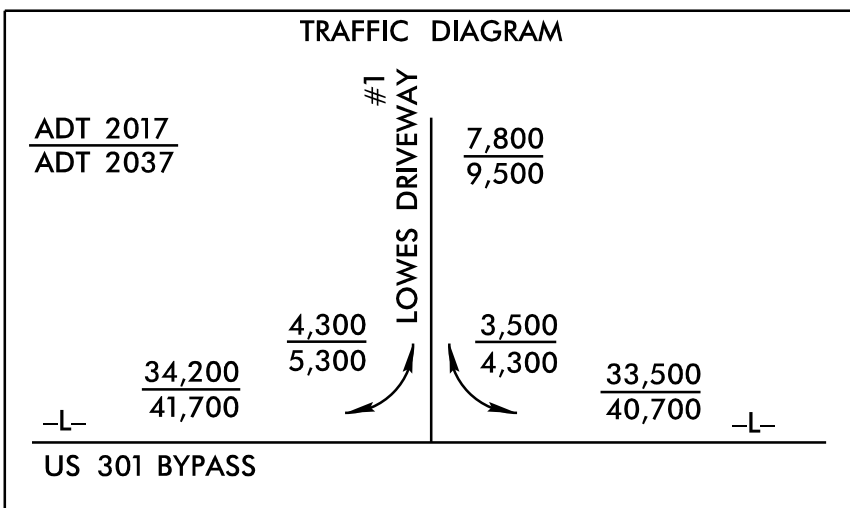
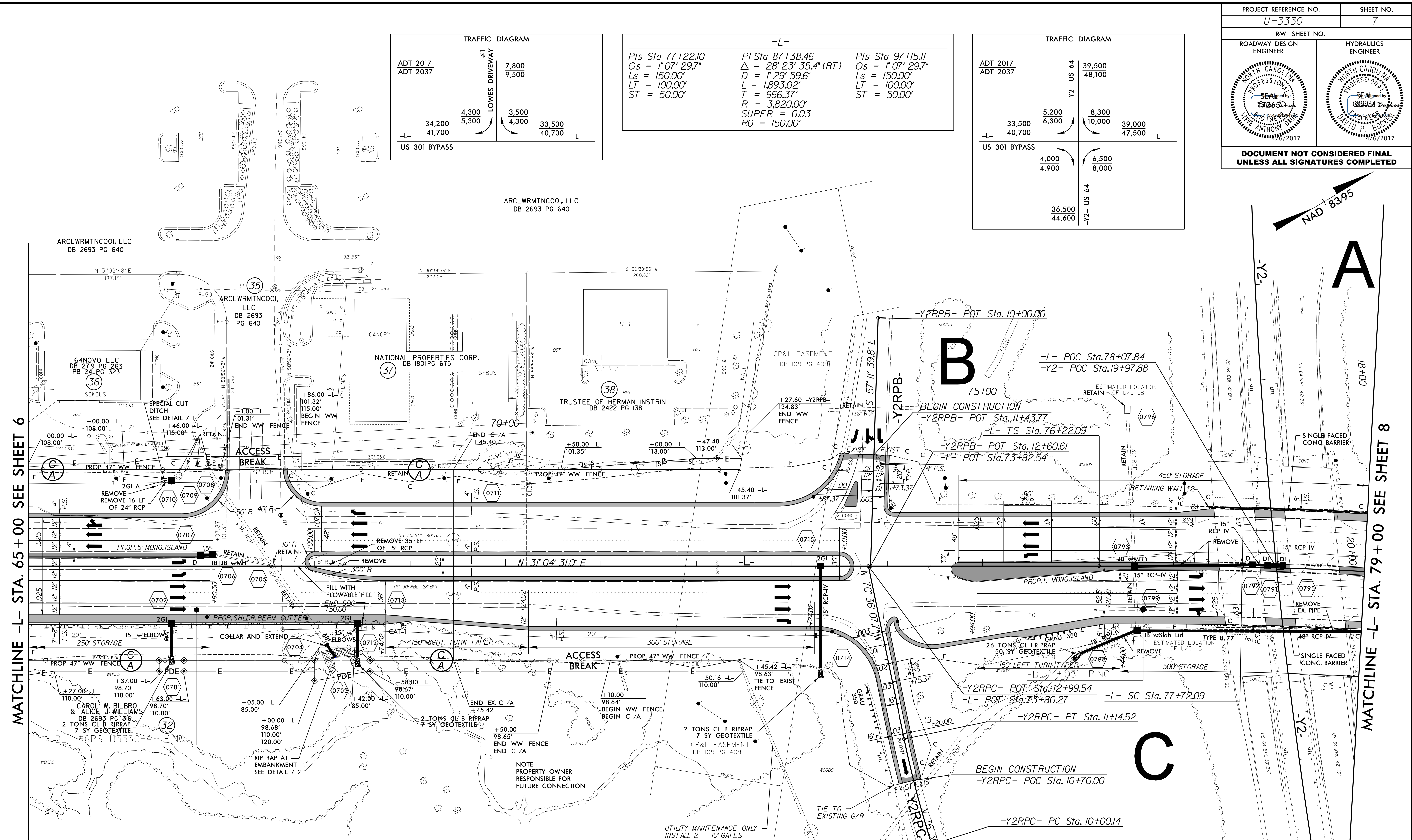
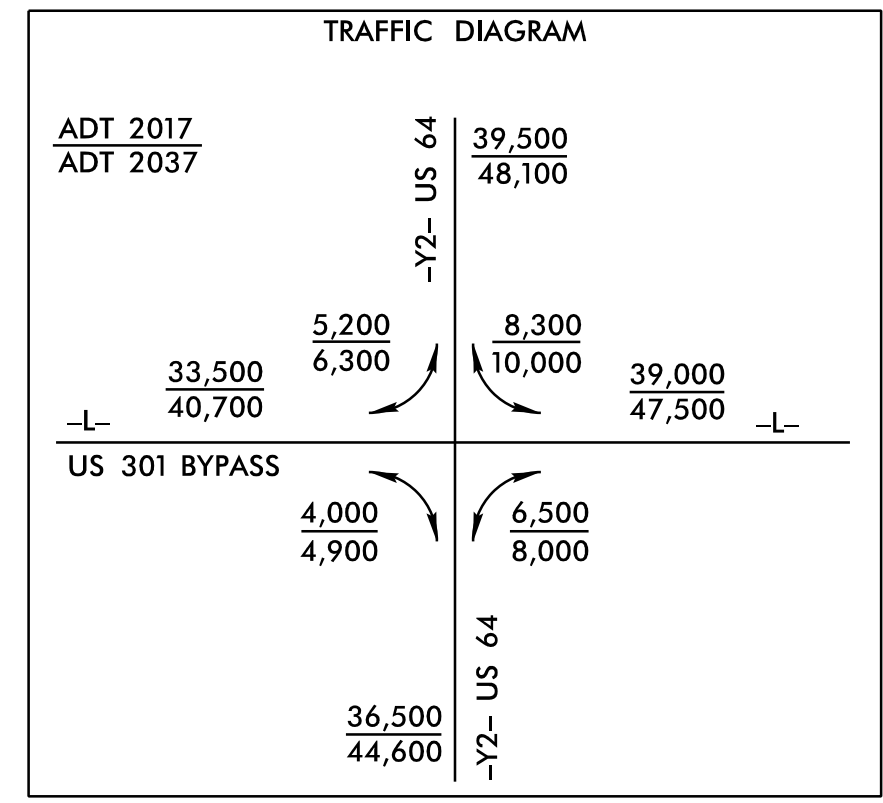


| | |
|--|---------------------|
| PROJECT REFERENCE NO. U-3330 | SHEET NO. 7 |
| RW SHEET NO. | |
| ROADWAY DESIGN ENGINEER | HYDRAULICS ENGINEER |
| | |
| DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED | |



-L-

| | | |
|--------------------------|--------------------------------|--------------------------|
| PI Sta 77+22.10 | PI Sta 87+38.46 | PI Sta 97+15.11 |
| $\Delta = 1' 07' 29.7''$ | $\Delta = 28' 23' 35.4''$ (RT) | $\Delta = 1' 07' 29.7''$ |
| $L_s = 150.00'$ | $D = 1' 29' 59.6''$ | $L_s = 150.00'$ |
| $LT = 100.00'$ | $L = 1,893.02'$ | $LT = 100.00'$ |
| $ST = 50.00'$ | $T = 966.37'$ | $ST = 50.00'$ |
| | $R = 3,820.00'$ | |
| | $SUPER = 0.03$ | |
| | $RO = 150.00'$ | |



MATCHLINE -L- STA. 65+00 SEE SHEET 6

MATCHLINE -L- STA. 79+00 SEE SHEET 8

-Y2RPC-

| |
|-------------------------------|
| PI Sta 10+57.38 |
| $\Delta = 5' 54' 14.6''$ (RT) |
| $D = 5' 09' 42.4''$ |
| $L = 114.38'$ |
| $T = 57.24'$ |
| $R = 1,110.00'$ |
| $SUPER = EXIST.$ |

-Y2RPC- POT Sta. 10+00.00

-Y2-

| |
|-------------------------------|
| PI Sta 20+46.86 |
| $\Delta = 9' 50' 42.2''$ (LT) |
| $D = 1' 00' 00.0''$ |
| $L = 984.51'$ |
| $T = 493.47'$ |
| $R = 5,729.58'$ |

TRANSITION MEDIAN FROM 30' TO 33' -L- STA. 73+90.00 TO STA. 74+65.00

DRIVEWAY RADII ARE 25' UNLESS OTHERWISE NOTED

FOR -L- PROFILE SEE SHT. 16
 FOR -Y2RPB- & -Y2RPC- PROFILE SEE SHT. 23
 SEE SHT. 2D-1 FOR DITCH DETAILS
 SEE SHT. 2B-2 FOR INTERSECTION DETAILS

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