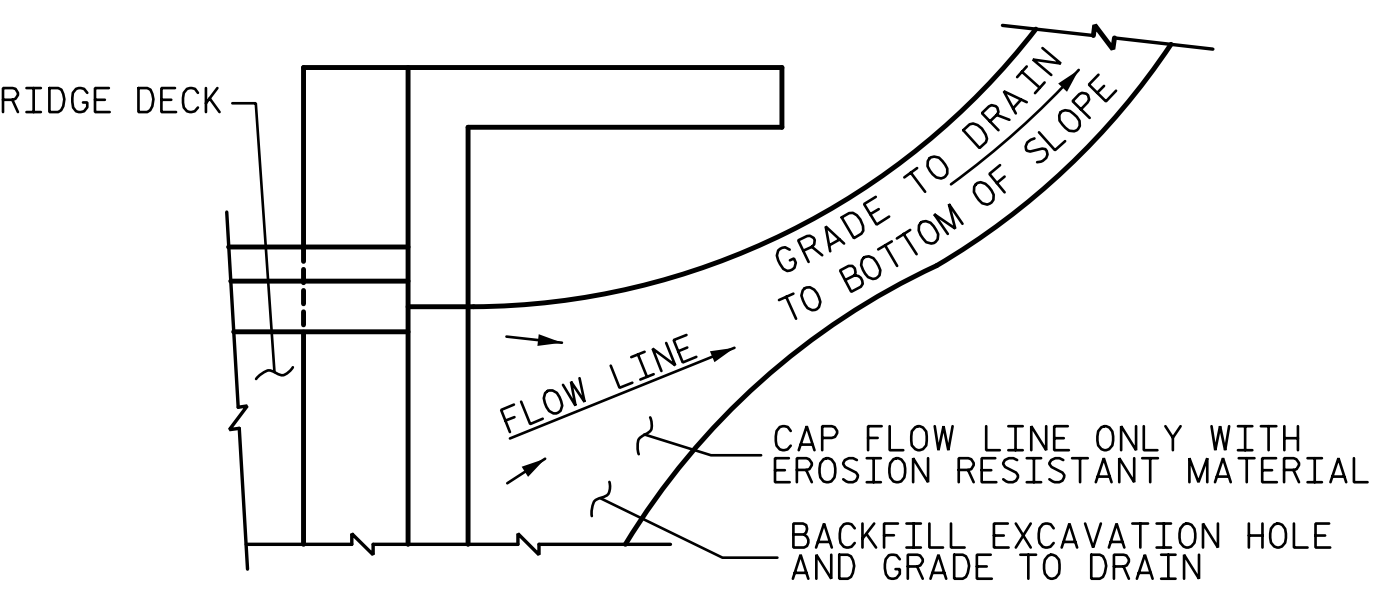


NOTE: IMMEDIATELY AFTER THE CONSTRUCTION OF THE APPROACH SLAB, THE CONTRACTOR SHALL PROVIDE TEMPORARY BERM AND SLOPE DRAIN. CONTRACTOR SHALL GRADE TO PIPE INLET AND PROVIDE EROSION RESISTANT MATERIAL AS SHOWN. THE EROSION RESISTANT MATERIAL SHALL BE EITHER 1) ASPHALT PLANT MIX, TYPE 1 OR TYPE 2, MIN. 2" DEPTH, 2) EROSION CONTROL MAT, OR 3) CONCRETE, AS DIRECTED BY THE ENGINEER. THE SLOPE DRAIN SHALL CONSIST OF A NON-PERFORATED TEMPORARY DRAINAGE PIPE, 12 INCHES IN DIAMETER.

PLAN VIEW



SECTION S-S



NOTE: IF THE APPROACH SLAB IS NOT CONSTRUCTED IMMEDIATELY AFTER THE BACKFILLING OF THE END BENT EXCAVATION, GRADE TO DRAIN TO THE BOTTOM OF THE SLOPE AND PROVIDE EROSION RESISTANT MATERIAL, SUCH AS FIBERGLASS ROVING OR AS DIRECTED BY THE ENGINEER TO PREVENT SOIL EROSION AND TO PROTECT THE AREA ADJACENT TO THE STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO REMOVE THESE MATERIALS PRIOR TO CONSTRUCTION OF THE APPROACH SLAB.

TEMPORARY DRAINAGE DETAIL

TEMPORARY BERM AND SLOPE DRAIN DETAILS

(TO BE USED WHEN SHOULDER BERM GUTTER IS REQUIRED)

BAR TYPES

ALL BAR DIMENSIONS ARE OUT TO OUT

BILL OF MATERIAL

BARRIER RAIL ONLY					
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
* B5	44	#5	STR	9'-8"	444
* S1	40	#5	1	5'-1"	212
* S2	32	#5	2	7'-0"	234
* S3	8	#5	2	5'-6"	46
* EPOXY COATED REINFORCING STEEL				LBS.	936
CLASS AA CONCRETE				C. Y.	5.6
CONCRETE BARRIER RAIL				LIN. FT.	40.17

PROJECT NO. U-4751
 NEW HANOVER COUNTY
 STATION: 225+92.26 -L-
 SHEET 2 OF 2

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BRIDGE APPROACH SLAB DETAILS

REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		

SHEET NO.		S3-36
TOTAL SHEETS		36

DRAWN BY: MBC DATE: 3-17
 CHECKED BY: VMW DATE: 5-17
 DESIGN ENGINEER OF RECORD: P. KELLY DATE: 5-17