

PIPE DETAIL
 TOP OF FLOOR DRAINS TO BE SET 3/8" BELOW SURFACE OF SLAB.
 4 - 1/2" SQUARE LUGS TO BE GLUED TO THE P.V.C. PLASTIC PIPE AT EQUAL SPACES AROUND THE PIPE DRAIN APPROXIMATELY 4" FROM THE TOP OF THE PIPE.
 THE 6" Ø PVC PLASTIC PIPE AND FITTINGS SHALL BE SCHEDULE 40 AND CONFORM TO ASTM D1785.

NOTES

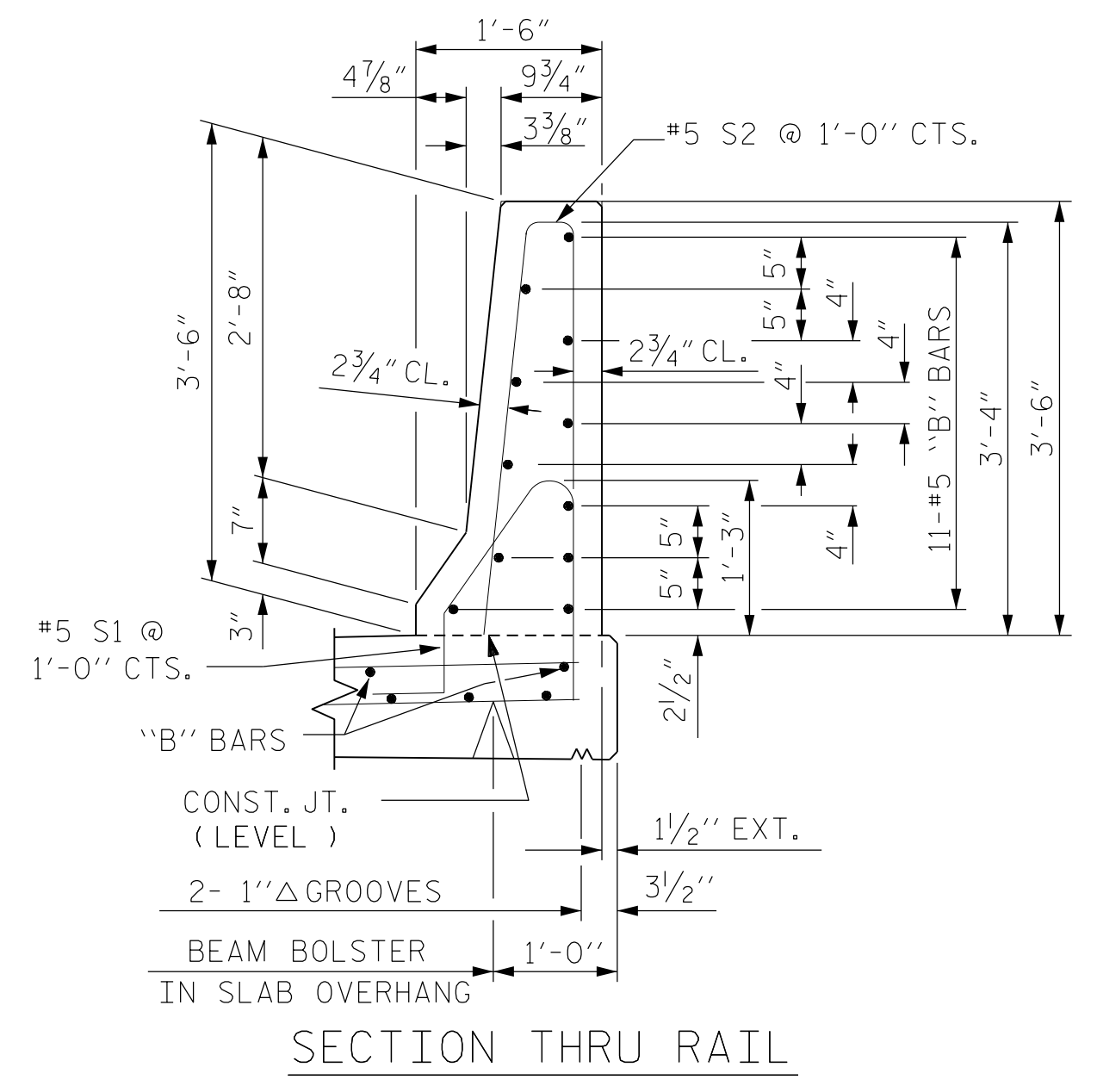
THE BARRIER RAIL IN EACH SPAN SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE IN THAT SPAN HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

WHEN FOAM JOINT SEAL IS REQUIRED, THE JOINT IN THE DECK SHALL BE SAWED PRIOR TO THE CASTING OF BARRIER RAIL.

ALL REINFORCING STEEL IN BARRIER RAILS SHALL BE EPOXY COATED.

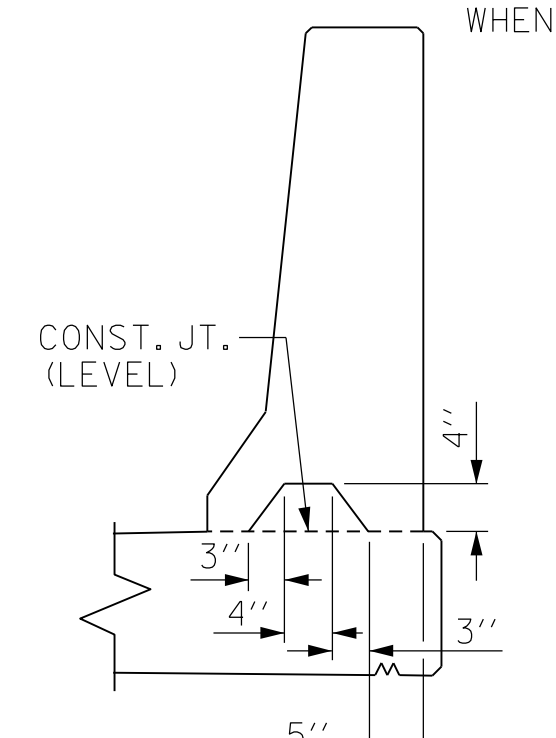
THE #5 S3, S4, S5 AND S6 BARS SHALL BE INSTALLED, USING AN ADHESIVE ANCHORING SYSTEM, AFTER SAWING THE JOINT. THE YIELD LOAD FOR THE #5 S3, S4, S5 AND S6 BARS IS 18.6 KIPS. FIELD TESTING FOR THE ADHESIVE BONDING SYSTEM IS NOT REQUIRED.

GROOVED CONTRACTION JOINTS, 1/2" IN DEPTH, SHALL BE TOOLED IN ALL EXPOSED FACES OF THE BARRIER RAIL AND IN ACCORDANCE WITH ARTICLE 825-10(B) OF THE STANDARD SPECIFICATIONS, THE CONTRACTION JOINT SHALL BE LOCATED AT EACH THIRD POINT BETWEEN BARRIER RAIL EXPANSION JOINTS. ONLY ONE CONTRACTION JOINT IS REQUIRED AT MIDPOINT OF BARRIER RAIL SEGMENTS LESS THAN 20 FEET IN LENGTH AND NO CONTRACTION JOINTS ARE REQUIRED FOR THOSE SEGMENTS LESS THAN 10 FEET IN LENGTH.

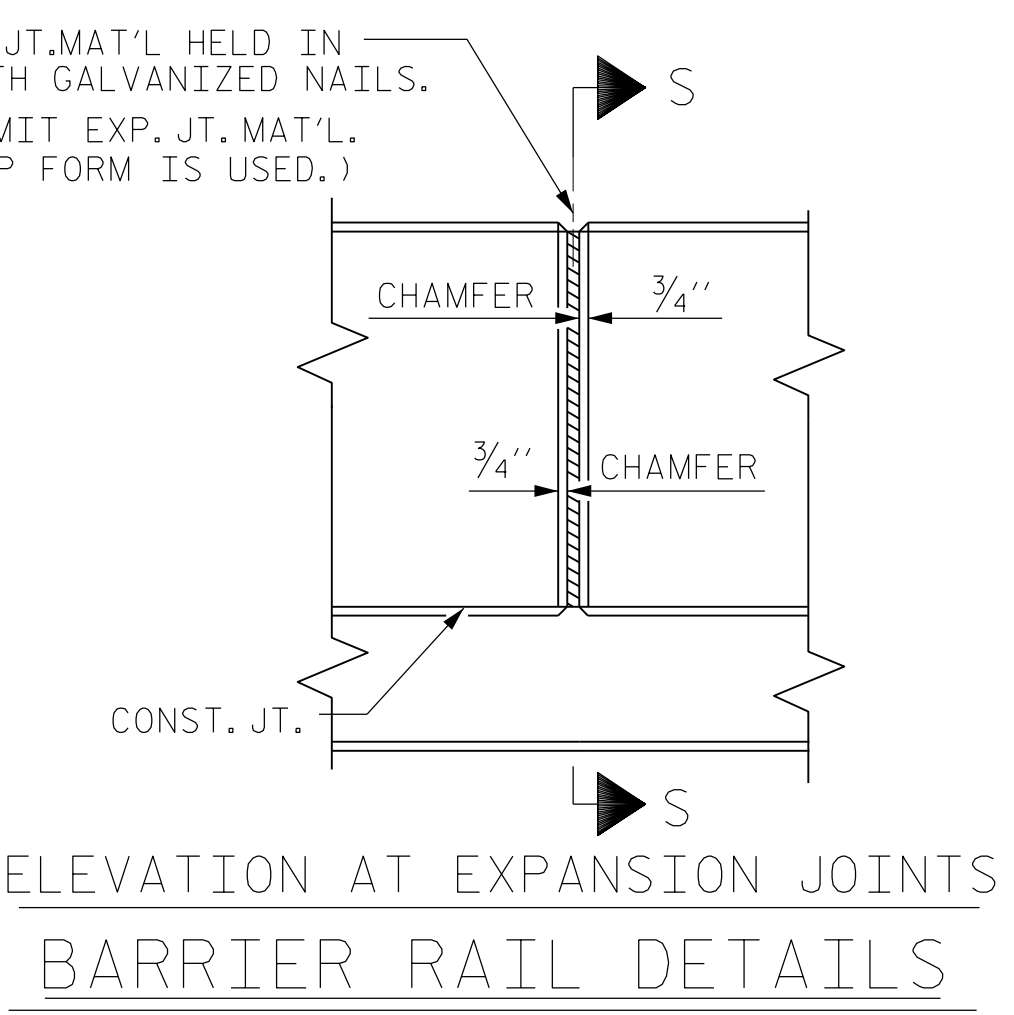


SECTION THRU RAIL

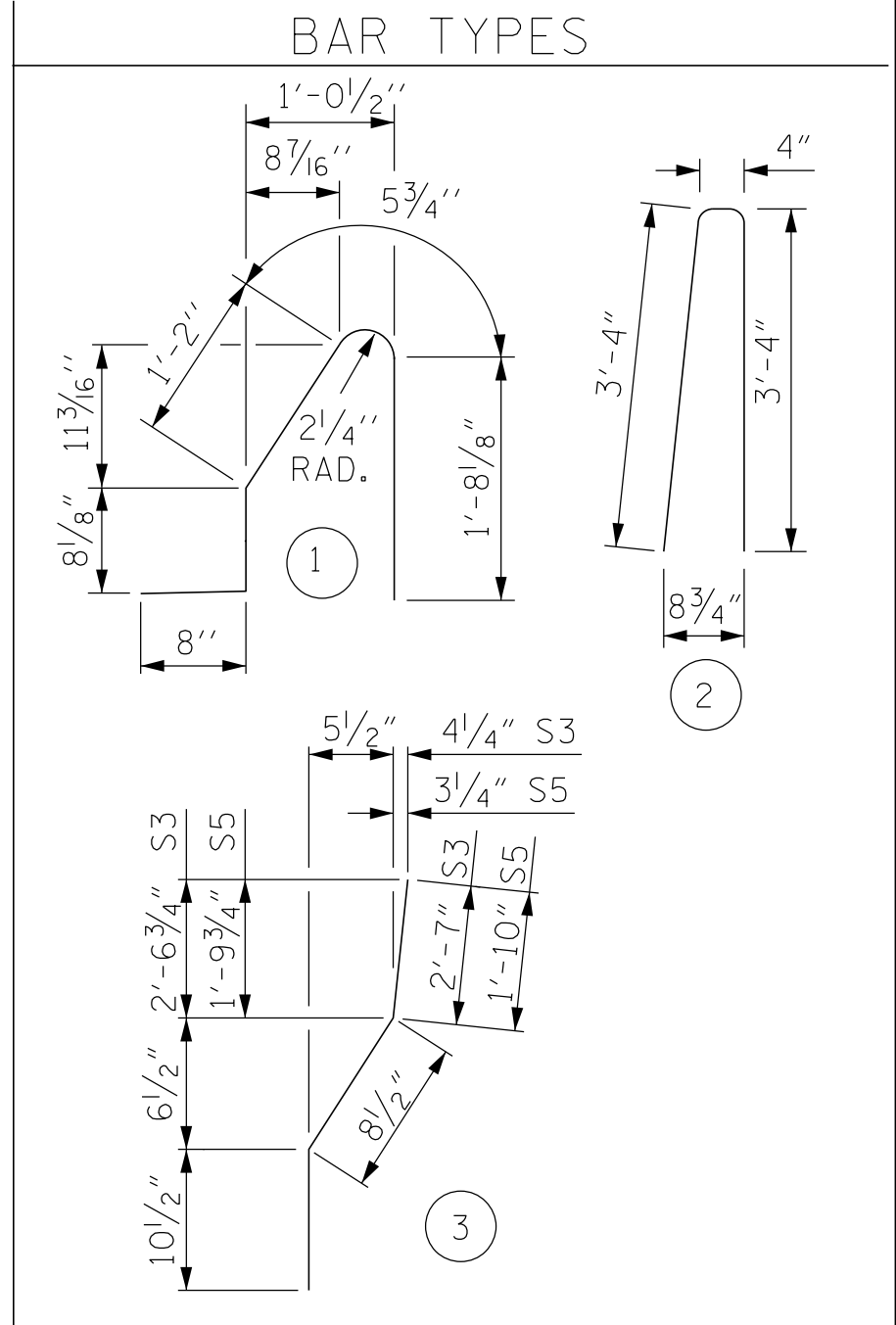
1/2" EXP. JT. MAT'L HELD IN PLACE WITH GALVANIZED NAILS.
 (NOTE: OMIT EXP. JT. MAT'L WHEN SLIP FORM IS USED.)



SECTION S-S
 AT DAM IN OPEN JOINT
 (THIS IS TO BE USED ONLY WHEN SLIP FORM IS USED)



ELEVATION AT EXPANSION JOINTS
 BARRIER RAIL DETAILS



ALL BAR DIMENSIONS ARE OUT TO OUT

BILL OF MATERIAL
 FOR CONCRETE BARRIER RAIL ONLY

BAR NO.	SIZE	TYPE	LENGTH	WEIGHT
* B1	176	#5	STR 26'-7"	4880
* B2	44	#5	STR 10'-0"	459
* B3	44	#5	STR 14'-3"	654
* S1	496	#5	1 4'-8"	2414
* S2	496	#5	2 7'-0"	3621
* S3	4	#5	3 4'-2"	17
* S4	4	#5	STR 4'-0"	17
* S5	8	#5	3 3'-5"	29
* S6	8	#5	STR 3'-3"	27

* EPOXY COATED REINFORCING STEEL	12,118 LBS.
CLASS AA CONCRETE	68.7 CU. YDS.
CONCRETE BARRIER RAIL	507.71 LIN. FT.

PROJECT NO. R-2707C
 CLEVELAND COUNTY
 STATION: 24+05.50-Y4-REV POT



DocuSigned by:
 Jeff Loftus 2/3/2017

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REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S5-16
1			3			TOTAL SHEETS
2			4			29

STR. #5 STD. NO. CBR1 (SHT 2)

DRAWN BY: H.ASSFOURA DATE: 02/16
 CHECKED BY: J.LOFTUS DATE: 11/16
 DESIGN ENGINEER OF RECORD: J.LOFTUS DATE: 01/17

R 2707C.5
 2/3/2017
 \\405-031-R2707C-SMU-CBR01-S5-16.dgn
 USER: jloftus