

## GENERAL NOTES / LOCAL NOTES (CONT)

### TRAFFIC BARRIER

V) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS. TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

W) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45-50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

### TRAFFIC CONTROL DEVICES

X) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.

Y) PLACE TYPE III BARRICADES WITH "ROAD CLOSED" SIGN R11-2 ATTACHED OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

Z) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES (DRUMS, CONES OR SKINNY DRUMS) PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 200 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

### PAVEMENT MARKINGS AND MARKERS

AA) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. ALL ASPHALT PAVEMENT	PAINT	TEMPORARY RAISED
2. PROPOSED BRIDGE DECKS	COLD APPLIED PLASTIC TYPE IV (REMOVABLE TAPE)	TEMPORARY RAISED

BB) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

CC) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

DD) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

EE) TRACE THE EXISTING AND/OR PROPOSED MONOLITHIC ISLAND LOCATIONS WITH THE PROPER COLOR PAVEMENT MARKING PRIOR TO REMOVAL AND/OR INSTALLATION. PLACE DRUMS TO DELINEATE ANY EXISTING AND/OR PROPOSED MONOLITHIC ISLANDS AFTER REMOVAL AND/OR BEFORE INSTALLATION.

### MISCELLANEOUS

FF) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

GG) ALL CURB RAMP LOCATIONS SHALL BE DERIVED FROM STATIONING SHOWN ON PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE SIGNING AND DELINEATION UNIT.

HH) CONTRACTOR SHALL MAINTAIN SIDEWALK ACCESS AT ALL TIME AS STATED IN THE PHASING. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY SIDEWALKS (CONCRETE, ASPHALT, OR OTHER SUITABLE MATERIAL AS APPROVED BY THE ENGINEER) AT ALL LOCATIONS WHERE THE OPEN PEDESTRIAN TRAVELWAY HAS BEEN REMOVED FOR CONSTRUCTION OPERATIONS (UTILITIES, DRAINAGE, ETC.).

II) WHEN CONSTRUCTING DRAINAGE STRUCTURES ADJACENT TO TRAFFIC, INSTALL TEMPORARY STEEL PLATES AS DIRECTED BY THE ENGINEER. MAY WORK EACH LOCATION INDEPENDENTLY OR CONCURRENTLY, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. WORK IN A CONTINUOUS MANNER TO PERFORM THE WORK IN THE FOLLOWING SEQUENCE, IN STEPS 'A' THRU 'E'.

A: CLOSE THE APPROPRIATE TRAVEL LANE TO TRAFFIC USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEETS 1, 2, 3, 8 & 10 OF 15.

B: CONSTRUCT PROPOSED STRUCTURE OR INSTALL PRE-CAST DRAINAGE STRUCTURE AS SHOWN IN THE CONSTRUCTION PLANS AND COVER WITH STEEL PLATES TO PROTECT STRUCTURE DURING CURING.

C: OPEN TRAVEL LANE TO EXISTING TRAFFIC PATTERN BY THE END OF EACH WORK PERIOD.

D: WHEN PROPERLY CURED, CLOSE THE APPROPRIATE TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEETS 1, 2, 3, 8 & 10 OF 15. BACKFILL & PAVE, IF REQUIRED, UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT (SEE CONSTRUCTION PLANS).

E: OPEN TRAVEL LANE TO EXISTING TRAFFIC PATTERN BY THE END OF THE WORK PERIOD.

JJ) CONSTRUCT ALL PAVEMENT (TEMPORARY AND/OR PROPOSED) TO MAINTAIN DRAINAGE AND NOT POND WATER IN THE TRAFFIC LANES.

KK) TEMPORARY PAVEMENT MAY BE UTILIZED IN THE CONSTRUCTION OF THE PROPOSED AS DIRECTED BY THE ENGINEER.

LL) PLACE TRAFFIC BACK INTO EXISTING PATTERN AT THE END OF EACH WORK PERIOD.

MM) PLACE TYPE III BARRICADES & DRUMS AT ALL -Y- LINES AND DRUMS AT ALL DRIVEWAYS TO KEEP PROPOSED/TEMPORARY WIDENING CLOSED TO TRAFFIC.

NN) TEMPORARY SHORING MAY BE ADJUSTED SO AS NOT TO CONFLICT WITH EXISTING UTILITIES.

OO) INSTALL TEMPORARY GLARE SCREEN ON PORTABLE CONCRETE BARRIER THAT SEPARATES TWO-WAY TRAFFIC ON CAPITAL BLVD.

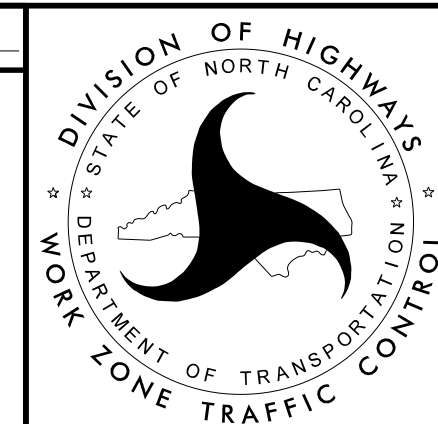
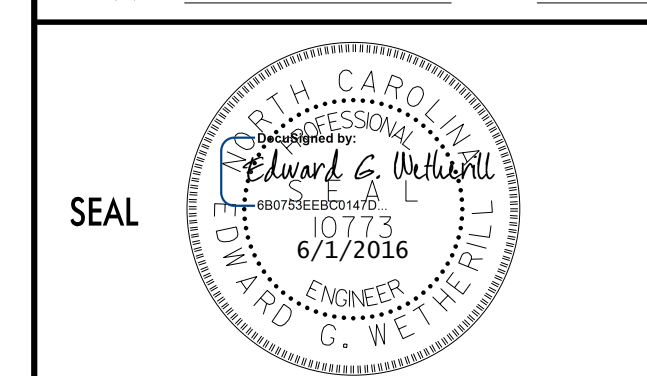
PP) PROTECT PEDESTRIANS FROM FALLING DEBRIS ALONG EXISTING SIDEWALKS BENEATH THE US 70/US 401/NC 50 BRIDGE STRUCTURES AT ALL TIMES DURING CONSTRUCTION (SEE SPECIAL PROVISIONS).

QQ) MOUNT 35 MPH ADVISORY SPEED PLAQUES (W13-1, BLACK ON ORANGE) BELOW ALL "TRAFFIC SHIFT" SIGNS (W1-4) ON CAPITAL BLVD. (-L-), UNLESS OTHERWISE SHOWN IN THE TMP. IN ADDITION, MOUNT 35 MPH ADVISORY SPEED PLAQUES (W13-1, BLACK ON ORANGE) BELOW ALL "LEFT/RIGHT LANE CLOSED AHEAD" SIGNS (W20-5), "LEFT/RIGHT LANE CLOSED" SIGNS (W20-5), "LEFT/RIGHT TWO LANES CLOSED AHEAD" SIGNS (W20-5a) AND "LEFT/RIGHT TWO LANES CLOSED" SIGNS (W20-5a) WHEN UTILIZING LANE CLOSURES ON CAPITAL BLVD. (-L-).

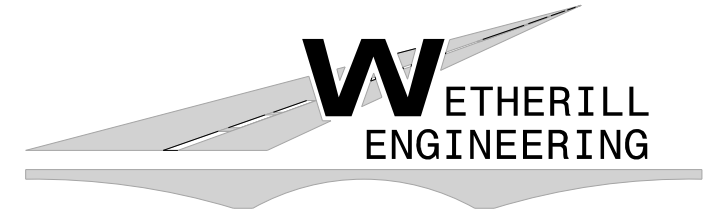
IF THE MERGE TAPER OR SIGNS FOR THE LANE CLOSURE ON CAPITAL BLVD. FALLS WITHIN THE LIMITS OF THE INSTALLED 25 MPH ADVISORY SPEED PLAQUES (W13-1, BLACK ON ORANGE) SHOWN IN THE PLAN (SEE SHEETS TMP-29 AND TMP-30), THEN DO NOT MOUNT THE 35 MPH ADVISORY SPEED PLAQUES BELOW THE LANE CLOSURE SIGNS.

**DOCUMENT NOT CONSIDERED FINAL  
UNLESS ALL SIGNATURES COMPLETED**

APPROVED: \_\_\_\_\_ DATE: \_\_\_\_\_



## GENERAL NOTES AND PROJECT NOTES



**WETHERILL  
ENGINEERING**

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TRANSPORTATION PLANNING/DESIGN - BRIDGE/STRUCTURE DESIGN  
CIVIL/SITE DESIGN - GIS/GPS - CONSTRUCTION OBSERVATION

6/1/2016 P:\2014\B-5121-B-5317\TrafficControl\Top\Current TMP Files\LANE'S CURRENT\Peace St\B5121B5317\_Tc\_Tmp\_Peace.pshd.dgn User:skennedy