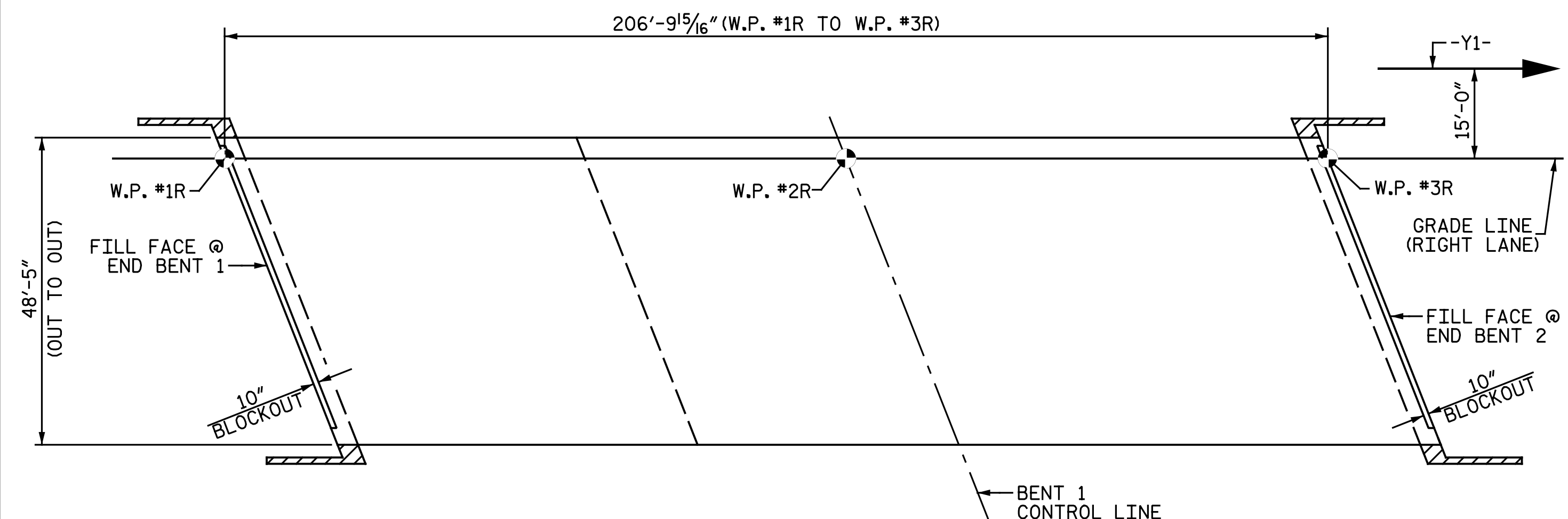
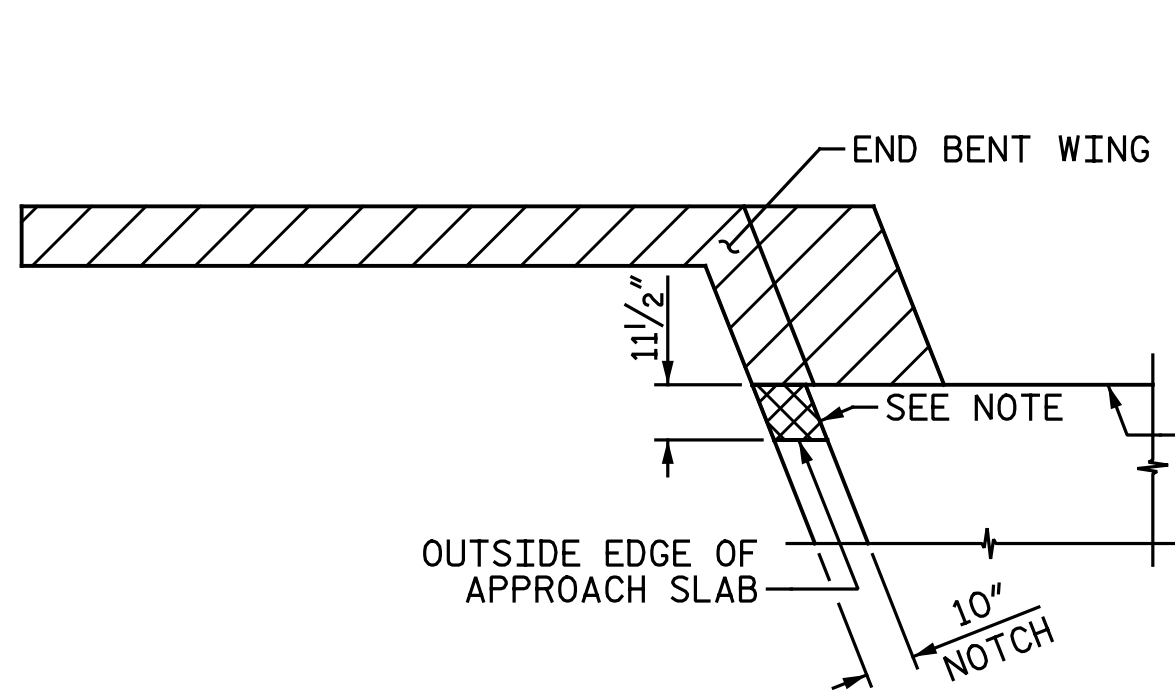


**POURING SEQUENCE**

# DENOTES POUR NUMBER AND DIRECTION

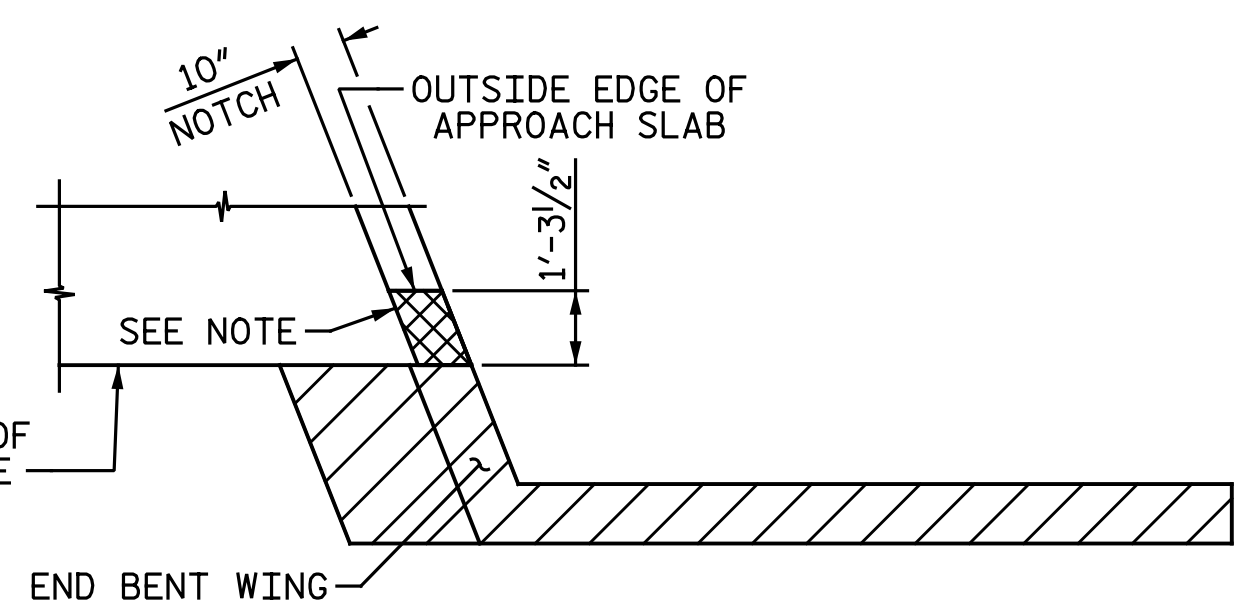


**LAYOUT FOR COMPUTING AREA OF REINFORCED CONCRETE DECK SLAB**  
(SQ. FT. = 10,011)



**DETAIL "A"**

LEFT SIDE OF BRIDGE AT END BENT 1 SHOWN, LEFT SIDE AT END BENT 2 SIMILAR



**DETAIL "B"**

RIGHT SIDE OF BRIDGE AT END BENT 2 SHOWN, RIGHT SIDE AT END BENT 1 SIMILAR

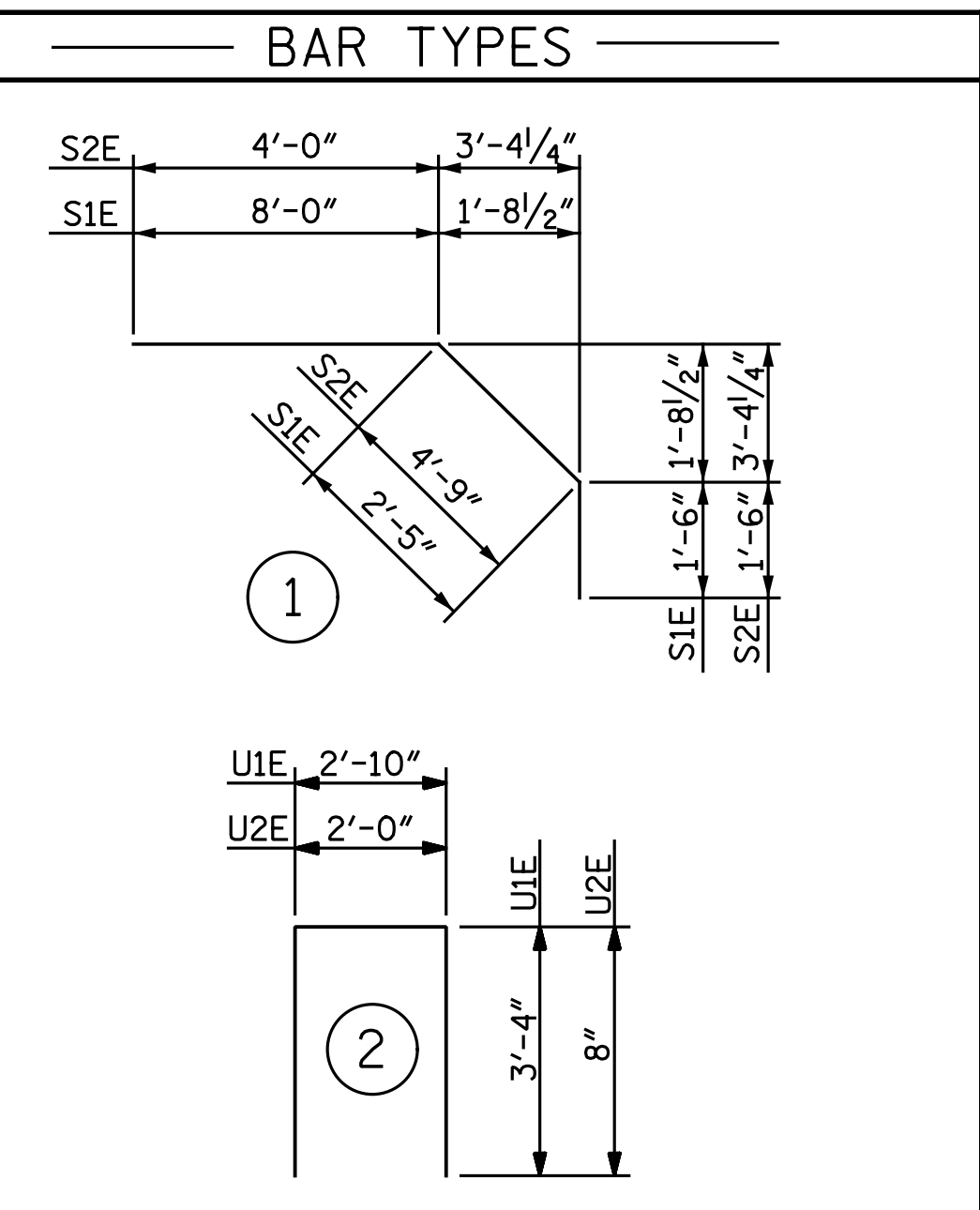
REINFORCING BAR SCHEDULE						REINFORCING BAR SCHEDULE					
SPANS A & B						SPANS A & B					
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
A1E	405	#5	STR.	48' - 1"	20,311	A201	2	#5	STR.	47' - 7"	99
A2	405	#5	STR.	48' - 1"	20,311	A202	2	#5	STR.	46' - 5"	97
A3E	10	#5	STR.	51' - 8"	539	A203	2	#5	STR.	45' - 3"	94
A101E	2	#5	STR.	47' - 7"	99	A204	2	#5	STR.	44' - 1"	92
A102E	2	#5	STR.	46' - 5"	97	A205	2	#5	STR.	42' - 11"	90
A103E	2	#5	STR.	45' - 3"	94	A206	2	#5	STR.	41' - 9"	87
A104E	2	#5	STR.	44' - 1"	92	A207	2	#5	STR.	40' - 7"	85
A105E	2	#5	STR.	42' - 11"	90	A208	2	#5	STR.	39' - 5"	82
A106E	2	#5	STR.	41' - 9"	87	A209	2	#5	STR.	38' - 3"	80
A107E	2	#5	STR.	40' - 7"	85	A210	2	#5	STR.	37' - 1"	77
A108E	2	#5	STR.	39' - 5"	82	A211	2	#5	STR.	35' - 11"	75
A109E	2	#5	STR.	38' - 3"	80	A212	2	#5	STR.	34' - 9"	72
A110E	2	#5	STR.	37' - 1"	77	A213	2	#5	STR.	33' - 7"	70
A111E	2	#5	STR.	35' - 11"	75	A214	2	#5	STR.	32' - 6"	68
A112E	2	#5	STR.	34' - 9"	72	A215	2	#5	STR.	31' - 4"	65
A113E	2	#5	STR.	33' - 7"	70	A216	2	#5	STR.	30' - 2"	63
A114E	2	#5	STR.	32' - 6"	68	A217	2	#5	STR.	29' - 0"	60
A115E	2	#5	STR.	31' - 4"	65	A218	2	#5	STR.	27' - 10"	58
A116E	2	#5	STR.	30' - 2"	63	A219	2	#5	STR.	26' - 8"	56
A117E	2	#5	STR.	29' - 0"	60	A220	2	#5	STR.	25' - 6"	53
A118E	2	#5	STR.	27' - 10"	58	A221	2	#5	STR.	24' - 4"	51
A119E	2	#5	STR.	26' - 8"	56	A222	2	#5	STR.	23' - 2"	48
A120E	2	#5	STR.	25' - 6"	53	A223	2	#5	STR.	22' - 0"	46
A121E	2	#5	STR.	24' - 4"	51	A224	2	#5	STR.	20' - 10"	43
A122E	2	#5	STR.	23' - 2"	48	A225	2	#5	STR.	19' - 8"	41
A123E	2	#5	STR.	22' - 0"	46	A226	2	#5	STR.	18' - 6"	39
A124E	2	#5	STR.	20' - 10"	43	A227	2	#5	STR.	17' - 4"	36
A125E	2	#5	STR.	19' - 8"	41	A228	2	#5	STR.	16' - 2"	34
A126E	2	#5	STR.	18' - 6"	39	A229	2	#5	STR.	15' - 0"	31
A127E	2	#5	STR.	17' - 4"	36	A230	2	#5	STR.	13' - 11"	29
A128E	2	#5	STR.	16' - 2"	34	A231	2	#5	STR.	12' - 9"	27
A129E	2	#5	STR.	15' - 0"	31	A232	2	#5	STR.	11' - 7"	24
A130E	2	#5	STR.	13' - 11"	29	A233	2	#5	STR.	10' - 5"	22
A131E	2	#5	STR.	12' - 9"	27	A234	2	#5	STR.	9' - 3"	19
A132E	2	#5	STR.	11' - 7"	24	A235	2	#5	STR.	8' - 1"	17
A133E	2	#5	STR.	10' - 5"	22	A236	2	#5	STR.	6' - 11"	14
A134E	2	#5	STR.	9' - 3"	19	A237	2	#5	STR.	5' - 9"	12
A135E	2	#5	STR.	8' - 1"	17	A238	2	#5	STR.	4' - 7"	10
A136E	2	#5	STR.	6' - 11"	14	A239	2	#5	STR.	3' - 5"	7
A137E	2	#5	STR.	5' - 9"	12	A240	2	#5	STR.	2' - 3"	5
A138E	2	#5	STR.	4' - 7"	10	A241	2	#5	STR.	1' - 1"	2
A139E	2	#5	STR.	3' - 5"	7	B1E	132	#5	STR.	53' - 1"	7,308
A140E	2	#5	STR.	2' - 3"	5	B2	256	#5	STR.	52' - 10"	14,107
A141E	2	#5	STR.	1' - 1"	2	B3E	64	#6	STR.	23' - 3"	2,235
						B4E	32	#6	STR.	21' - 5"	1,029
						B5E	64	#6	STR.	60' - 0"	5,768
						B6E	32	#6	STR.	25' - 1"	1,206
						B7E	48	#4	STR.	27' - 5"	879
						G1E	205	#4	STR.	6' - 9"	924
						H1	16	#4	STR.	2' - 10"	30
						K1E	16	#4	STR.	21' - 9"	232
						K2E	16	#4	STR.	32' - 11"	352
						S1E	92	#4	1	11' - 11"	732
						S2E	88	#4	1	10' - 3"	603
						U1E	92	#4	2	9' - 6"	584
						U2E	60	#4	2	3' - 4"	134
						V1	12	#4	STR.	3' - 10"	31
						REINFORCING STEEL		LBS.	36,559		
						EPOXY COATED REINF. STEEL		LBS.	44,916		

**GROOVING BRIDGE FLOORS**

APPROACH SLABS	1,798 SQ.FT.
BRIDGE DECK	7,573 SQ.FT.
TOTAL	9,371 SQ.FT.

**NOTE:**

CONCRETE SHALL BE POURED IN THE CROSS-HATCHED AREAS SHOWN IN DETAIL "A" AND DETAIL "B" TO MATCH THE TOP OF END BENT WINGS. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THE CONCRETE IN THESE AREAS SHALL BE POURED AT THE SAME TIME THE BLOCKOUTS IN THE END BENT WINGS ARE FILLED WITH CONCRETE AS NOTED ON SHEET 1 OF "INTEGRAL END BENT 1" AND "INTEGRAL END BENT 2" SHEETS.



ALL BAR DIMENSIONS ARE OUT TO OUT

**SUPERSTRUCTURE BILL OF MATERIAL**

	CLASS AA CONCRETE (CU. YDS.)	REINFORCING STEEL (LBS.)	EPOXY COATED REINFORCING STEEL (LBS.)
SPANS A & B		36,559	44,916
POUR 1	77.4		
POUR 2	208.6		
POUR 3*	64.3		
SIDEWALK	36.8		***
TOTALS **	387.1	36,559	44,916

\* POUR 3 QUANTITY INCLUDES DIAPHRAGM PORTION OF INTEGRAL END BENT  
 \*\* QUANTITIES FOR CONCRETE BARRIER RAIL ARE NOT INCLUDED  
 \*\*\* QUANTITIES INCLUDED WITH SPAN TOTALS

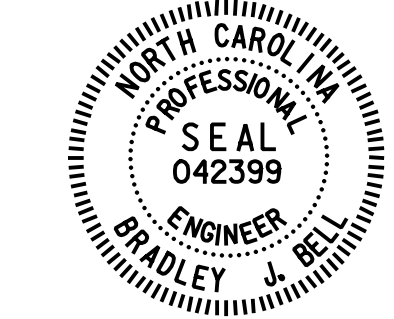
**SUPERSTRUCTURE REINFORCING STEEL LENGTHS ARE BASED ON THE FOLLOWING MINIMUM SPLICE LENGTHS**

BAR SIZE	SUPERSTRUCTURE EXCEPT APPROACH SLABS, PARAPET, AND BARRIER RAIL		APPROACH SLABS		PARAPET AND BARRIER RAIL
	EPOXY COATED	UNCOATED	EPOXY COATED	UNCOATED	
#4	2'-0"	1'-9"	2'-0"	1'-9"	2'-9"
#5	2'-6"	2'-2"	2'-6"	2'-2"	3'-5"
#6	3'-0"	2'-7"	3'-10"	2'-7"	4'-4"
#7	5'-3"	3'-6"			
#8	6'-10"	4'-7"			

PROJECT NO. U-3330  
 NASH COUNTY  
 STATION: 18+22.61 -Y1-

DRAWN BY: N. B. SPEAKS DATE: 9-8-16  
 CHECKED BY: B. J. BELL DATE: 9-8-16

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



DocuSigned by:  
 Bradley J. Bell  
 CA1A5F8E3C3A34  
 1/27/2017

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STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH  
 SUPERSTRUCTURE  
 BILL OF MATERIAL  
 RIGHT LANES

REVISIONS						SHEET NO. S2-25
NO.	BY:	DATE:	NO.	BY:	DATE:	
1			3			TOTAL SHEETS 39
2			4			