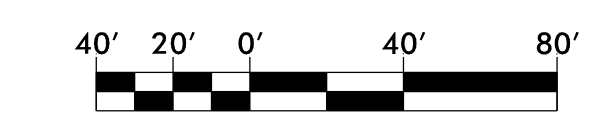
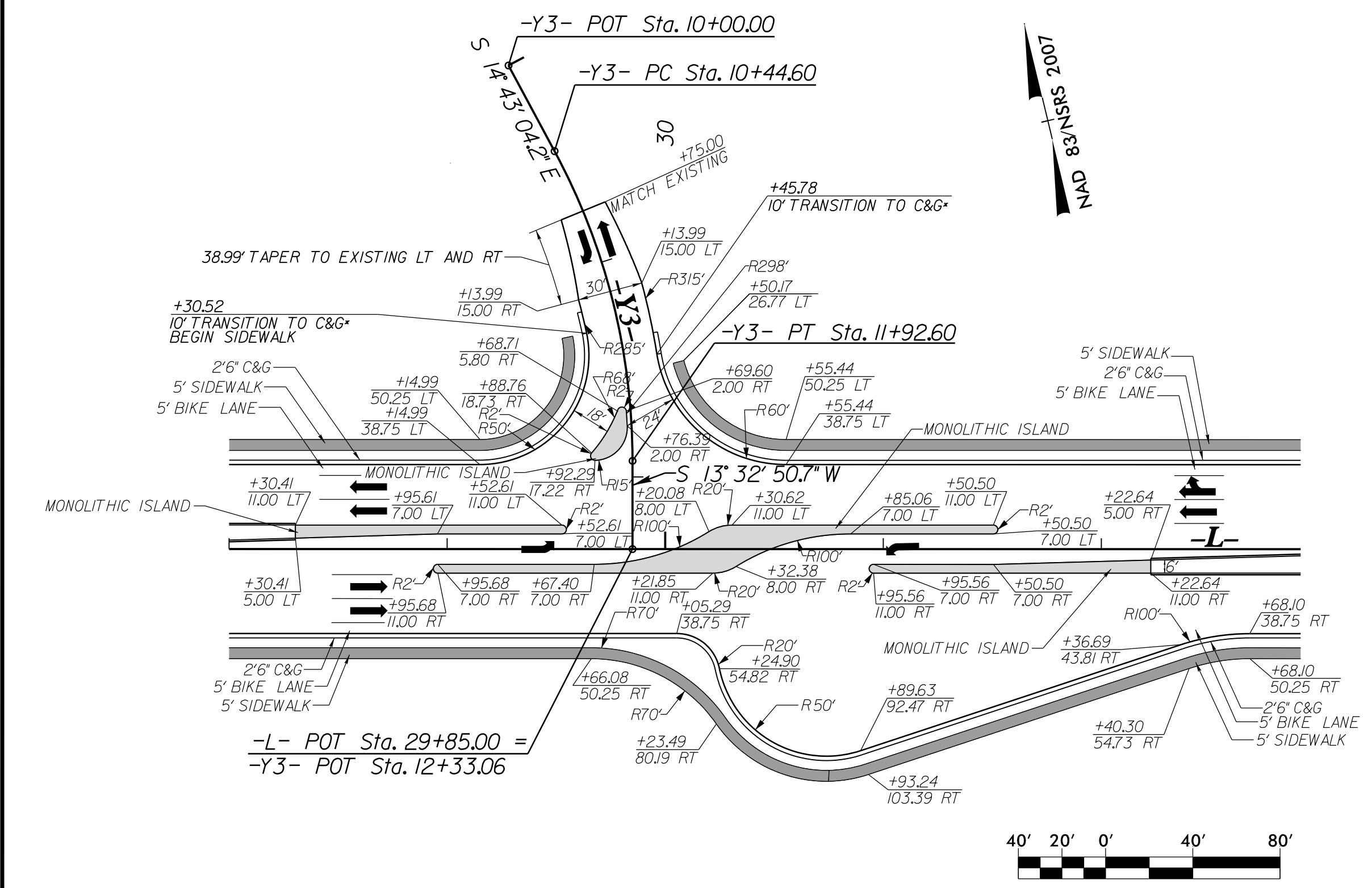


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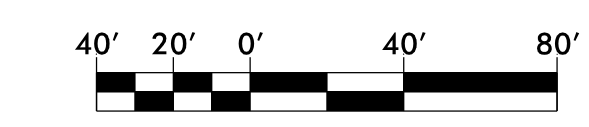
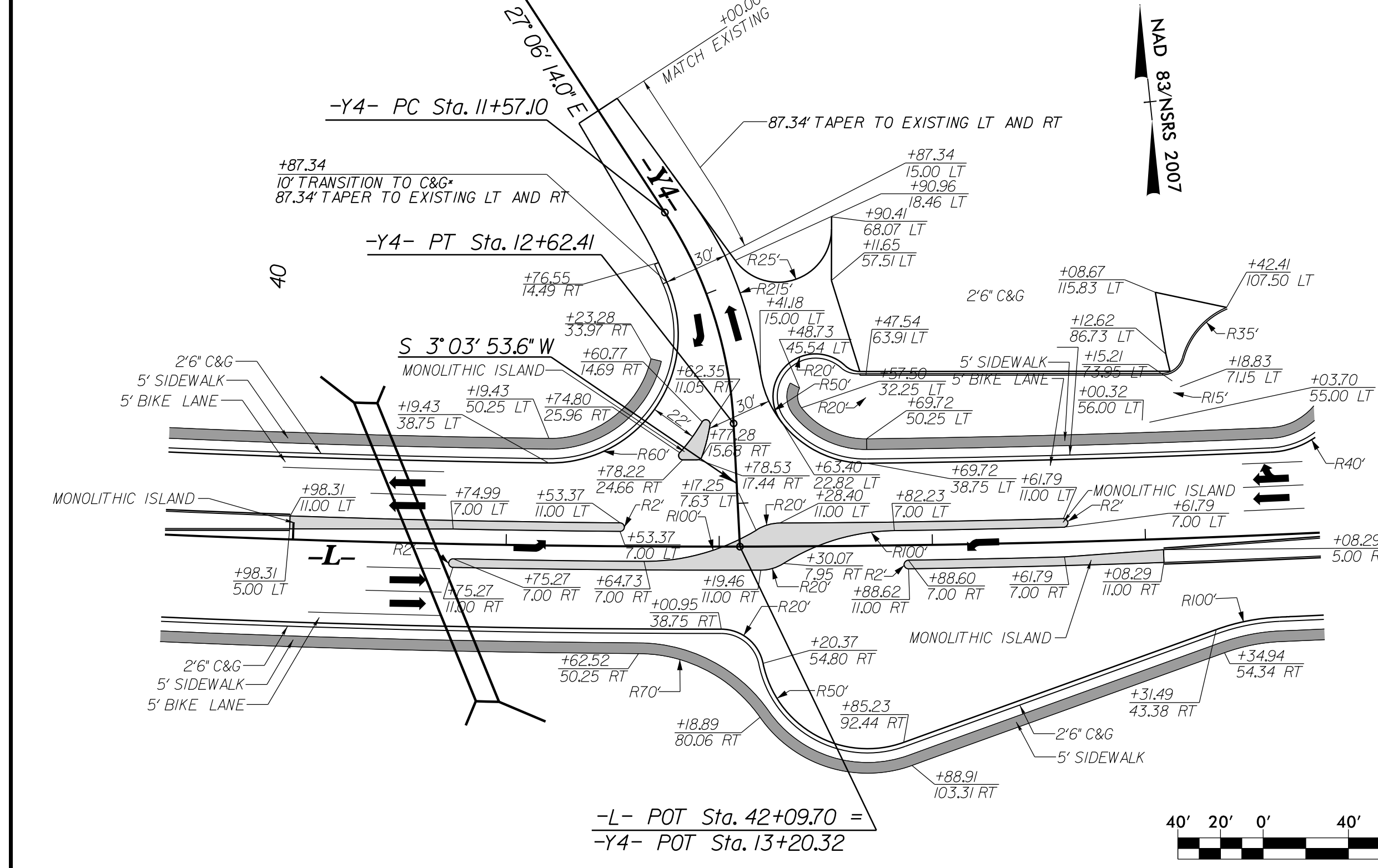
INTERSECTION DETAIL -L- AND -Y3-

NOTE: ALL OFFSETS ARE TAKEN OFF THE DIRECTLY ADJACENT ALIGNMENT (-L-, -Y3-) UNLESS NOTED OTHERWISE
*SEE DETAIL SHEET 2C-2 FOR CURB AND GUTTER 10' TRANSITION TO PAVED SHOULDER



INTERSECTION DETAIL -L- AND -Y4-

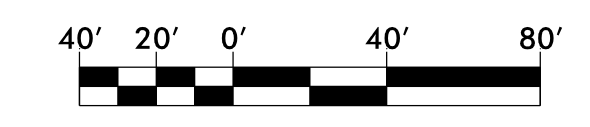
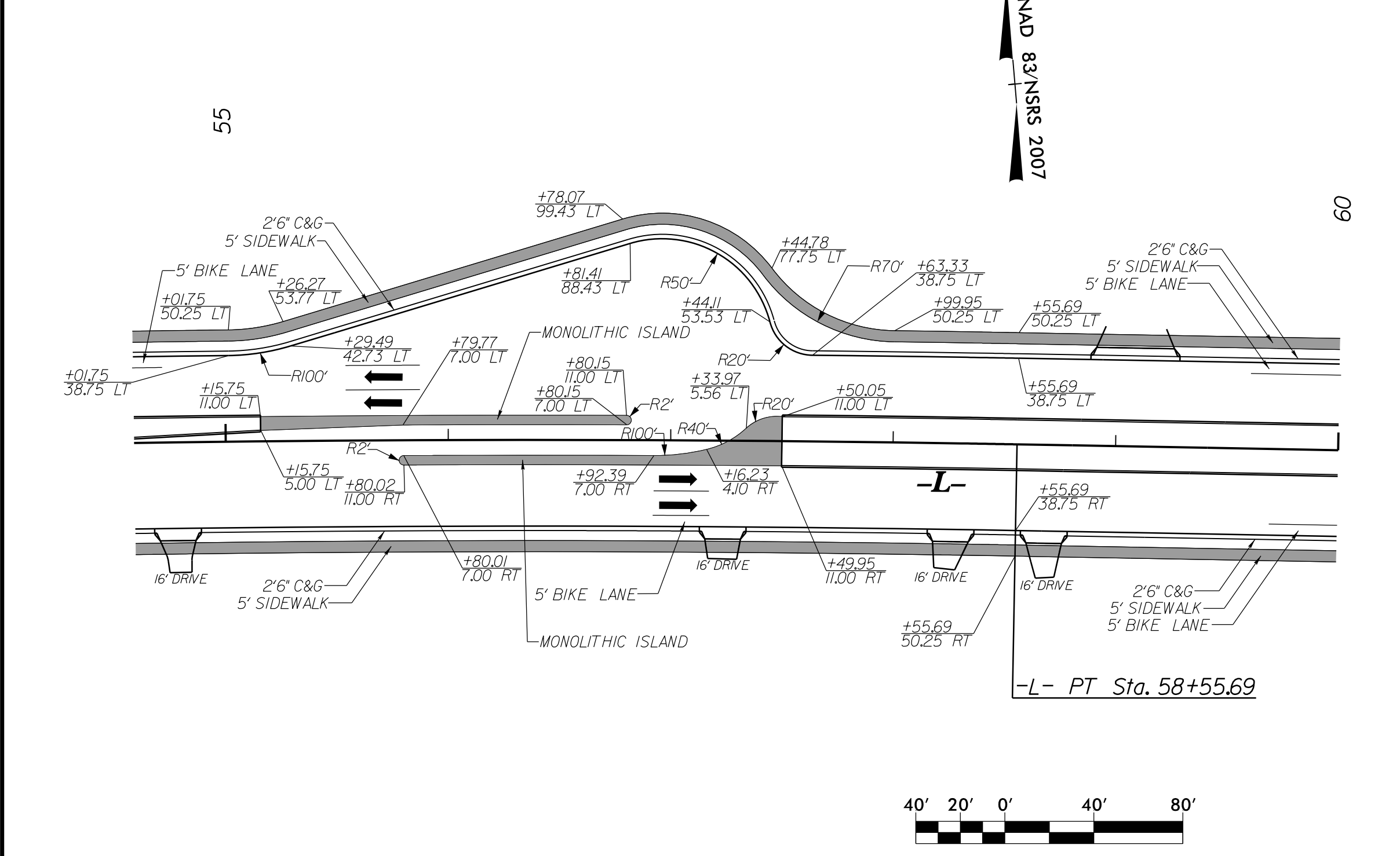
NOTE: ALL OFFSETS ARE TAKEN OFF THE DIRECTLY ADJACENT ALIGNMENT (-L-, -Y4-) UNLESS NOTED OTHERWISE
*SEE DETAIL SHEET 2C-2 FOR CURB AND GUTTER 10' TRANSITION TO PAVED SHOULDER



PROJECT REFERENCE NO.	SHEET NO.
U-3440	2B-1
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	
8/17/2016	
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	

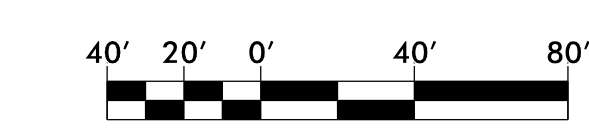
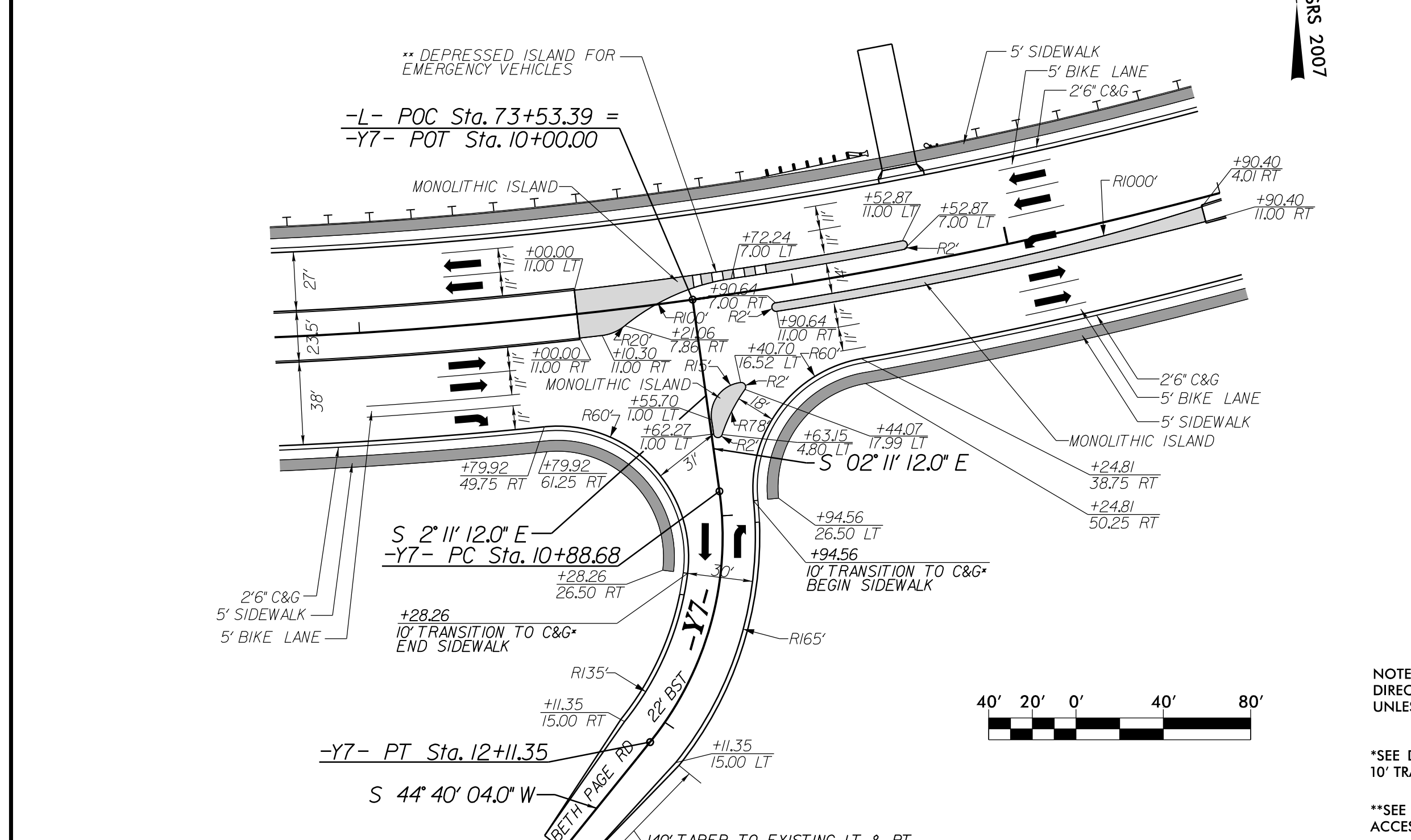
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BULB-OUT DETAIL -L- STA. 57+00 LT



INTERSECTION DETAIL -L- AND -Y7-

NOTE: ALL OFFSETS ARE TAKEN OFF THE DIRECTLY ADJACENT ALIGNMENT (-L-, -Y7-) UNLESS NOTED OTHERWISE
*SEE DETAIL SHEET 2C-2 FOR CURB AND GUTTER 10' TRANSITION TO PAVED SHOULDER
**SEE DETAIL SHEET 2C-1 FOR EMERGENCY VEHICLE ACCESS.



8/17/2016
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