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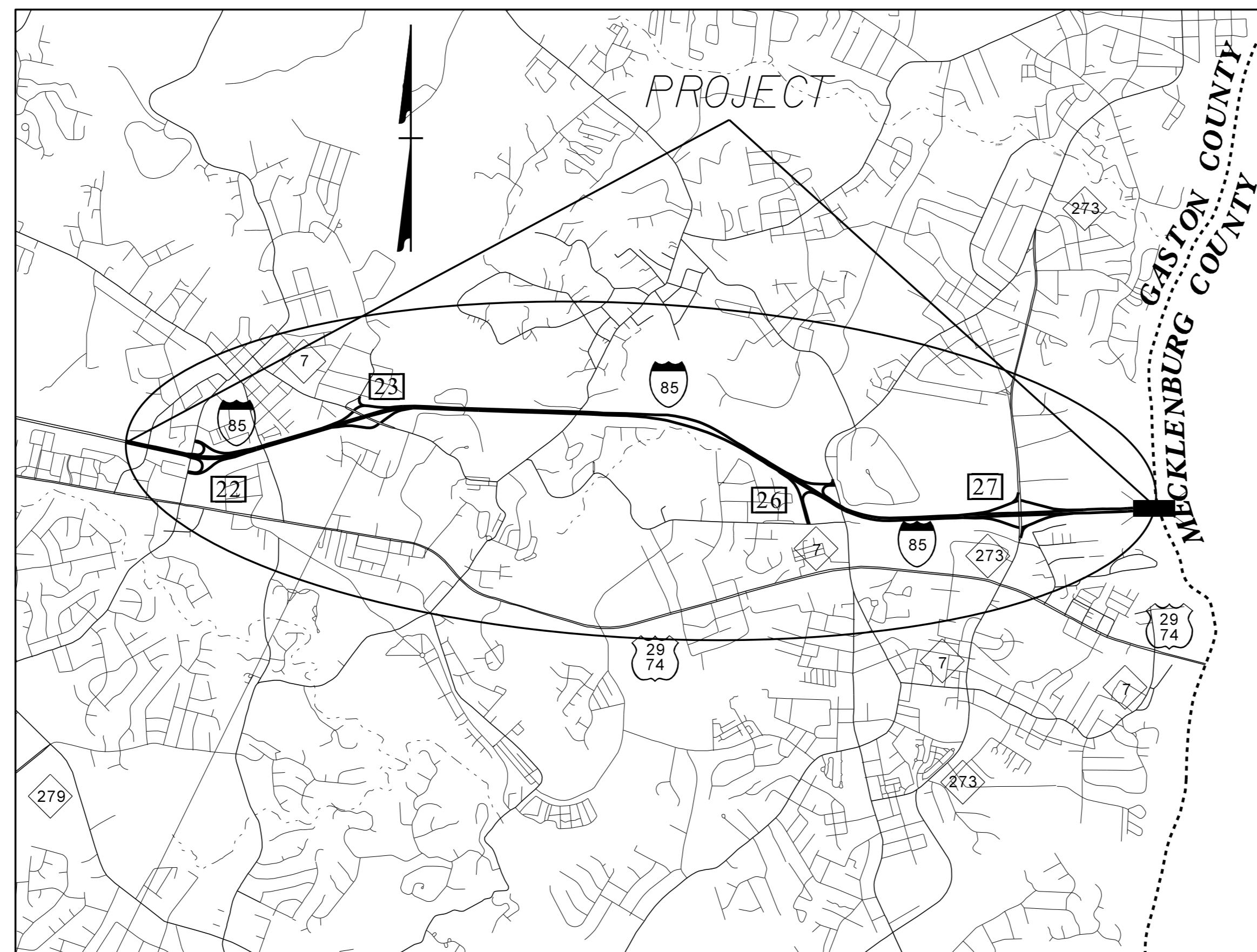
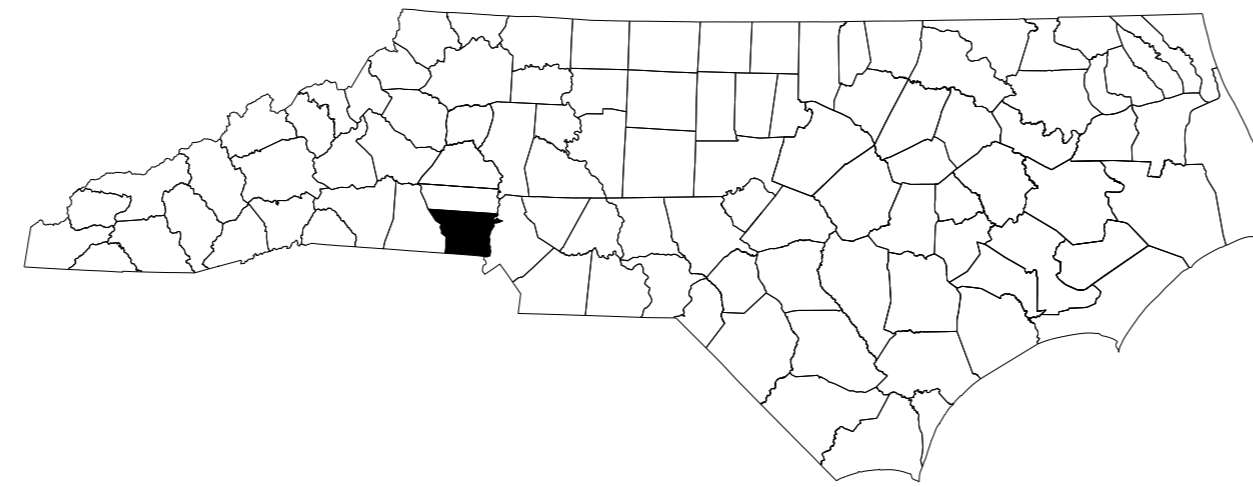
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STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

GASTON COUNTY



VICINITY MAP

INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-1B	TRANSPORTATION OPERATIONS PLAN: (MANAGEMENT STRATEGIES, GENERAL NOTES, AND LOCAL NOTES)
TMP-2-9	DETOURS FOR RAMP AND LOOP CLOSURES
TMP-10	STATIONARY ADVANCE WARNING SIGNS
TMP-11	WORK ZONE VARIABLE SPEED LIMIT DIGITAL SIGNS
TMP-12	WORK ZONE PRESENCE LIGHTING

SHEET NO.
TMP-1

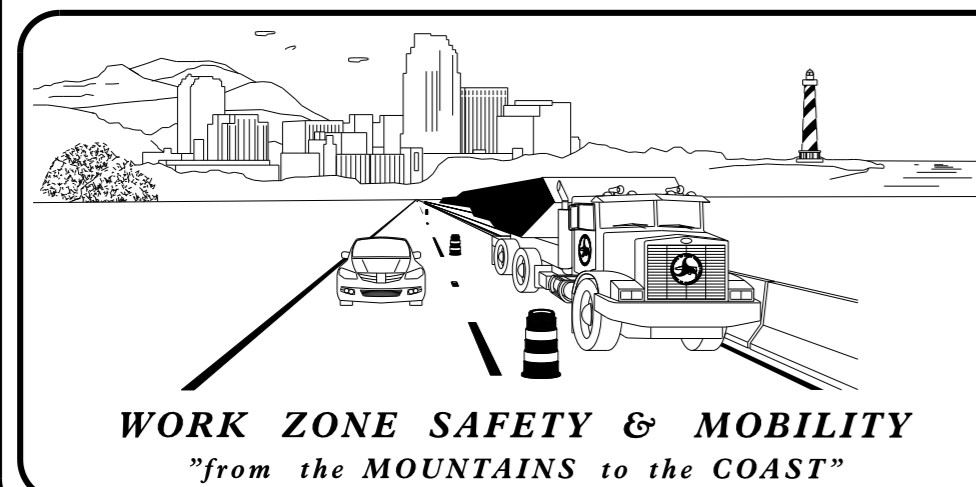
I-5894

TIP PROJECT:

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED

APPROVED: Bryan Sowell
DATE: 1/31/2017

SEAL



N.C.D.O.T. WORK ZONE TRAFFIC CONTROL
1561 MAIL SERVICE CENTER (MSC) RALEIGH, NC 27699-1561
750 N. GREENFIELD PARKWAY, GARNER, NC 27529 (DELIVERY)
PHONE: (919) 773-2800 FAX: (919) 771-2745

J. S. BOURNE, P.E. STATE TRAFFIC MANAGEMENT ENGINEER

TRAFFIC CONTROL PROJECT ENGINEER

TRAFFIC CONTROL PROJECT DESIGN ENGINEER

TRAFFIC CONTROL DESIGN ENGINEER



31-JAN-2017 13:19 S:\DDC\DISTRICT\Gaston\I-5894\I-5894_01.dgn to County\I-5894\I-5894_01.dgn

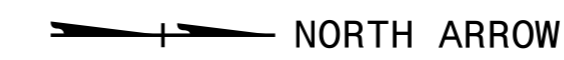
ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

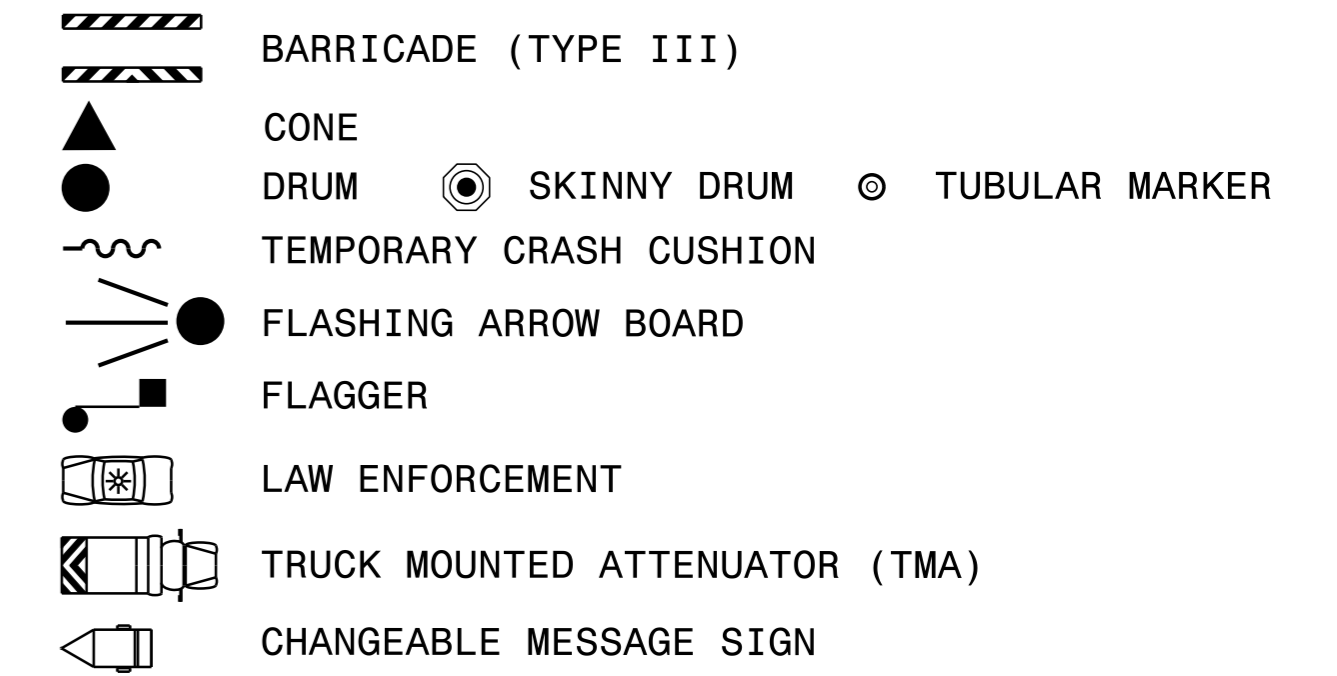
<u>STD. NO.</u>	<u>TITLE</u>
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO-LANE AND MULTI-LANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMPS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

LEGEND

GENERAL



TRAFFIC CONTROL DEVICES



04-NOV-2016 11:01 S:\DDC\DISTRICT\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-TC-TMP-1A.dgn \$\$\$USERNAME\$\$\$

APPROVED: DATE: 11/4/2016 <div style="text-align: center;">SEAL</div>			<h3>ROADWAY STANDARD DRAWINGS & LEGEND</h3>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED			

GENERAL NOTES / LOCAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
I85 (NB&SB) AND RAMPS & LOOPS	MONDAY through SUNDAY 6:00AM TO 8:00PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
I85 (NB&SB) AND RAMPS&LOOPS

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 8:00 A.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 8:00 P.M. THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 8:00 P.M. MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 8:00 P.M. TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 8:00 P.M. TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 8:00 P.M. MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.
9. FOR CAROLINA PANTHER GAMES IN CHARLOTTE, BETWEEN THE HOURS OF 6:00 A.M. THE DAY OF THE EVENT AND 8:00 P.M. THE DAY AFTER THE EVENT.
10. FOR AND NASCAR RACE EVENTS AT CHARLOTTE MOTOR SPEEDWAY BETWEEN THE HOURS 6:00 A.M. THE WEDNESDAY BEFORE THE FIRST TRACK EVENT AND 8:00 P.M. THE MONDAY AFTER THE LAST TRACK EVENT.
11. FOR THE McADENVILLE CHRISTMAS TOWN USA SEASON BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 1st, 2017 AND 8:00 P.M. DECEMBER 27th, 2017.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- C) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- G) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- H) DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON .

PAVEMENT EDGE DROP OFF REQUIREMENTS

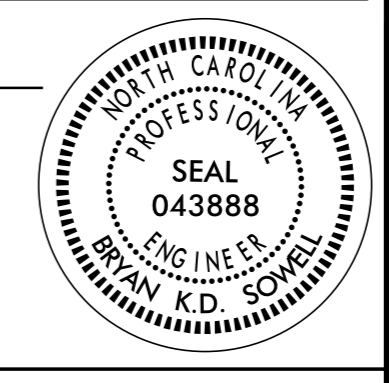
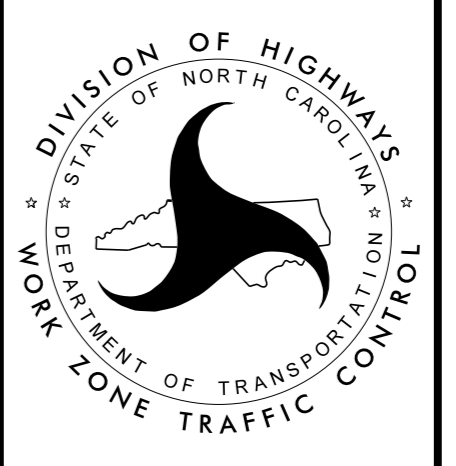
- I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

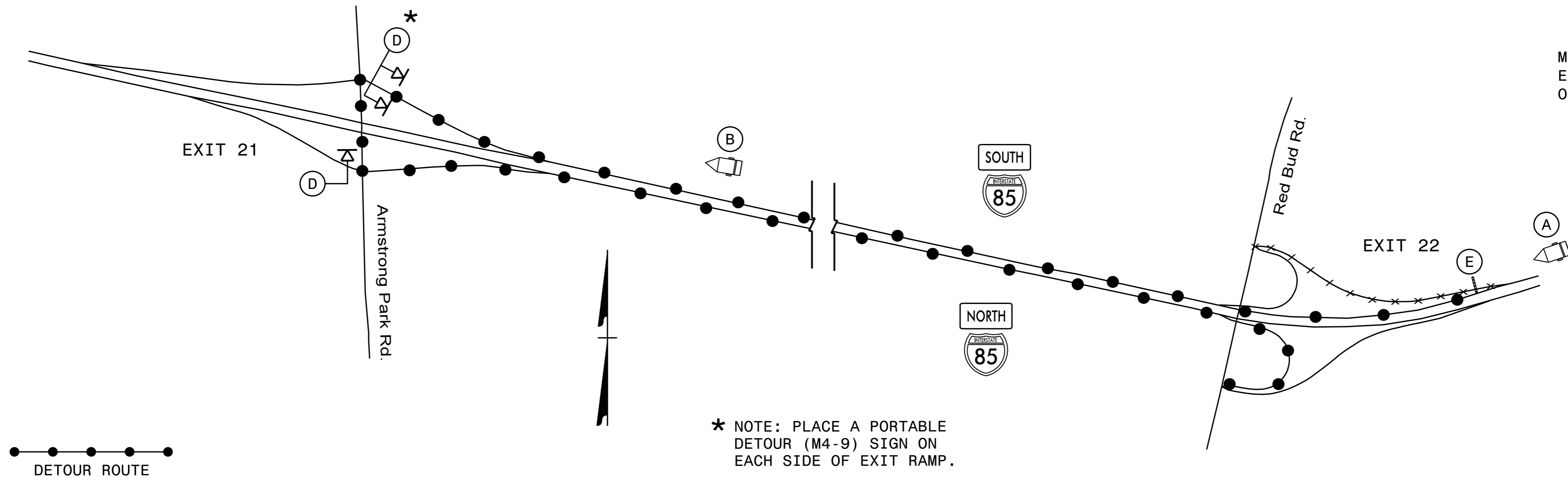
BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- J) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.
- K) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

04-JAN-2017 11:17 S:\DD\CODISTRIC\Gaston\I-5894\Interstate_Maint-Gaston-2017\I-5894\Interstate_Maintenance-2017\Gaston\Traffic Control Plans\I-5894_TC_TMP_IB.dgn \$\$\$USERNAME\$\$\$

<p>DocuSigned by: <i>Bryan Sowell</i> 79EABDC9D6E4ED1</p> <p>APPROVED: _____</p> <p>DATE: 1/4/2017</p> <p style="text-align: center;">SEAL</p>			<h3>TRANSPORTATION OPERATIONS PLAN</h3>
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>			

**DETOUR 1: CLOSURE AT I-85 S EXIT 22
EXIT RAMP TO RED BUD RD.**



* NOTE: PLACE A PORTABLE
DETOUR (M4-9) SIGN ON
EACH SIDE OF EXIT RAMP.

(A)

MESSAGE NO. 1	MESSAGE NO. 2
EXIT 22 RED BUD CLOSED	DETOUR TO EXIT 21

CHANGEABLE MESSAGE SIGN

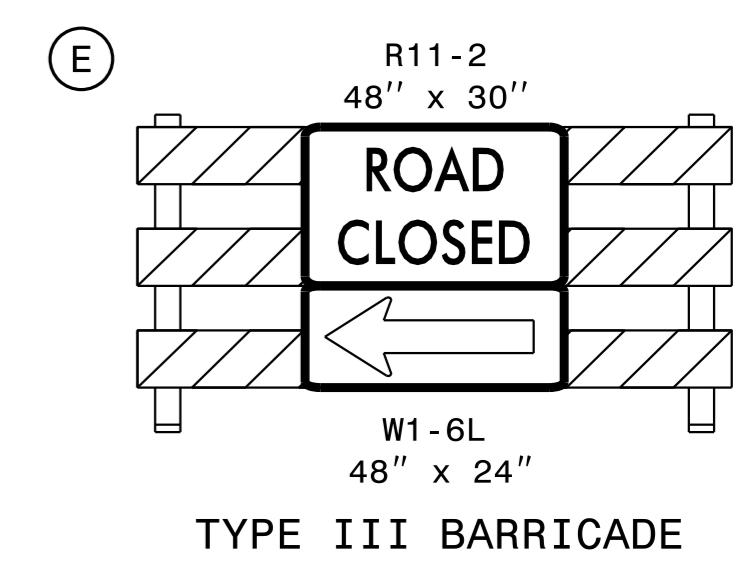
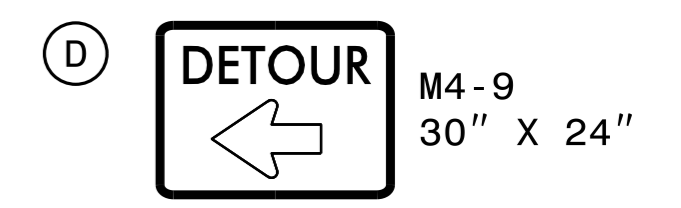
MIN. 1/2 MILE IN ADVANCE OF
EXIT 22 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

(B)

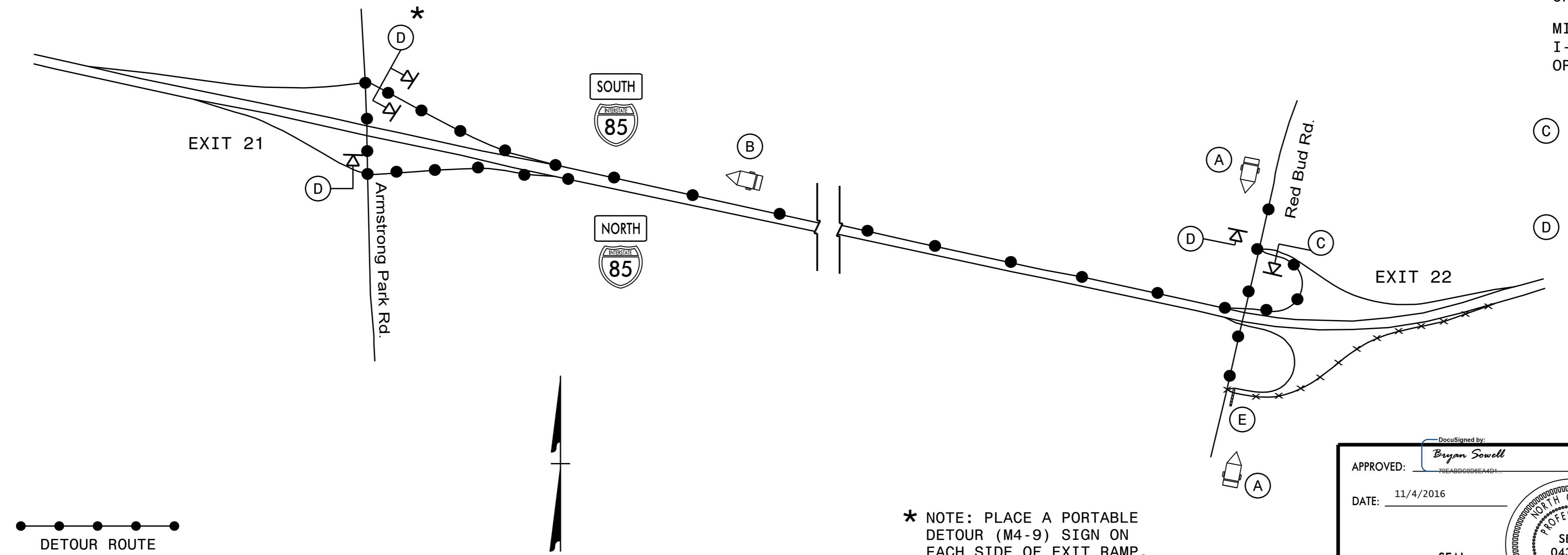
MESSAGE NO. 1	MESSAGE NO. 2
RED BUD DETOUR EXIT 21	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF
EXIT 21 DECELERATION LANE
OR AS DIRECTED BY ENGINEER



**DETOUR 2: CLOSURE AT I-85 N EXIT 22
ENTRANCE RAMP TO I-85 NORTH**



* NOTE: PLACE A PORTABLE
DETOUR (M4-9) SIGN ON
EACH SIDE OF EXIT RAMP.

(A)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N RAMP CLOSED	DETOUR USE I-85 S

CHANGEABLE MESSAGE SIGN

MIN. 350 FT IN ADVANCE OF
I-85 N ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

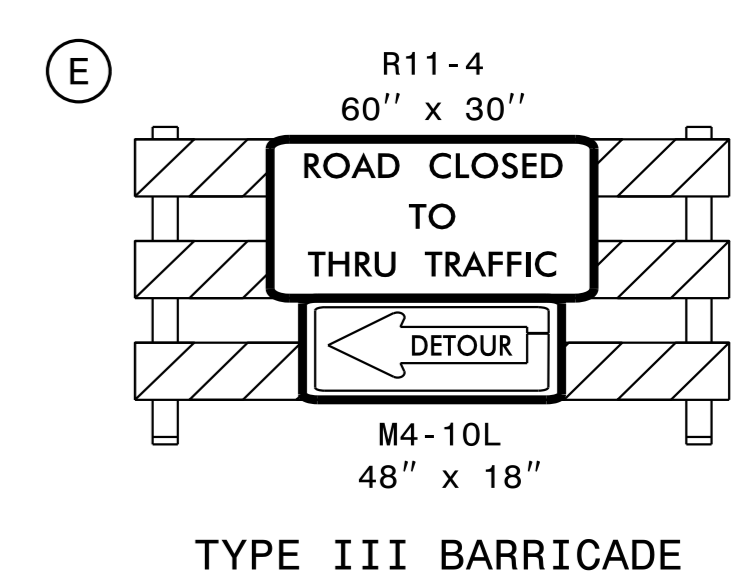
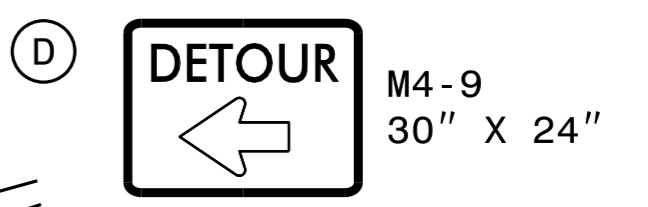
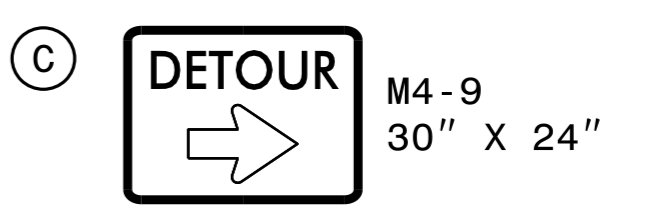
MIN. 350 FT IN ADVANCE OF
I-85 S ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

(B)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N DETOUR EXIT 21	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

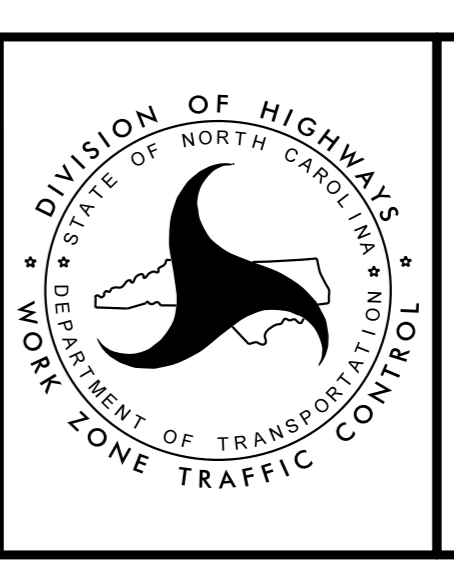
MIN. 1500 FT IN ADVANCE OF
EXIT 21 DECELERATION LANE
OR AS DIRECTED BY ENGINEER



APPROVED: *Bryan Sowell*
DATE: 11/4/2016

SEAL

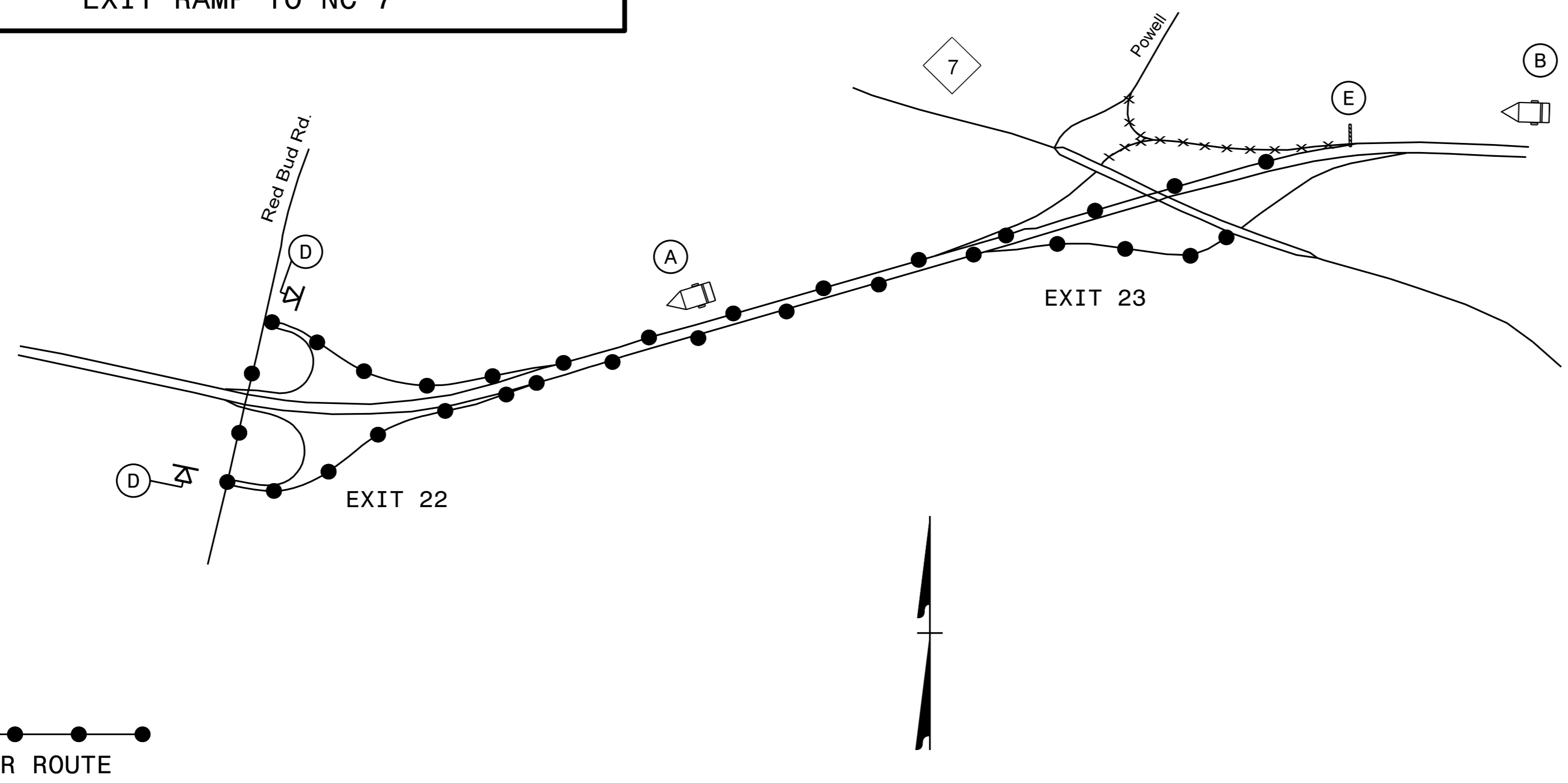
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I-85 EXIT 22
RAMP DETOURS
1 & 2

04-NOV-2016 11:04 \\ADOT\DFSR001\GROUPOS-Div2\CC\DDC\District\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-TMP-02-Exit 22 Detours I & 2.dgn
Bksowell AT DICADZ10892

**DETOUR 5: CLOSURE AT I-85 S EXIT 23
EXIT RAMP TO NC 7**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
NC 7 DETOUR EXIT 22	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

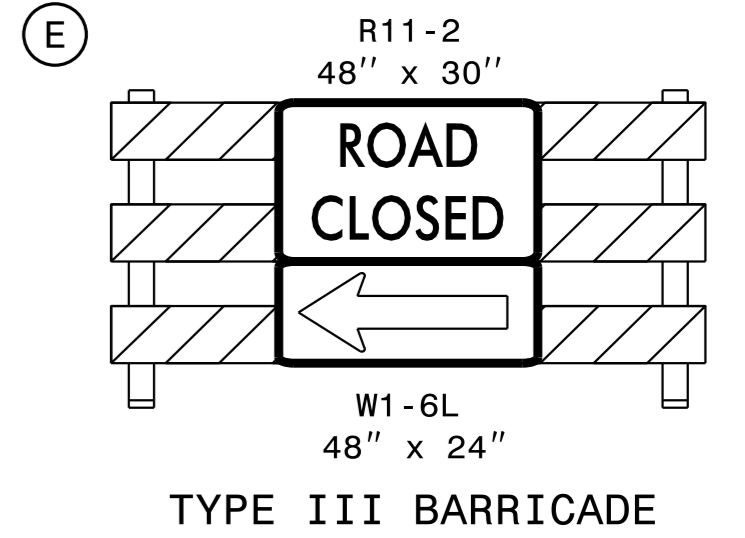
MIN. 1500 FT IN ADVANCE OF
EXIT 22 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

(B)

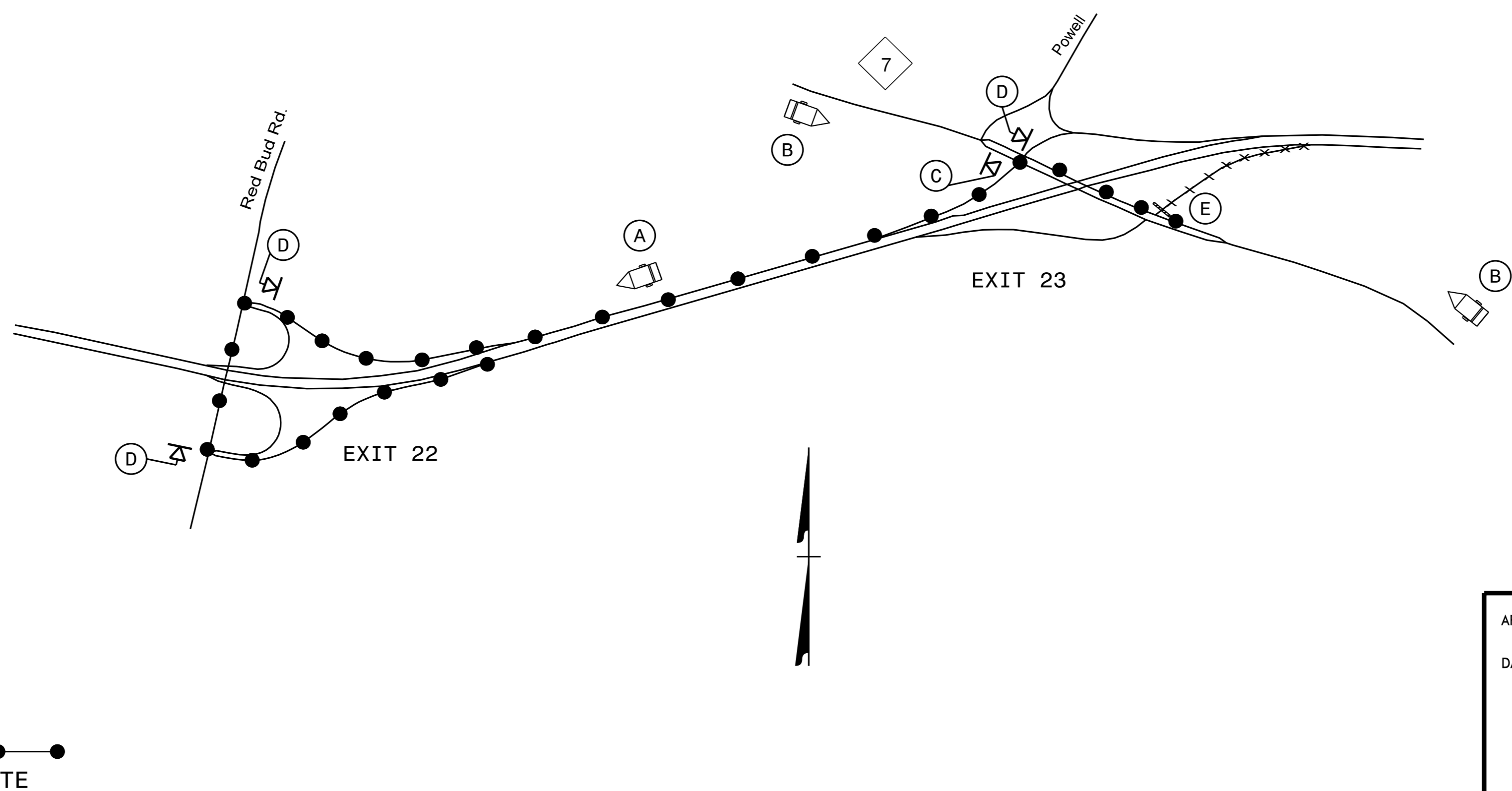
MESSAGE NO. 1	MESSAGE NO. 2
EXIT 23 NC 7 CLOSED	DETOUR TO EXIT 22

CHANGEABLE MESSAGE SIGN

MIN. 1/2 MILE IN ADVANCE OF
EXIT 23 DECELERATION LANE
OR AS DIRECTED BY ENGINEER



**DETOUR 6: CLOSURE AT I-85 N EXIT 23
ENTRANCE RAMP TO I-85 N**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N DETOUR EXIT 22	

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF
EXIT 22 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

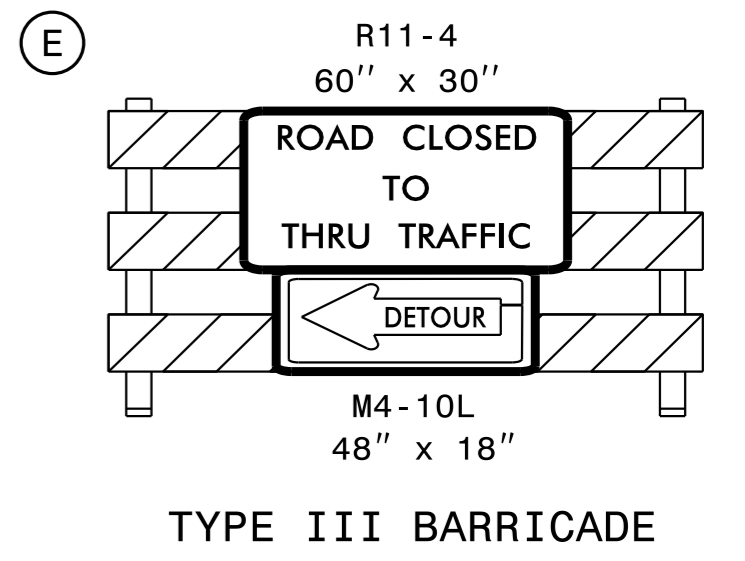
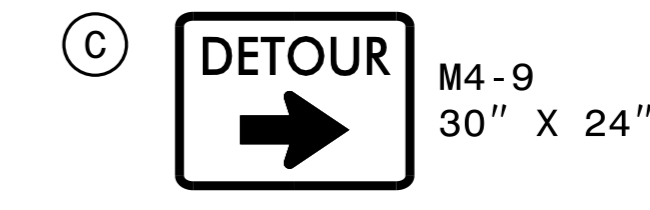
(B)

MESSAGE NO. 1	MESSAGE NO. 2
EXIT 23 RAMP CLOSED	FOLLOW DETOUR AHEAD

CHANGEABLE MESSAGE SIGN

MIN. 350 FT IN ADVANCE OF
I-85 N ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

MIN. 350 FT IN ADVANCE OF
I-85 S ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

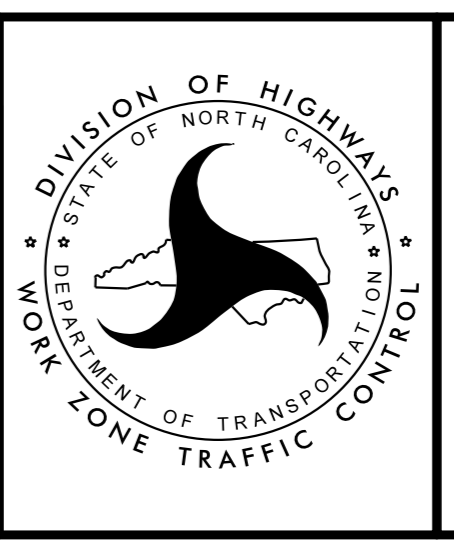


04-NOV-2016 14:07 \\ADOT\DFSR00101\GROUPS-Div\2CC\DDC\District\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-TC-TMP-04-Exit 23 Detours 5 & 6.dgn
Bksowell AT DICCADZ10892

APPROVED: *Bryan Sowell*
DATE: 11/4/2016

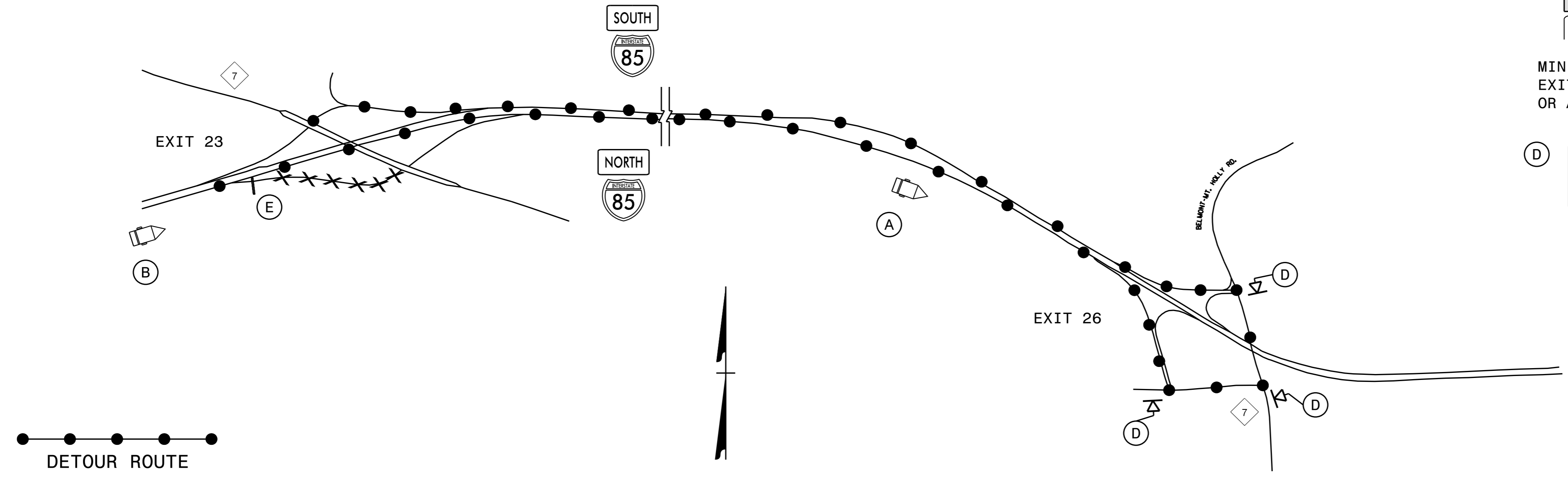
SEAL

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED



I-85 EXIT 23
RAMP DETOURS
5 & 6

**DETOUR 7: CLOSURE AT I-85 N EXIT 23
EXIT RAMP TO NC 7**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
NC 7 DETOUR EXIT 26	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

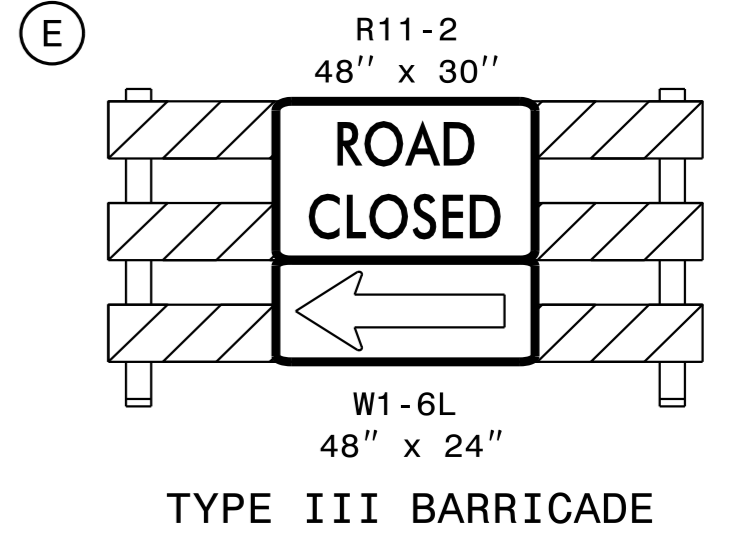
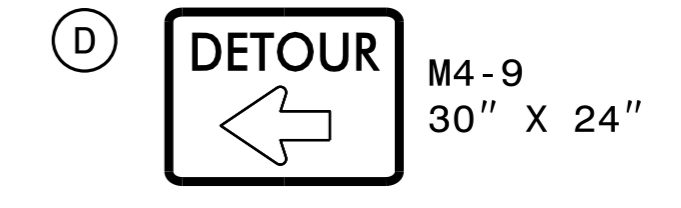
MIN. 1500 FT IN ADVANCE OF
EXIT 26 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

(B)

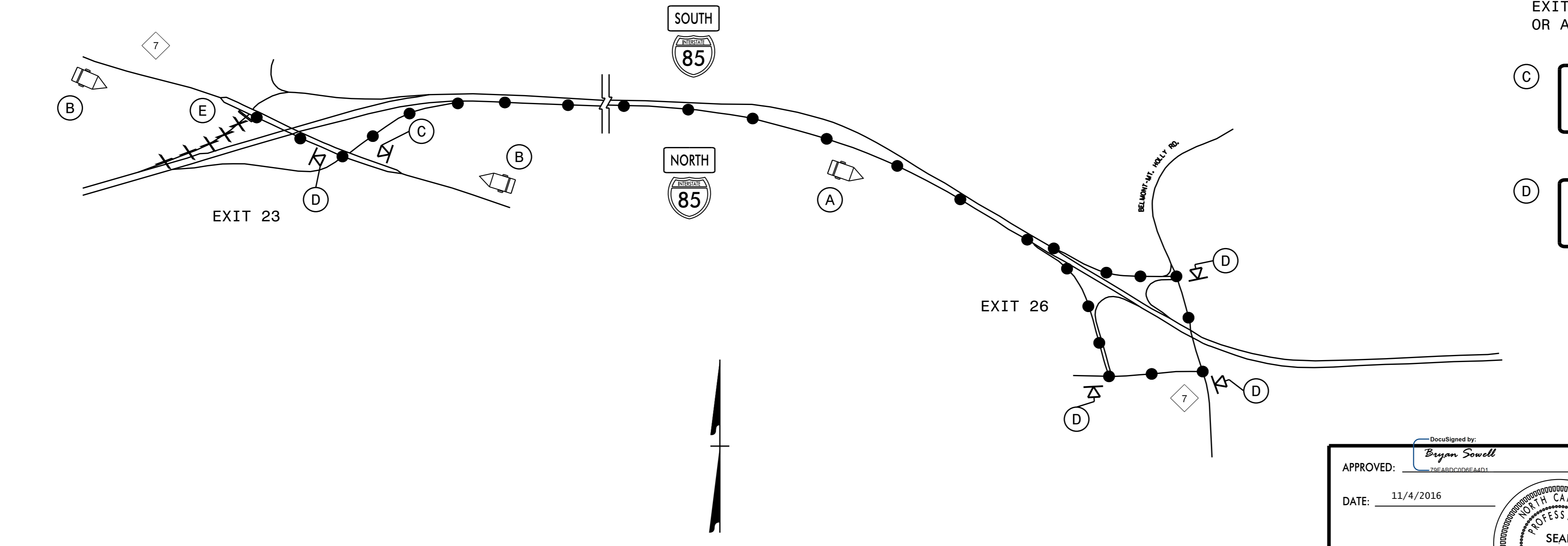
MESSAGE NO. 1	MESSAGE NO. 2
EXIT 23 NC 7 CLOSED	DETOUR TO EXIT 26

CHANGEABLE MESSAGE SIGN

MIN. 1/2 MILE IN ADVANCE OF
EXIT 23 DECELERATION LANE
OR AS DIRECTED BY ENGINEER



**DETOUR 8: CLOSURE AT I-85 S EXIT 23
ENTRANCE RAMP TO I-85 S**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 S DETOUR EXIT 26	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF
EXIT 26 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

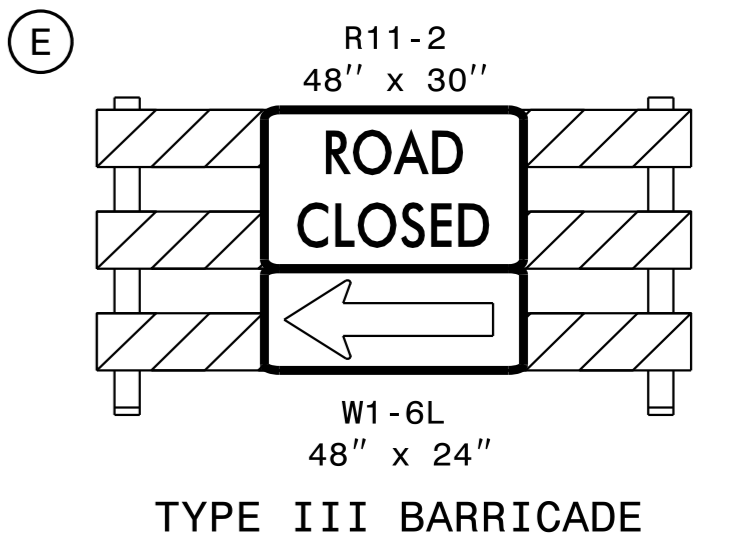
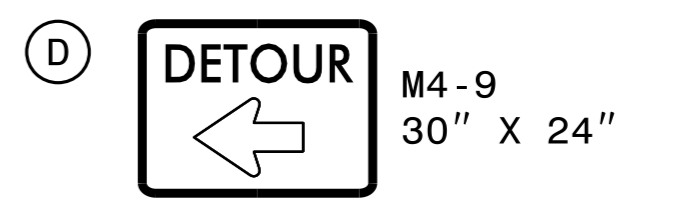
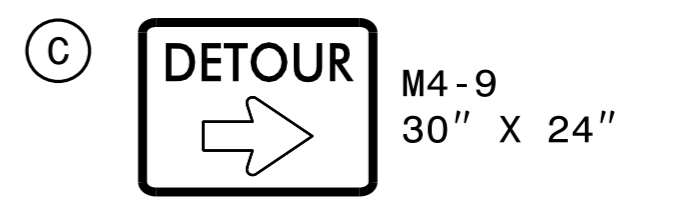
(B)

MESSAGE NO. 1	MESSAGE NO. 2
EXIT 23 RAMP CLOSED	FOLLOW DETOUR AHEAD

CHANGEABLE MESSAGE SIGN

MIN. 350 FT IN ADVANCE OF
I-85 N ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

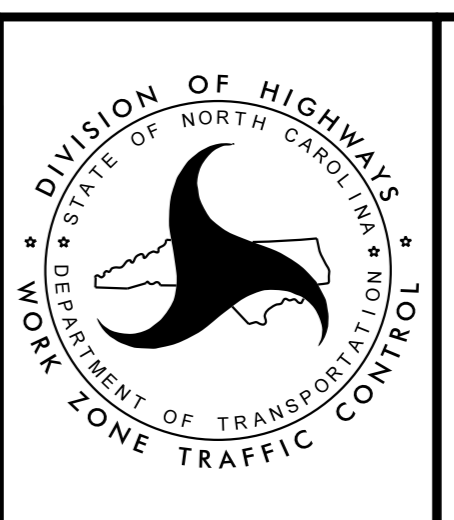
MIN. 350 FT IN ADVANCE OF
I-85 S ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER



APPROVED: *Byron Swell*
DATE: 11/4/2016

SEAL

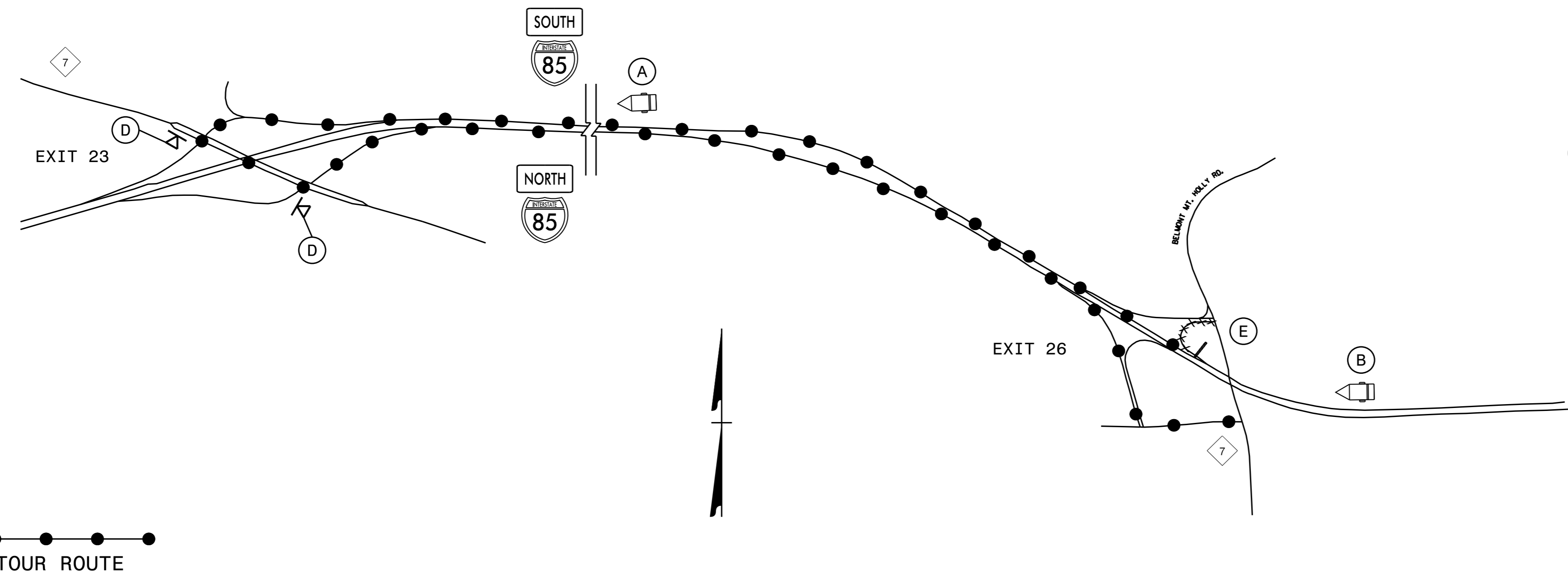
DOCUMENT NOT CONSIDERED FINAL
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I-85 EXIT 23
RAMP DETOURS
7 & 8

04-NOV-2016 14:09 \\ADOT\DFSR00101\GROUPS-Div\2CC\DDC\District\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-TC-TMP-05-Exit 23 Detours 7 & 8.dgn
Bksowell AT DIZCADZ10892

**DETOUR 9: CLOSURE AT I-85 S EXIT 26
EXIT LOOP TO BELMONT-MT. HOLLY RD.**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
BELMONT MT HOLLY RD	DETOUR EXIT 23

CHANGEABLE MESSAGE SIGN

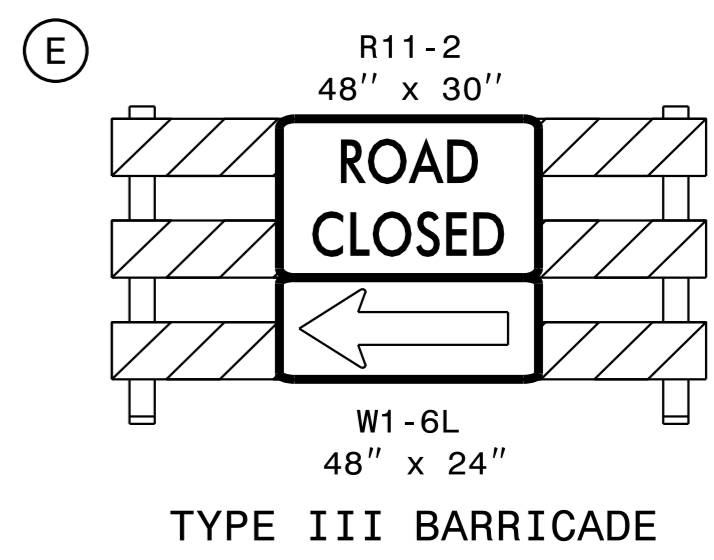
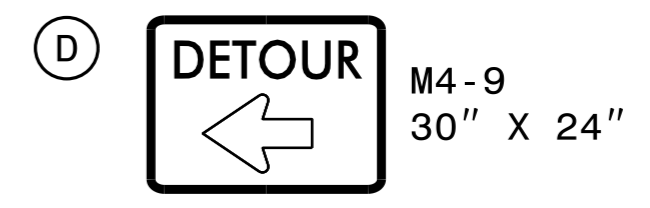
MIN. 1500 FT IN ADVANCE OF EXIT 23 DECELERATION LANE OR AS DIRECTED BY ENGINEER

(B)

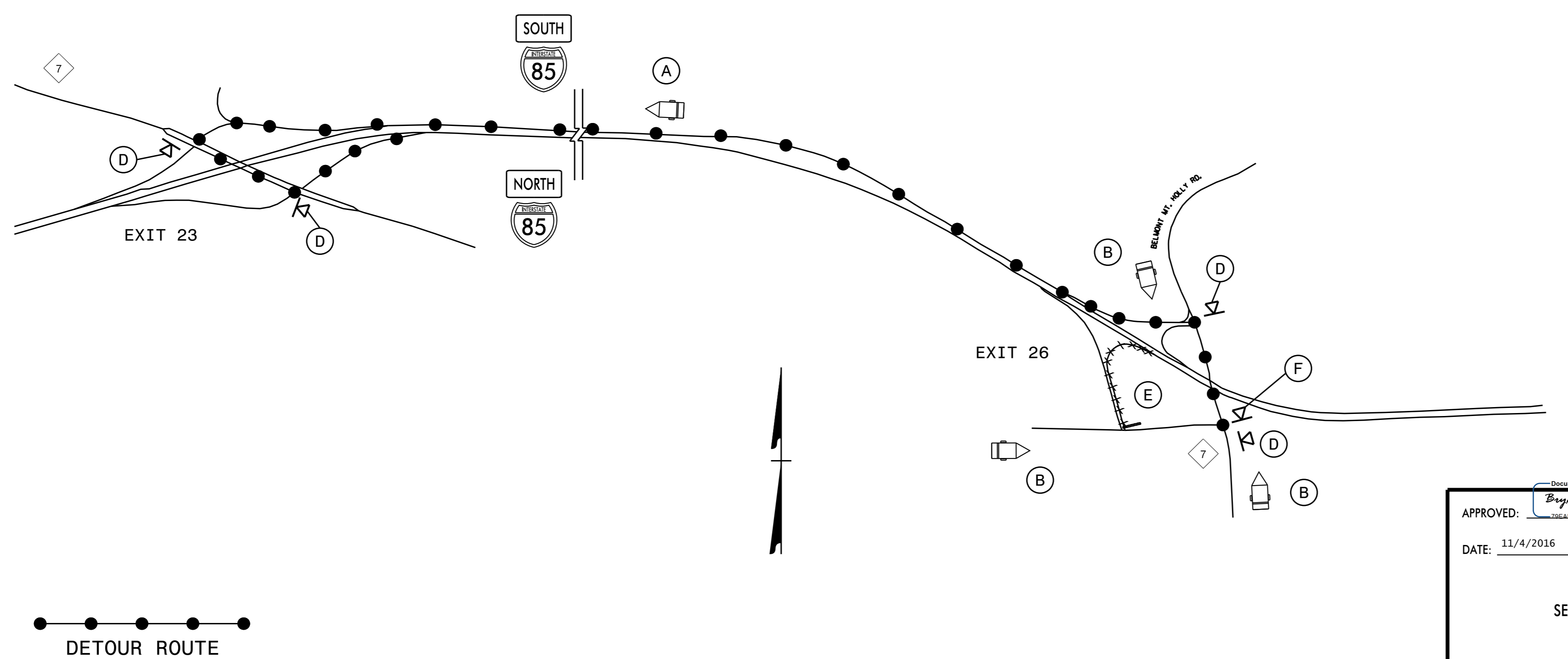
MESSAGE NO. 1	MESSAGE NO. 2
EXIT 26 BELMONT MT HOLLY	DETOUR TO EXIT 23

CHANGEABLE MESSAGE SIGN

MIN. 1/2 MILE IN ADVANCE OF EXIT 26 DECELERATION LANE OR AS DIRECTED BY ENGINEER



**DETOUR 10: CLOSURE AT I-85 N EXIT 26
ENTRANCE LOOP TO I-85 N**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N	DETOUR EXIT 23

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF EXIT 23 DECELERATION LANE OR AS DIRECTED BY ENGINEER

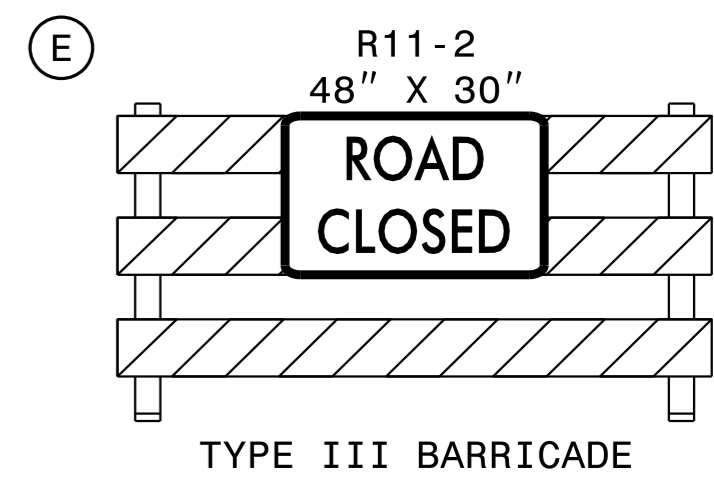
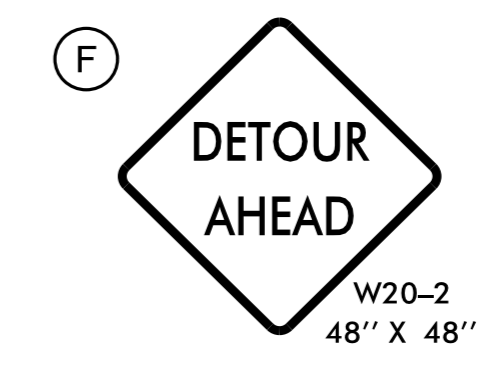
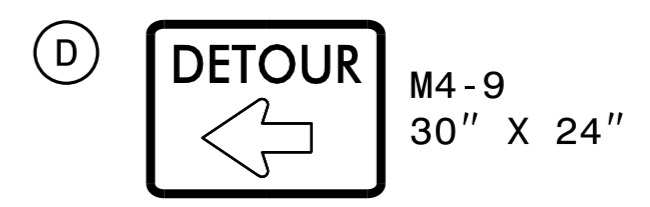
(B)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N RAMP CLOSED	FOLLOW DETOUR AHEAD

CHANGEABLE MESSAGE SIGN

MIN. 350 FT IN ADVANCE OF I-85 N ENTRANCE RAMP OR AS DIRECTED BY ENGINEER

MIN. 350 FT IN ADVANCE OF I-85 S ENTRANCE RAMP OR AS DIRECTED BY ENGINEER

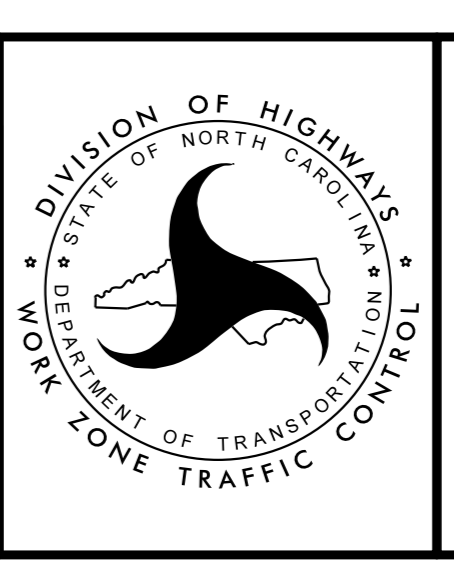


04-NOV-2016 10:51 \\ADOT\DFSR00101\GROUPS-Div2\CC\DDC\District\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-I-85-Exit 26 Detours 9 & 10.dgn
Bksowell AT: DZCADZ10892

APPROVED: *Bryan Somell*
DATE: 11/4/2016

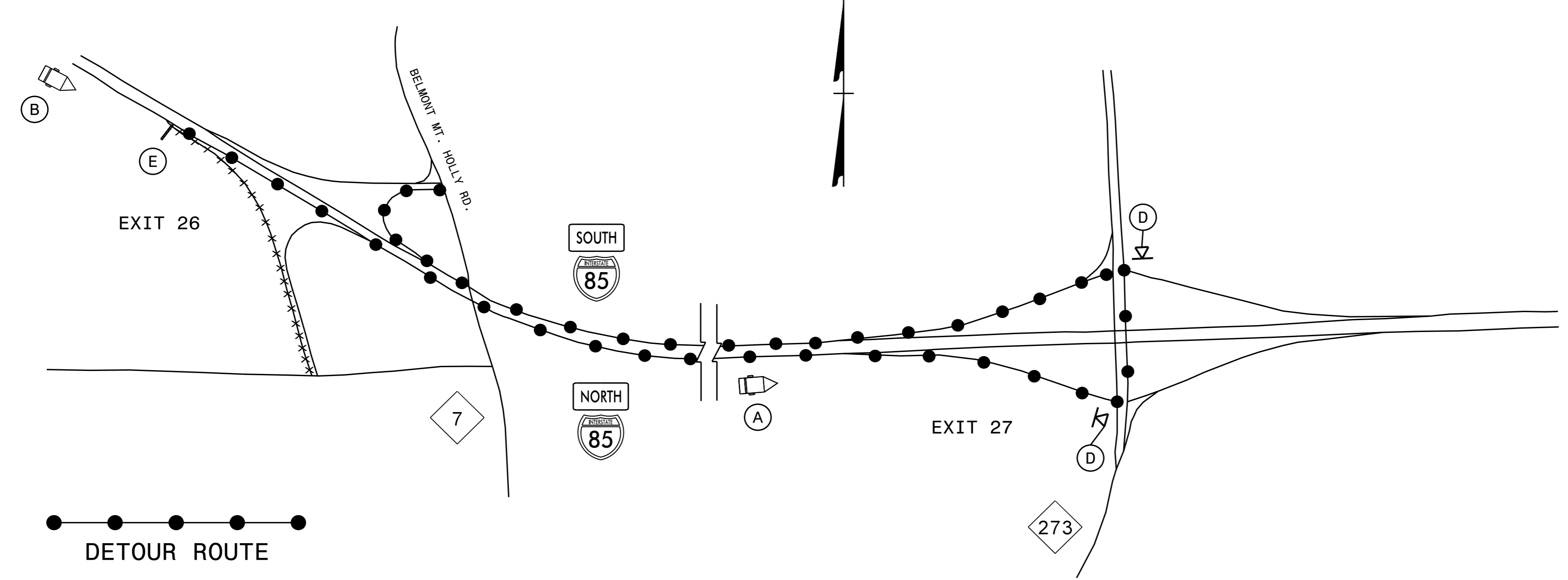
SEAL

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



I-85 EXIT 26
LOOP DETOURS
9 & 10

**DETOUR 11: CLOSURE AT I-85 N EXIT 26
EXIT RAMP TO NC 7 & BELMONT MT. HOLLY RD.**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
NC 7 DETOUR EXIT 27	

CHANGEABLE MESSAGE SIGN

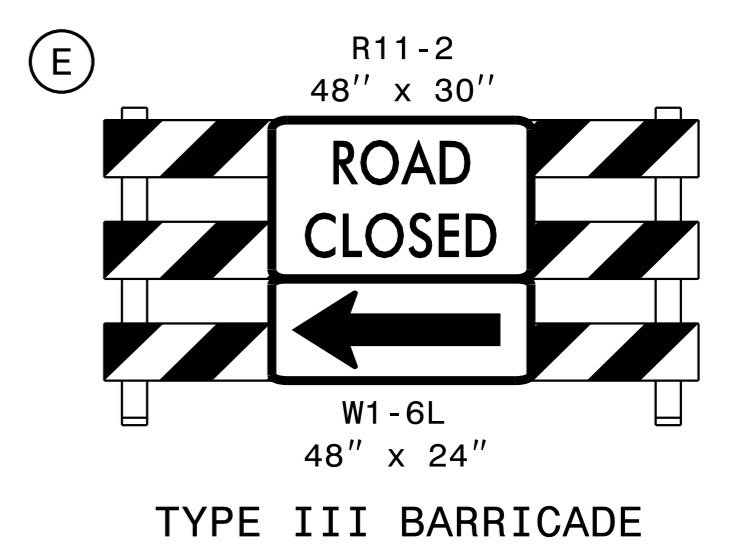
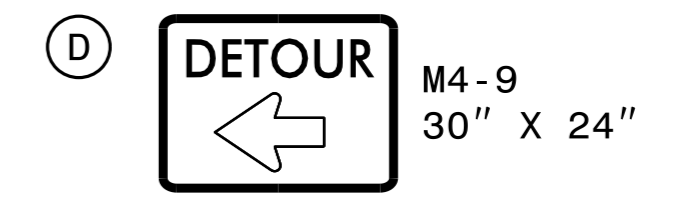
MIN. 1500 FT IN ADVANCE OF
EXIT 27 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

(B)

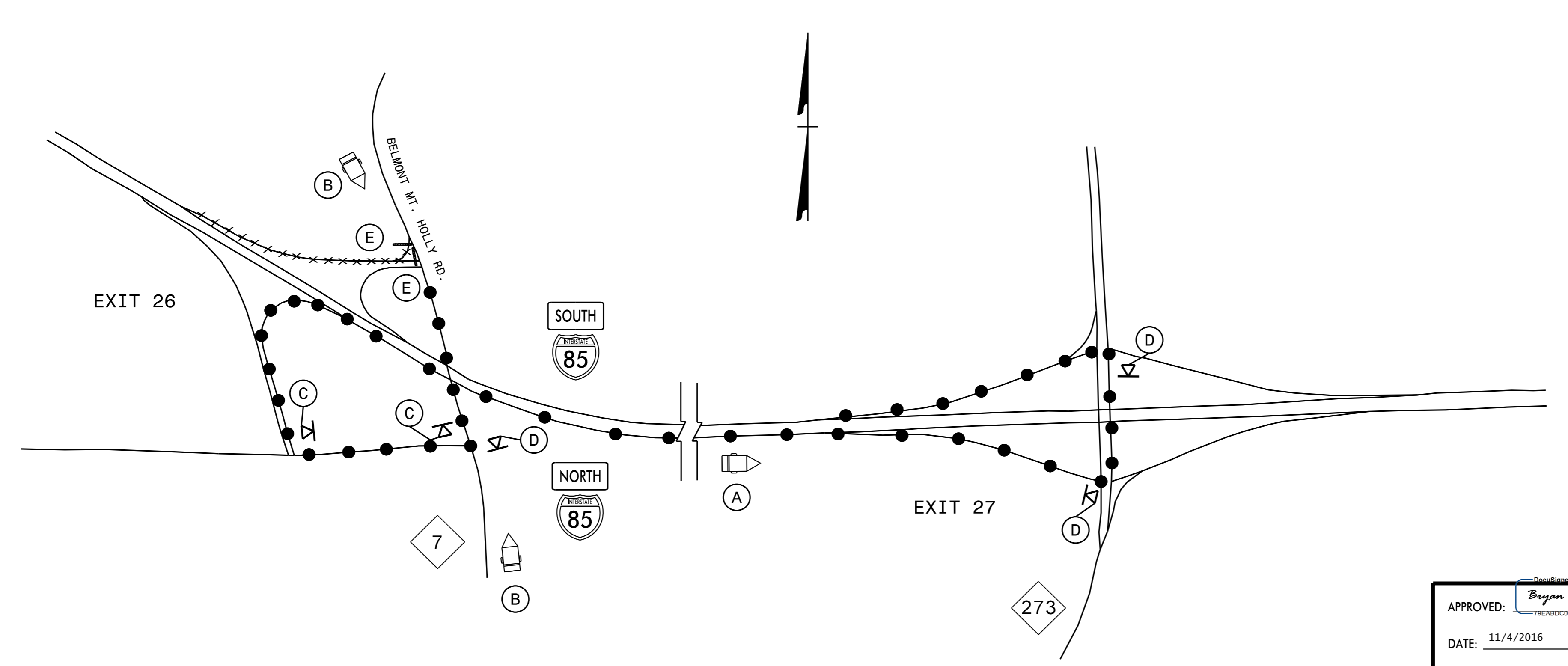
MESSAGE NO. 1	MESSAGE NO. 2
EXIT 26 NC 7 CLOSED	DETOUR TO EXIT 27

CHANGEABLE MESSAGE SIGN

MIN. 1/2 MILE IN ADVANCE OF
EXIT 26 DECELERATION LANE
OR AS DIRECTED BY ENGINEER



**DETOUR 12: CLOSURE AT I-85 S EXIT 26
ENTRANCE RAMP TO I-85 S**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 S DETOUR EXIT 27	

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF
EXIT 27 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

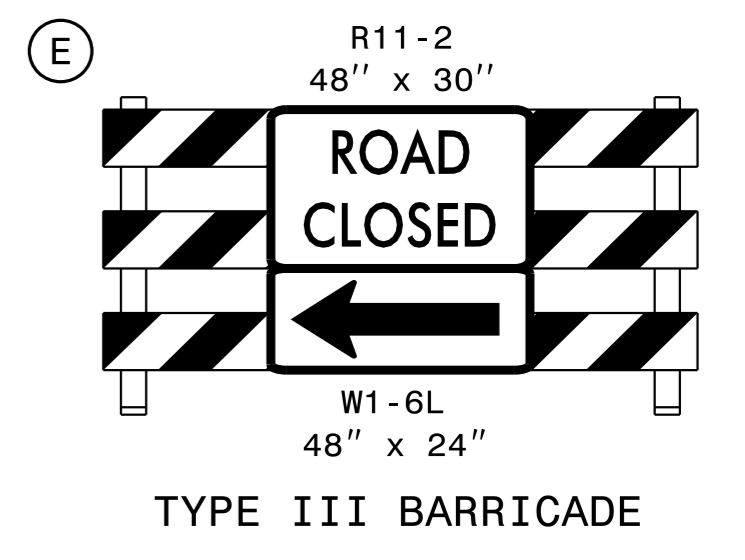
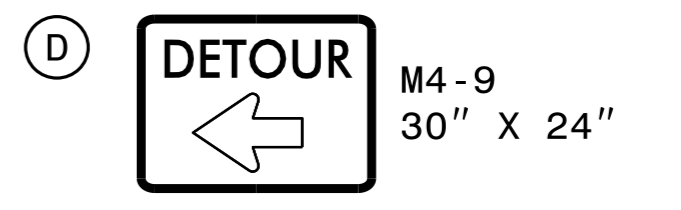
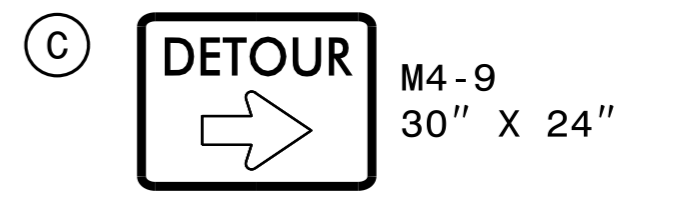
(B)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 S RAMP CLOSED	FOLLOW DETOUR AHEAD

CHANGEABLE MESSAGE SIGN

MIN. 350 FT IN ADVANCE OF
I-85 S ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

MIN. 350 FT IN ADVANCE OF
I-85 N ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

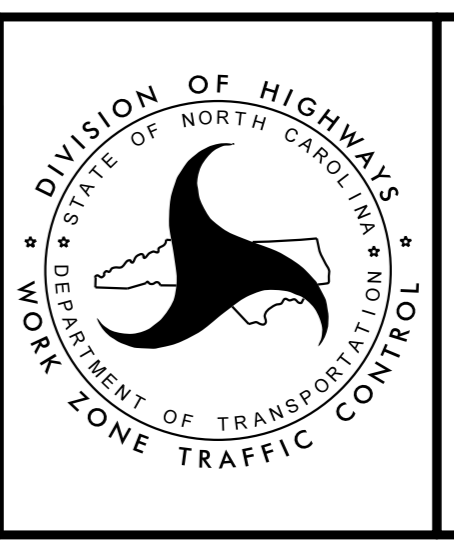


04-NOV-2016 ihl \\ADOT\DF\SR00101\GROUPS-Div\2CC\DDC\District\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-TC-TMP-07-Exit 26 Detours I& 12.dgn
Bksowell AT DICADZ10892

APPROVED: *Bryan Sowell*
DATE: 11/4/2016

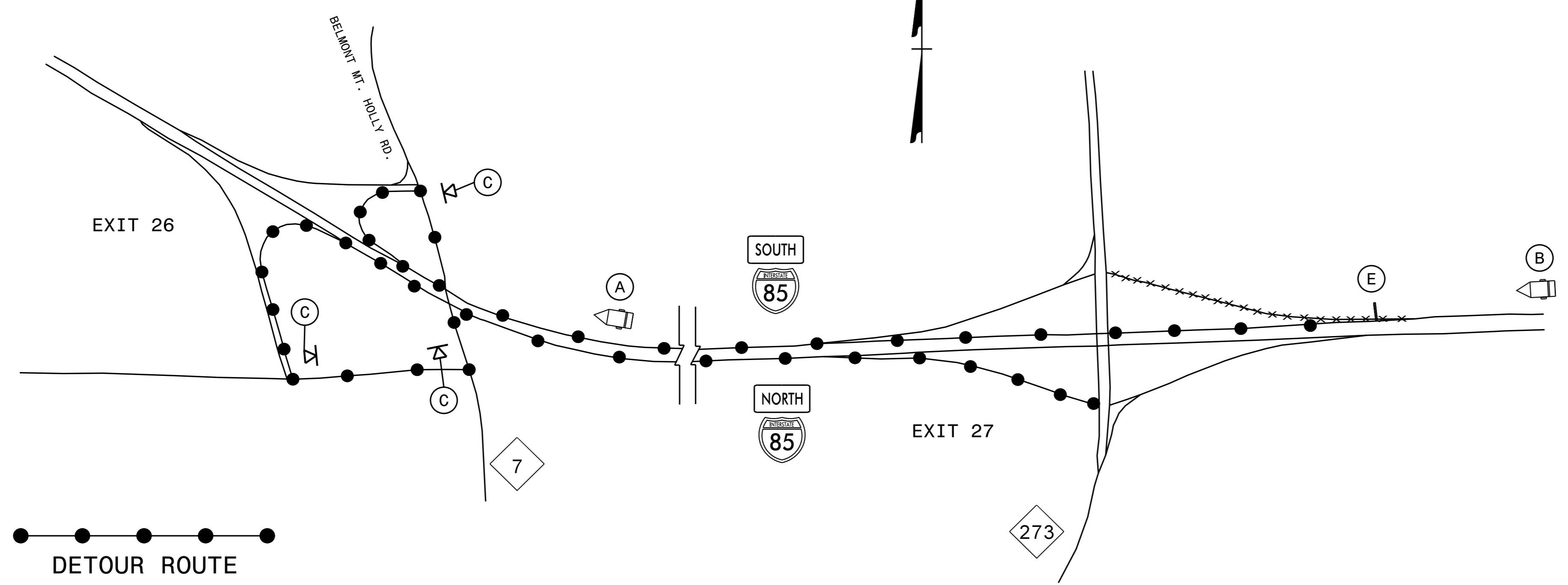
SEAL

**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**



I-85 EXIT 27
RAMP DETOURS
11 & 12

**DETOUR 13: CLOSURE AT I-85 S EXIT 27
EXIT RAMP TO NC 7**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
NC 273 DETOUR EXIT 26	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

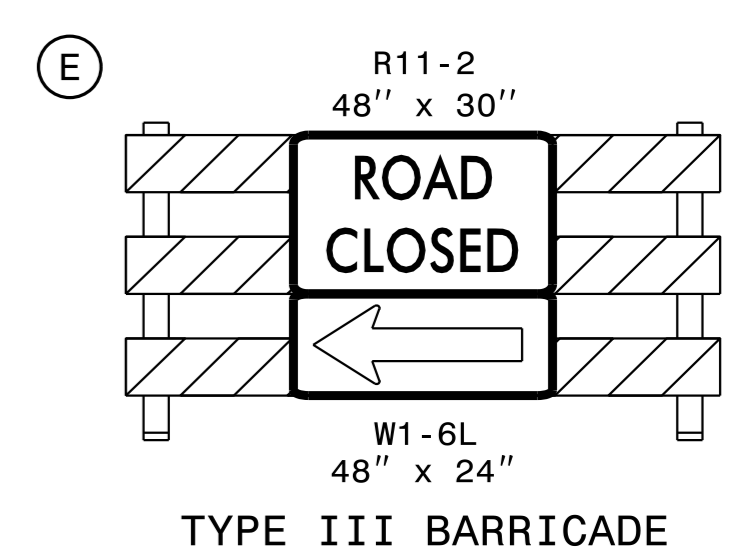
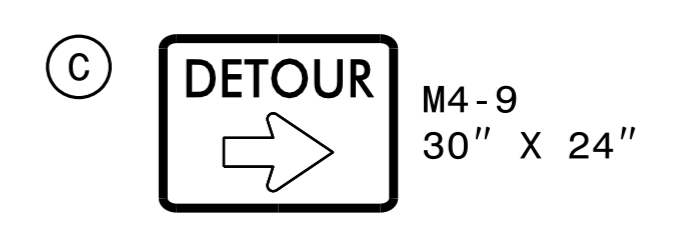
MIN. 1500 FT IN ADVANCE OF
EXIT 26 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

(B)

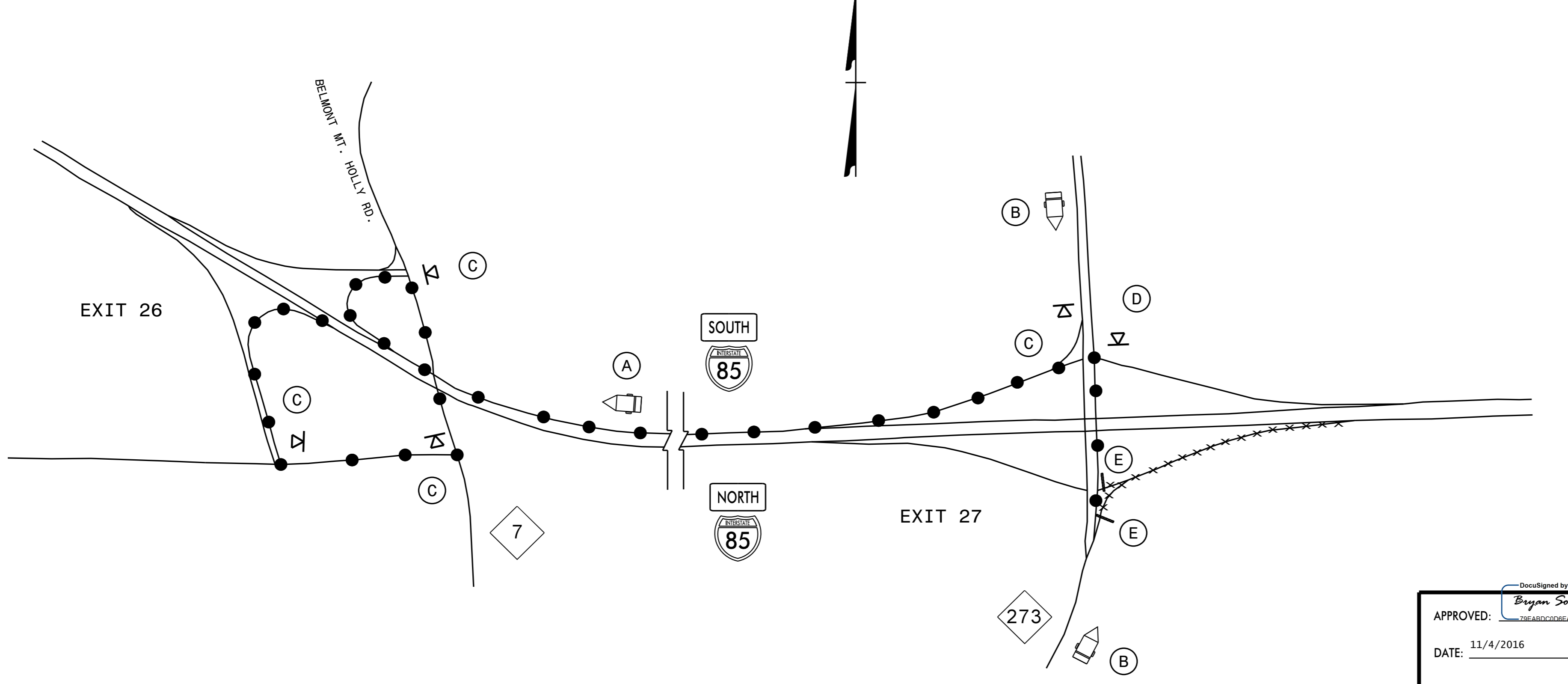
MESSAGE NO. 1	MESSAGE NO. 2
EXIT 27 NC 273 CLOSED	DETOUR TO EXIT 26

CHANGEABLE MESSAGE SIGN

MIN. 1/2 MILE IN ADVANCE OF
EXIT 27 DECELERATION LANE
OR AS DIRECTED BY ENGINEER



**DETOUR 14: CLOSURE AT I-85 N EXIT 27
ENTRANCE RAMP TO I-85 N**



(A)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N DETOUR EXIT 26	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

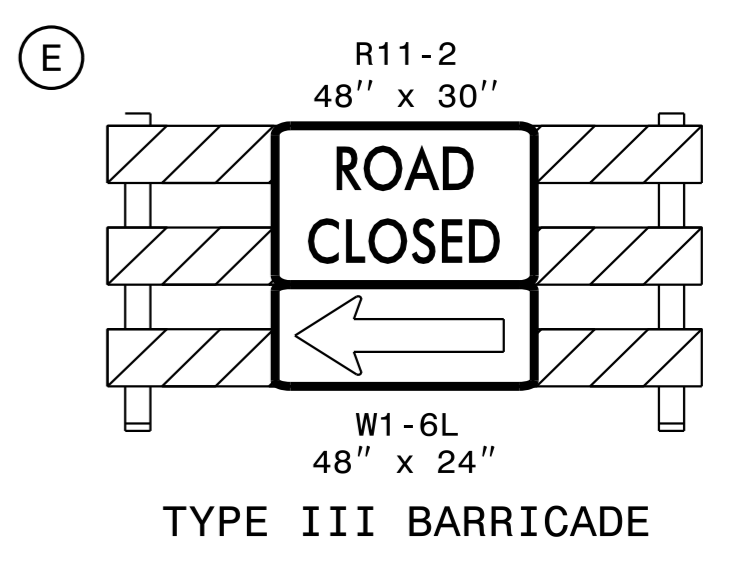
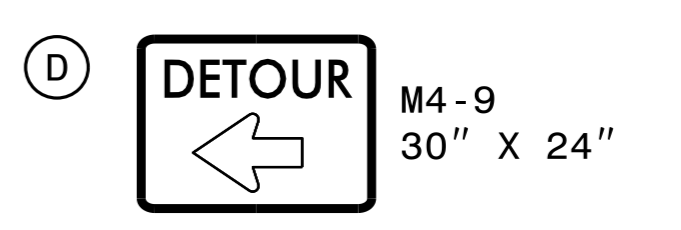
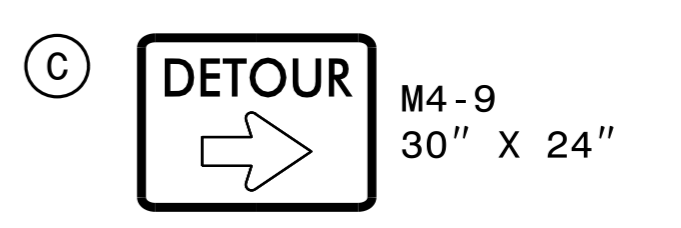
MIN. 1500 FT IN ADVANCE OF
EXIT 26 DECELERATION LANE
OR AS DIRECTED BY ENGINEER

(B)

MESSAGE NO. 1	MESSAGE NO. 2
I-85 N RAMP CLOSED	FOLLOW DETOUR AHEAD

CHANGEABLE MESSAGE SIGN

MIN. 350 FT IN ADVANCE OF
I-85 N ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER
MIN. 1/2 MILE IN ADVANCE OF
I-85 S ENTRANCE RAMP
OR AS DIRECTED BY ENGINEER

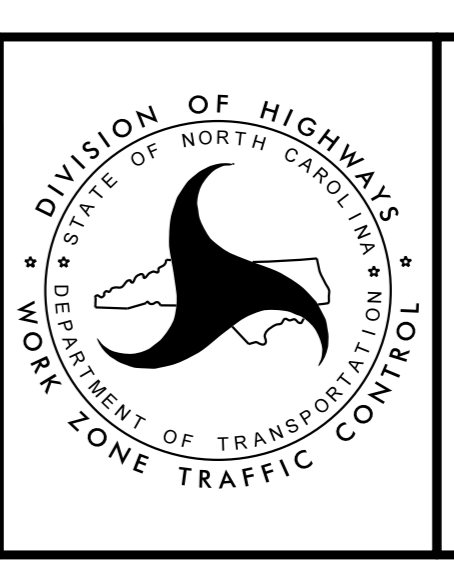


DETOUR ROUTE

APPROVED: *Bryan Sowell*
DATE: 11/4/2016

SEAL

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED



I-85 EXIT 27
RAMP DETOURS
13 & 14

04-NOV-2016 ihz \\ADOT\DF\GROUPS-Div\2CC\DDC\District\Gaston\I-5894 I-85 MM22 to Countyline\Traffic Control\I-5894-TC-TMP-08-Exit 27 Detours 13 & 14.dgn
Bksowell AT DICADZ10892

**DETOUR 15: CLOSURE AT I-85 S EXIT 27
ENTRANCE RAMP TO I-85 S**

A	MESSAGE NO. 1	MESSAGE NO. 2
	I-85 S DETOUR EXIT 26	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF EXIT 29 DECELERATION LANE OR AS DIRECTED BY ENGINEER

B	MESSAGE NO. 1	MESSAGE NO. 2
	I-85 N RAMP CLOSED	FOLLOW DETOUR AHEAD

CHANGEABLE MESSAGE SIGN

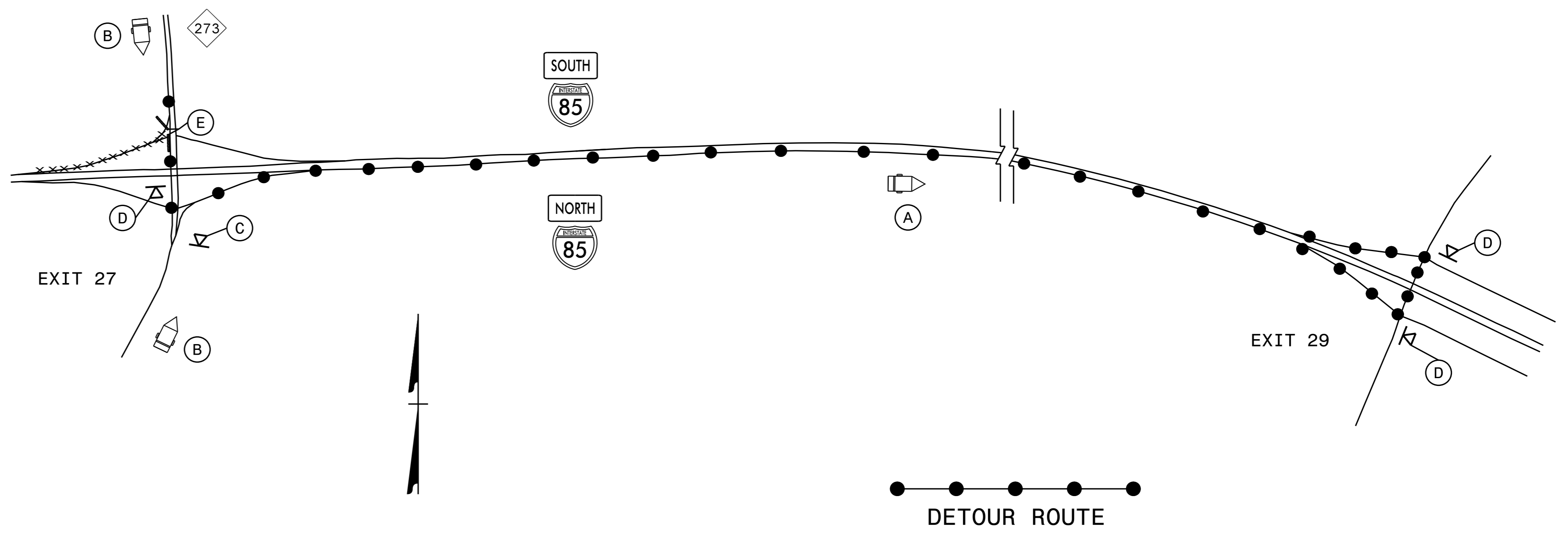
MIN. 350 FT IN ADVANCE OF I-85 N ENTRANCE RAMP OR AS DIRECTED BY ENGINEER
MIN. 350 FT IN ADVANCE OF I-85 S ENTRANCE RAMP OR AS DIRECTED BY ENGINEER

C	DETOUR	M4-9 30" X 24"

D	DETOUR	M4-9 30" X 24"

E	R11-2 48" X 30"	ROAD CLOSED
	W1-6L 48" X 24"	

TYPE III BARRICADE



**DETOUR 16: CLOSURE AT I-85 N EXIT 27
EXIT RAMP TO NC 273**

A	MESSAGE NO. 1	MESSAGE NO. 2
	NC 273 DETOUR EXIT 29	NEXT RIGHT

CHANGEABLE MESSAGE SIGN

MIN. 1500 FT IN ADVANCE OF EXIT 29 DECELERATION LANE OR AS DIRECTED BY ENGINEER

B	MESSAGE NO. 1	MESSAGE NO. 2
	EXIT 27 NC 273 CLOSED	DETOUR TO EXIT 29

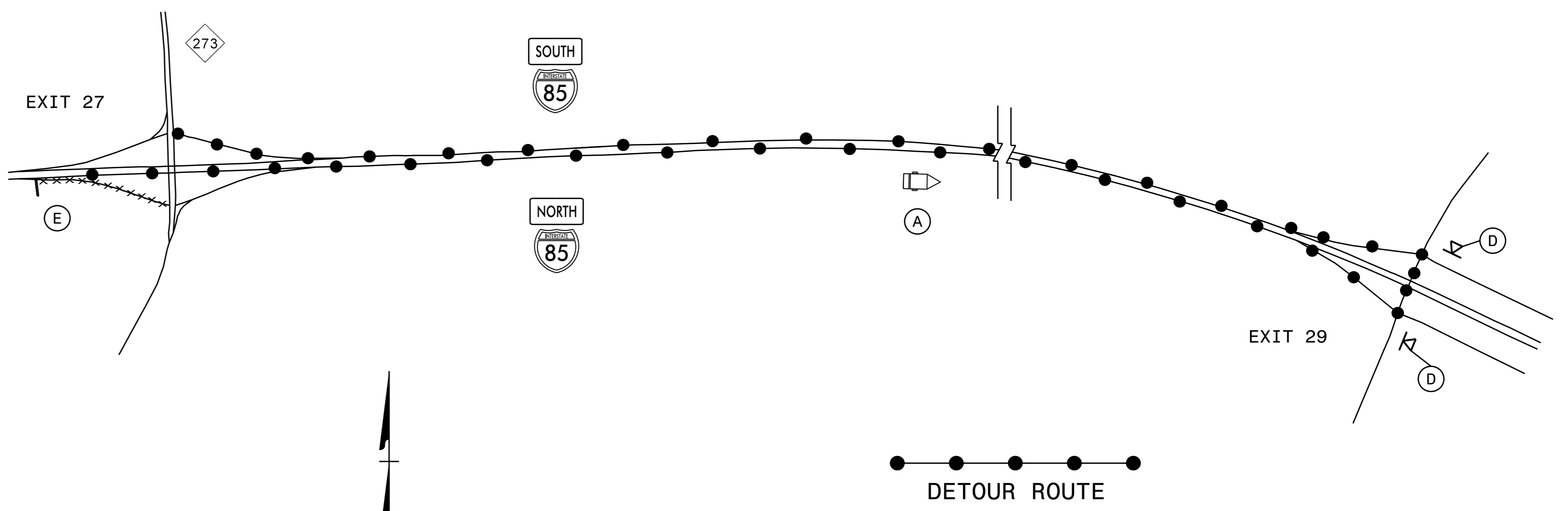
CHANGEABLE MESSAGE SIGN

MIN. 1/2 MILE IN ADVANCE OF EXIT 27 DECELERATION LANE OR AS DIRECTED BY ENGINEER

D	DETOUR	M4-9 30" X 24"

E	R11-2 48" X 30"	ROAD CLOSED
	W1-6L 48" X 24"	

TYPE III BARRICADE



04-NOV-2016 ih7 \\ADOT\DFSR00101\GROUPS-Div\2CC\DDC\District\Caston\5894 I-85 MM22 to Countyline\Traffic Control\I-5894_TC_TMP-09-Exit 27 Detours 15 & 16.dgn
Bksowell AT DICADZ10892

APPROVED: DATE: 11/4/2016 SEAL			<p>I-85 EXIT 27 RAMP DETOURS 15 & 16</p>
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>			

DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER

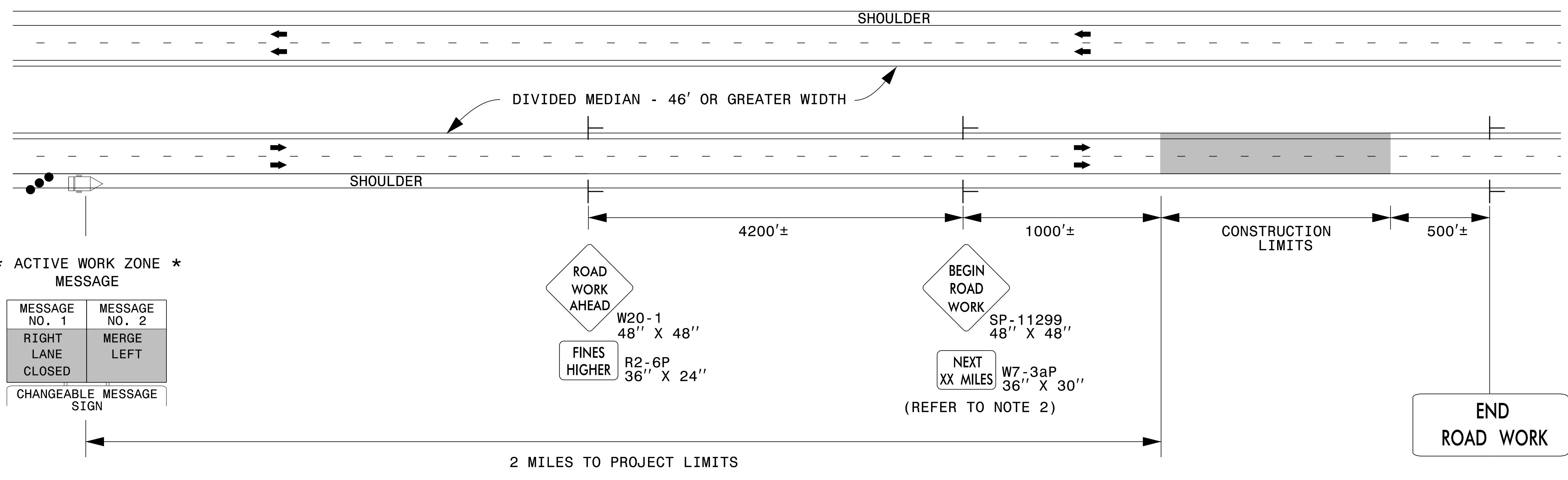
* NOTE: ADVANCE THIS CMS CONTINUOUSLY AS WORK OPERATIONS PROGRESS.

* INACTIVE WORK ZONE MESSAGE

MESSAGE NO. 1	MESSAGE NO. 2
ROAD WORK	2 MILES AHEAD
CHANGEABLE MESSAGE SIGN	

* ACTIVE WORK ZONE MESSAGE

MESSAGE NO. 1	MESSAGE NO. 2
RIGHT LANE CLOSED	MERGE LEFT
CHANGEABLE MESSAGE SIGN	



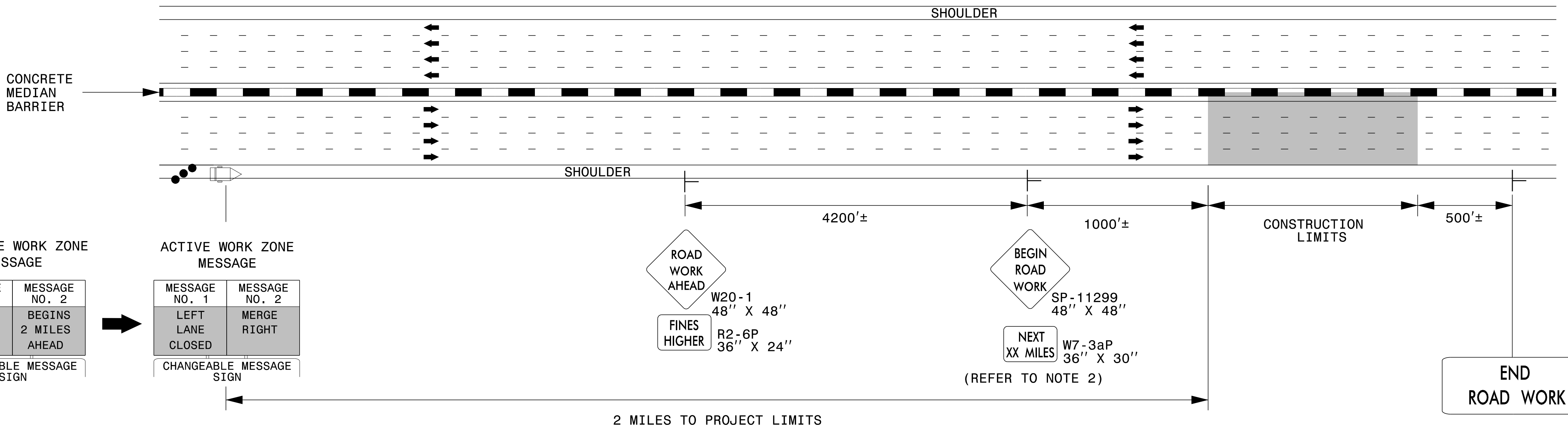
DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER

INACTIVE WORK ZONE MESSAGE

MESSAGE NO. 1	MESSAGE NO. 2
ROAD WORK	BEGINS 2 MILES AHEAD
CHANGEABLE MESSAGE SIGN	

ACTIVE WORK ZONE MESSAGE

MESSAGE NO. 1	MESSAGE NO. 2
LEFT LANE CLOSED	MERGE RIGHT
CHANGEABLE MESSAGE SIGN	



NOTES

1. THIS DRAWING IS TO BE USED IN CONJUNCTION WITH THE WORK ZONE VARIABLE SPEED LIMIT USING DIGITAL SPEED LIMIT SIGNS FOR INTERSTATE/FREEWAY RESURFACING PROJECTS DETAIL.
2. FOR SIGN W7-3aP, ROUND TO THE NEAREST MILE.
3. FOR ENTRANCE AND EXIT RAMP, REFER TO RSD 1101.01, SHEET 1, DETAIL B & C.
4. FOR ADDITIONAL NOTES, REFER TO RSD 1101.01, SHEET 1.

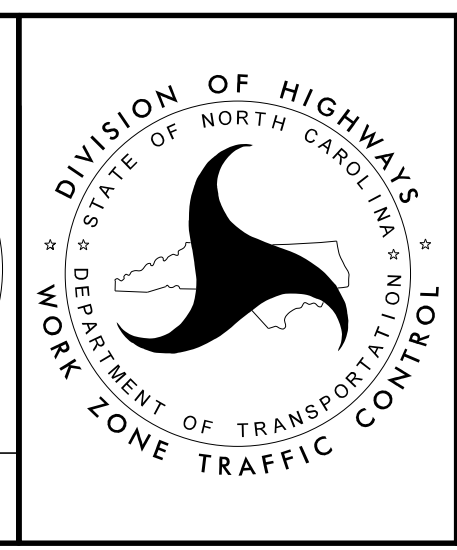
LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM

APPROVED: *Don A. Parker*
40D48286261841D

DATE: 1/4/2017

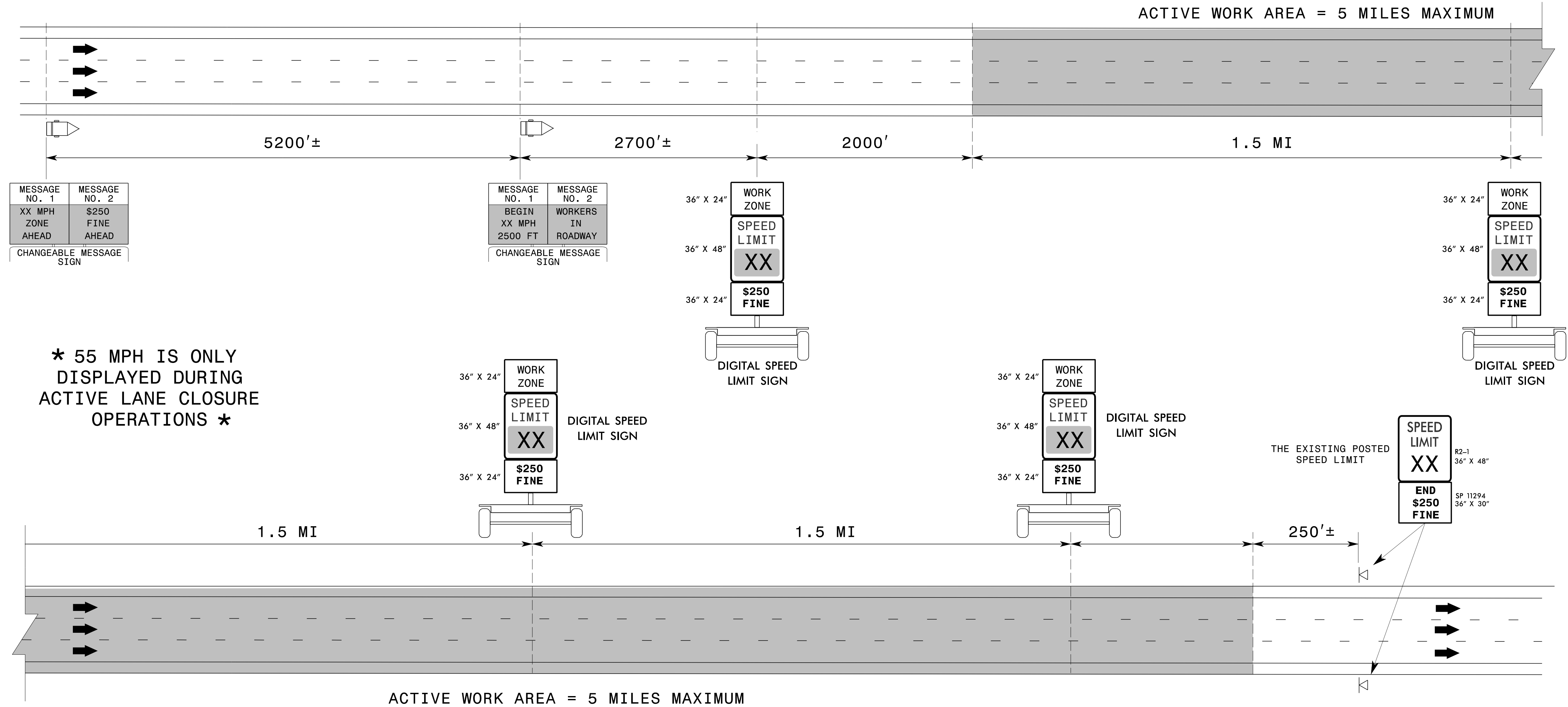
**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**



**STATIONARY ADVANCE
WARNING SIGNS FOR
INTERSTATE/FREEWAY
RESURFACING PROJECTS**

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INTERSTATE RESURFACING OPERATIONS WITH DIGITAL SPEED LIMIT SIGNS



*** 55 MPH IS ONLY DISPLAYED DURING ACTIVE LANE CLOSURE OPERATIONS ***

WHEN THERE IS NOT ACTIVE WORK IN THE TRAVEL LANE

SPEED LIMIT DISPLAY	CONDITIONS	
	DROP-OFFS BETWEEN OPEN TRAVEL LANES	PAVED SHOULDER DROP-OFFS
USE EXISTING SPEED LIMIT	< 1.0"	≤ 3.0"
REDUCE SPEED LIMIT 5 MPH	1.0" - 2.0"	> 3.0"

DROP-OFFS BETWEEN OPEN TRAVEL LANES SHOULD NOT EXCEED 2.0"

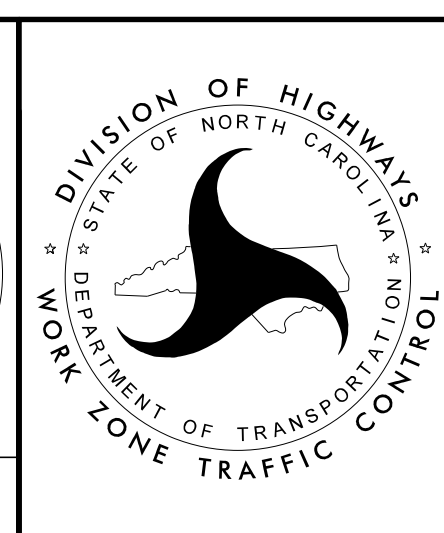
- NOTES**

 1. THE SPEED LIMITS DISPLAYED WITHIN THE ACTIVE WORK AREA MAY VARY BETWEEN 55 MPH AND 70 MPH, DEPENDENT UPON ROAD WORK CONDITIONS AND THE EXISTING SPEED LIMIT. 55 MPH IS ONLY DISPLAYED DURING ACTIVE LANE CLOSURE OPERATIONS.
 2. THE ENGINEER MAY DETERMINE TO INSTALL THE DIGITAL SPEED LIMIT SIGNS ON THE OUTSIDE SHOULDER OR ON THE MEDIAN SIDE IF THE SIGNS ARE NOT HIGHLY VISIBLE TO ALL MOTORISTS.
 3. THIS APPLICATION IS FOR SHORT-TERM ACTIVITIES. THE MAXIMUM ACTIVE WORK AREA IS 5 MILES.
 4. THE DIGITAL SPEED LIMIT SIGNS TAKE PRECEDENCE OVER EXISTING SPEED LIMIT SIGNS. ALL EXISTING SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED.
 5. THE DIGITAL SPEED LIMITS SIGNS WILL BE INSTALLED (TRAILOR MOUNTED OR STATIONARY MOUNTED) IN ADVANCE AND SPACED APPROXIMATELY 1.5 MILES THROUGHOUT THE ACTIVE WORK AREA, UNLESS DIRECTED OTHERWISE.
 6. NCDOT HAS SOLE AUTHORITY OF THE SPEED LIMITS DISPLAYED ON THE DIGITAL SPEED LIMIT SIGNS.
 7. THE WORK ZONE VARIABLE SPEED LIMIT AND THE \$250 SPEEDING PENALTY ARE SEPARATE ORDINANCES THAT MUST BE SIGNED BY THE STATE TRAFFIC ENGINEER TO BE VALID AND ENFORCEABLE. WITHOUT A SIGNED ORDINANCE, THE SPEED LIMIT ON A FACILITY SHALL REMAIN UNCHANGED.

APPROVED: *Don A. Parker*
40D46286261841D...
 DATE: 1/4/2017

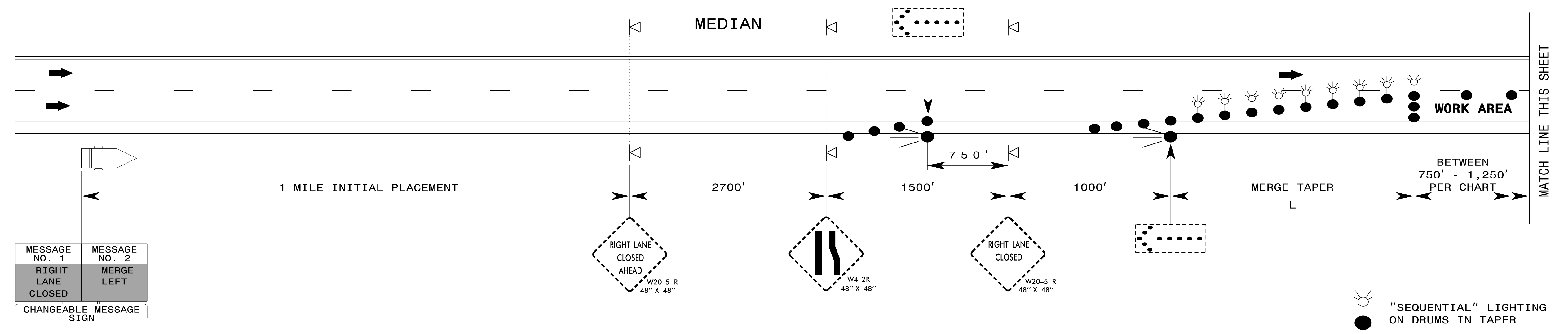
NORTH CAROLINA
PROFESSIONAL
ENGINEERS
DON A. PARKER
SEAL
043251

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UNLESS ALL SIGNATURES COMPLETED**

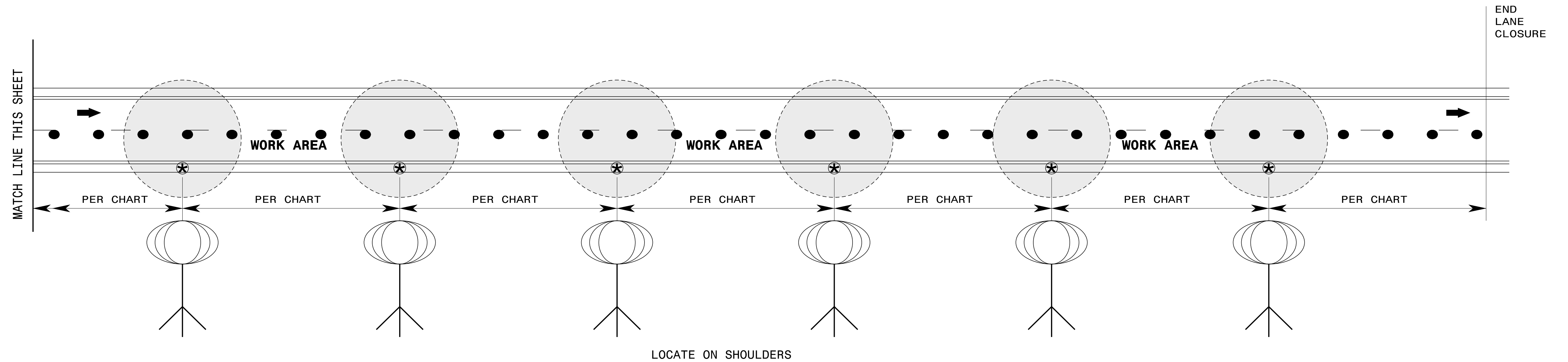


WORK ZONE "VARIABLE"
SPEED LIMIT USING
DIGITAL SPEED LIMIT
SIGNS FOR INTERSTATE/
FREEWAY RESURFACING
PROJECTS

ADVANCE WARNING AREA



WORK ZONE AREA



SPACING CHART

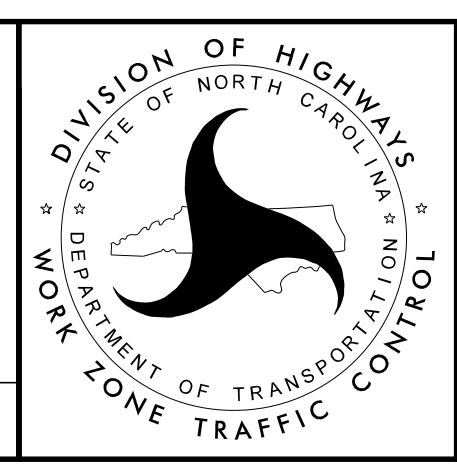
LIGHT OUTPUT (LUMENS)	MINIMUM LIGHTED FIXTURE AREA (SQUARE FEET)	MAXIMUM SPACING (FEET)	LIGHT UNITS (PER MILE)
50,000 TO 65,000	5.5	750'	6
66,000 TO 80,000	5.5	1,000'	5
81,000 TO 100,000	36	1,250'	4

NOTES

- 1) SPACE LIGHT UNITS ACCORDING TO THE CHART.
- 2) EACH LIGHT UNIT SHALL BE CAPABLE OF ELEVATING TO A MINIMUM HEIGHT OF 14' ABOVE THE PAVEMENT.
- 3) PLACE ON PAVED SHOULDER IF POSSIBLE.

APPROVED: Don A. Parker
 DATE: 1/4/2017

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



WORK ZONE PRESENCE LIGHTING

I:\4\2017\0\div\division2\5894\Traffic\TrafficControl\Turn_in_Docs\PDF\WZ_Presence_Lighting_20161219.dgn
 User:rmgarrrett