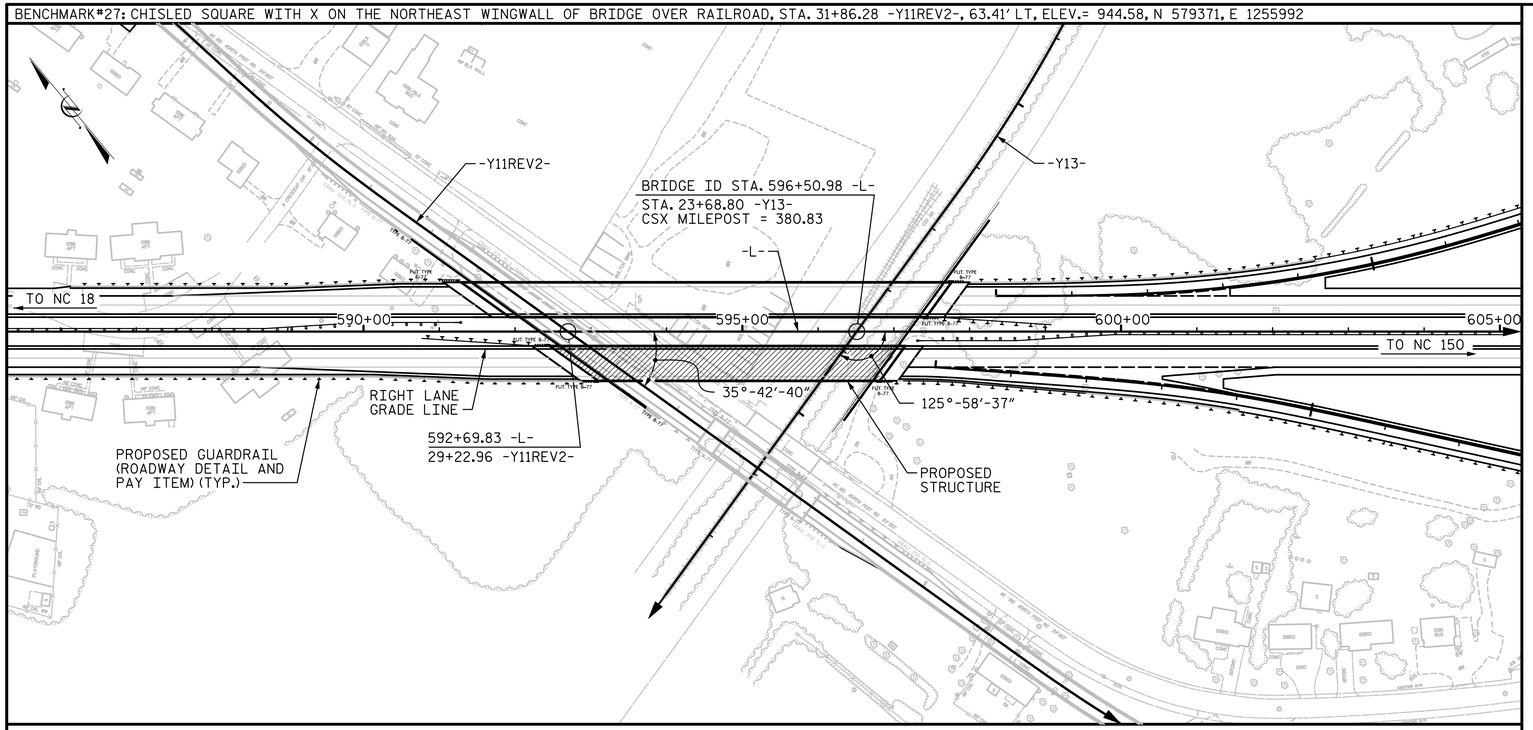


LOCATION SKETCH

FOR UTILITY INFORMATION, SEE UTILITY PLANS AND SPECIAL PROVISIONS.

GENERAL NOTES:

ASSUMED LIVE LOAD = HL-93 OR ALTERNATE LOADING.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.

THIS BRIDGE IS LOCATED IN SEISMIC ZONE 1.

REMOVABLE FORMS MAY BE USED IN LIEU OF METAL STAY-IN-PLACE FORMS IN ACCORDANCE WITH ARTICLE 420-3 OF THE STANDARD SPECIFICATIONS.

NEEDLE BEAMS WILL NOT BE ALLOWED UNLESS OTHERWISE CALLED FOR ON THE PLANS OR APPROVED BY THE ENGINEER.

FOR EROSION CONTROL MEASURES, SEE EROSION CONTROL PLANS.

FOR FOUNDATION NOTES, SEE "FOUNDATION LAYOUT" SHEET.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

THE ELEVATION AND CLEARANCE SHOWN ON THE PLANS AT THE POINT OF MINIMUM VERTICAL CLEARANCE ARE FROM THE BEST INFORMATION AVAILABLE. PRIOR TO BEGINNING BRIDGE CONSTRUCTION, VERIFY THE ELEVATION ON THE EXISTING PAVEMENT AND CHECK THE CLEARANCE. REPORT ANY VARIATIONS TO THE ENGINEER. ANY PLAN REVISIONS NECESSARY TO ACHIEVE THE REQUIRED MINIMUM VERTICAL CLEARANCE WILL BE PROVIDED BY THE DEPARTMENT.

THE RAILROAD TRACK TOP OF RAIL ELEVATIONS SHOWN ON THE PLANS ARE FROM THE BEST INFORMATION AVAILABLE. PRIOR TO BEGINNING BRIDGE CONSTRUCTION, VERIFY THE TOP OF RAIL ELEVATIONS AND REPORT ANY VARIATIONS TO THE ENGINEER. ANY PLAN REVISIONS NECESSARY TO ACHIEVE THE REQUIRED MINIMUM VERTICAL CLEARANCE WILL BE PROVIDED BY THE DEPARTMENT.

THE CLASS AA CONCRETE IN THE BRIDGE DECK SHALL CONTAIN FLY ASH OR GROUND GRANULATED BLAST FURNACE SLAG AT THE SUBSTITUTION RATE SPECIFIED IN ARTICLE 1024-1 AND IN ACCORDANCE WITH ARTICLES 1024-5 AND 1024-6 OF THE STANDARD SPECIFICATIONS. NO PAYMENT WILL BE MADE FOR THIS SUBSTITUTION AS IT IS CONSIDERED INCIDENTAL TO THE COST OF THE REINFORCED CONCRETE DECK SLAB.

FOR LIMITS OF TEMPORARY SHORING FOR MAINTENANCE OF TRAFFIC, SEE TRAFFIC CONTROL PLANS. FOR PAY ITEM FOR TEMPORARY SHORING FOR MAINTENANCE OF TRAFFIC, SEE ROADWAY PLANS.

ANY SOIL EXCAVATED DUE TO CONSTRUCTION ACTIVITIES ON CSXT RIGHT OF WAY SHALL NOT BE REMOVED FROM THE PROPERTY. ANY EXCESS SOIL THAT IS NOT REUSED WITHIN THE CSXT RIGHT OF WAY SHALL BE TESTED BY A RAILROAD REPRESENTATIVE FOR CONTAMINATION AND DISPOSAL ACCORDINGLY AT AN APPROVED LANDFILL. CSXT WILL NOT BEAR ANY COSTS RELATED TO DISPOSAL OF SOILS GENERATED DUE TO CONSTRUCTION ACTIVITY RELATED TO THIS PROJECT.

ALL STRUCTURAL STEEL SHALL BE AASHTO M270 GRADE 50W AND PAINTED IN ACCORDANCE WITH SYSTEM 4 OF ARTICLE 442-8 OF THE STANDARD SPECIFICATIONS UNLESS OTHERWISE NOTED ON THE PLANS.

FOR PLACING LOAD ON STRUCTURE MEMBERS, SEE SPECIAL PROVISIONS.

FOR MAINTENANCE AND PROTECTION OF TRAFFIC BENEATH PROPOSED STRUCTURE, SEE SPECIAL PROVISIONS.

TOTAL BILL OF MATERIAL

	FOUNDATION EXCAVATION FOR BENT	REINFORCED CONCRETE DECK SLAB	GROOVING BRIDGE FLOORS	CLASS A CONCRETE	BRIDGE APPROACH SLABS	REINFORCING STEEL	SPIRAL COLUMN REINFORCING STEEL	APPROX. 1,294,000 LBS STRUCTURAL STEEL	PILE DRIVING EQUIP. SETUP FOR HP 12x53 STEEL PILES	HP 12x53 STEEL PILES		CONCRETE BARRIER RAIL	60" CHAIN LINK FENCE	4" SLOPE PROTECTION	DISC BEARINGS	EXPANSION JOINT SEALS
	LUMP SUM	SQ. FT.	SQ. FT.	CU. YD.	LUMP SUM	LBS.	LBS.	LBS.	EA.	NO.	LIN. FT.	LIN. FT.	LIN. FT.	SQ. YD.	LUMP SUM	LUMP SUM
SUPERSTRUCTURE		20,563	19,977		LUMP SUM			LUMP SUM				835.0	428.0		LUMP SUM	LUMP SUM
END BENT 1				108.5		13,999			13	13	915			24		
BENT 1	LUMP SUM			136.5		20,878	1,496		48	48	4,048					
END BENT 2				79.6		10,383			13	13	685			17		
TOTAL	LUMP SUM	20,563	19,977	324.6	LUMP SUM	45,260	1,496	LUMP SUM	74	74	5,648	835.0	428.0	41	LUMP SUM	LUMP SUM

NOTES:

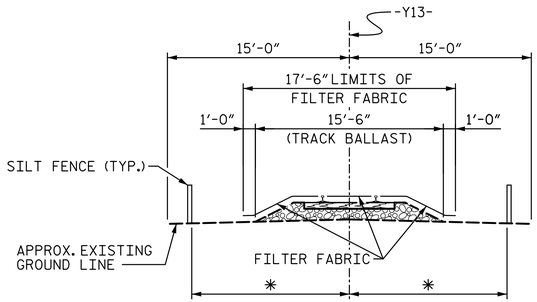
RAILROAD EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO PERFORMING ANY WORK IN THE RAILROAD RIGHT-OF-WAY.

ADDITIONAL EROSION CONTROL MEASURES FOR PROTECTION OF RAILROAD DITCHES MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.

NO SEPARATE PAYMENT WILL BE MADE FOR RAILROAD EROSION CONTROL MEASURES.

LIMITS OF SILT FENCE AND FILTER FABRIC PARALLEL TO RAILROAD SHALL EXTEND A MINIMUM OF 25'-0" OUTSIDE EDGE OF SUPERSTRUCTURE OR TOE OF SLOPE ON CONSTRUCTION. A GREATER LENGTH OF SILT FENCE OR FILTER FABRIC MAY BE REQUIRED IF SO DIRECTED BY THE ENGINEER.

FILTER FABRIC TO BE NAILED TO TIMBER RAIL TIES WITH PRIME SOURCE "GRIP CAP" OR EQUIVALENT. FILTER FABRIC ON SHOULDER TO BE SECURED AS DIRECTED BY THE ENGINEER AND RAILROAD.



RAILROAD EROSION CONTROL DETAIL

* TO BE DETERMINED BY THE RESIDENT ENGINEER IN CONSULTATION WITH THE RAILROAD ENGINEER.

DRAWN BY : MBC DATE : 10-16
CHECKED BY : AJP DATE : 10-16
DESIGN ENGINEER OF RECORD: K. BAILEY DATE : 10-16

NORTH CAROLINA PROFESSIONAL SEAL 28479 ENGINEER KEVIN G. BAILEY
DocuSigned by: Kevin Bailey 6825EECDF7B4401...
5/5/2017

NORTH CAROLINA PROFESSIONAL SEAL 40317 ENGINEER TONY R. LAWS, JR.
DocuSigned by: Tony R. Laws, JR. C40CE06F6B764F7...
5/5/2017

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

STV 100 Years
STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

PROJECT NO. R-2707C
CLEVELAND COUNTY
STATION: 596+50.98 -L-
23+68.80 -Y13-
SHEET 3 OF 3

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

GENERAL DRAWING

LOCATION SKETCH, GENERAL NOTES AND TOTAL BILL OF MATERIAL

(SITE 6R)

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S8-3
1			3			TOTAL SHEETS
2			4			44