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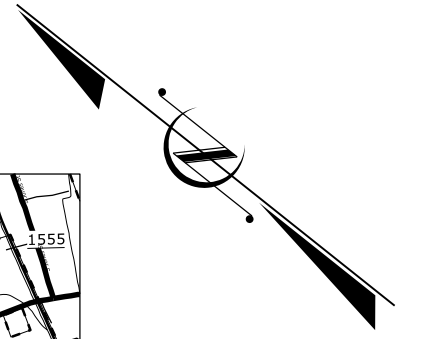
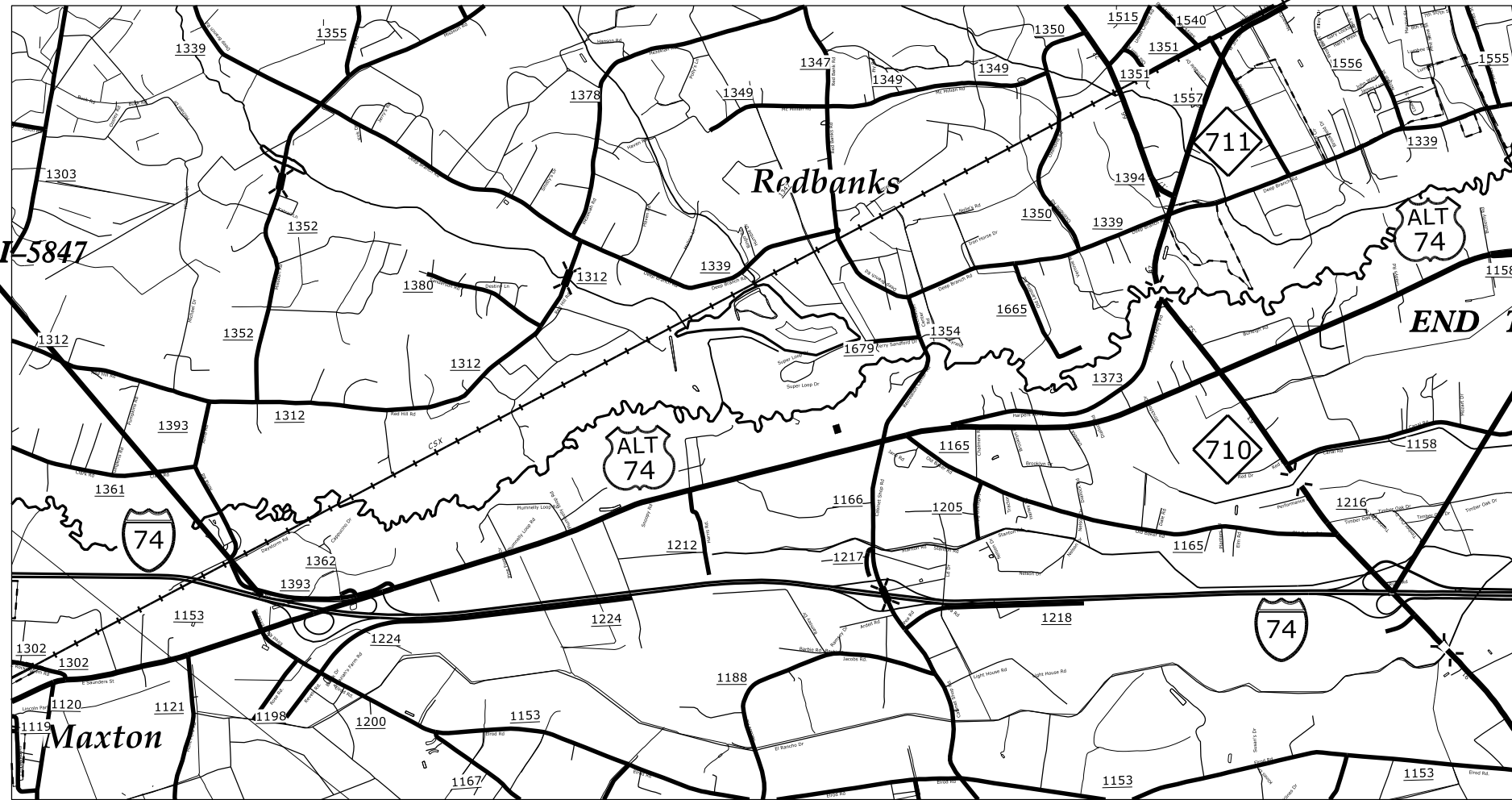
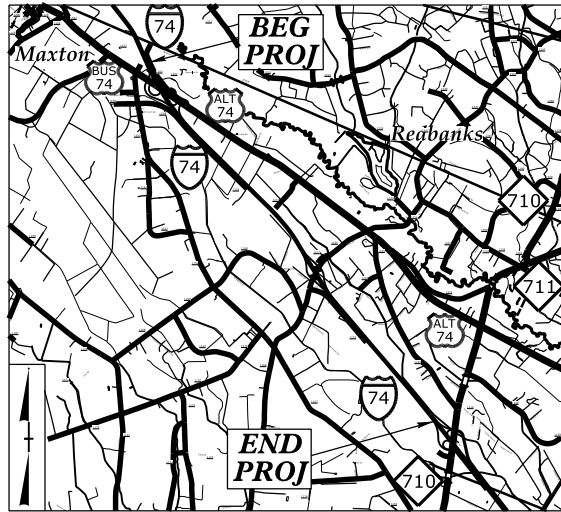
STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5847	1	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
53052.1.FS1	NHPP-0074(183)	P.E.	
53052.3.FS1	NHPP-0074(183)	CONSTR.	

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

ROBESON COUNTY

LOCATION: I-74 FROM US 74 ALTERNATE TO NC 710

TYPE OF WORK: PAVEMENT REHABILITATION



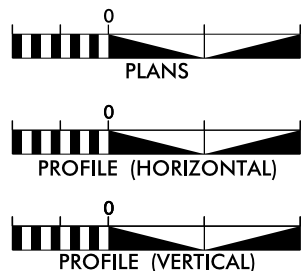
TIP PROJECT: I-5847

CONTRACT: C203841

BEGIN TIP PROJECT I-5847
-L- STA 10+00.00

END TIP PROJECT I-5847
-L- STA 353+20.00

GRAPHIC SCALES



DESIGN DATA

ADT 2016 = 10600

PROJECT LENGTH

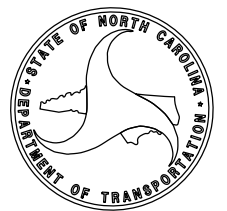
TOTAL PROJECT LENGTH = 6.5 MILES

Prepared in the Office of:
DIVISION OF HIGHWAYS
1000 Birch Ridge Dr., Raleigh NC, 27610

2012 STANDARD SPECIFICATIONS

RIGHT OF WAY DATE:
NA

LETTING DATE:
MAY 17, 2016



PROJECT NO.	SHEET NO.
I-5847	3B-2

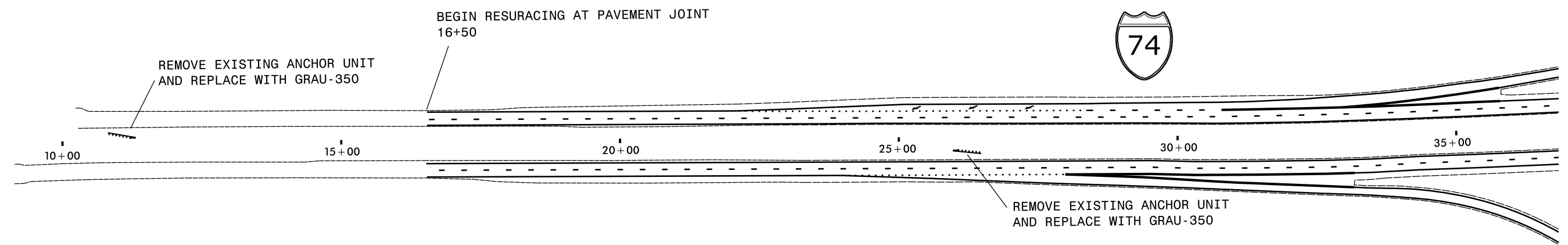
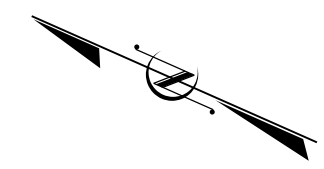
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	AGGREGATE SHOULDER BORROW TON	SHOULDER RECONSTRUCTION SMI	2" MILLING SY	0" TO 2" MILLING SY	INCIDENTAL MILLING SY	ASPHALT CONCRETE SURFACE COURSE, S9.5C TONS	ASPHALT CONCRETE SURFACE COURSE, TYPE SA-1 TON	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TON	MILLED RUMBLE STRIPS (ASPHALT CEMENT CONCRETE) LF	GUARDRAIL ANCHOR UNITS, TYPE 350 EA	REMOVE EXISTING GUARDRAIL LF	SEED & MULCHING AC
I-5847	Robeson	1	I-74	FROM ALT. 74 MP 193.3 TO NC 710 MP 199.8	1 & 2	4	MD	NO	NO	6.5	80	5,901	35.40	3,970	12,674	612	42,161	200	2,501	155	137,280	3	150	1.00
TOTAL FOR MAP NO. 1										6.5		5,901	35.40	3,970	12,674	612	42,161	200	2,501	155	137,280	3	150	1.00
TOTAL FOR PROJ NO. I-74										6.5		5,901	35.40	3,970	12,674	612	42,161	200	2,501	155	137,280	3	150	1.00
GRAND TOTAL										6.5		5,901	35.40	3,970	12,674	612	42,161	200	2,501	155	137,280	3	150	1.00

PROJECT NO. I-5847	SHEET NO. 3B-3
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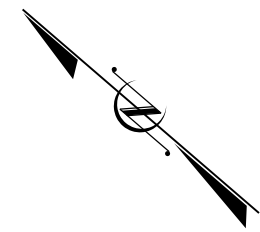
THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4400000000-E STATIONARY WORK ZONE SIGN	4405000000-E PORTABLE WORK ZONE SIGN	4410000000-E BARRICADE MOUNTED WORK ZONE SIGN	4415000000-N FLASHING ARROW BOARD	4420000000-N PORTABLE CHANGEABLE MESSAGE SIGN	4422000000-N PORTABLE CHANGEABLE MESSAGE SIGN (SHORT TERM)	4430000000-N DRUMS	4435000000-N CONES	4445000000-E BARRICADES (TYPE III)	4450000000-N FLAGGERS	4480000000-N TMA	4510000000-N LAW ENFORCE- MENT	4650000000-N TEMPORARY RAISED PAVEMENT MARKERS	4688000000-E THERMO- PLASTIC PAVEMENT MARKING LINES (6", 90 MILS)	4690000000-E THERMO- PLASTIC PAVEMENT MARKING LINES (6", 120 MILS)	4695000000-E THERMO- PLASTIC PAVEMENT MARKING LINES (8", 90 MILS)	4700000000-E THERMO- PLASTIC PAVEMENT MARKING LINES (12", 90 MILS)	4702000000-E THERMO- PLASTIC PAVEMENT MARKING LINES (12", 120 MILS)	4710000000-E THERMO- PLASTIC PAVEMENT MARKING LINES (24", 120 MILS)	4725000000-E THERMO- PLASTIC PAVEMENT MARKING SYMBOL (90 MILS)	4815000000-E PAINT PAVEMENT MARKING LINES (6")	4820000000-E PAINT PAVEMENT MARKING LINES (8")	4825000000-E PAINT PAVEMENT MARKING LINES (12")	4835000000-E PAINT PAVEMENT MARKING LINES (24")	4845000000-N PAINT PAVEMENT MARKING SYMBOL	4855000000-E REMOVAL OF PAVEMENT MARKING LINES (6")	4905000000-N SNOW- PLOWABLE PAVEMENT MARKERS
I-5847	Robeson	1	I-74	FROM ALT. 74 MP 193.3 TO NC 710 MP 199.8	1 & 2	4	MD	6.5	80	244	571	44	2	4	80	200	40	32	80.00	1.00	80.00	900.00	154,013	17,572	435	5,841	200	204	26	350,000	435	641	204	26	175,000	1,435
TOTAL FOR MAP NO. 1										244	571	44	2	4	80	200	40	32	80	1	80	900	154,013	17,572	435	5,841	200	204	26	350,000	435	641	204	26	175,000	1,435
TOTAL FOR PROJ NO. I-74										244	571	44	2	4	80	200	40	32	80	1	80	900	154,013	17,572	435	5,841	200	204	26	350,000	435	641	204	26	175,000	1,435
GRAND TOTAL										244	571	44	2	4	80	200	40	32	80	1	80	900	154,013	17,572	435	5,841	200	204	26	350,000	435	641	204	26	175,000	1,435

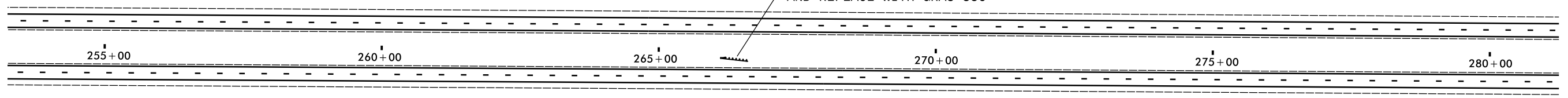


REVISIONS

24-FEB-2016 13:30
8/17/99
C:\Users\robson\OneDrive\Documents\Projects\74\74-Resurface and Bridges\Robeson Co\Proj\1-5847_Rdy.psh-4.dgn



REMOVE EXISTING ANCHOR UNIT
AND REPLACE WITH GRAU-350



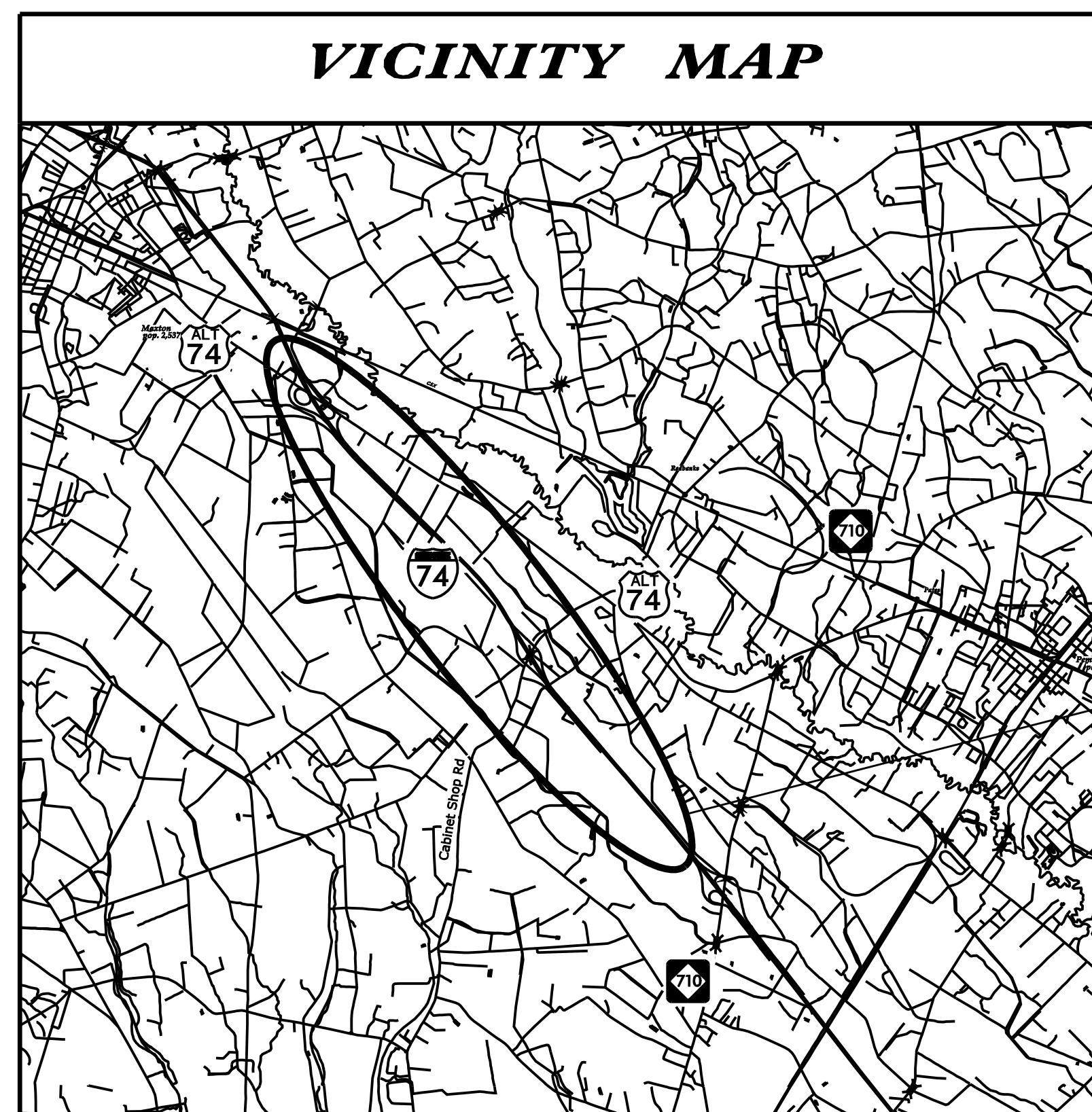
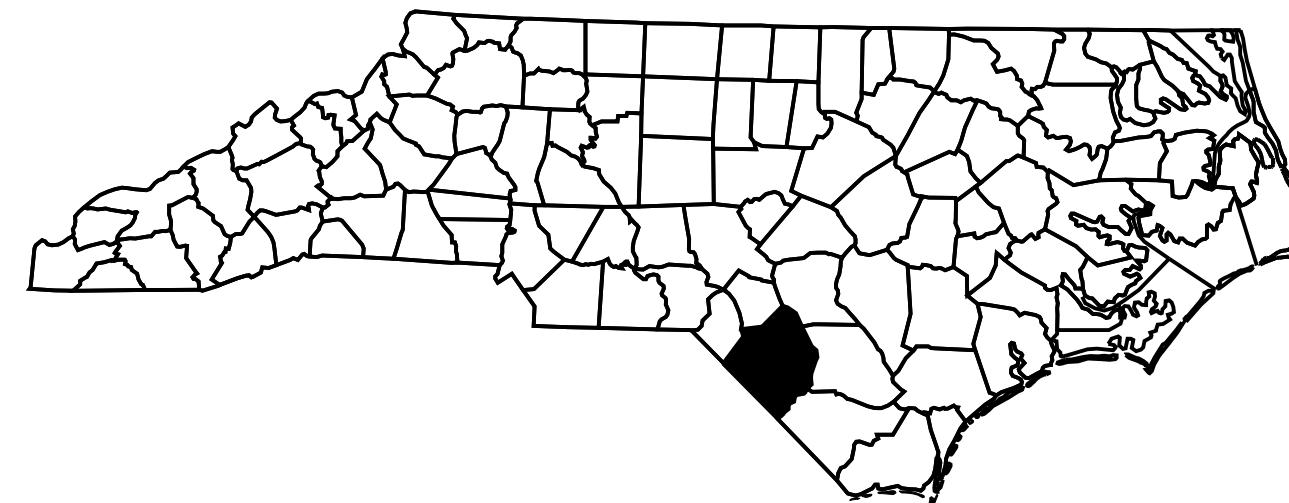
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REVISIONS

8/17/99

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

ROBESON COUNTY



APPROXIMATE
PROJECT LOCATION

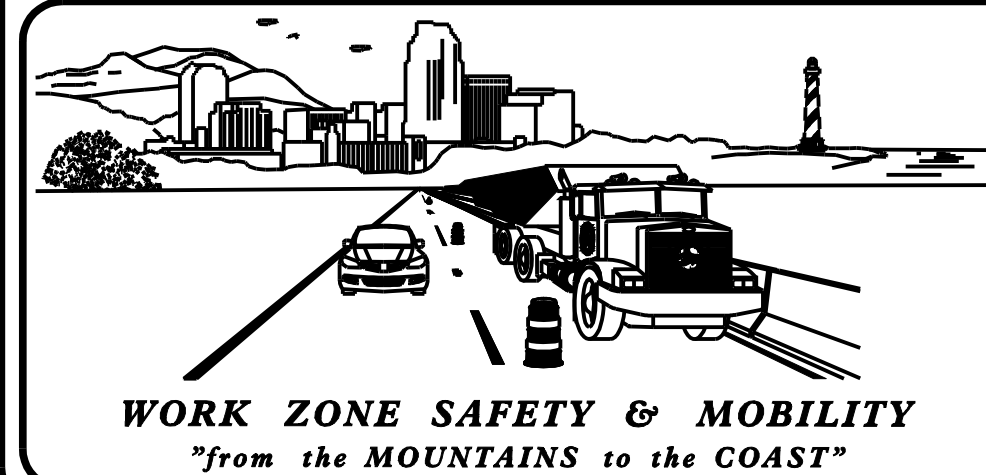
INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-2	TRANSPORTATION OPERATIONS PLAN: (WORK ZONE TRAFFIC CONTROL REQUIREMENTS)
TMP-2A AND TMP-2B	OFF SITE DETOUR DETAILS
TMP-2C	RESURFACING ADVANCE WARNING SIGNS FOR HIGH SPEED FACILITIES >= 60 MPH
TMP-2D	WORK ZONE VARIABLE SPEED LIMIT DETAIL
TMP-3	WORK ZONE TRAFFIC CONTROL NARRATIVE

SHEET NO.
TMP-1

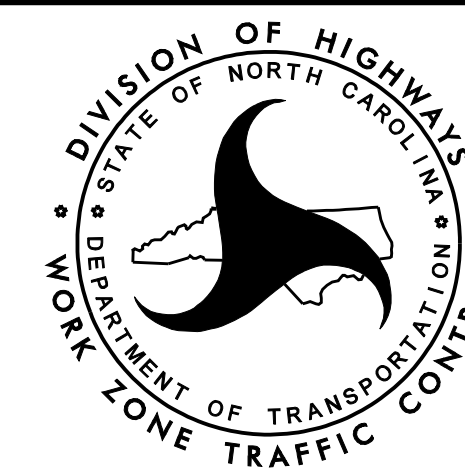
I-5847

TIP PROJECT:



N.C.D.O.T. WORK ZONE TRAFFIC CONTROL
1561 MAIL SERVICE CENTER (MSC) RALEIGH, NC 27699-1561
750 N. GREENFIELD PARKWAY, GARNER, NC 27529 (DELIVERY)
PHONE: (919) 773-2800 FAX: (919) 771-2745

J. S. BOURNE, P.E. STATE TRAFFIC MANAGEMENT ENGINEER
STEVE KITE, P.E. TRAFFIC CONTROL PROJECT ENGINEER
DON PARKER, P.E. TRAFFIC CONTROL PROJECT DESIGN ENGINEER
TRAFFIC CONTROL DESIGN ENGINEER



PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE
DESIGN GROUP, INC.

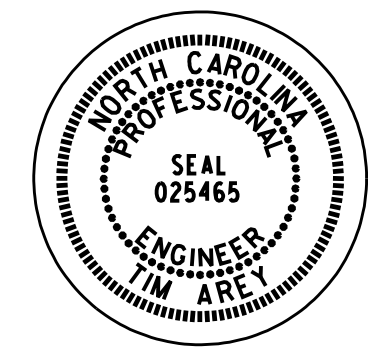


ENGINEERS • CONSULTANTS

PH: 704 573 3003

APPROVED: *Tom Arsy*
DATE: 3/1/2016

SEAL



ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1180.01	SKINNY - DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXIT AND ENTRANCE RAMP
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)

- WORK AREA
- REMOVAL
- TEMPORARY PAVEMENT
- ON-GOING CONSTRUCTION

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM SKINNY DRUM TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

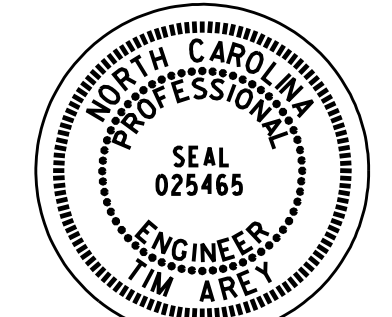
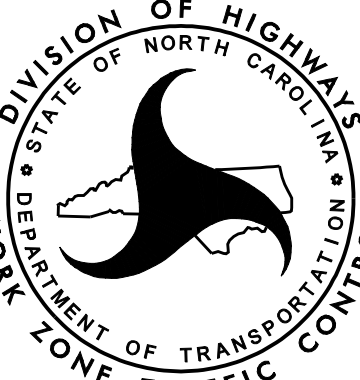
- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS PH 704 573 3003</p>	<p>APPROVED: <i>Ann Gray</i> DATE: 3/1/2016</p> <p>SEAL</p> 		<p>ROADWAY STANDARD DRAWINGS & LEGEND</p>
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WORK ZONE TRAFFIC CONTROL GENERAL REQUIREMENTS

PROJ. REFERENCE NO.	SHEET NO.
I-5847	TMP-2

GENERAL PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF MILLING UNDER BRIDGES AND IN TIE-IN AREAS, RESURFACING THE MAINLINE AND RAMPS, AND PLACING LONGLIFE PAVEMENT MARKINGS AND SNOWPLOWABLE MARKERS. THE CONSTRUCTION OPERATIONS ALONG THE MAINLINE WILL BE ACCOMPLISHED USING LANE CLOSURES. CONSTRUCTION OPERATIONS ON THE RAMPS AND LOOPS WILL BE ACCOMPLISHED WITH OFFSITE DETOURS AS SHOWN IN THESE PLANS.

LANE CLOSURE RESTRICTIONS

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS (SEE SPECIAL PROVISION, INTERMEDIATE CONTRACT TIME AND LIQUIDATED DAMAGES)

A) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME

I-74
ALL I-74 RAMPS/LOOPS

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S DAY, BETWEEN THE HOURS OF 7:00AM DECEMBER 31st AND 6:00PM JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 6:00PM THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 7:00AM THURSDAY AND 6:00PM TUESDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 7:00AM FRIDAY AND 6:00PM WEDNESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 7:00AM THE FRIDAY BEFORE THE WEEK OF INDEPENDENCE DAY AND 6:00PM THE FOLLOWING MONDAY AFTER THE WEEK OF INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 7:00AM FRIDAY AND 6:00PM WEDNESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 7:00AM TUESDAY AND 6:00PM MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 7:00AM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00PM THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- B) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- C) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

WORK ZONE SIGNING:

(A) INSTALLATION

IF THERE IS A PERIOD OF CONSTRUCTION INACTIVITY LONGER THAN 14 CALENDAR DAYS, REMOVE OR COVER ADVANCE/GENERAL WARNING WORK ZONE SIGNS. UNCOVER ADVANCE/GENERAL WARNING WORK ZONE SIGNS NO MORE THAN 7 CALENDAR DAYS BEFORE WORK RESUMES. ALL OTHER OPERATIONS MAY BE SUSPENDED UPON FAILURE TO COMPLY WITH THE ABOVE REQUIREMENTS. SUCH SUSPENDED OPERATIONS WOULD NOT BE RESUMED UNTIL THE ABOVE REQUIREMENTS ARE FULFILLED.

(B) SIGN REMOVAL

ALL STATIONARY WORK ZONE SIGNS SHALL BE REMOVED ONCE THE PROJECT IS SUBSTANTIALLY COMPLETE. THE PROJECT IS SUBSTANTIALLY COMPLETE WHEN THE RESURFACING OPERATIONS ARE COMPLETED AND THE SHOULDERS ARE BROUGHT UP TO THE SAME ELEVATION AS THE PROPOSED PAVEMENT AND WHEN PAVEMENT MARKINGS ARE INSTALLED. THE PAVEMENT MARKING DOESN'T HAVE TO BE THE FINAL MARKING MATERIAL TO BE CONSIDERED SUBSTANTIALLY COMPLETE. ANY REMAINING PUNCH LIST ITEMS ARE TO BE COMPLETED WITH PORTABLE WORK ZONE SIGNING.

(C) LANE CLOSURE WORK ZONE SIGNS

INSTALL ANY REQUIRED LANE CLOSURE SIGNING NEEDED DURING THE LIFE OF THE PROJECT IN ACCORDANCE WITH THE STANDARD DRAWING NO. 1101.02, 1101.11 AND 1110.02 OF THE 2012 ROADWAY STANDARD DRAWINGS.

(D) ROAD CLOSURE SIGNS

NOTIFY THE ENGINEER FOURTEEN (14) CALENDAR DAYS PRIOR TO RAMP/LOOP CLOSURE. PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS AND PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS. COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD AND ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION. ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

PAVEMENT MARKING AND MARKERS:

DESCRIPTION

TEMPORARY PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH STANDARD DRAWINGS 1205.01 THROUGH 1205.13 OF THE 2012 ROADWAY STANDARD DRAWINGS AND SECTION 1205 OF THE 2012 STANDARD SPECIFICATIONS.

(A) MARKERS

REMOVE EXISTING SNOWPLOWABLE PAVEMENT MARKERS IN PREPARATION FOR PAVING. REPAIR ANY PAVEMENT DAMAGE DUE TO EXISTING PAVEMENT MARKER REMOVAL PRIOR TO THE END OF THE WORK DAY. DISPOSE OF EXISTING PAVEMENT MARKERS AS DIRECTED BY THE ENGINEER. NO DIRECT PAYMENT WILL BE MADE FOR THIS WORK AS IT WILL BE INCIDENTAL TO THE PAVING OPERATION.

INSTALL PERMANENT PAVEMENT MARKERS WITHIN 60 CALENDAR DAYS AFTER COMPLETING THE RESURFACING. PAVEMENT MARKERS SHALL BE INSTALLED IN ACCORDANCE WITH STANDARD DRAWING 1205.12 AND STANDARD DRAWINGS 1250.01 THROUGH 1253.01 OF THE 2012 ROADWAY STANDARD DRAWINGS AND SECTIONS 1250 THROUGH 1253 OF THE 2012 STANDARD SPECIFICATIONS.

(B) MARKING AND MARKERS

FOR TEMPORARY PAVEMENT MARKING USE RSD 1205.01, 1205.02, 1205.03, 1205.06, 1205.08, 1205.12, 1205.13.

TEMPORARY TRAFFIC CONTROL (TTC):

MAINTAIN TRAFFIC IN ACCORDANCE WITH DIVISIONS 10, 11 AND 12 OF THE 2012 STANDARD SPECIFICATIONS AND THE FOLLOWING PROVISIONS:

INSTALL WORK ZONE ADVANCE WARNING SIGNS IN ACCORDANCE WITH THE DETAIL DRAWING PROVIDED IN THESE PLANS PRIOR TO BEGINNING ANY OTHER WORK.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AND WITHIN 10 FEET OF OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING STANDARD DRAWING NO. 1101.02 OF THE 2012 ROADWAY STANDARD DRAWINGS. WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, 2012 ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

WHEN UTILIZING A SLOW-MOVING OPERATION FOR SUCH ITEMS AS PAVEMENT MARKING AND MARKER PLACEMENT, AS A MINIMUM THE SLOW MOVING OPERATION CARAVAN SHALL CONSIST OF THE VEHICLES AND DEVICES SHOWN ON THE MOVING OPERATION CARAVAN DETAILS ACCORDING TO ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 13 OF THE 2012 ROADWAY STANDARD DRAWINGS. TRAFFIC CONES MAY BE USED WHEN NECESSARY TO PROVIDE ADDITIONAL PROTECTION OF WET PAVEMENT MARKINGS. BALLAST ALL TRAFFIC CONES SO THEY WILL NOT BE BLOWN OVER BY TRAFFIC.

CONSTRUCTION OPERATIONS:

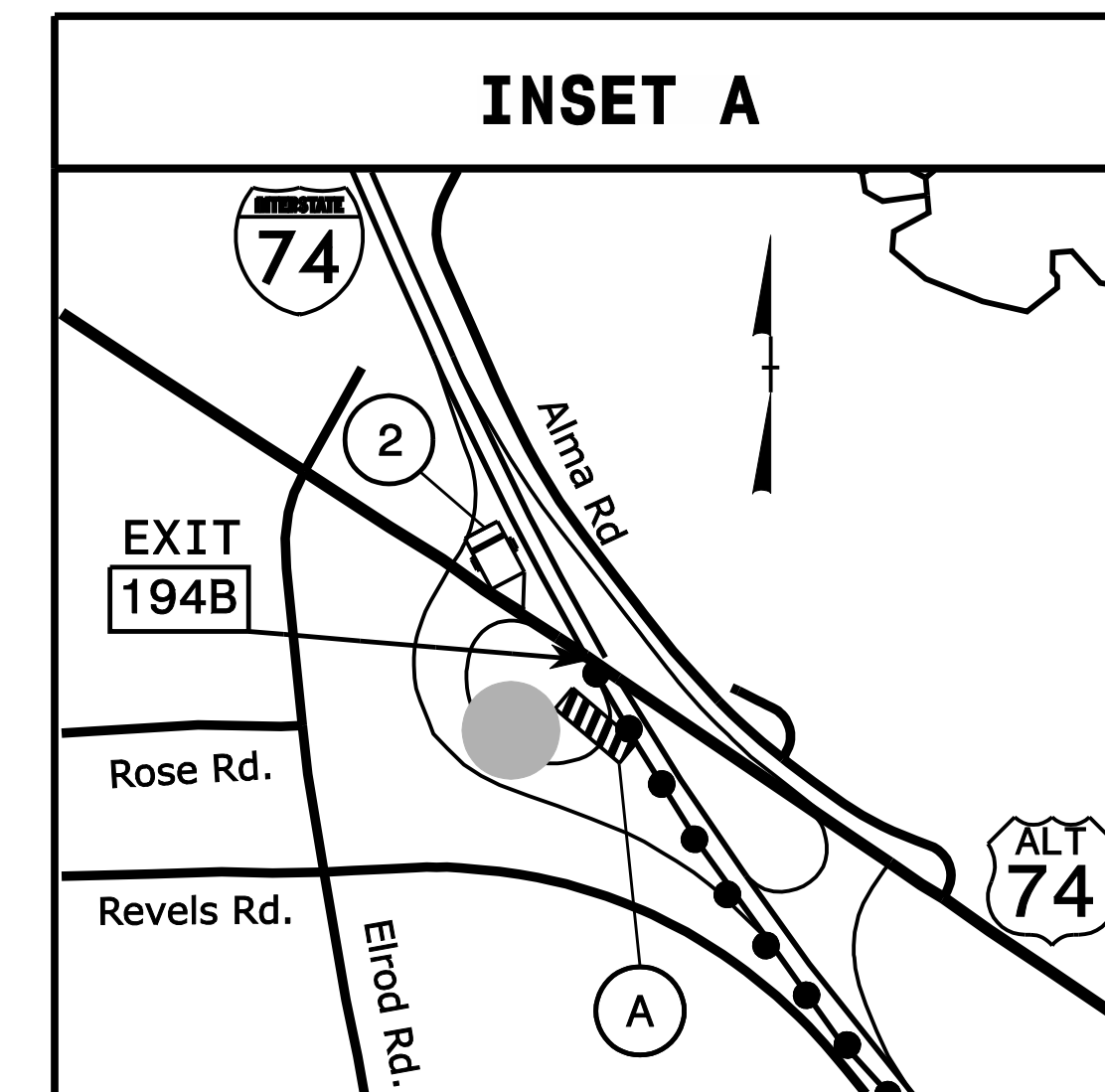
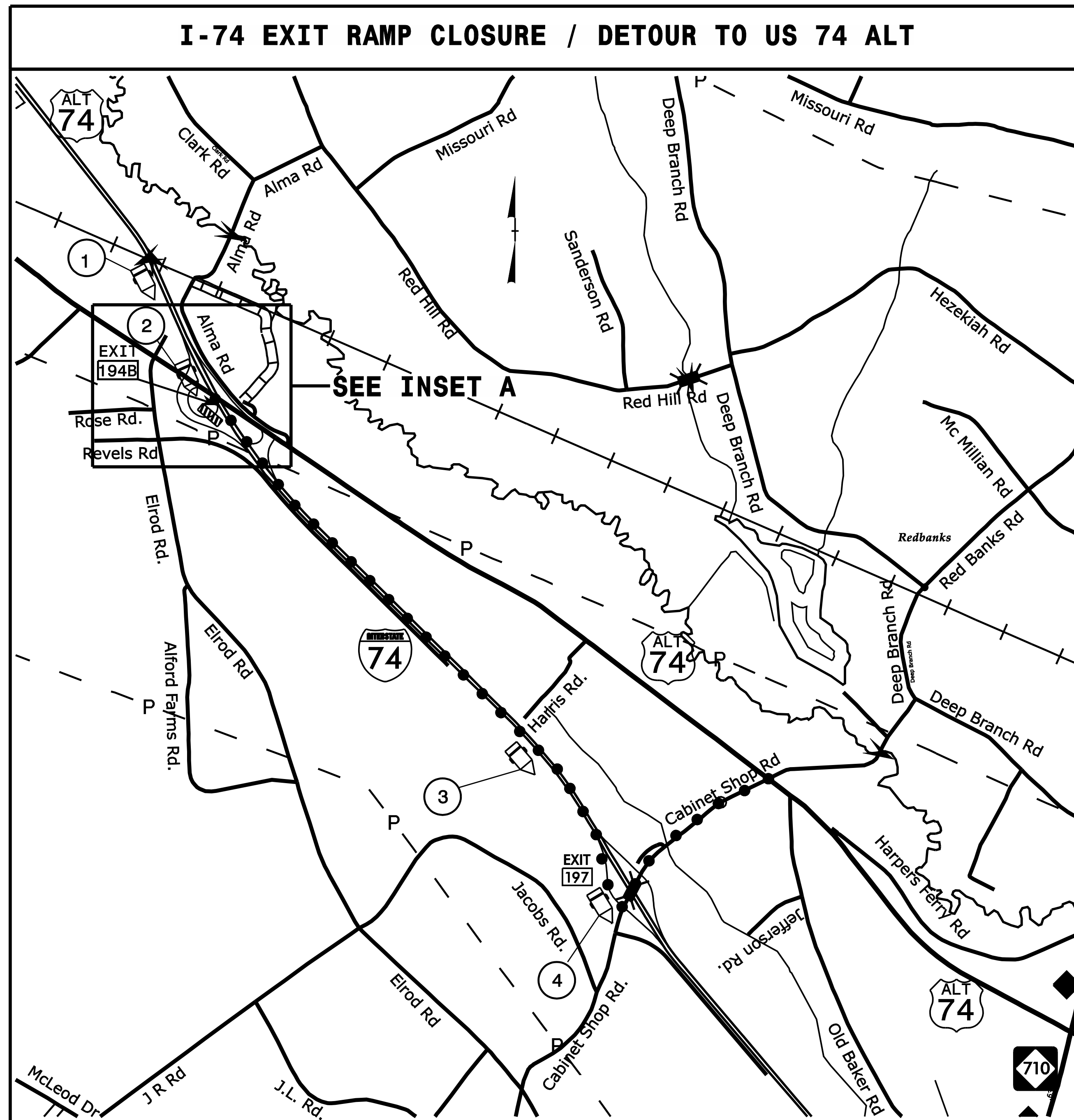
1. CONTRACTOR SHALL MILL AND PAVE LANES IN AN ORDER SUCH THAT WATER SHALL NOT ACCUMULATE.
2. IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, THE CONTRACTOR IS TO FURNISH AND INSTALL THE FOLLOWING PORTABLE SIGNS TO WARN DRIVERS OF THE CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" (W8-8), "UNEVEN LANES" (W8-11), AND "GROOVED PAVEMENT" (W8-15) W/ MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DUAL INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS.
3. REMOVE /REPLACE ANY CONFLICTING OR DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.
4. OPERATE EQUIPMENT AND CONDUCT OPERATIONS IN THE SAME DIRECTION AS THE FLOW OF TRAFFIC. MAINTAIN VEHICULAR ACCESS IN ACCORDANCE WITH ARTICLE 1101-05 OF THE 2012 STANDARD SPECIFICATIONS.
5. DO NOT INSTALL MORE THAT 2 MILES OF LANE CLOSURE ON I-74, MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE, AND DO NOT INSTALL ANY MORE THAN ONE PER DIRECTION.

MISCELLANEOUS

A) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS DIRECTED BY THE ENGINEER.

<p style="font-size: x-small;">PLAN PREPARED IN THE OFFICE OF:</p> <p style="font-weight: bold; font-size: large;">PROGRESSIVE</p> <p style="font-weight: bold; font-size: large;">DESIGN GROUP, INC.</p> <div style="display: flex; justify-content: center; gap: 5px;"> <div style="width: 15px; height: 15px; background-color: #ccc;"></div> <div style="width: 15px; height: 15px; background-color: #ccc;"></div> <div style="width: 15px; height: 15px; background-color: #ccc;"></div> <div style="width: 15px; height: 15px; background-color: #ccc;"></div> <div style="width: 15px; height: 15px; background-color: #ccc;"></div> <div style="width: 15px; height: 15px; background-color: #ccc;"></div> </div> <p style="font-size: x-small;">ENGINEERS • CONSULTANTS</p> <p style="font-size: x-small;">PH 704 573 3003</p>	<p style="font-size: x-small;">APPROVED: <i>Jim Arvey</i> DATE: 3/1/2016</p> <div style="border: 1px solid black; border-radius: 50%; padding: 5px; width: 40px; height: 40px; margin: auto;"> <p style="font-size: x-small; text-align: center;">SEAL</p> <p style="font-size: x-small; text-align: center;">NORTH CAROLINA PROFESSIONAL ENGINEER JIM ARVEY 025465</p> </div>	<div style="border: 1px solid black; border-radius: 50%; padding: 5px; width: 40px; height: 40px; margin: auto;"> <p style="font-size: x-small; text-align: center;">DIVISION OF HIGHWAYS & DEPARTMENT OF TRANSPORTATION STATE OF NORTH CAROLINA</p> </div>	<p style="font-weight: bold; font-size: large;">TRANSPORTATION MANAGEMENT PLAN</p> <p style="font-weight: bold; font-size: large;">TRANSPORTATION OPERATIONS PLAN</p>
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I-74 EXIT RAMP CLOSURE / DETOUR TO US 74 ALT



1	MESSAGE NO. 1	MESSAGE NO. 2
	EXIT 194B CLOSED	USE EXIT 197

CHANGEABLE MESSAGE SIGN

PLACE CMS APPROX. 1/2 MILE IN ADVANCE OF CLOSED RAMP

3	MESSAGE NO. 1	MESSAGE NO. 2
	DETOUR TO US 74 ALT	USE EXIT 197

CHANGEABLE MESSAGE SIGN

PLACE CMS 800-1000 FEET IN ADVANCE OF EXIT 197

2	MESSAGE NO. 1	MESSAGE NO. 2
	EXIT 194B CLOSED	USE EXIT 197

CHANGEABLE MESSAGE SIGN

PLACE CMS 1000+/- FEET IN ADVANCE OF CLOSED RAMP

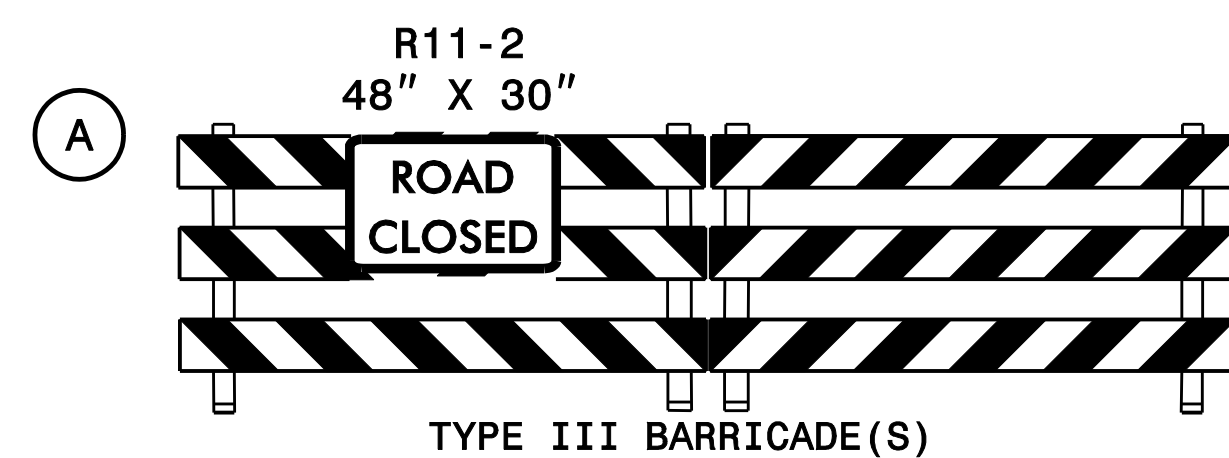
4	MESSAGE NO. 1	MESSAGE NO. 2
	DETOUR TO US 74 ALT	TURN LEFT

CHANGEABLE MESSAGE SIGN

PLACE CMS 300-600 FEET FROM TOP OF RAMP

NOTES:

- USE THIS OFF SITE DETOUR EXAMPLE FOR CLOSURES OF FREEWAY EXIT RAMP. MODIFY CMS MESSAGES AND NUMBER OF CMS LOCATIONS REQUIRED FOR SPECIFIC DETOUR.
- NO MORE THAN ONE RAMP OR LOOP SHOULD BE CLOSED AT THE SAME TIME.
- CHANGEABLE MESSAGE SIGNS LOCATED AT INTERSECTIONS SHALL NOT BE PLACED WHERE THEY BLOCK STOP SIGNS, STOP AHEAD SIGNS, OR OTHER REGULATORY OR WARNING SIGNS. DO NOT PLACE CMS IN LOCATION THAT MAY INTERFERE WITH PROPER SIGHT DISTANCE OF ANY SIGNS.



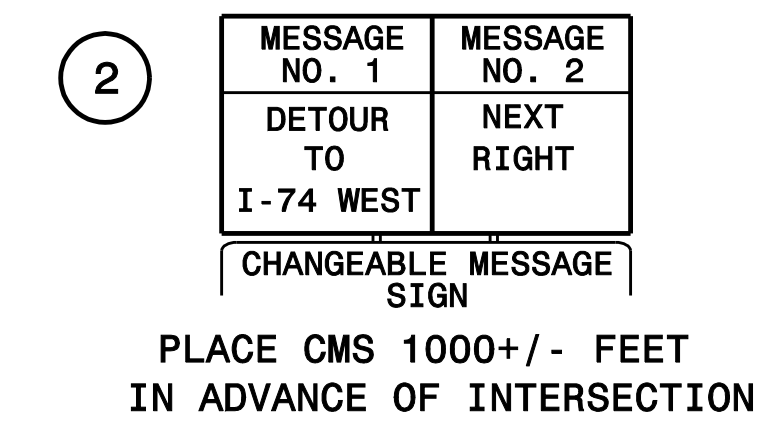
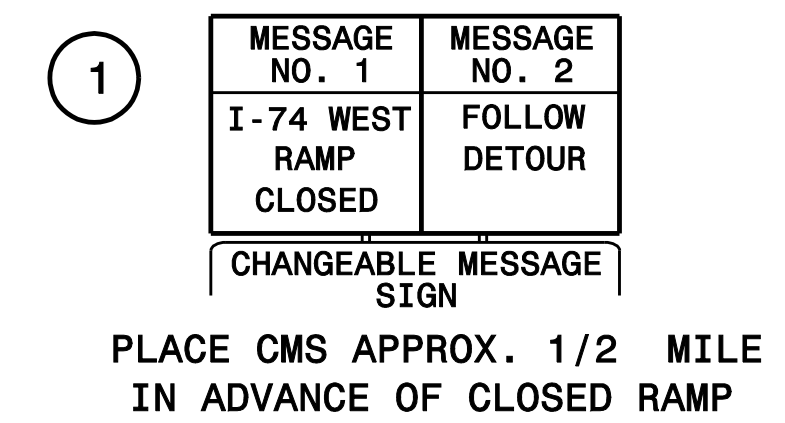
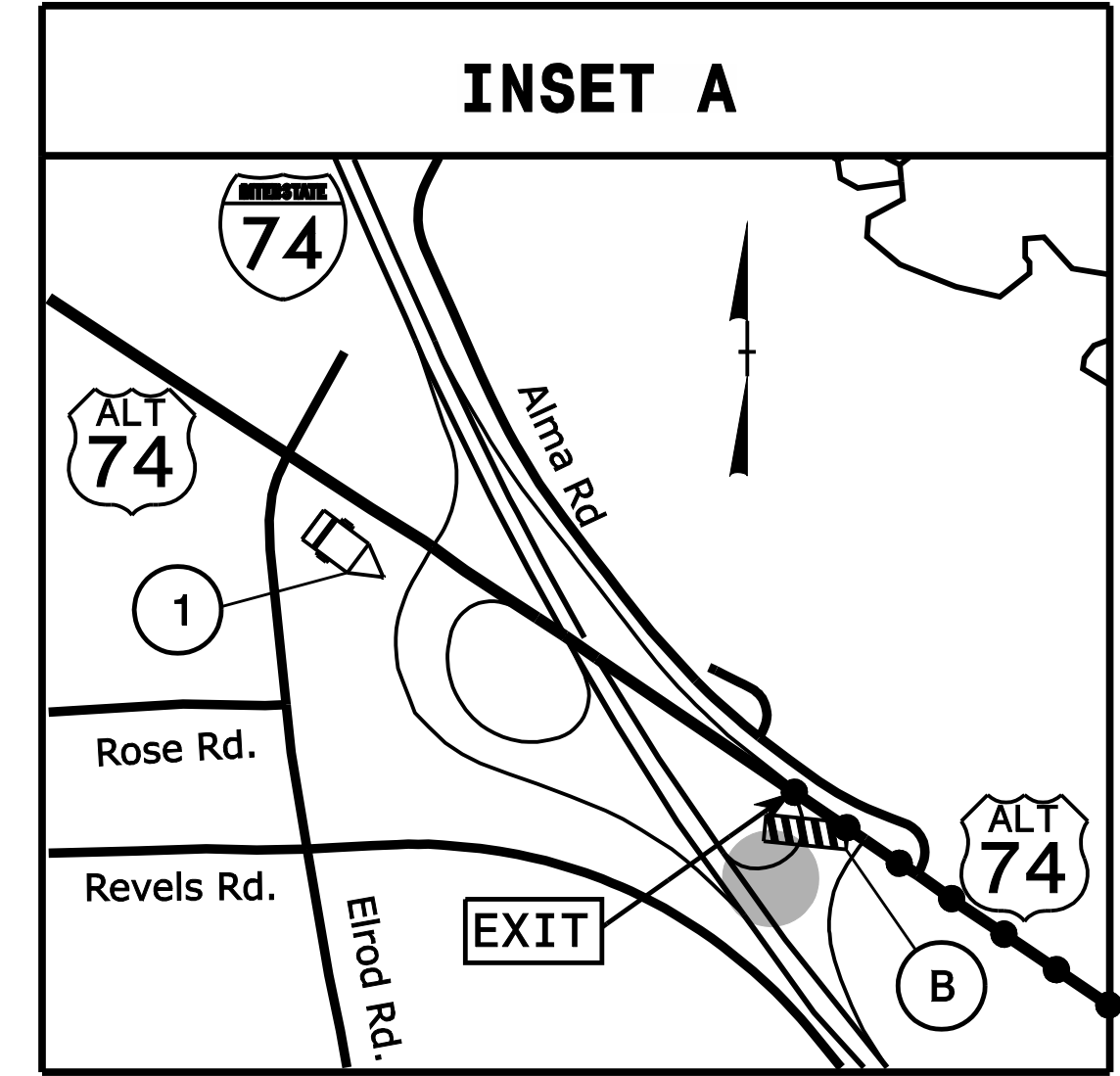
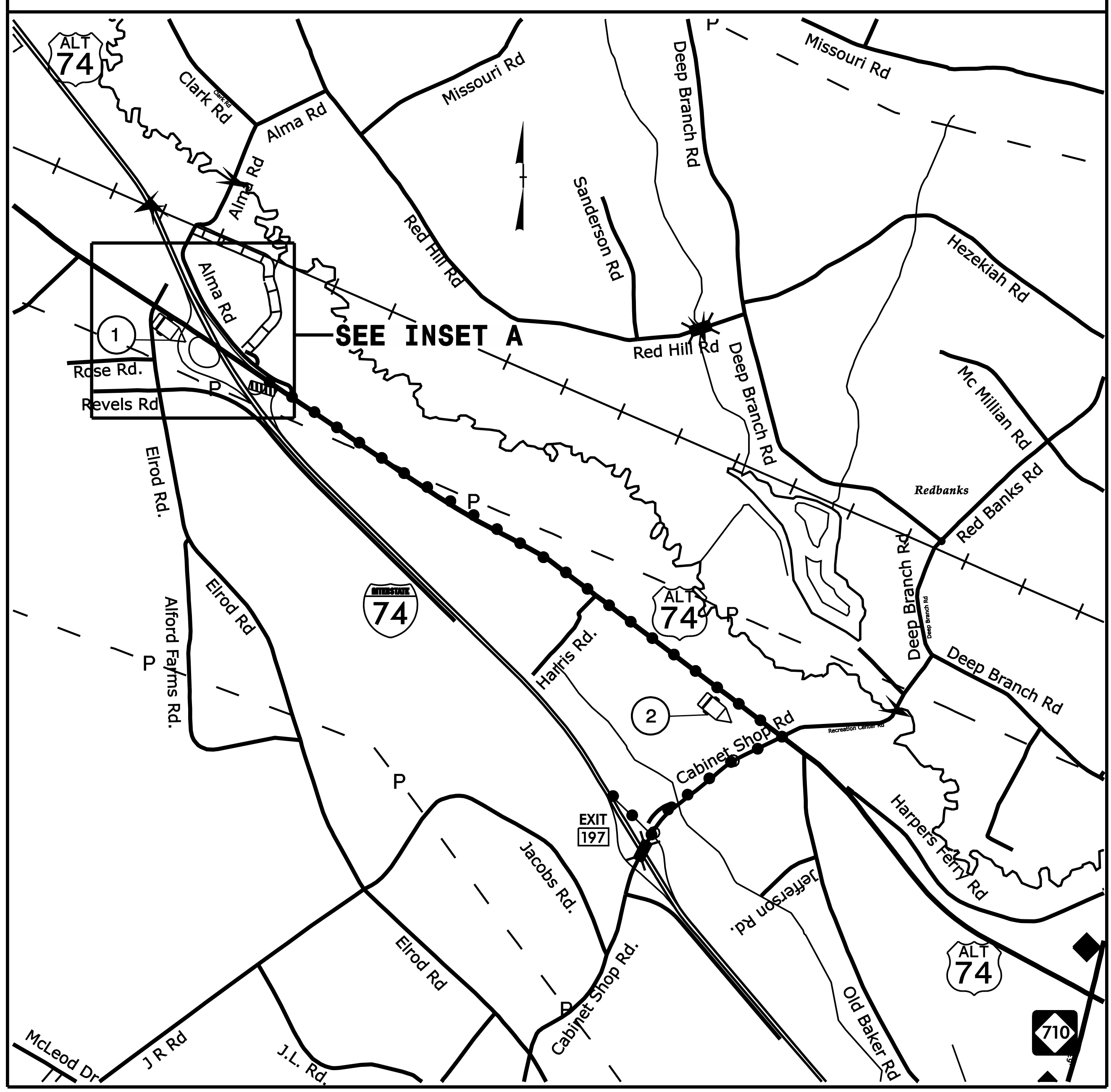
USE WHEN CLOSING A FREEWAY EXIT RAMP (AT -L- LINE MOVEMENT TO -Y- LINE)

- CHANGEABLE MESSAGE SIGN
- CLOSED RAMP
- OFFSITE DETOUR

EXAMPLE CLOSURE AT FREEWAY EXIT RAMP

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS PH 704 573 3003	APPROVED: <i>Tommy</i> DATE: 3/1/2016 		TRANSPORTATION MANAGEMENT PLAN OFF SITE DETOURS
	SEAL		

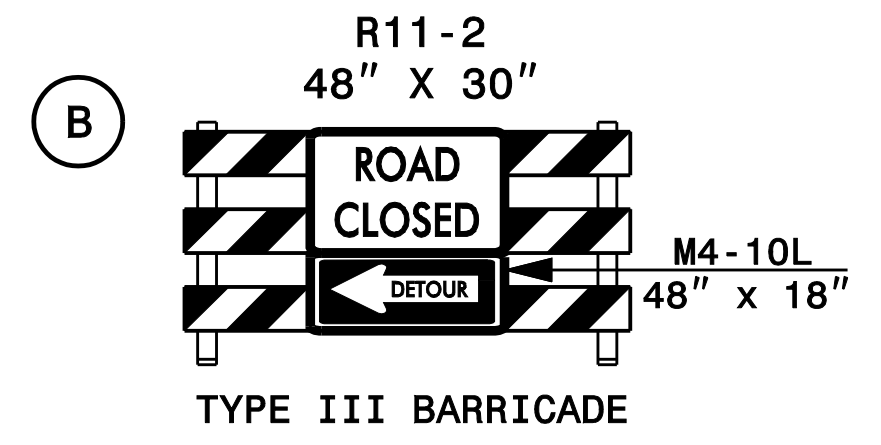
EB US 74 ALT ENTRANCE RAMP CLOSURE TO I-74 WEST / DETOUR TO I-74 WEST



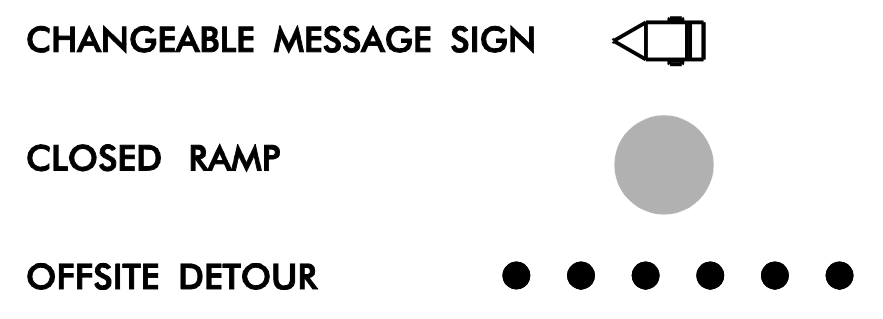
NOTES:

- USE THIS OFF SITE DETOUR EXAMPLE FOR CLOSURES OF FREEWAY ENTRANCE RAMPS. MODIFY CMS MESSAGES AND NUMBER OF CMS LOCATIONS REQUIRED FOR SPECIFIC DETOUR.
- NO MORE THAN ONE RAMP OR LOOP SHOULD BE CLOSED AT THE SAME TIME.
- CHANGEABLE MESSAGE SIGNS LOCATED AT INTERSECTIONS SHALL NOT BE PLACED WHERE THEY BLOCK STOP SIGNS, STOP AHEAD SIGNS, OR OTHER REGULATORY OR WARNING SIGNS. DO NOT PLACE CMS IN LOCATION THAT MAY INTERFERE WITH PROPER SIGHT DISTANCE OF ANY SIGNS.

EXAMPLE CLOSURE AT FREEWAY ENTRANCE RAMP



USE WHEN CLOSING A FREEWAY ENTRANCE RAMP (AT -Y- LINE MOVEMENT TO -L- LINE)



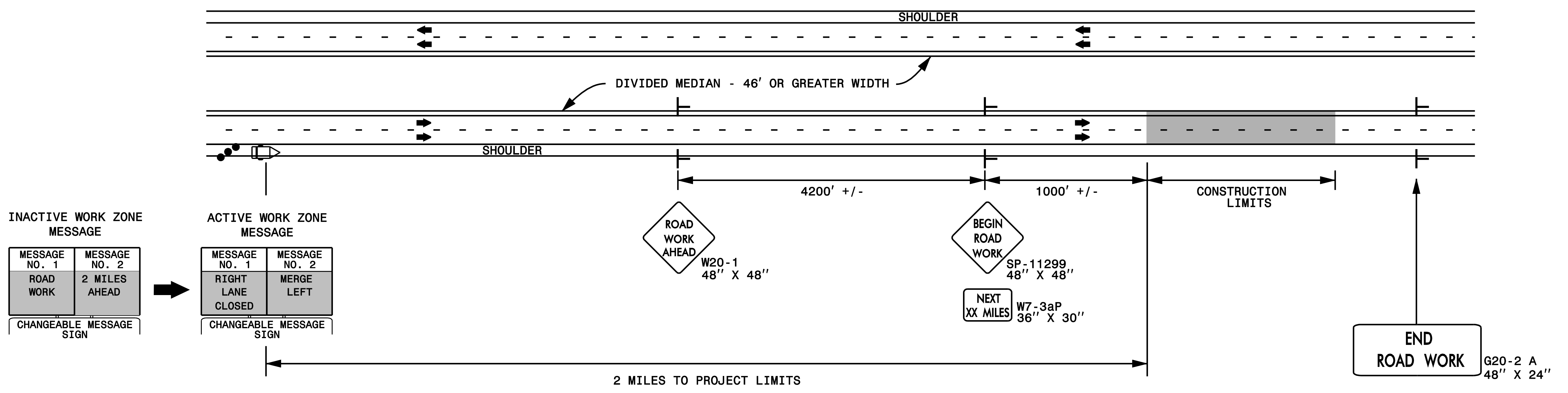
PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE DESIGN GROUP, INC.
ENGINEERS • CONSULTANTS
PH 704 573 3003

APPROVED: *Jim Arey* DATE: 3/1/2016

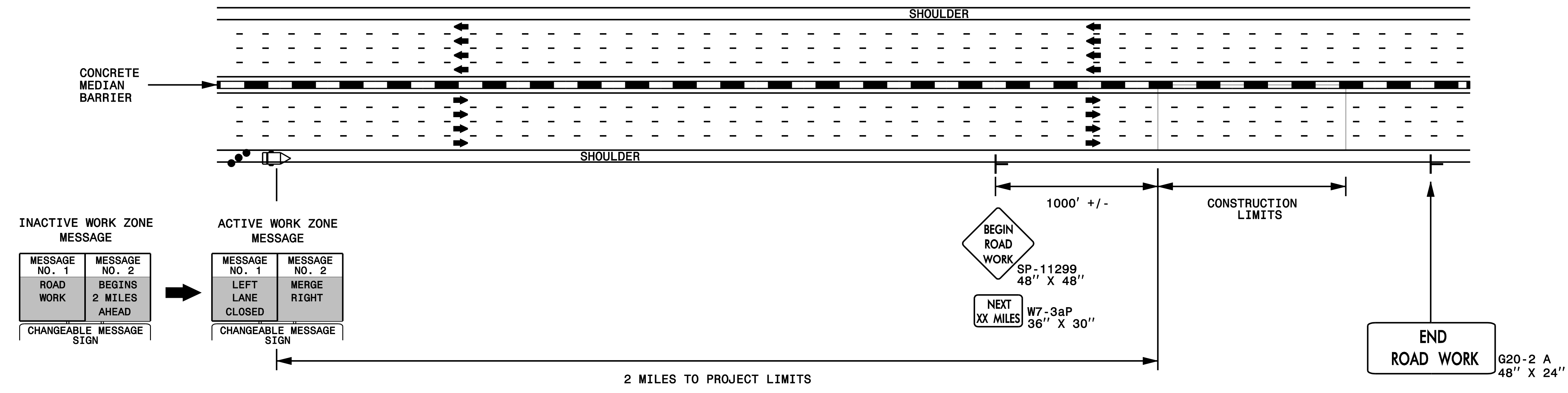
SEAL

TRANSPORTATION MANAGEMENT PLAN OFF SITE DETOURS

DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER



NOTES:

- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
- 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
- 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM

***** SYSTEM *****
 ***** USER *****
 ***** DATE *****

PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE

DESIGN GROUP, INC.

ENGINEERS • CONSULTANTS

PH 704 573 3003

APPROVED: *Jan Arny* DATE: 3/1/2016

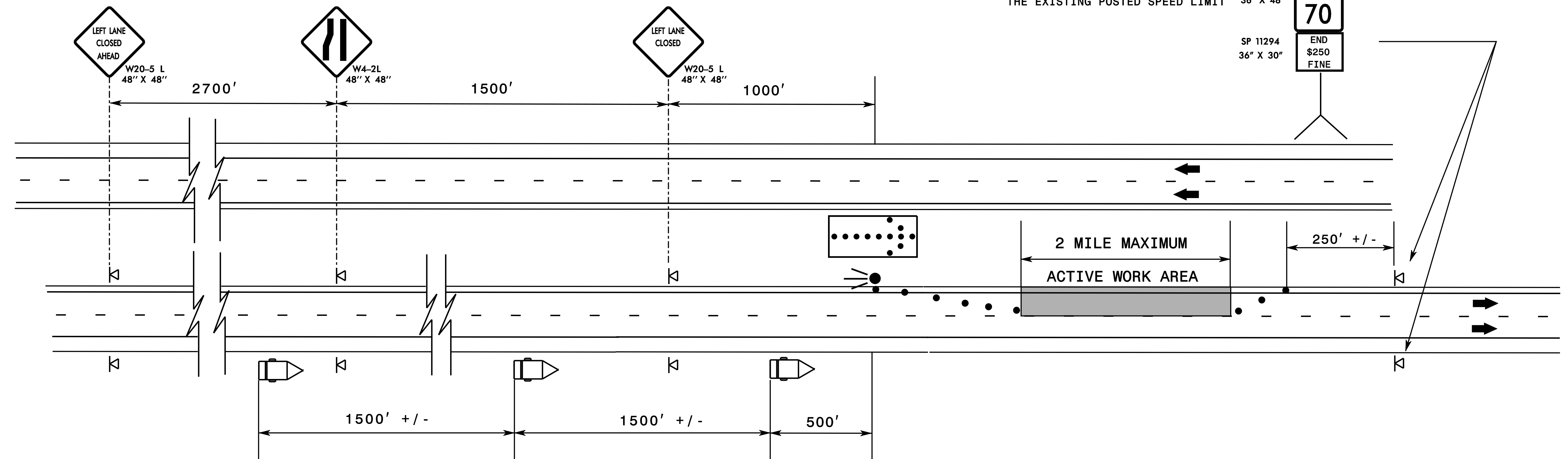
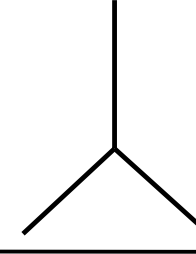
SEAL

RESURFACING ADVANCE
WARNING SIGNS FOR
HIGH SPEED FACILITIES
≥ 60 MPH

THE EXISTING POSTED SPEED LIMIT R2-1 36" X 48"



SP 11294 36" X 30"



MESSAGE NO. 1	MESSAGE NO. 2
60 MPH ZONE AHEAD	\$250 FINE AHEAD

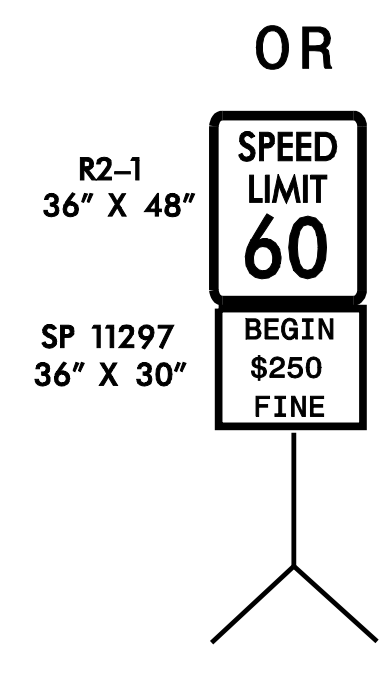
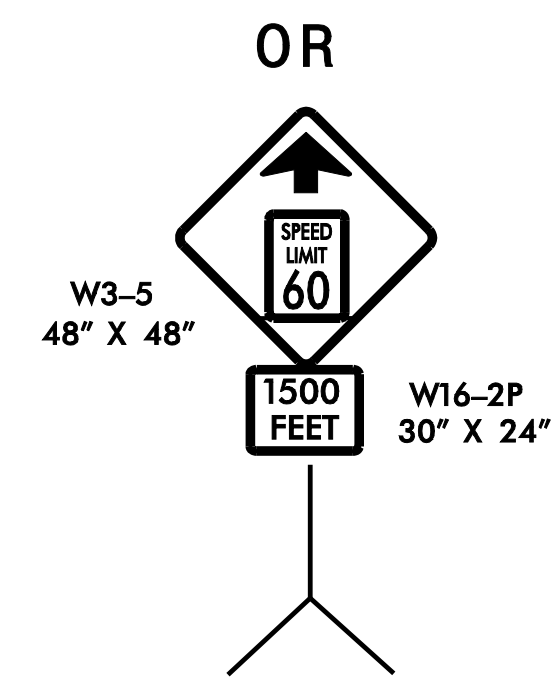
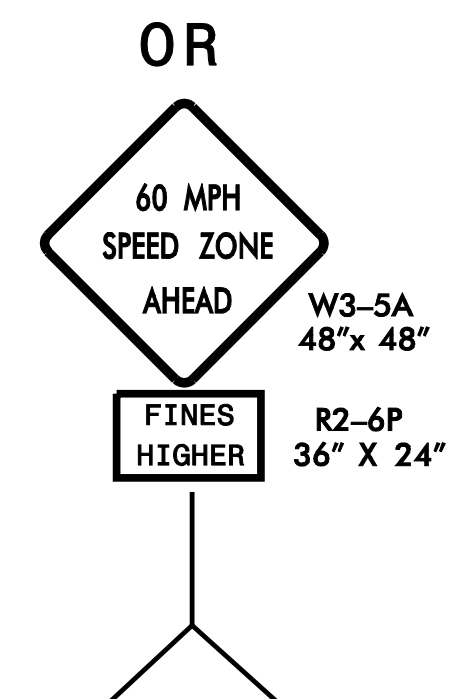
CHANGEABLE MESSAGE SIGN

MESSAGE NO. 1	MESSAGE NO. 2
BEGIN 60 MPH 1500 FT	WORKERS IN ROADWAY

CHANGEABLE MESSAGE SIGN

MESSAGE NO. 1	MESSAGE NO. 2
SPEED LIMIT 60	BEGIN \$250 FINE

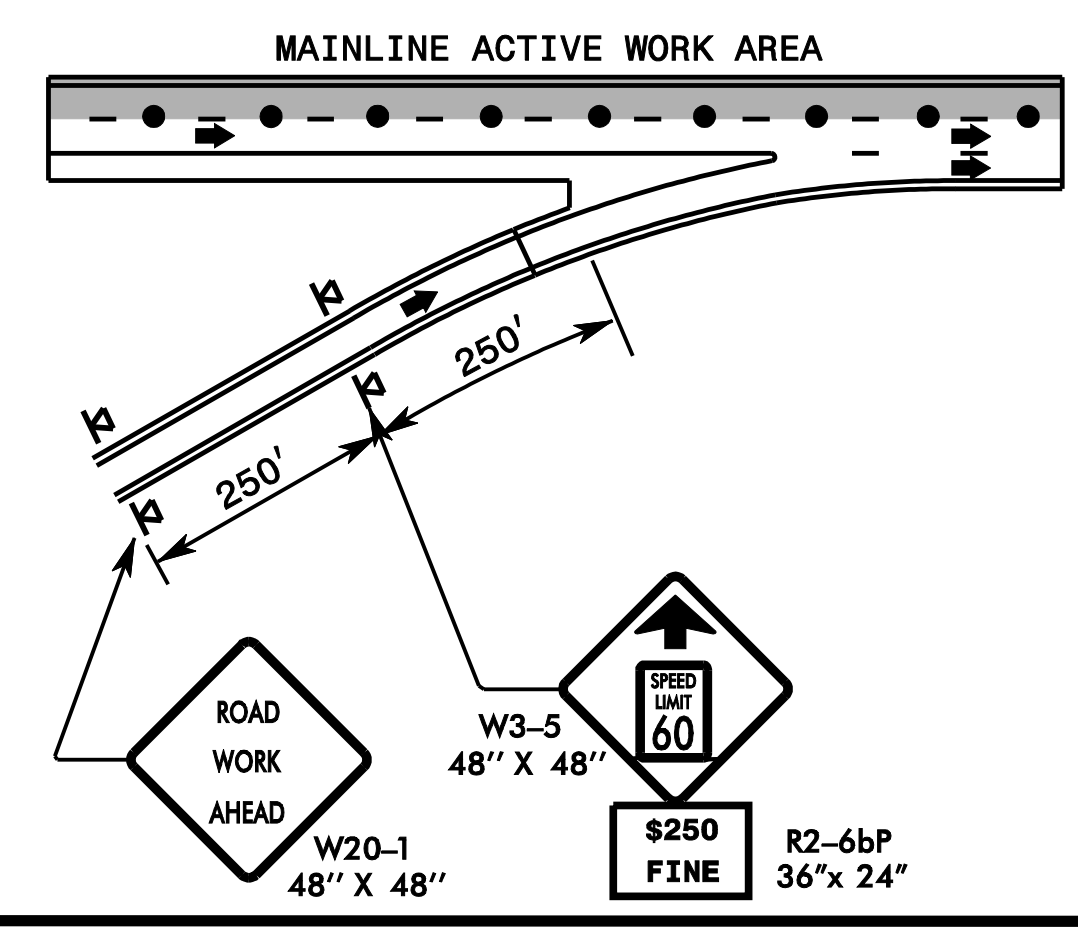
CHANGEABLE MESSAGE SIGN



GUIDELINES

1. THIS DRAWING IS FOR USE ONLY AFTER AN ENGINEERING INVESTIGATION HAS BEEN PERFORMED BY THE REGIONAL TRAFFIC ENGINEER AND WORK ZONE TRAFFIC CONTROL SECTION. THE WORK ZONE "VARIABLE" SPEED LIMIT IS INTENDED FOR USE ON FREEWAYS WITH SPEED LIMITS 65 MPH OR GREATER. SEE WORK ZONE "VARIABLE" SPEED LIMIT GUIDELINES FOR CRITERIA.
2. THE STATE TRAFFIC ENGINEER HAS TO ORDINANCE THE SPEED LIMIT REDUCTION IN ORDER FOR THE REDUCTION AND/OR \$250 SPEEDING FINE TO BE VALID AND ENFORCEABLE. NO SPEED LIMIT MESSAGES/SIGNS SHALL BE INSTALLED PRIOR TO RECEIVING A SIGNED ORDINANCE. IN ADDITION, THE \$250 SPEEDING FINE ALSO REQUIRES A SEPARATE SIGNED ORDINANCE BY THE STATE TRAFFIC ENGINEER.
3. EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.
4. THE MAXIMUM LANE CLOSURE LENGTH IS 2 MILES UNLESS OTHERWISE SHOWN IN THE PLANS. IN ADDITION, FOR ACTIVE WORK AREAS THAT EXCEED 1 MILE IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF ADDITIONAL CMS'S/SIGNS ARE NEEDED TO SUPPLEMENT THE INITIAL ONES. PORTABLE MOUNTED W3-5 SIGNS WITH SPEEDING PENALTY SIGNS ARE TO BE PLACED ALONG ENTRANCE RAMP LOCATED WITHIN THE ACTIVE WORK AREA.
5. THE \$250 SPEEDING FINE PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION.
6. THIS APPLICATION IS FOR SHORT-TERM ACTIVITIES SUCH AS LANE CLOSURES AND ROAD CLOSURES. THE WORK ZONE "VARIABLE" SPEED LIMIT SHALL NOT BE IN OPERATION CONTINUOUSLY (24/7) FOR A PERIOD EXCEEDING 30 CALENDAR DAYS. THE WORK ZONE "VARIABLE" SPEED LIMIT MESSAGING/SIGNAGE SHALL BE REMOVED AT THE COMPLETION OF THE ACTIVITY. THE REGIONAL TRAFFIC ENGINEER WILL BE NOTIFIED BY THE RESIDENT ENGINEER TO RESCIND THE ORDINANCE.
7. WHEN WORK ZONE "VARIABLE" SPEED LIMIT REDUCTIONS ARE IN EFFECT, THE CONTRACTOR IS TO COVER ANY EXISTING SPEED LIMIT SIGNS LOCATED WITHIN THE ACTIVE WORK AREA THAT CONFLICT WITH THE WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION.

ENTRANCE RAMP

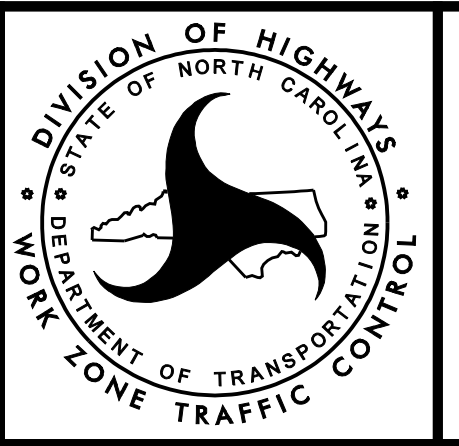


NOTES

1. THE WORK ZONE "VARIABLE" SPEED LIMIT WILL BE ESTABLISHED IN COLLABORATION BETWEEN THE REGIONAL TRAFFIC ENGINEER, THE DIVISION, AND THE WORK ZONE TRAFFIC CONTROL SECTION. THIS DRAWING SHOWS THE TYPICAL APPLICATION OF REDUCING THE SPEED LIMIT TO 60 MPH.
2. TYPICALLY THE WORK ZONE "VARIABLE" SPEED LIMIT APPLIES TO A SPECIFIC PORTION AND NOT THE ENTIRE PROJECT. THE WORK AREA AFFECTED BY THE SPEED REDUCTION SHOULD NOT EXCEED 2 MILES UNLESS THE MAXIMUM LENGTH OF LANE CLOSURE ALLOWED BY THE PLANS EXCEEDS THE 2 MILE MAXIMUM.
3. THE WORK ZONE "VARIABLE" SPEED LIMIT AND THE \$250 SPEEDING FINE IS TO BEGIN 500' IN ADVANCE OF THE FLASHING ARROW PANEL AT THE MERGE TAPER. THE EXISTING SPEED LIMIT SIGNS AND THE "END \$250 FINE" SIGNS ARE TO BE INSTALLED AT THE LOCATION WHERE THE EXISTING SPEED LIMIT IS TO RESUME.
4. THE NEED AND LOCATION OF ADDITIONAL POSTED WORK ZONE "VARIABLE" SPEED LIMIT SIGNS WITHIN THE LANE CLOSURE IS TO BE DETERMINED BY THE REGIONAL TRAFFIC ENGINEER.

PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE DESIGN GROUP, INC.
 ENGINEERS • CONSULTANTS
 PH 704 573 3003

APPROVED: *Jim Arrey* DATE: 3/1/2016
 SEAL



WORK ZONE "VARIABLE" SPEED LIMIT REDUCTION

WORK ZONE TRAFFIC CONTROL NARRATIVE

STEP 1: INSTALL WORK ZONE ADVANCE WARNING SIGNS FOR THE PROJECT LIMITS AS SHOWN ON TMP-2C (RESURFACING ADVANCE WARNING SIGNS FOR HIGH SPEED FACILITIES >= 60 MPH.

NOTE: OPERATIONS DESCRIBED IN STEP 2 AND STEPS 3 THRU 5 MAY BE COMPLETED SIMULTANEOUSLY.

STEP 2: USING THE WORK ZONE VARIABLE SPEED LIMIT DETAIL SHOWN ON SHEET TMP-2D, COMPLETE THE REMOVAL OF EXISTING PAVEMENT MARKINGS AND MARKERS, MILLING, AND RESURFACING FOR BOTH DIRECTIONS OF I-74. PLACE TEMPORARY PAVEMENT MARKINGS ALONG I-74 BACK TO THE ORIGINAL PATTERN AT THE END OF EACH OPERATION BEFORE OPENING LANES BACK TO TRAFFIC. PLACE PERMANENT PAVEMENT MARKINGS AND MARKERS ON THE FINAL SURFACE.

NOTE: UTILIZE THE CONSTRUCTION OPERATION DESCRIBED IN STEP 3 THRU STEP 5 FOR EACH RAMP AND LOOP AT THE US 74 ALTERNATE AND SR 1166 (CABINET SHOP RD) INTERCHANGES. DO NOT SIMULTANEOUSLY CLOSE MORE THAN ONE RAMP AT A TIME.

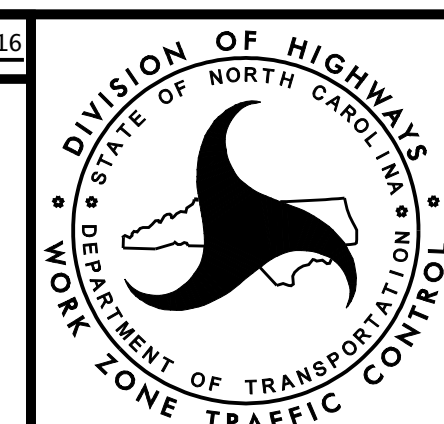
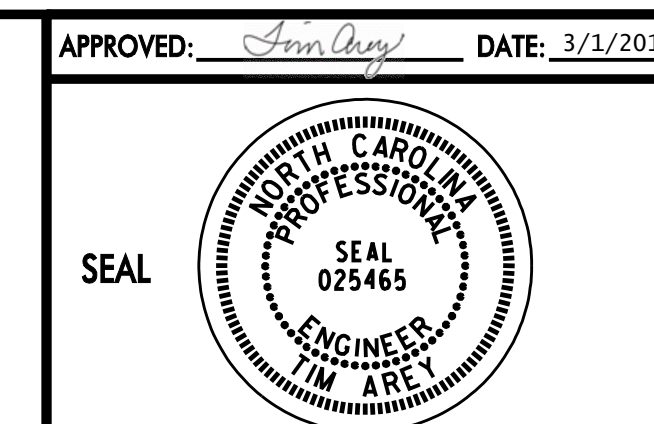
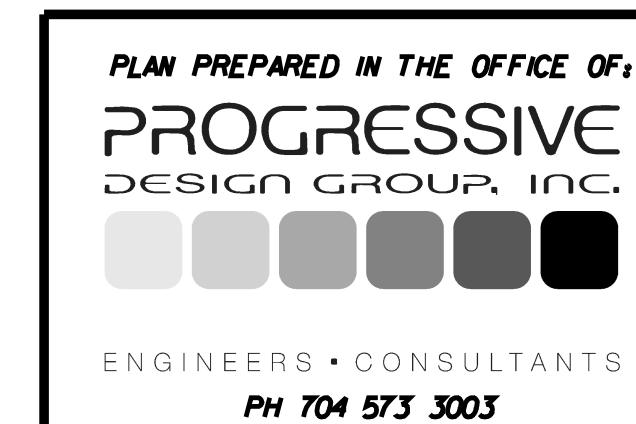
STEP 3: FOR THE APPROPRIATE RAMP OR LOOP THAT IS UNDER CONSTRUCTION, PLACE THE OFF SITE DETOUR DEVICES AS SHOWN ON SHEET TMP-2A FOR I-74 EXIT RAMP AND LOOPS (OFF RAMP FROM I-74). TMP-2A INCLUDES AN EXAMPLE DETOUR TO BE REFERENCED FOR THESE LOCATIONS. THE SPECIFIC DETOUR FOR EACH LOCATION WILL BE APPROVED BY THE ENGINEER. IN ADDITION TO THE CHANGEABLE MESSAGE SIGNS REQUIRED ON I-74, A MINIMUM OF ONE CMS PER TURN ON THE DETOUR IS REQUIRED. CLOSE THE MAINLINE EXIT LANE USING DRUMS. CLOSE THE RAMP USING TYPE III BARRICADES.

USE THE WORK ZONE VARIABLE SPEED LIMIT DETAIL SHOWN ON SHEET TMP-2D AND ROADWAY STANDARD DRAWING 1101.02 SHEETS 4, 9, AND 10, TO CLOSE THE OUTSIDE LANE OF I-74 WHEN RAMP CONSTRUCTION OPERATIONS ARE WITHIN 10' OF THE OPEN TRAVEL LANE.

STEP 4: FOR THE APPROPRIATE RAMP OR LOOP THAT IS UNDER CONSTRUCTION, PLACE THE OFF SITE DETOUR DEVICES AS SHOWN ON SHEET TMP-2B FOR THE -Y-LINE ENTRANCE RAMP AND LOOPS (ON RAMP TO I-74). TMP-2B INCLUDES AN EXAMPLE DETOUR TO BE REFERENCED FOR THESE LOCATIONS. THE SPECIFIC DETOUR FOR EACH LOCATION WILL BE APPROVED BY THE ENGINEER. IN ADDITION TO THE CHANGEABLE MESSAGE SIGN REQUIRED ON THE -Y-LINE IN ADVANCE OF THE RAMP CLOSURE, A MINIMUM OF ONE CMS PER TURN ON THE DETOUR IS REQUIRED. USING DRUMS, CLOSE THE EXCLUSIVE RIGHT AND LEFT TURN LANES THAT ACCESS THE CLOSED ENTRANCE RAMP. CLOSE THE RAMP USING TYPE III BARRICADES.

USE THE WORK ZONE VARIABLE SPEED LIMIT DETAIL SHOWN ON SHEET TMP-2D AND ROADWAY STANDARD DRAWING 1101.02 SHEETS 4, 9, AND 10, TO CLOSE THE OUTSIDE LANE OF I-74 WHEN RAMP CONSTRUCTION OPERATIONS ARE WITHIN 10' OF THE OPEN TRAVEL LANE.


STEP 5: PLACE PERMANENT MARKINGS AND MARKERS ALONG THE NEWLY RESURFACED RAMPS AND REOPEN THE RAMPS BACK TO THE ORIGINAL PATTERN.



TRANSPORTATION
MANAGEMENT PLAN
TRAFFIC CONTROL NARRATIVE

**STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
ROBESON COUNTY**

TIP NO. I-5847	SHEET NO. PMP-1
APPROVED: <i>Ayman Alqudwah</i> DATE: 3/4/2016	
	
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	

T.I.P.: I-5847

ROADWAY STANDARD DRAWING

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO-LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXIT AND ENTRANCE RAMP
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1253.01	RAISED PAVEMENT MARKERS - SNOWPLOWABLE

PAVEMENT MARKINGS SCHEDULE

- THERMOPLASTIC(12", 120 MILS)
WHITE SOLID LANE LINE
- THERMOPLASTIC(12", 90 MILS)
WHITE GORELINE
- THERMOPLASTIC(24", 120 MILS)
WHITE STOPBAR
- THERMOPLASTIC(6", 120 MILS)
2 FT. - 6 FT./SP WHITE MINISKIP
10 FT. WHITE SKIP
3 FT. - 9 FT./SP WHITE MINISKIP
- THERMOPLASTIC(6", 90 MILS)
WHITE EDGELINE
YELLOW EDGELINE
- THERMOPLASTIC(8", 90 MILS)
WHITE GORELINE

- THERMOPLASTIC PAVEMENT MARKING SYMBOLS (90 MILS)
- LEFT TURN ARROW
 - RIGHT TURN ARROW
 - COMBO.STRAIGHT/RIGHT
 - MERGE ARROW
 - RAMP ARROW SYMBOL
- MARKERS
- SNOWPLOWABLE RAISED PAVEMENT MARKERS
 - CRYSTAL & RED

GENERAL NOTES

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

- A) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
I-74/RAMPS	THERMOPLASTIC	SNOWPLOWABLE

- B) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
 C) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS.
 D) PASSING ZONES WILL BE DETERMINED IN THE FIELD AND MUST BE APPROVED BY THE ENGINEER.
 E) STOP BAR LOCATION AT NON-SIGNALIZED INTERSECTIONS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER.
 F) UNLESS OTHERWISE SPECIFIED, HEATED-IN-PLACE THERMOPLASTIC MAY BE USED IN LIEU OF EXTRUDED THERMOPLASTIC FOR STOP BARS, SYMBOLS, CHARACTERS AND DIAGONALS. IF HEATED-IN-PLACE IS USED, IT SHALL BE PAID FOR USING THE EXTRUDED THERMOPLASTIC PAY ITEM.

PLAN PREPARED BY: N.C.D.O.T. SIGNING AND DELINEATION UNIT

A. I. ALQUDWAH, P.E. SIGNING & DELINEATION REGIONAL ENGINEER
D. M. EATON SIGNING & DELINEATION PROJECT DESIGN ENGINEER/TECHNICIAN



INDEX

SHEET NO.	DESCRIPTION
PMP-1	PAVEMENT MARKING PLAN TITLE, NOTES, AND SCHEDULE