

TYPICAL SECTION AT INTERMEDIATE DIAPHRAGMS

NOTES:

PROVIDE 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0" CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF "A" BARS, WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M.) @ 4'-0" CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF "A" BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

BARRIER RAIL SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

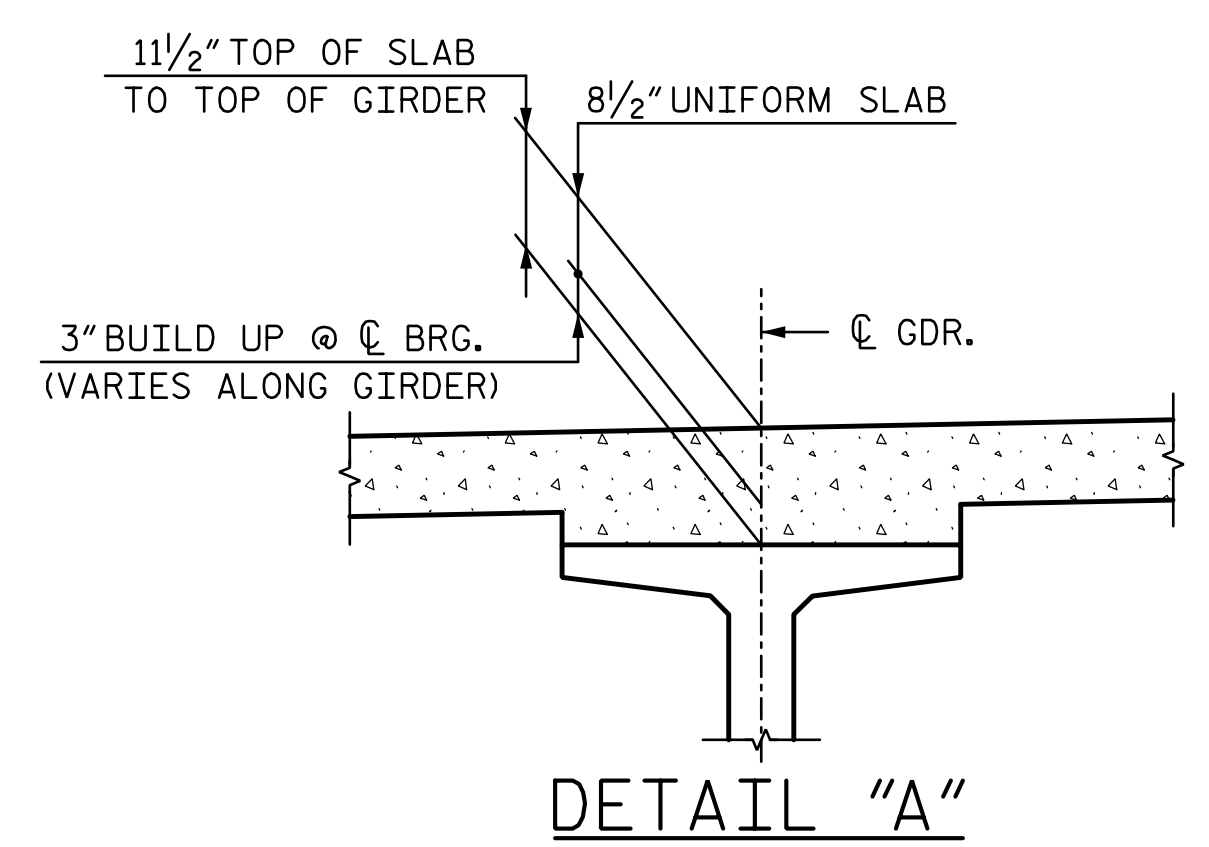
FOR INTERMEDIATE DIAPHRAGM DETAILS, SEE "INTERMEDIATE STEEL DIAPHRAGMS FOR 63" MODIFIED BULB TEE PRESTRESSED CONCRETE GIRDERS" SHEET.

FOR BARRIER RAIL DETAILS, SEE "CONCRETE BARRIER RAIL" SHEET.

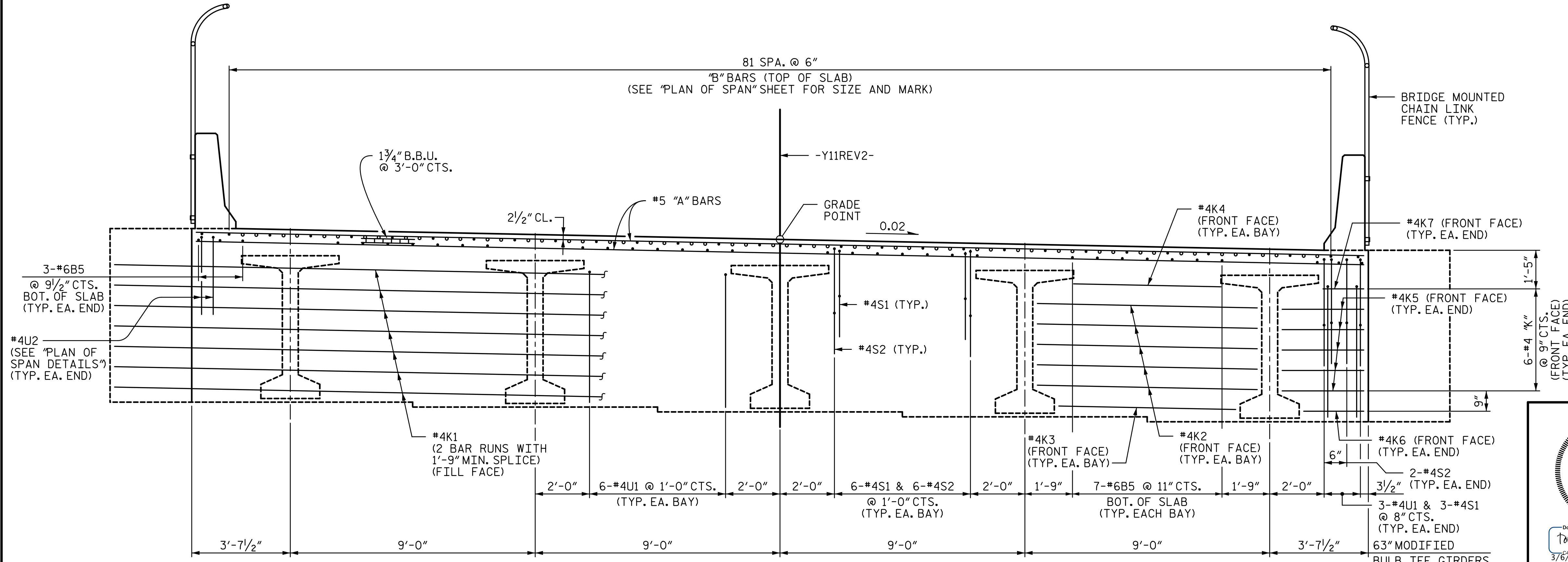
FOR INTEGRAL BACKWALL AND WING WALL DIMENSIONS, DETAILS AND ELEVATIONS, SEE "PLAN OF SPANS DETAILS" SHEETS.

HEIGHT OF BEAM BOLSTER IS CALCULATED @ @ BEARING. CONTRACTOR SHALL ADJUST HEIGHTS, AS NECESSARY TO MAINTAIN PROPER CLEARANCE, DUE TO GIRDER CAMBER.

PREVIOUSLY CAST CONCRETE SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.

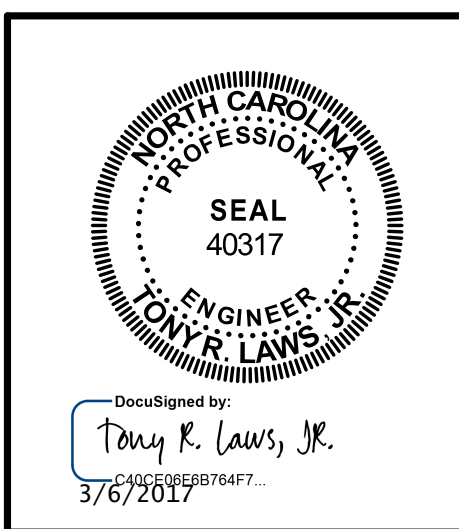


DETAIL "A"



ELEVATION OF INTEGRAL END BENT DIAPHRAGM

PROJECT NO. R-2707C
CLEVELAND COUNTY
 STATION: 32+31.41 -Y11REV2-



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 SUPERSTRUCTURE
 TYPICAL SECTION &
 INTEGRAL BACKWALL

DRAWN BY: MBC DATE: 9-16
 CHECKED BY: JTG DATE: 9-16
 DESIGN ENGINEER OF RECORD: T. LAWS DATE: 9-16

(END BENT 1 SHOWN, END BENT 2 SIMILAR)
 (LOOKING IN DIRECTION OF STATIONING,
 WING REINFORCEMENT NOT SHOWN FOR CLARITY.)

DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED

REVISIONS				SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		

TOTAL SHEETS: 27

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