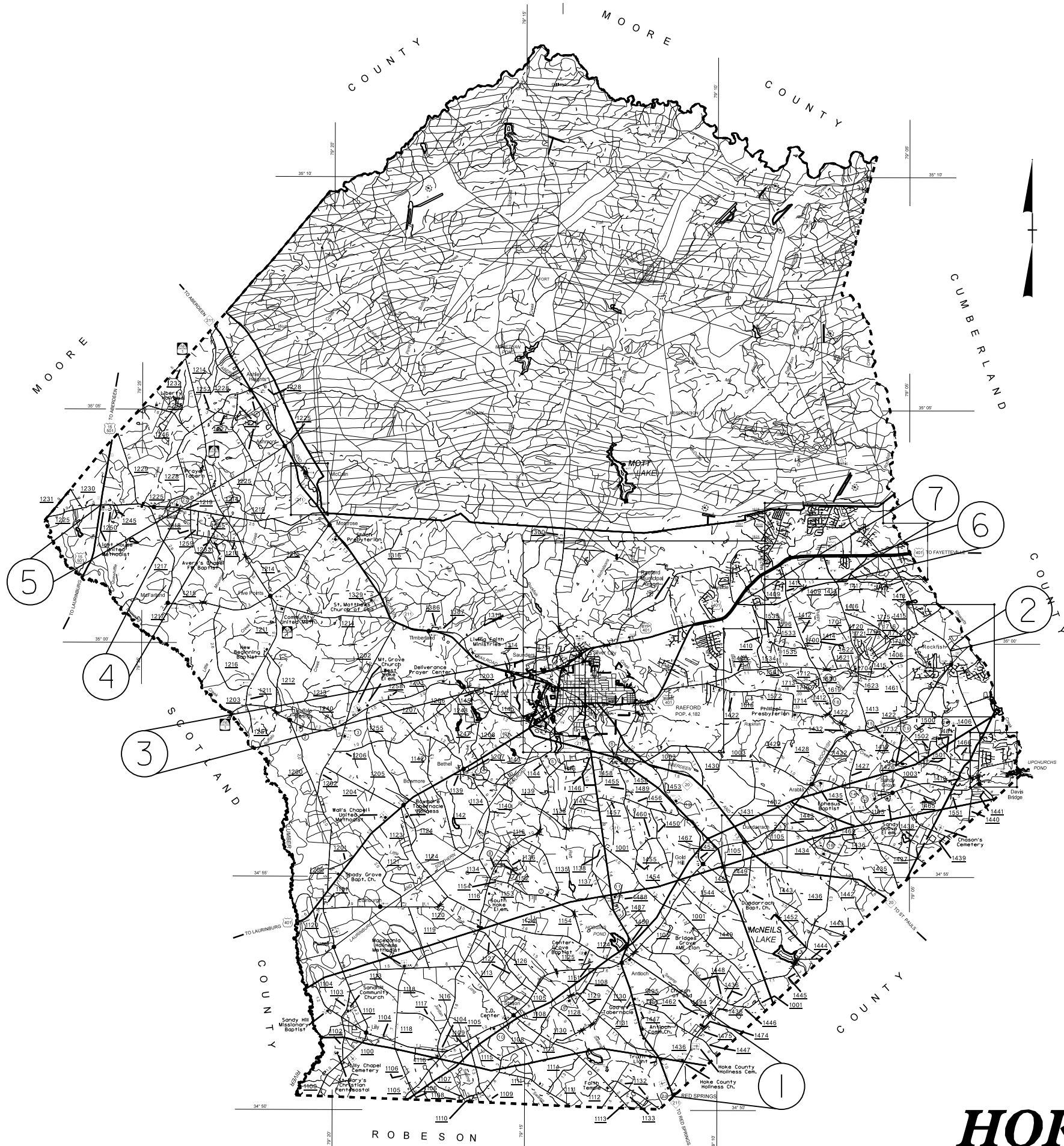


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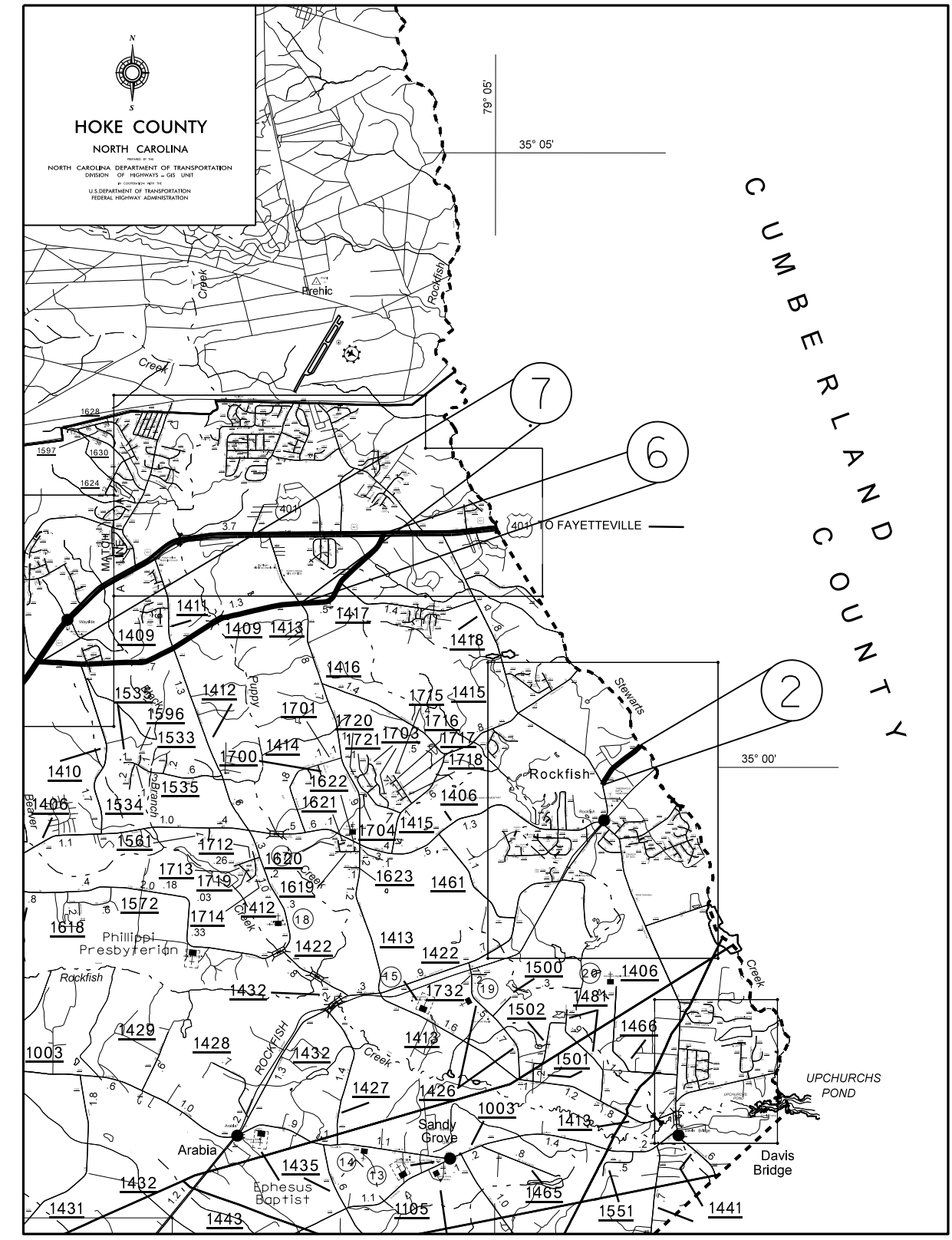
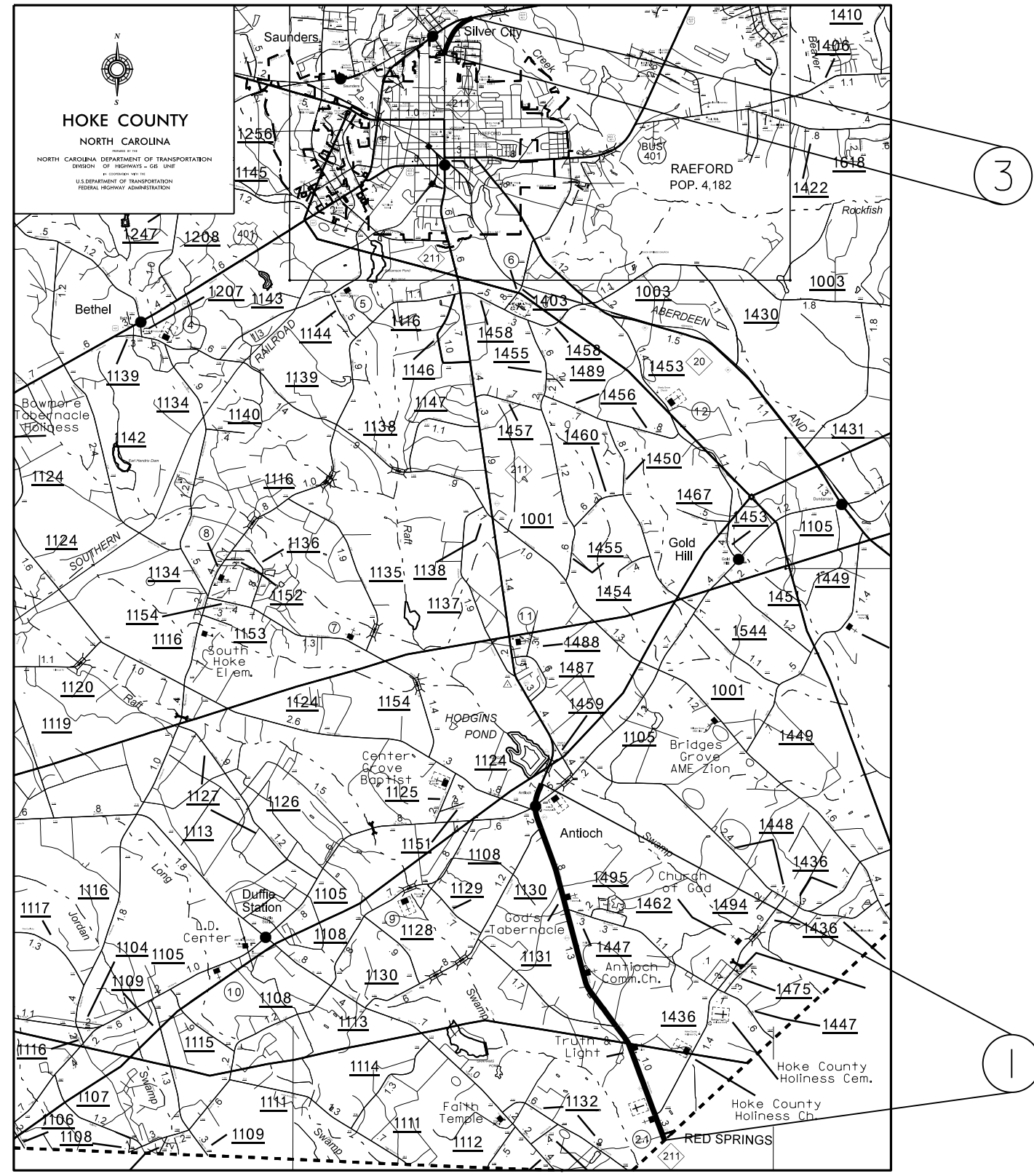
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5/28/99



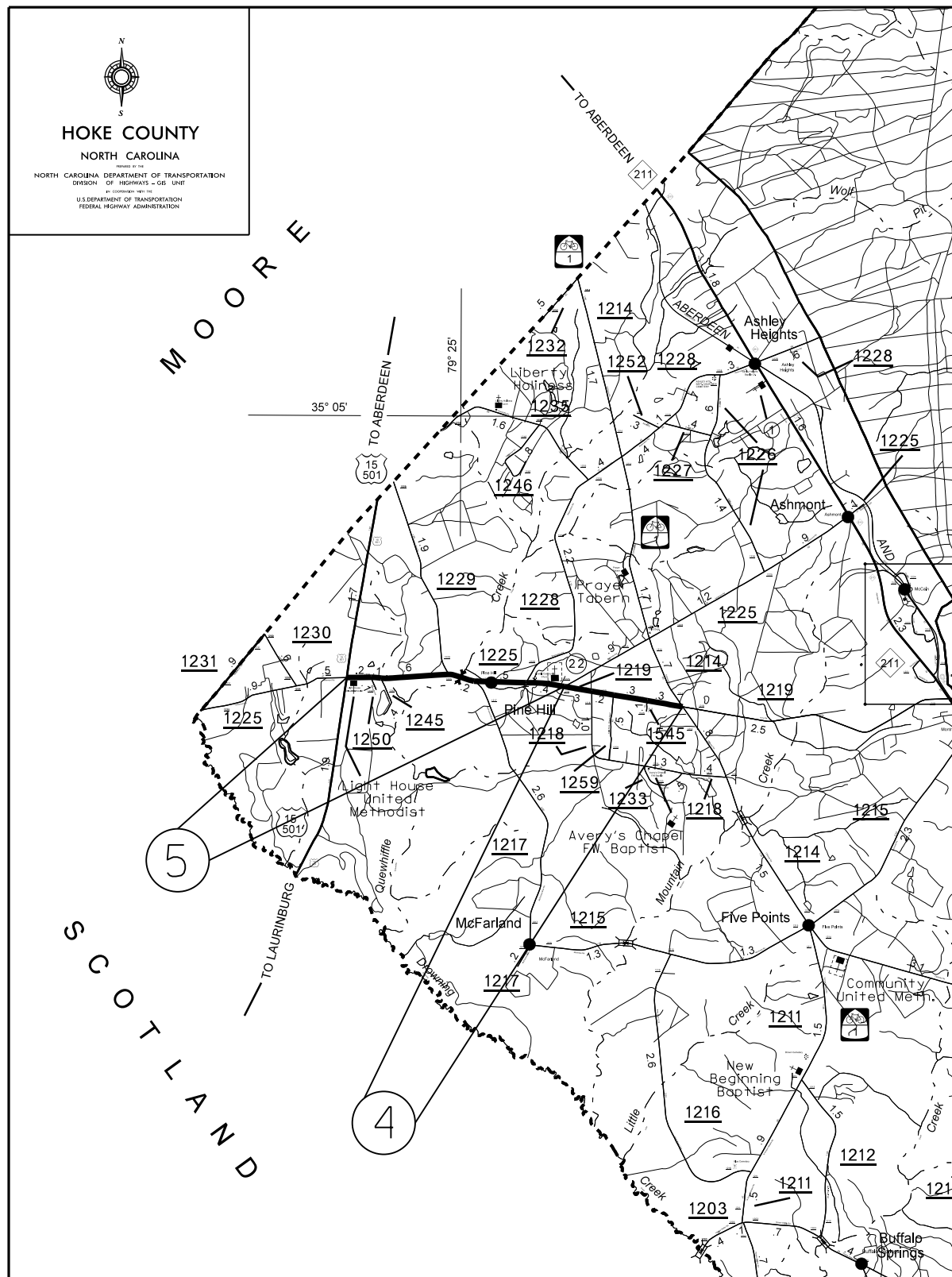
HOKE COUNTY

 SYSTEMS TIME *****



CUMBERLAND COUNTY

040397
DATE PLOTTED: 08/16/2016 10:47:11 AM
SCALE: 1" = 1 MILE
DRAWN BY: J. W. BROWN
CHECKED BY: J. W. BROWN
DATE: 08/16/2016



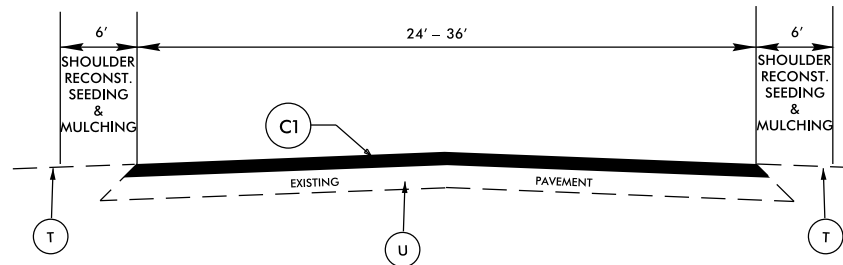
HOKE COUNTY
NORTH CAROLINA

NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - GS LINE
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U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

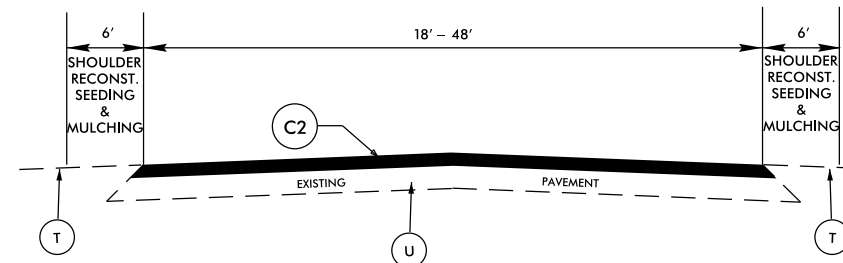
040397

SYSTEMS
CONSTRUCTION
DIVISION
PLANNING
SECTION
DATE: 08/16/2016
TIME: 10:00 AM
BY: J. W. BROWN
CHECKED BY: J. W. BROWN
APPROVED BY: J. W. BROWN

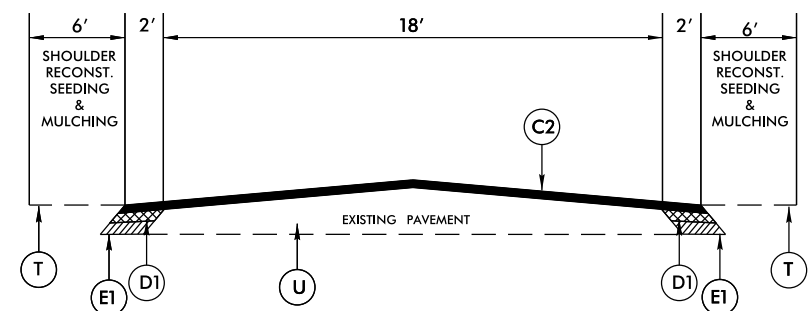
HOKE COUNTY TYPICAL SECTIONS



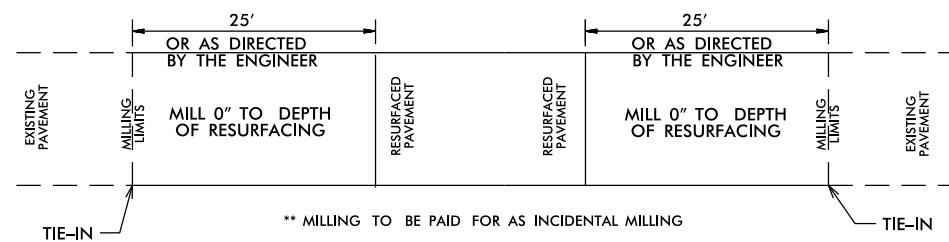
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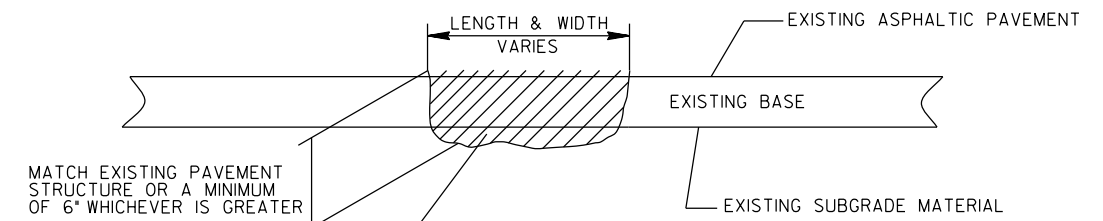
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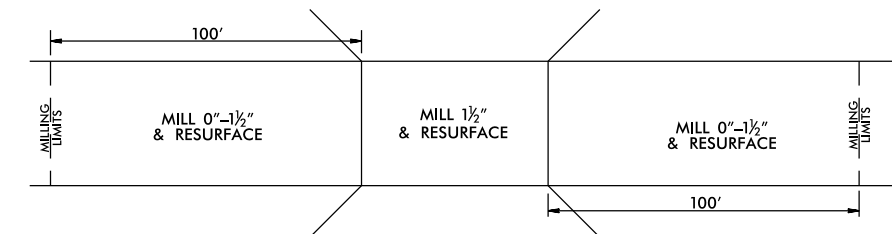
PAVEMENT TIE-IN DETAIL

DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING

DETAIL



SAW AND REMOVE EXISTING ASPHALT PAVEMENT TO NEAT LINES AND REMOVE EXISTING LOOSE BASE AND/OR SUBGRADE MATERIAL AND REPLACE WITH ASPHALT CONCRETE SURFACE COURSE B-25.0 B OR I-19.0 B, AS DIRECTED BY THE ENGINEER



BRIDGE DRAWING FOR SR 1409 (BRIDGE NO 61)

* MILLING SHALL BE PAID FOR UNDER INCIDENTAL MILLING AND AT LOCATIONS AS DIRECTED BY THE ENGINEER.

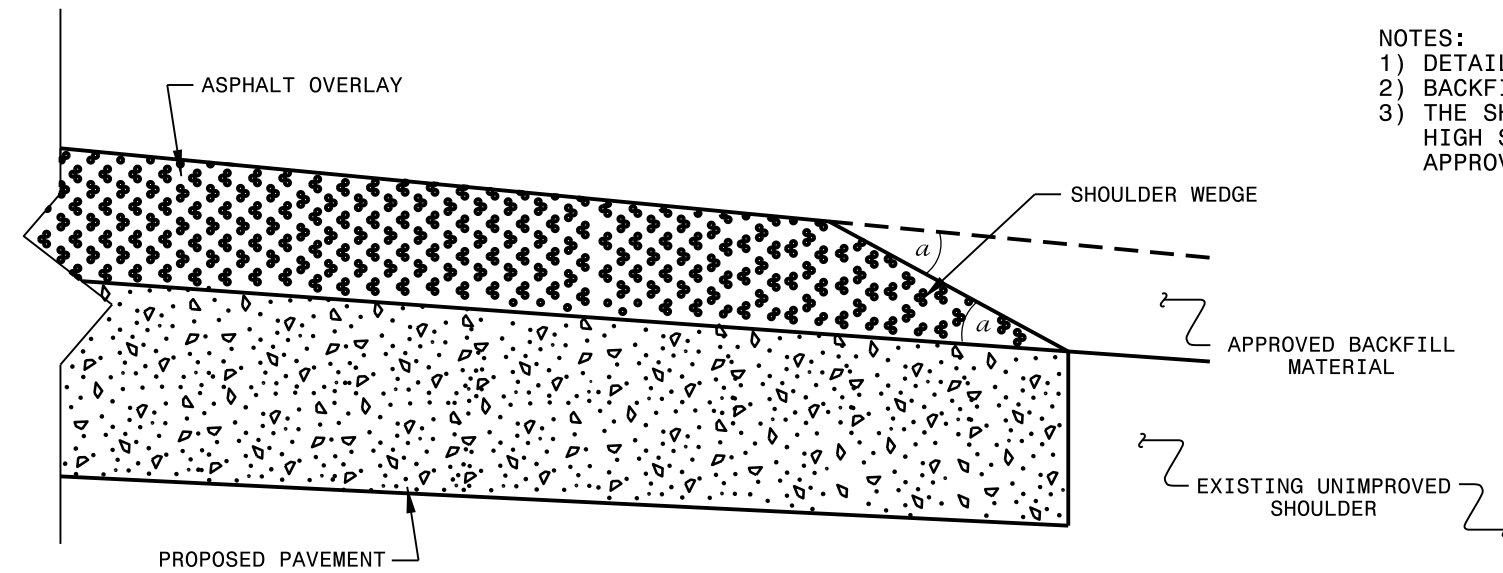
PAVEMENT SCHEDULE

C1	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
D1	PROP. APPROX. 4" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
E1	PROP. APPROX. 5.5" ASPHALT CONC. BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YD.
T	EARTH MATERIAL
U	EXISTING PAVEMENT

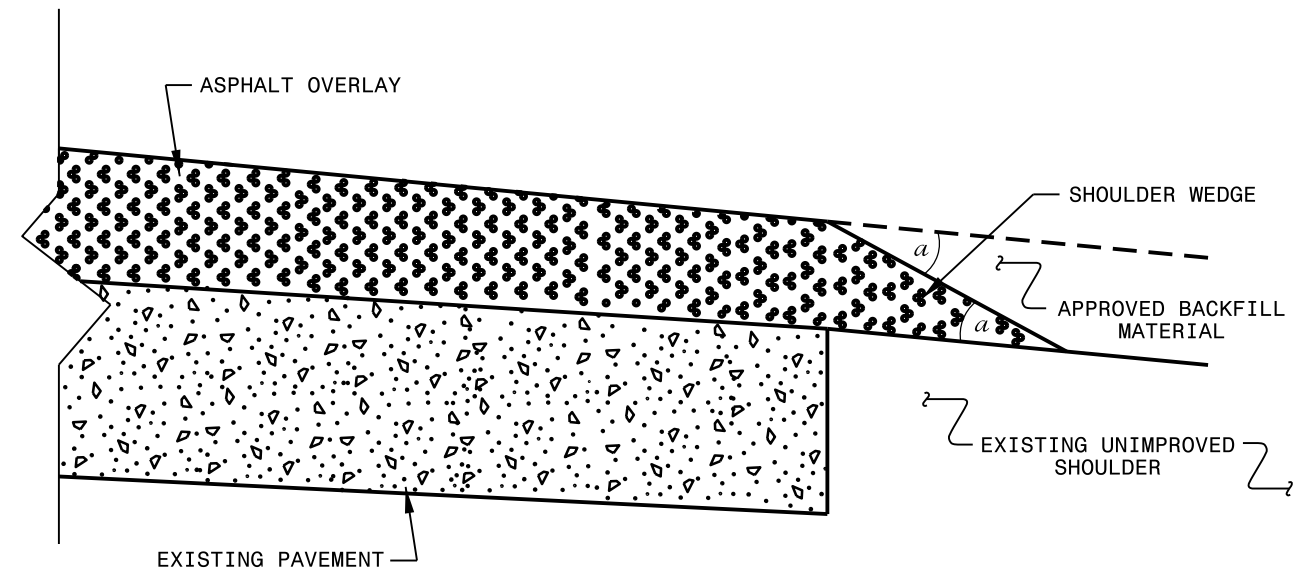
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 2016CPT.08.16.20471

NOTES:

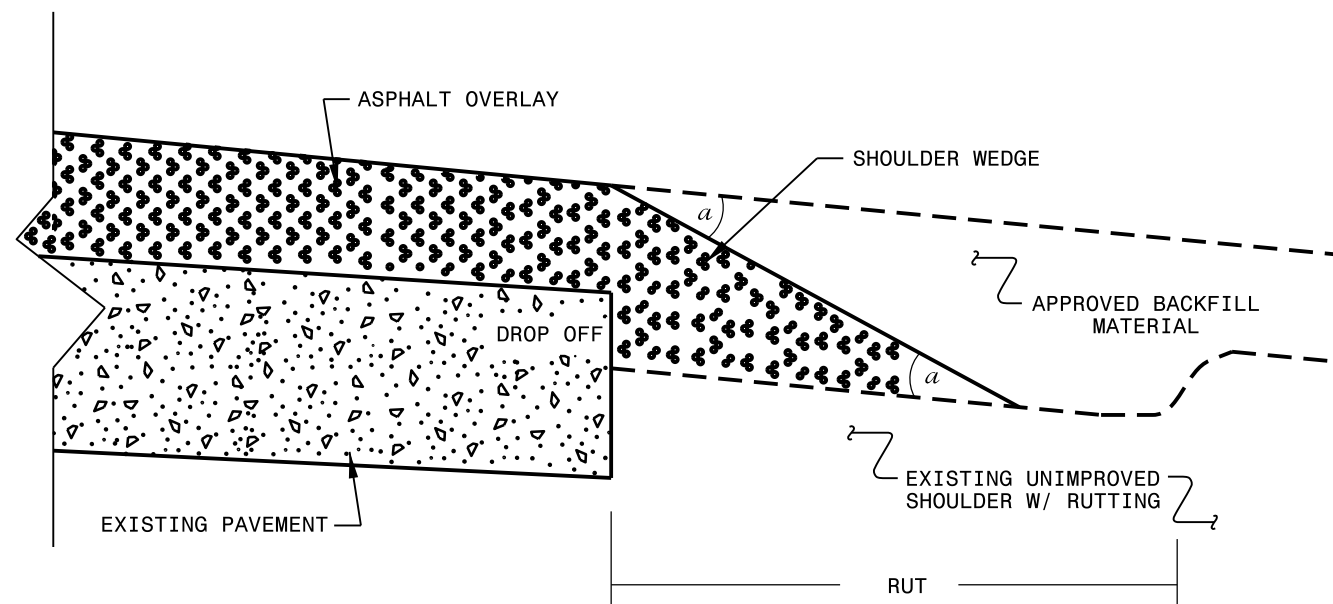
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: szusr/details/stand/shoulderwedgedetail.dgn			

24-MAR-2016 11:45
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 \$\$\$USERNAME\$\$\$

PROJECT NO.	SHEET NO.	TOTAL NO.
2016CPT.08.16.10471, 2016CPT.08.16.20471	6	

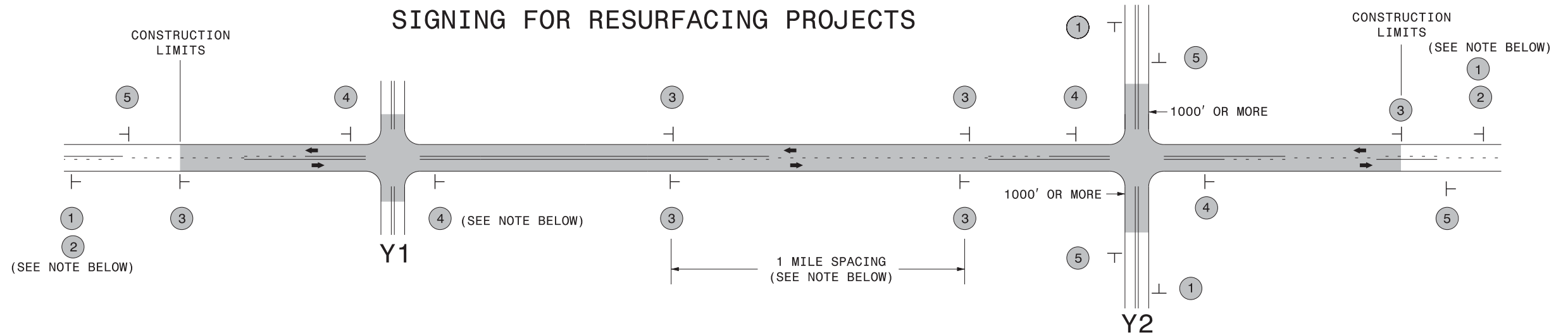
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	BORROW EXCAVATION CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	INCIDENTAL MILLING SY	BASE COURSE, B25.0B TONS	INTERMEDIATE COURSE, I19.0B TONS	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, SF9.5A TON	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	ADJ. OF MAN-HOLES EA	ADJ. OF METER OR VALVE BOXES EA	TEMP. SILT FENCE LF	WATTLE LF	SEED & MULCHING AC	
2016CPT.08.16.10471	Hoke	1	NC 211	FROM CONST JOINT @ BRIDGE NO 4 TO ROBESON COUNTY LINE	1	2	2WU	NO	NO	3.967	24-36	582	575	8.00	430			6,270		376	215			400	60	5.70	
TOTAL FOR PROJ NO. 2016CPT.08.16.10471										3.967		582	575	8.00	430			6,270		376	215			400	60	5.70	
2016CPT.08.16.20471	Hoke	2	SR 1420 (GILLIS HILL RD)	FROM CUMBERLAND COUNTY LINE TO SR 1418 (LINDSAY RD)	2	2	2WU	NO	NO	0.493	24-48	73	20	1.00	120				840	56	500		3	50	10	0.72	
2016CPT.08.16.20471	Hoke	3	SR 1405 (N. MAIN ST)	FROM CONST JOINT @ SR 1300 (VASS RD) TO CONST JOINT BEFORE US 401 BYPASS	2	2	2WU	NO	NO	0.341	24	50	45	0.68	120				480	32	125	1		35	10	0.50	
2016CPT.08.16.20471	Hoke	4	SR 1219 (ARMY RD)	FROM SR 1225 (ASHEMONT RD) TO CONST JOINT @ SR 1214 (CALLOWAY RD)	2	2	2WU	NO	NO	1.077	22	142	90	2.15	75				1,465	98	360			110	20	1.57	
2016CPT.08.16.20471	Hoke	5	SR 1225 (ASHEMONT RD)	FROM US 15-501 TO SR 1219 (ARMY RD)	2	2	2WU	NO	NO	1.97	22	289	150	3.94	260				2,470	165				200	30	2.87	
2016CPT.08.16.20471	Hoke	6	SR 1409 (JOHNSON MILL RD)	FROM CONST JOINT @ US 401 TO SR 1413 (PITTMAN GROVE CHURCH RD)	2,3	2	2WU	NO	NO	1.04	22	153	55	2.08	530	805	585		1,295	150	320		2	105	20	1.51	
2016CPT.08.16.20471	Hoke	7	SR 1409 (JOHNSON MILL RD)	FROM SR 1413 (PITTMAN GROVE CHURCH RD) TO CONST JOINT @ US 401	2	2	2WU	NO	NO	2.534	20	372	60	5.07	648				2,980	200	575			255	40	3.69	
TOTAL FOR PROJ NO. 2016CPT.08.16.20471										7.455		1,079	420	14.92	1,753	805	585		9,530	701	1,880	1	5	755	130	10.86	
GRAND TOTAL										11.422		1,661	995	22.92	2,183	805	585		6,270	9,530	1,077	2,095	1	5	1,155	190	16.56

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-E	4685000000-E	4686000000-E		4695000000-E		4710000000-E	4725000000-E			4900000000-N	
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING SF	TEMP. TRAFFIC CONTROL LS	4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	4" X 120 M WHITE THERMO LF	8" X 90 M WHITE THERMO LF	8" X 90 M YELLOW THERMO LF	24" X 120 M WHITE THERMO LF	THERMO LT ARROW 90 M EA	THERMO STR ARROW 90 M EA	THERMO RT ARROW 90 M EA	CRYSTAL & RED MARKERS EA	
2016CPT.08.16.10471	Hoke	1	NC 211	FROM CONST JOINT @ BRIDGE NO 4 TO ROBESON COUNTY LINE	1	2	2WU	3.967	24-36	445	*	41,890	25,884	650	75	112	70	7				265
TOTAL FOR PROJ NO. 2016CPT.08.16.10471								3.967		445		41,890	26,534	650	187	70	7				265	
2016CPT.08.16.20471	Hoke	2	SR 1420 (GILLIS HILL RD)	FROM CUMBERLAND COUNTY LINE TO SR 1418 (LINDSAY RD)	2	2	2WU	0.493	24-48	56	*	5,205	6,560	590	250			5	2	2	35	
2016CPT.08.16.20471	Hoke	3	SR 1405 (N. MAIN ST)	FROM CONST JOINT @ SR 1300 (VASS RD) TO CONST JOINT BEFORE US 401 BYPASS	2	2	2WU	0.341	24	39		3,600	3,600								25	
2016CPT.08.16.20471	Hoke	4	SR 1219 (ARMY RD)	FROM SR 1225 (ASHEMONT RD) TO CONST JOINT @ SR 1214 (CALLOWAY RD)	2	2	2WU	1.077	22	121		11,372	10,460									
2016CPT.08.16.20471	Hoke	5	SR 1225 (ASHEMONT RD)	FROM US 15-501 TO SR 1219 (ARMY RD)	2	2	2WU	1.97	22	221		20,822	17,344									
2016CPT.08.16.20471	Hoke	6	SR 1409 (JOHNSON MILL RD)	FROM CONST JOINT @ US 401 TO SR 1413 (PITTMAN GROVE CHURCH RD)	2,3	2	2WU	1.04	22	117		10,986	7,810				40					
2016CPT.08.16.20471	Hoke	7	SR 1409 (JOHNSON MILL RD)	FROM SR 1413 (PITTMAN GROVE CHURCH RD) TO CONST JOINT @ US 401	2	2	2WU	2.534	20	284		26,762	19,210				50					
TOTAL FOR PROJ NO. 2016CPT.08.16.20471								7.455		838		78,747	64,984	590	250		90	5	2	2	60	
GRAND TOTAL								11.422		1,283	1	120,637	90,868	1,240	325	112	160	12	2	2	325	
												92,108		437				16				

SIGNING FOR RESURFACING PROJECTS



LEGEND	
┆	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" </div> <div style="text-align: center;"> W20-7 A 48" X 48" </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	2	 W7-3aP 24" X 18"	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3	 SP 13107 48" X 48"	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
	4	 SP 13106 48" X 48"	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.		

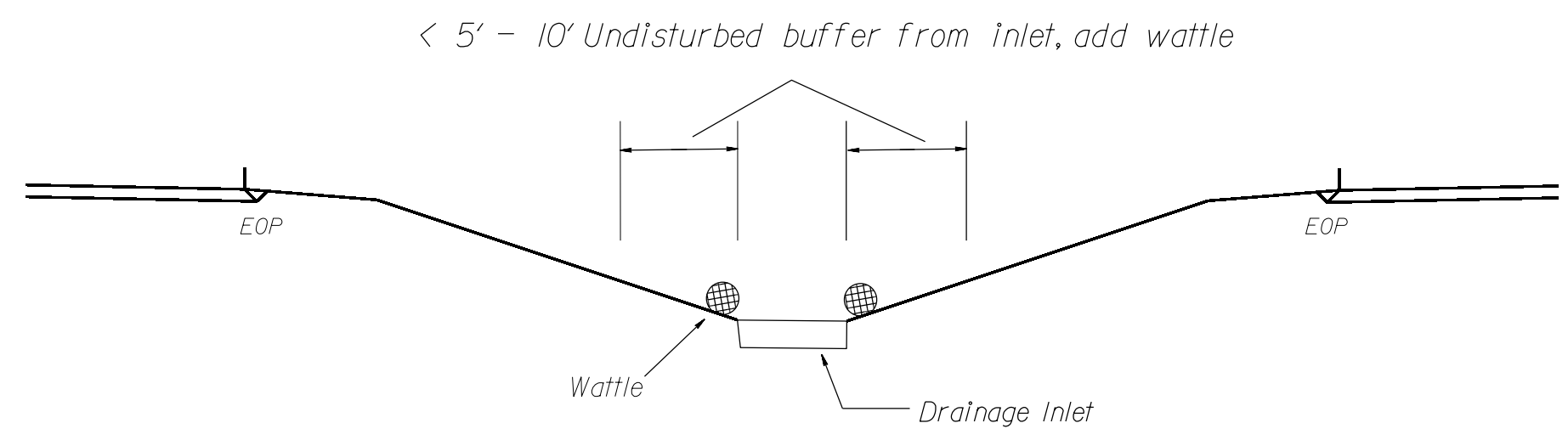
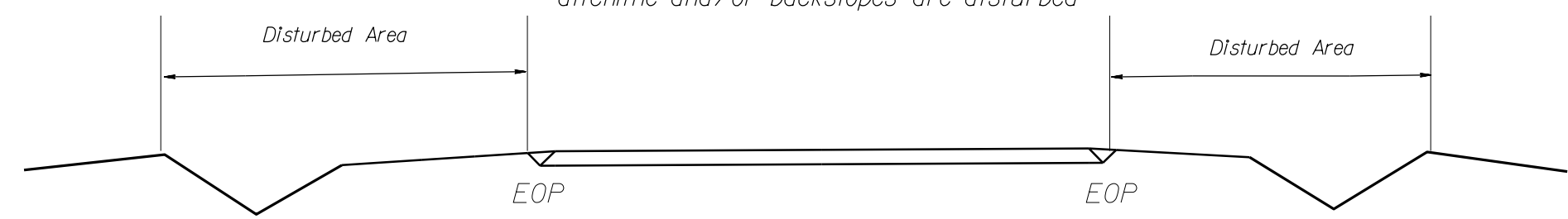
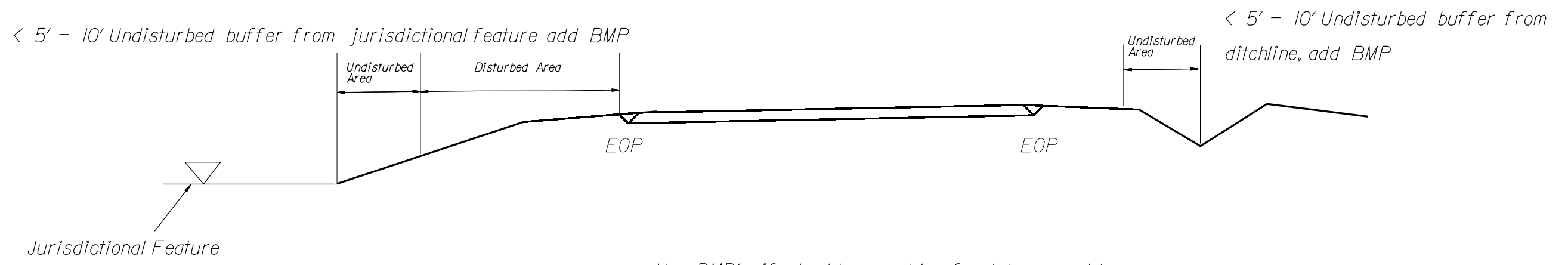
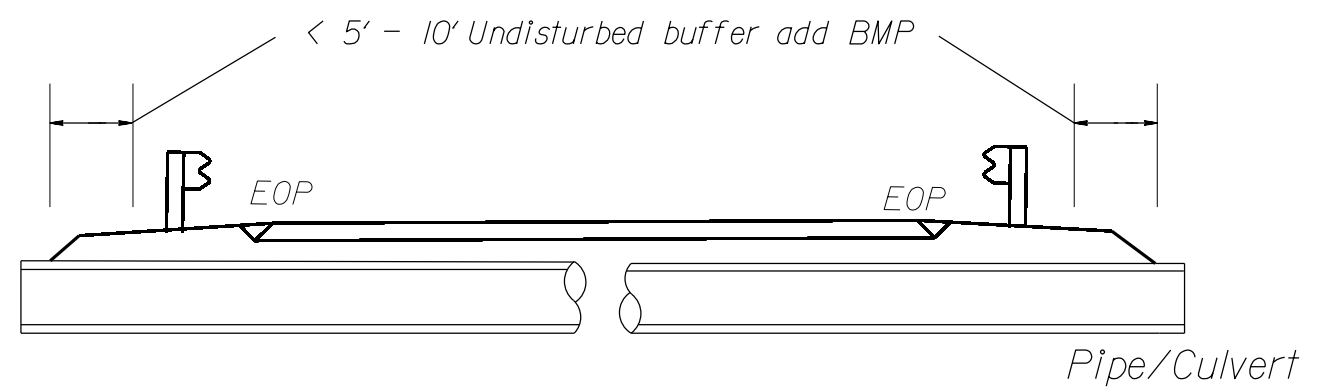
3/19/2015
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 User:rmgarrett

**RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS**

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

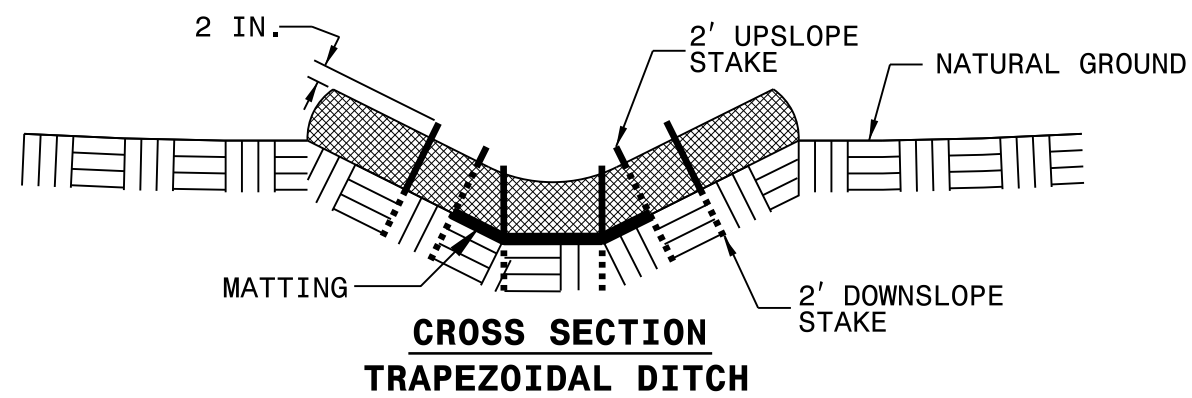
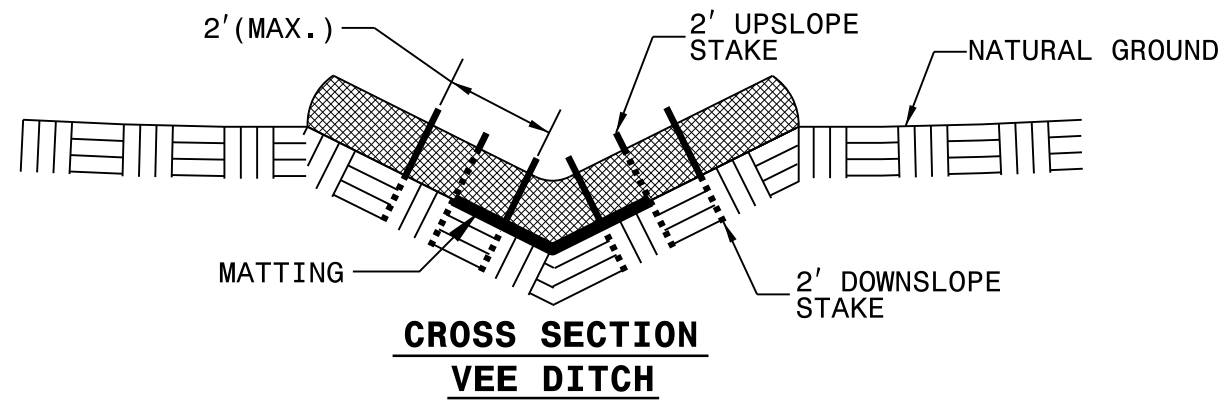
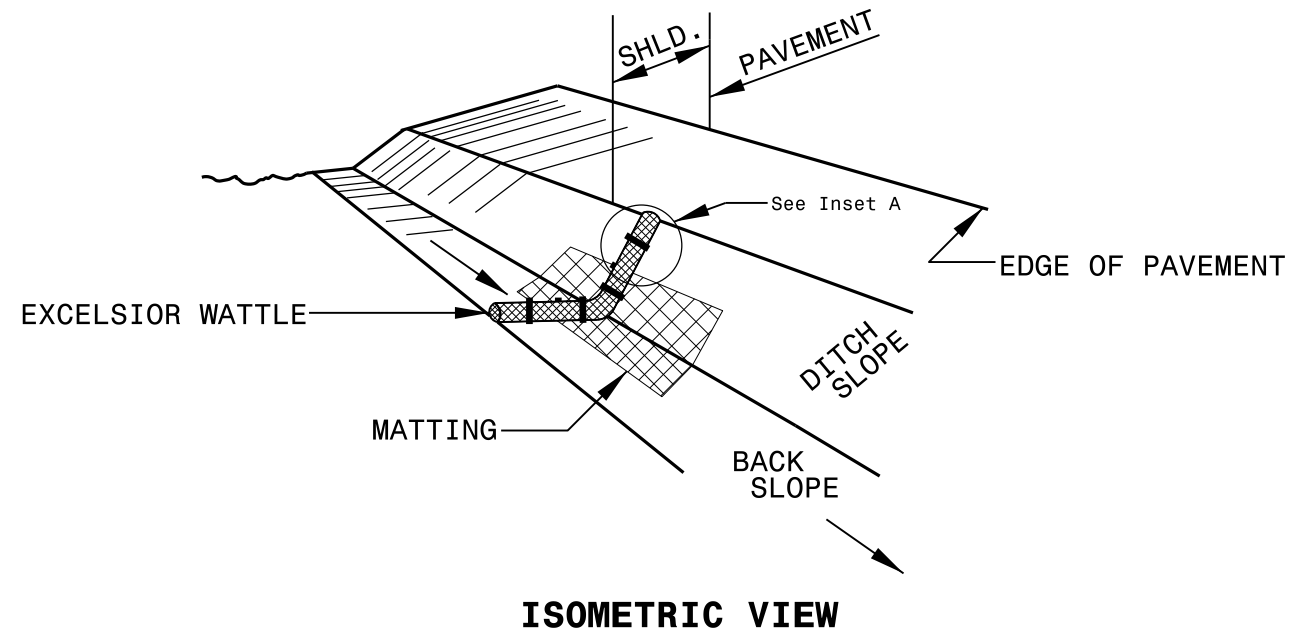
EROSION CONTROL DETAIL

BMP Options: Wattle or Silt Fence



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

