

FOUNDATION LAYOUT

NOTES

ALL END BENT PILES ARE VERTICAL HP 12 x 53 STEEL PILES. DIMENSIONS LOCATING PILES ARE SHOWN TO PILE CENTERLINES AT THE BOTTOM OF THE END BENT CAPS.

FOR PILES, SEE GEOTECHNICAL SPECIAL PROVISIONS AND SECTION 450 OF THE STANDARD SPECIFICATIONS.

PILES AT END BENT 1 AND END BENT 2 ARE DESIGNED FOR A FACTORED RESISTANCE OF 105 TONS PER PILE.

DRIVE PILES AT END BENT 1 TO A REQUIRED DRIVING RESISTANCE OF 240 TONS PER PILE. THIS REQUIRED DRIVING RESISTANCE INCLUDES ADDITIONAL RESISTANCE FOR DOWNDRAG.

DRIVE PILES AT END BENT 2 TO A REQUIRED DRIVING RESISTANCE OF 225 TONS PER PILE. THIS REQUIRED DRIVING RESISTANCE INCLUDES ADDITIONAL RESISTANCE FOR DOWNDRAG.

DRIVE PILES AT END BENT 1 AND END BENT 2 AFTER MSE WALL CONSTRUCTION AND AFTER THE 1 MONTH WAITING PERIOD.

IT HAS BEEN ESTIMATED THAT A HAMMER WITH AN EQUIVALENT RATED ENERGY IN THE RANGE OF 35,000 TO 55,000 FT.-LBS. PER BLOW WILL BE REQUIRED TO DRIVE PILES AT THE END BENTS. THIS ESTIMATED ENERGY RANGE DOES NOT RELEASE THE CONTRACTOR FROM PROVIDING DRIVING EQUIPMENT IN ACCORDANCE WITH SUBARTICLE 450-3(D)(2) OF THE STANDARD SPECIFICATIONS.

TESTING PILES WITH THE PDA DURING DRIVING, RESTRIKING OR REDRIVING MAY BE REQUIRED. THE ENGINEER WILL DETERMINE THE NEED FOR PDA TESTING. FOR PDA TESTING, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

INSTALLATION OF 30" DIAMETER CORRUGATED METAL CANS FROM THE BOTTOM OF THE PILE CAP TO THE LEVELING PAD ELEVATION IS REQUIRED FOR PILES AT END BENT 1 AND END BENT 2. THE CANS SHALL BE DESIGNED TO WITHSTAND THE PRESSURES FROM COMPACTION OPERATIONS ON ADJACENT FILLS WITHOUT DISTORTION. AT A MINIMUM, CORRUGATED METAL CANS SHALL BE 16-GAUGE WITH A WALL THICKNESS OF 0.064".

LOOSELY BACKFILL CORRUGATED METAL CANS USING SAME MATERIAL AS MSE REINFORCEMENT ZONE PRIOR TO CONSTRUCTION OF THE END BENT PILE CAP. DO NOT COMPACT MATERIAL WITHIN THE CAN.

OBSERVE A 1 MONTH WAITING PERIOD AFTER CONSTRUCTING MSE RETAINING WALL AT END BENT 1 AND END BENT 2 TO THE BOTTOM OF CAP ELEVATION BEFORE BEGINNING END BENT CONSTRUCTION. FOR BRIDGE WAITING PERIODS, SEE ROADWAY PLANS AND SPECIAL PROVISIONS.

OBSERVE A 1 MONTH WAITING PERIOD AFTER CONSTRUCTING THE EMBANKMENT, END BENT AND REINFORCED BRIDGE APPROACH FILL, IF APPLICABLE, BEFORE BEGINNING APPROACH SLAB CONSTRUCTION AT END BENT 1. FOR BRIDGE WAITING PERIODS, SEE ROADWAY PLANS AND SPECIAL PROVISIONS.

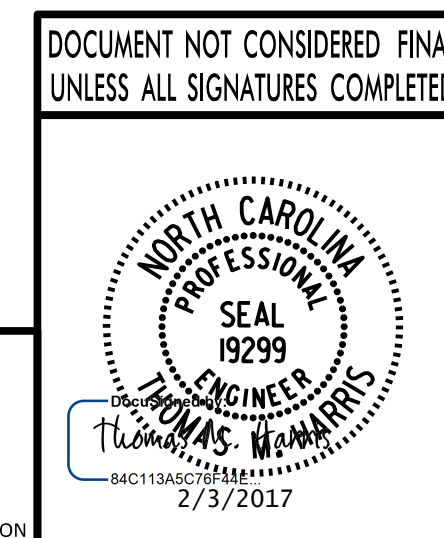
PROJECT NO. R-2707C
CLEVELAND COUNTY
 STATION: 611+32.01 -L-

SHEET 2 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GENERAL DRAWING
**BRIDGE OVER NC 150 (-Y14-) ON
 SHELBY BYPASS (-L-) BETWEEN
 NC 180 AND US 74 BUSINESS
 (LEFT LANE)**

REVISIONS						SHEET No.
No.	BY:	DATE:	No.	BY:	DATE:	TOTAL SHEETS
1			3			25
2			4			



PLANS PREPARED BY :
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 FOR NORTH CAROLINA DEPARTMENT OF TRANSPORTATION

DRAWN BY : K. E. LOFTON DATE : 6-16
 CHECKED BY : T. M. HARRIS DATE : 10-16
 DESIGN ENGINEER : T. M. HARRIS DATE : 10-16