

TOTAL BILL OF MATERIAL

	CONST. MAINT. & REMOVAL OF TEMPORARY STRUCTURE	CONST. MAINT. & REMOVAL OF TEMP. ACCESS	REMOVAL OF EXISTING STRUCTURE	3'-0" Ø DRILLED PIERS IN SOIL	3'-0" Ø DRILLED PIERS NOT IN SOIL	PERMANENT STEEL CASING FOR 3'-0" Ø DRILLED PIER	SID INSPECTIONS	SPT TESTING	CSL TESTING	UNCLASSIFIED STRUCTURE EXCAVATION	CONCRETE WEARING SURFACE	GROOVING BRIDGE FLOORS	CLASS A CONCRETE	BRIDGE APPROACH SLABS	REINFORCING STEEL	SPIRAL COLUMN REINFORCING STEEL	HP 12 X 53 STEEL PILES	VERTICAL CONCRETE BARRIER RAIL	RIP RAP CLASS II (2'-0" THICK)	GEOTEXTILE FOR DRAINAGE	ELASTOMERIC BEARINGS	3'-0" X 1'-9" PRESTRESSED CONCRETE CORED SLABS	3'-0" X 2'-0" PRESTRESSED CONCRETE CORED SLABS	ASBESTOS ASSESSMENT				
	LUMP SUM	LUMP SUM	LUMP SUM	LINE FT.	LINE FT.	LINE FT.	EA.	EA.	EA.	LUMP SUM	SQ. FT.	SQ. FT.	CU. YDS.	LUMP SUM	LBS.	LBS.	NO.	LINE FT.	LINE FT.	TON	SQ. YD.	LUMP SUM	NO.	LINE FT.	NO.	LINE FT.	LUMP SUM	
SUPERSTRUCTURE											4,577	4,840		LUMP SUM					270.87			LUMP SUM	24	840	12	780		
END BENT 1													25.5		3,105		7	175		60	65							
BENT 1				83.0	22.0	57.0							18.7		11,015	1,965												
BENT 2				55.0	28.0	33.3							21.9		11,917	1,898												
END BENT 2													25.5		3,105		7	180		60	65							
TOTAL	LUMP SUM	LUMP SUM	LUMP SUM	138.0	50.0	90.3	1	2	2	LUMP SUM	4,577	4,840	91.6	LUMP SUM	29,142	3,863	14	355	270.87	120	130	LUMP SUM	24	840	12	780	LUMP SUM	

NOTES:

ASSUMED LIVE LOAD = HL-93 OR ALTERNATE LOADING.

THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.

THIS BRIDGE IS LOCATED IN SEISMIC ZONE 1.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

FOR EROSION CONTROL MEASURES, SEE EROSION CONTROL PLANS.

THIS STRUCTURE HAS BEEN DESIGNED IN ACCORDANCE WITH HEC 18, "EVALUATING SCOUR AT BRIDGES", MAY, 2001.

THE MATERIAL SHOWN IN THE CROSS-HATCHED AREA ON SHEET S-1 SHALL BE EXCAVATED FOR A DISTANCE OF 32 FT. EACH SIDE OF CENTERLINE ROADWAY AT END BENT 1 AND 30 FT. LEFT SIDE, 25 FT. RIGHT SIDE OF CENTERLINE ROADWAY AT END BENT 2 AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR UNCLASSIFIED STRUCTURE EXCAVATION. SEE SECTION 412 OF THE STANDARD SPECIFICATIONS.

INASMUCH AS THE PAINT SYSTEM ON THE EXISTING STRUCTURAL STEEL CONTAINS LEAD, THE CONTRACTOR'S ATTENTION IS DIRECTED TO ARTICLE 107-1 OF THE STANDARD SPECIFICATIONS. ANY COSTS RESULTING FROM COMPLIANCE WITH APPLICABLE STATE OR FEDERAL REGULATIONS PERTAINING TO HANDLING OF MATERIALS CONTAINING LEAD BASED PAINT SHALL BE INCLUDED IN THE BID PRICE FOR "REMOVAL OF EXISTING STRUCTURE AT STATION 19+12.00 -L-".

THE EXISTING STRUCTURE CONSISTING OF 3 SPANS: 2 @ 30'-0", 1 @ 50'-0", WITH A 4" AWS CLEAR ROADWAY WIDTH OF 22'-0" AND REINFORCED CONCRETE DECK ON I-BEAMS; WITH SUBSTRUCTURE CONSISTING OF END BENTS AND TWO INTERIOR BENTS WITH REINFORCED CONCRETE CAPS AND TIMBER PILES AND SEVEN STEEL CRUTCH BENTS LOCATED AT MIDSPAN AND ST INTERIOR BENTS AT THE PROPOSED STRUCTURE SITE SHALL BE REMOVED. THE EXISTING BRIDGE IS PRESENTLY NOT POSTED. FOR REMOVAL OF EXISTING STRUCTURE, SEE SPECIAL PROVISIONS.

REMOVAL OF THE EXISTING BRIDGE SHALL BE PERFORMED SO AS NOT TO ALLOW DEBRIS TO FALL INTO THE WATER. THE CONTRACTOR SHALL REMOVE THE BRIDGE AND SUBMIT PLANS FOR DEMOLITION IN ACCORDANCE WITH ARTICLE 402-2 OF STANDARD SPECIFICATIONS.

THE SUBSTRUCTURE OF THE EXISTING BRIDGE INDICATED ON THE PLANS IS FROM THE BEST INFORMATION AVAILABLE. SINCE THIS INFORMATION IS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR, THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN THE EXISTING BRIDGE SUBSTRUCTURE SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

AT THE CONTRACTOR'S OPTION, AND UPON REMOVAL OF THE CAUSEWAY, THE CLASS II RIP RAP USED IN THE CAUSEWAY MAY BE PLACED AS RIP RAP SLOPE PROTECTION. SEE SPECIAL PROVISIONS FOR CONSTRUCTION, MAINTENANCE AND REMOVAL OF TEMPORARY ACCESS AT STATION 19+12.00 -L-.

THE CONTRACTOR'S ATTENTION IS DRAWN TO THE FACT THAT A 115KV TRANSMISSION LINE NEAR END BENT 2 SHALL REMAIN IN PLACE. FOR "CONSTRUCTABILITY IN CLOSE PROXIMITY OF OVERHEAD POWER LINES", SEE SPECIAL PROVISIONS.

THE CONTRACTOR WILL BE REQUIRED TO CONSTRUCT, MAINTAIN AND AFTERWARDS REMOVE A TEMPORARY STRUCTURE AT STATION 14+91.00 -LDET- FOR USE DURING CONSTRUCTION OF THE PROPOSED STRUCTURE. FOR CONSTRUCTION, MAINTENANCE AND REMOVAL OF TEMPORARY STRUCTURE, SEE SPECIAL PROVISIONS.

THE BRIDGE RAILS ON THE TEMPORARY STRUCTURE SHALL BE DESIGNED FOR THE AASHTO LRFD TEST LEVEL 3 (TL-3) CRASH TEST CRITERIA. FOR CONSTRUCTION, MAINTENANCE AND REMOVAL OF TEMPORARY STRUCTURE, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

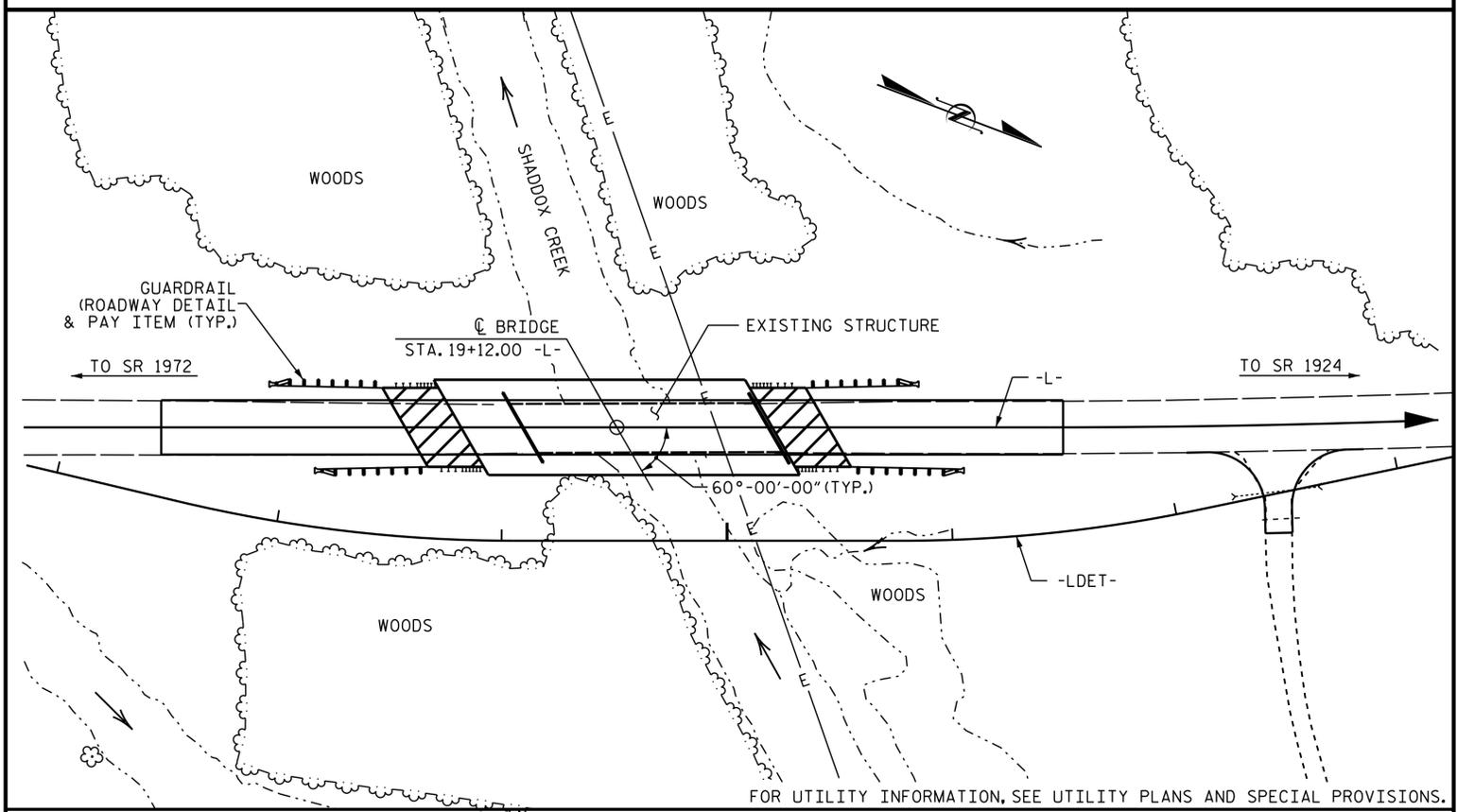
FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

ALL PAVEMENT MARKING WILL BE IN ACCORDANCE WITH THE PAVEMENT MARKING PLANS AND SHALL PROVIDE FOR BICYCLES.

FOR ASBESTOS ASSESSMENT FOR BRIDGE DEMOLITION AND RENOVATION ACTIVITIES, SEE SPECIAL PROVISIONS.

BM #2: RAILROAD SPIKE IN BASE OF POWER POLE, 260.8' LEFT OF STA. 20+72.56 -L-, EL. 171.04'.



LOCATION SKETCH

HYDRAULIC DATA

DESIGN DISCHARGE	=	2800 CFS
FREQUENCY OF DESIGN FLOOD	=	25 YRS.
DESIGN HIGH WATER ELEVATION	=	172.4
DRAINAGE AREA	=	15.0 SQ. MI.
BASIC DISCHARGE (Q100)	=	3900 CFS
BASIC HIGH WATER ELEVATION	=	174.1

OVERTOPPING FLOOD DATA

OVERTOPPING DISCHARGE	=	2247 CFS
FREQUENCY OF OVERTOPPING FLOOD	=	10 + YRS.
OVERTOPPING FLOOD ELEVATION	=	171.1 (ROADWAY STA. 12+00)

DRAWN BY : S. WANCE DATE : 03/14
 CHECKED BY : W. F. PARKER DATE : 09/15

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DocuSigned by:
 Ting Fang
 E7208400977435 4/11/2016

PROJECT NO. B-4461
CHATHAM COUNTY
 STATION: 19+12.00 -L-

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
GENERAL DRAWING
 FOR BRIDGE ON
 SR 1916 (CORINTH RD.) OVER
 SHADDOX CREEK BETWEEN
 SR 1972 AND SR 1924

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-3
1			3			TOTAL SHEETS 23
2			4			

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED