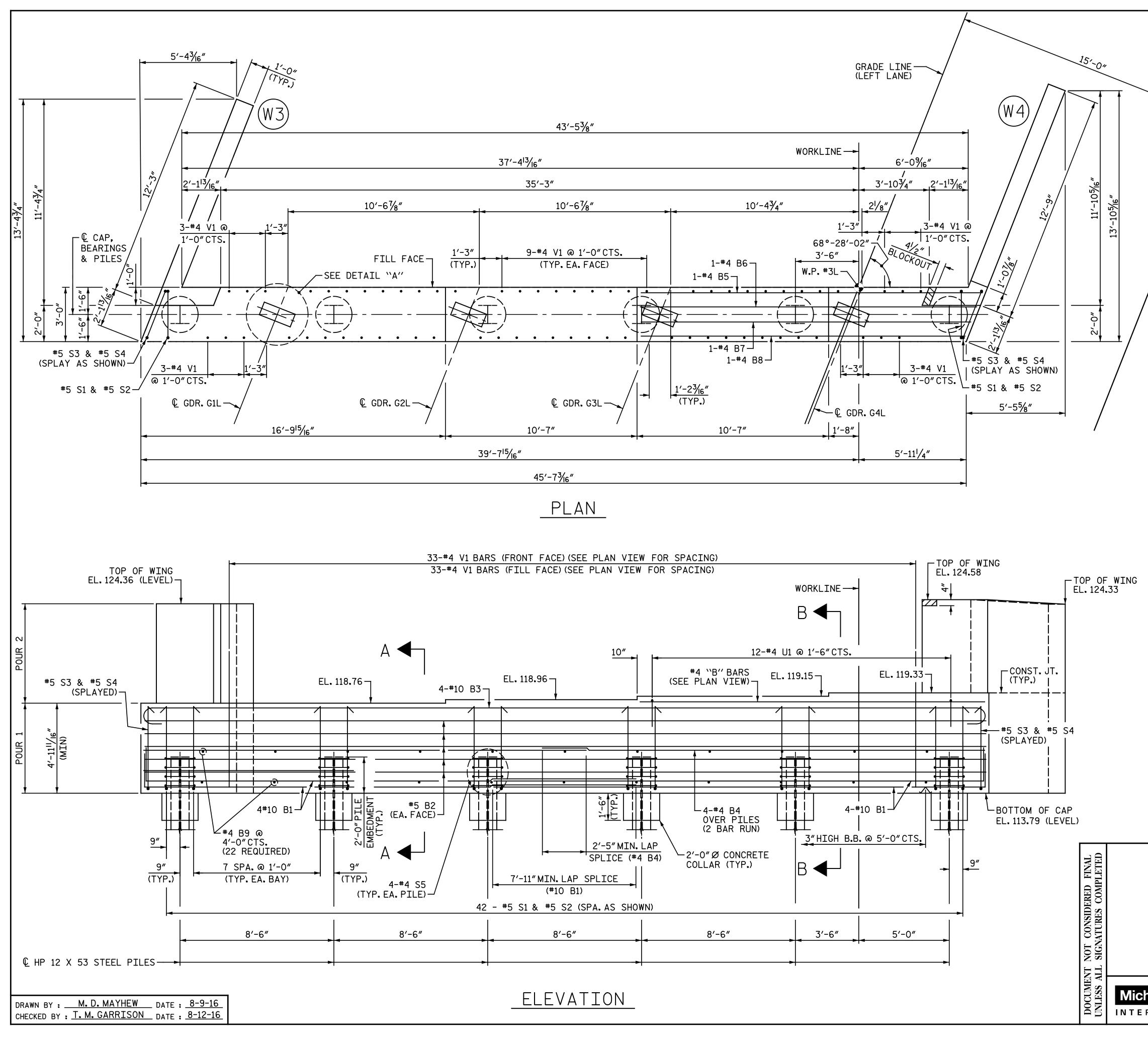
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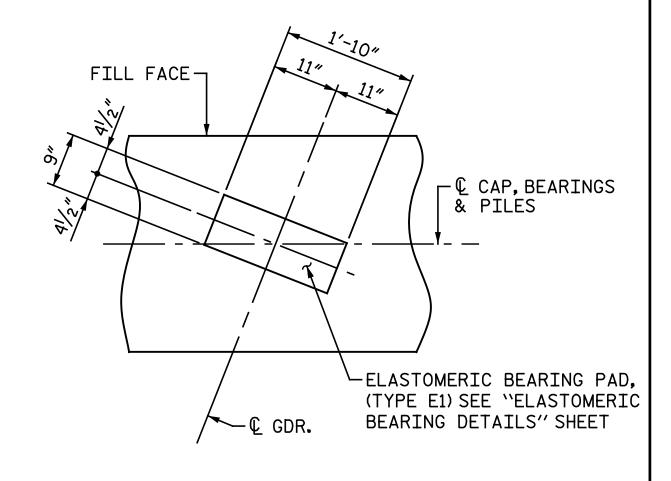
FOR ``SECTION A-A'' AND ``SECTION B-B'', SEE ``INTEGRAL END BENT 2 DETAILS'' SHEET.

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR #4 V1 BARS.

THE TOP SURFACE OF THE END BENT CAP, EXCLUDING THE BEARING AREA, SHALL BE RAKED TO A DEPTH OF $\frac{1}{4}$ ".

THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE BARRIER RAIL IS CAST IF SLIP FORMING IS USED.

#4 B9 BARS MAY BE SHIFTED AS NECESSARY TO CLEAR THE STEEL PILES.



DETAIL ``A'' ALL DIMENSIONS AND DETAILS SHOWN ARE TYPICAL FOR ALL BEARINGS AT EACH BRIDGE SEAT LOCATION.

U-3330 PROJECT NO.____ NASH . COUNTY 18+22.61 -Y1-STATION: SHEET 1 OF 2 TH CAROL STATE OF NORTH CAROLINA OFESSION N DEPARTMENT OF TRANSPORTATION RALEIGH 042399 SUBSTRUCTURE ADLEY J. INTEGRAL END BENT 2 Bradley J. Bell C41A3F8ECA3C434... 1/27/2017 LEFT LANES REVISIONS SHEET NO. Michael Baker Engineering 8000 Regency Parkway, Suite 600 Cary, North Carolina 27518 NC License No.: F-1084 DATE: NO. BY: DATE: SI-32 Michael Baker BY: NO. total sheets 38 INTERNATIONAL