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See Sheet 1-A For Index of Sheets
See Sheet 1-B For Conventional Symbols
See Sheet 1C-1 For Survey Control

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSYLVANIA COUNTY

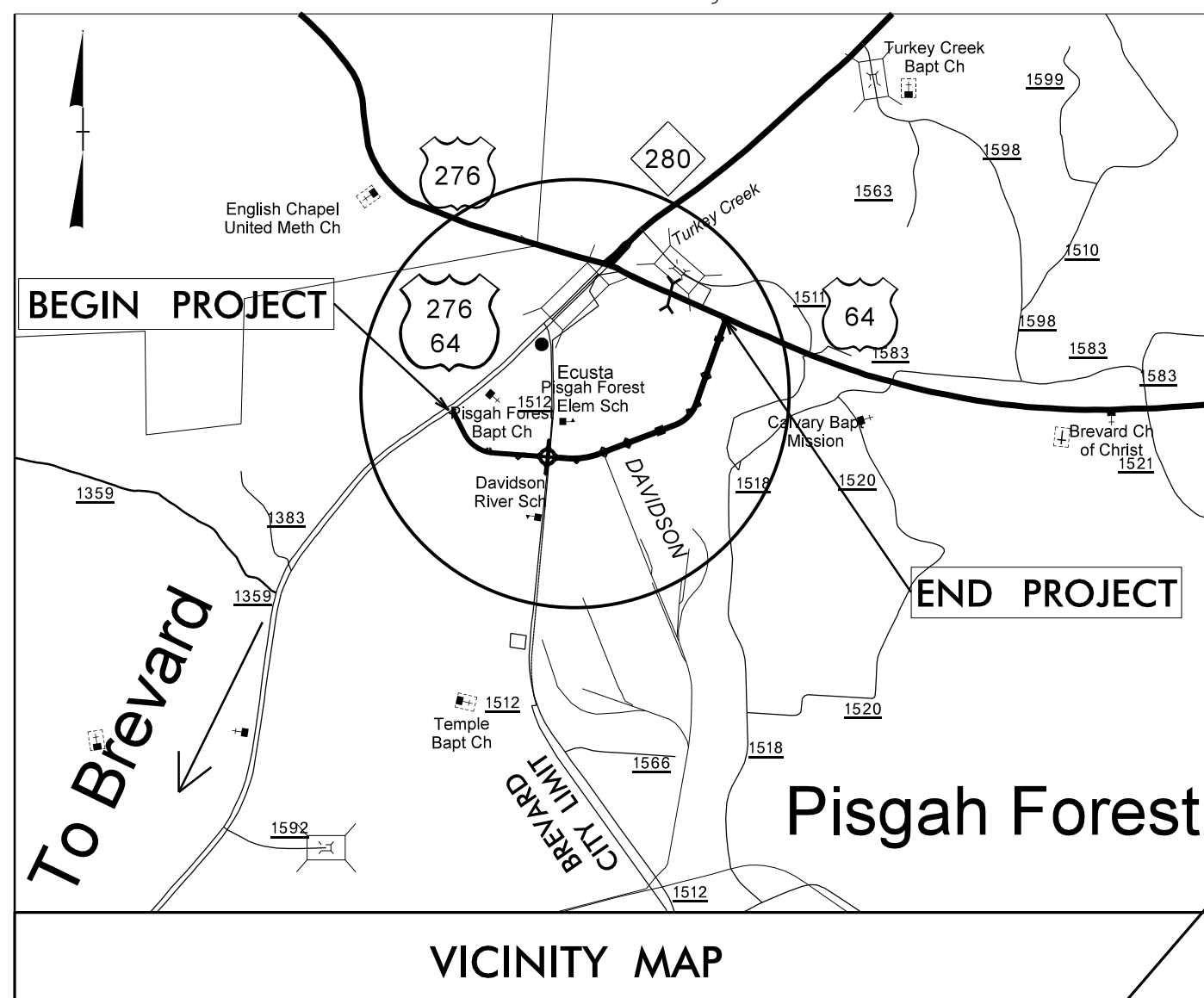
LOCATION: DAVIDSON RIVER VILLAGE CONNECTOR FROM
US 64/276 TO US 64, CITY OF BREVARD

TYPE OF WORK: GRADING, DRAINAGE, PAVING, SIGNALS AND STRUCTURES

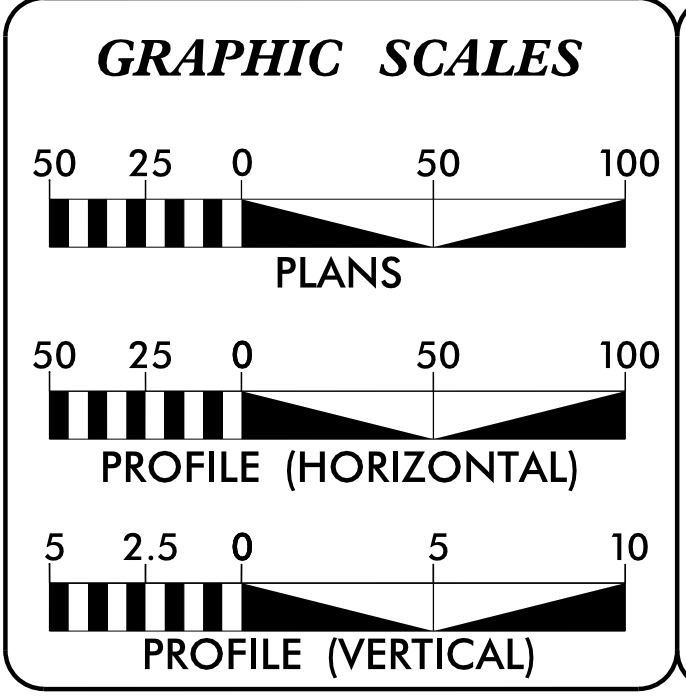
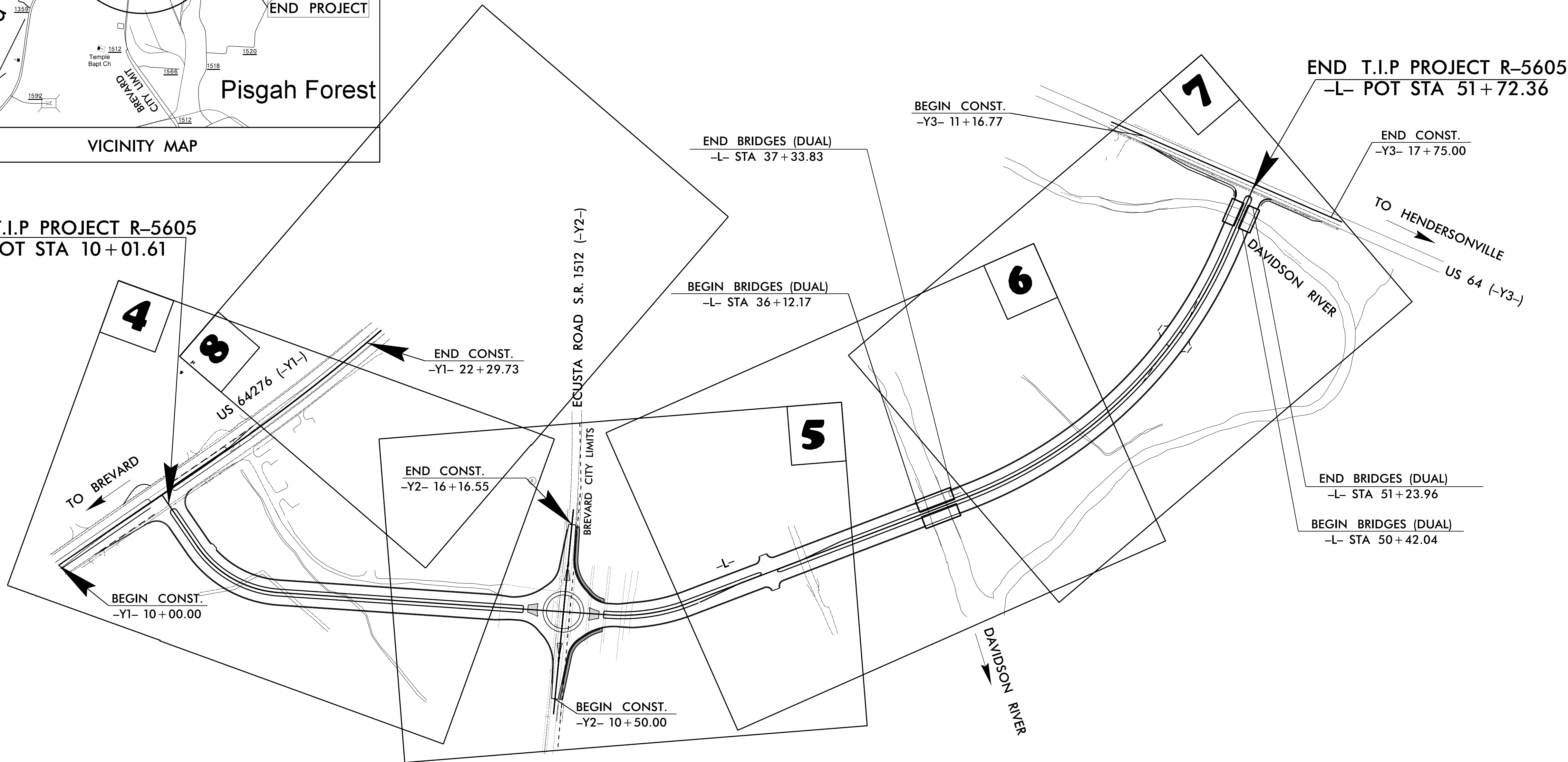
STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	R-5605	1	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
43587.1.1		PE	
43587.2.2		RW, UTIL.	
43587.3.1		CONST.	

TIP PROJECT: R-5605

CONTRACT: C203752



BEGIN T.I.P. PROJECT R-5605
-L- POT STA 10+01.61



DESIGN DATA

ADT 2016 =	17,400
ADT 2036 =	21,100
K =	10 %
D =	55 %
T =	6 % *
V =	30 MPH
* TTST =	4 DUAL 2
CLASS =	COLLECTOR
REGIONAL TIER	

PROJECT LENGTH

LENGTH OF ROADWAY PROJECT R-5605 =	0.751 MI
LENGTH OF STRUCTURES PROJECT R-5605 =	0.039 MI
TOTAL LENGTH OF PROJECT R-5605 =	0.790 MI

Prepared for Division of Highways in the Office of:
WGLA Engineering, PLLC
214 N. King Street, Hendersonville NC 28792

2012 STANDARD SPECIFICATIONS

RIGHT OF WAY DATE:
AUGUST 13, 2015

LETTING DATE:
FEBRUARY 16, 2016

WILLIAM R. BUJE, PE PROJECT ENGINEER
G. THOMAS JONES III, PE PROJECT DESIGN ENGINEER
STEVE WILLIAMS NCDOT CONTACT

HYDRAULICS ENGINEER

DocuSigned by:
Dana J. Bolden
28945

SIGNATURE: _____

ROADWAY DESIGN ENGINEER

DocuSigned by:
G. Thomas Jones III
023091

SIGNATURE: _____

1/11/2016

**DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA**















STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

CONVENTIONAL PLAN SHEET SYMBOLS












Note: Not to Scale

*S.U.E. = *Subsurface Utility Engineering*










BOUNDARIES AND PROPERTY:

State Line	-----
County Line	-----
Township Line	-----
City Line	-----
Reservation Line	-----
Property Line	-----
Existing Iron Pin	----- 
Property Corner	----- 
Property Monument	----- 
Parcel/Sequence Number	----- 
Existing Fence Line	----- 
Proposed Woven Wire Fence	----- 
Proposed Chain Link Fence	----- 
Proposed Barbed Wire Fence	----- 
Existing Wetland Boundary	----- 
Proposed Wetland Boundary	----- 
Existing Endangered Animal Boundary	----- 
Existing Endangered Plant Boundary	----- 
Known Soil Contamination: Area or Site	----- 
Potential Soil Contamination: Area or Site	----- 

BUILDINGS AND OTHER CULTURE:

Gas Pump Vent or U/G Tank Cap	----- 
Sign	----- 
Well	----- 
Small Mine	----- 
Foundation	----- 
Area Outline	----- 
Cemetery	----- 
Building	----- 
School	----- 
Church	----- 
Dam	----- 



















HYDROLOGY:

Stream or Body of Water	-----
Hydro, Pool or Reservoir	-----
Jurisdictional Stream	----- 
Buffer Zone 1	----- 
Buffer Zone 2	----- 
Flow Arrow	----- 
Disappearing Stream	----- 
Spring	----- 
Wetland	----- 
Proposed Lateral, Tail, Head Ditch	----- 
False Sump	----- 














RAILROADS:

Standard Gauge	----- 
RR Signal Milepost	----- 
Switch	----- 
RR Abandoned	-----
RR Dismantled	-----

RIGHT OF WAY:

Baseline Control Point	----- 
Existing Right of Way Marker	----- 
Existing Right of Way Line	----- 
Proposed Right of Way Line	----- 
Proposed Right of Way Line with Iron Pin and Cap Marker	----- 
Proposed Right of Way Line with Concrete or Granite RW Marker	----- 
Proposed Control of Access Line with Concrete CA Marker	----- 
Existing Control of Access	----- 
Proposed Control of Access	----- 
Existing Easement Line	----- 
Proposed Temporary Construction Easement	----- 
Proposed Temporary Drainage Easement	----- 
Proposed Permanent Drainage Easement	----- 
Proposed Permanent Drainage / Utility Easement	----- 
Proposed Permanent Utility Easement	----- 
Proposed Temporary Utility Easement	----- 
Proposed Aerial Utility Easement	----- 
Proposed Permanent Easement with Iron Pin and Cap Marker	----- 

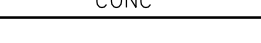

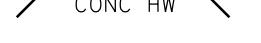


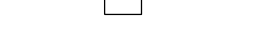



ROADS AND RELATED FEATURES:

Existing Edge of Pavement	-----
Existing Curb	-----
Proposed Slope Stakes Cut	----- 
Proposed Slope Stakes Fill	----- 
Proposed Curb Ramp	----- 
Existing Metal Guardrail	----- 
Proposed Guardrail	----- 
Existing Cable Guiderail	----- 
Proposed Cable Guiderail	----- 
Equality Symbol	----- 
Pavement Removal	----- 
Single Tree	----- 
Single Shrub	----- 
Hedge	----- 
Woods Line	----- 









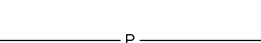
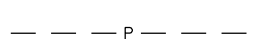

VEGETATION:

Orchard	----- 
Vineyard	----- 







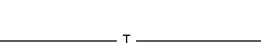

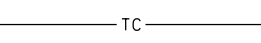
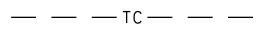

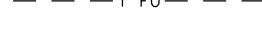

EXISTING STRUCTURES:

MAJOR:	
Bridge, Tunnel or Box Culvert	----- 
Bridge Wing Wall, Head Wall and End Wall	----- 
MINOR:	
Head and End Wall	----- 
Pipe Culvert	----- 
Footbridge	----- 
Drainage Box: Catch Basin, DI or JB	----- 
Paved Ditch Gutter	----- 
Storm Sewer Manhole	----- 
Storm Sewer	----- 







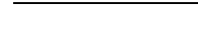
UTILITIES:

POWER:	
Existing Power Pole	----- 
Proposed Power Pole	----- 
Existing Joint Use Pole	----- 
Proposed Joint Use Pole	----- 
Power Manhole	----- 
Power Line Tower	----- 
Power Transformer	----- 
U/G Power Cable Hand Hole	----- 
H-Frame Pole	----- 
Recorded U/G Power Line	----- 
Designated U/G Power Line (S.U.E.*)	----- 







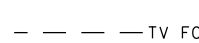

TELEPHONE:

Existing Telephone Pole	----- 
Proposed Telephone Pole	----- 
Telephone Manhole	----- 
Telephone Booth	----- 
Telephone Pedestal	----- 
Telephone Cell Tower	----- 
U/G Telephone Cable Hand Hole	----- 
Recorded U/G Telephone Cable	----- 
Designated U/G Telephone Cable (S.U.E.*)	----- 
Recorded U/G Telephone Conduit	----- 
Designated U/G Telephone Conduit (S.U.E.*)	----- 
Recorded U/G Fiber Optics Cable	----- 
Designated U/G Fiber Optics Cable (S.U.E.*)	----- 






WATER:

Water Manhole	----- 
Water Meter	----- 
Water Valve	----- 
Water Hydrant	----- 
Recorded U/G Water Line	----- 
Designated U/G Water Line (S.U.E.*)	----- 
Above Ground Water Line	----- 







TV:

TV Satellite Dish	----- 
TV Pedestal	----- 
TV Tower	----- 
U/G TV Cable Hand Hole	----- 
Recorded U/G TV Cable	----- 
Designated U/G TV Cable (S.U.E.*)	----- 
Recorded U/G Fiber Optic Cable	----- 
Designated U/G Fiber Optic Cable (S.U.E.*)	----- 






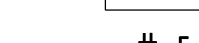






GAS:

Gas Valve	----- 
Gas Meter	----- 
Recorded U/G Gas Line	----- 
Designated U/G Gas Line (S.U.E.*)	----- 
Above Ground Gas Line	----- 

SANITARY SEWER:

Sanitary Sewer Manhole	----- 
Sanitary Sewer Cleanout	----- 
U/G Sanitary Sewer Line	----- 
Above Ground Sanitary Sewer	----- 
Recorded SS Forced Main Line	----- 
Designated SS Forced Main Line (S.U.E.*)	----- 

MISCELLANEOUS:

Utility Pole	----- 
Utility Pole with Base	----- 
Utility Located Object	----- 
Utility Traffic Signal Box	----- 
Utility Unknown U/G Line	----- 
U/G Tank; Water, Gas, Oil	----- 
Underground Storage Tank, Approx. Loc.	----- 
A/G Tank; Water, Gas, Oil	----- 
Geoenvironmental Boring	----- 
U/G Test Hole (S.U.E.*)	----- 
Abandoned According to Utility Records	----- 
End of Information	----- 

SURVEY CONTROL SHEET NCGS MONUMENT "OLIN" -FINAL-

PROJECT REFERENCE NO.	SHEET NO.
R-5605	1C-1
LOCATIONS AND SURVEYS	

FINAL RIGHT-OF-WAY MARKERS TO BE SET

ALIGN	STATION	OFFSET	NORTH	EAST
L	10+37.32	RT EX RW	573418.93	892824.23
L	10+37.39	LT EX RW	573485.68	892922.20
L	11+63.58	80' RT	573314.58	892896.84
L	11+63.58	LT EX PL	573363.35	892996.05
L	12+40.18	LT EX PL	573331.58	893033.41
L	14+00.82	60' LT	573268.81	893150.54
L	14+78.20	60' LT	573257.23	893214.37
L	14+78.20	80' RT	573137.50	893206.30
L	22+40.00	60' LT	573205.96	893974.45
L	22+40.00	80' RT	573086.23	893968.37
L	23+17.00	114' RT	573027.17	894038.56
L	23+26.50	102' LT	573242.07	894053.08
L	24+77.00	102' RT	573028.37	894200.01
L	24+78.00	104' LT	573233.77	894215.87
L	25+49.80	60' LT	573185.10	894283.65
L	25+49.80	80' RT	573065.37	894275.57
L	28+08.26	60' LT	573219.20	894512.11
L	28+08.00	80' RT	573108.85	894504.53
L	37+70.39	60' LT	573561.50	895411.29
L	37+70.39	80' RT	573440.35	895403.38
L	49+65.86	60' LT	574333.38	896219.68
L	49+65.86	80' RT	574285.55	896329.73
L	51+14.86	LT EX RW	574470.48	896279.25
L	51+14.86	RT EX RW	574422.21	896389.12
Y2	10+50.00	LT EX RW	572856.31	894073.99
Y2	10+50.00	RT EX RW	572850.78	894131.15
Y2	11+62.00	42' RT	572960.82	894155.54
Y2	15+60.00	LT EX RW	573363.88	894123.06
Y2	15+60.00	40' RT	573327.25	894192.00
Y2	16+18.55	40' RT	573413.54	894197.46
Y2	16+18.55	22.83 RT	573415.20	894180.37

I, Phillip B. White, certify that this map was drawn under my supervision from an actual survey made under my supervision using GNSS (GPS) and traditional field procedures; that the ratio of precision as calculated [(21 NCAC 56.1603)] is 1:10,000 or better (Class A); that this survey is oriented to NC Grid, NAD '83 (CORS 96)(2002) based upon global navigation satellite system (GNSS) observations; that the vertical control was completed to meet the Class C standard [(21 NCAC 56.1605 (a))].

The following information was used to perform the GNSS portion of the survey.
 Horizontal Positional Accuracy: 0.005 Meters
 Vertical Positional Accuracy: 0.003 Meters
 Type of GNSS Field Procedure: Static (Post-Processed using Online Positioning User Service, OPUS-RS)
 Date of GNSS Survey: 8-16-2006
 Horizontal Datum/Epoch: NAD '83 (CORS 96) Epoch 2002.00
 Vertical Datum: NAVD '88, Geoid 03
 Combined Grid Factor: 0.9997750
 Base Point of Localization: NCGS MONUMENT "OLIN"
 Units: U.S. Survey Feet

Witness my signature, License Number, and Seal this 23rd Day of December, A.D., 2015



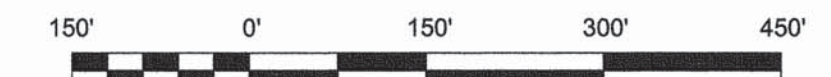
12-23-2015

DATUM DESCRIPTION

THE LOCALIZED COORDINATE SYSTEM DEVELOPED FOR THIS PROJECT IS BASED UPON THE NAD 83 (CORS 96)(2002) STATE PLANE COORDINATES ESTABLISHED BY GLOBAL NAVIGATION SATELLITE SYSTEM OBSERVATIONS ON SITE AND ON NCGS MONUMENT "OLIN". POSITIONS ARRIVED AT BY ONLINE POSITIONING USER SERVICE (OPUS-RS). MONUMENT "OLIN" HAVING NO GRID NAD 83 (CORS 96)(2002) COORDINATES OF N: 572877.403; E: 894079.146; ELEV: 2131.88'. THE AVERAGE COMBINED GRID FACTOR USED FOR THIS PROJECT IS 0.9997750. ALL COORDINATES SHOWN ARE GROUND COORDINATES LOCALIZED TO NCGS MONUMENT "OLIN". THE NC GRID BEARING AND HORIZONTAL GROUND DISTANCE FROM NCGS MONUMENT "OLIN" TO L-STATION 9+62.83 IS N62°57'27"W 1400.88'. VERTICAL DATUM IS REFERENCED TO NAVD 1988.

NOTES

- * Horizontal Datum is North Carolina Grid, North American Datum of 1983, based on multiple five to six hour sessions of GPS reception at monument OLIN. Position arrived at by OPUS solution provided by National Geodetic Survey. Combined Grid Factor is: 0.9997750
- * All bearings are North Carolina Grid Bearings (NAD '83) unless otherwise noted.
- * All distances shown hereon are horizontal ground distances unless otherwise noted.
- * Station and offset information shown hereon was obtained from road right-of-way construction plans and have not been calculated hereon. Curves shown on said plans and corresponding stations along centerline were calculated using chord definition curves. See said construction plans for more details.

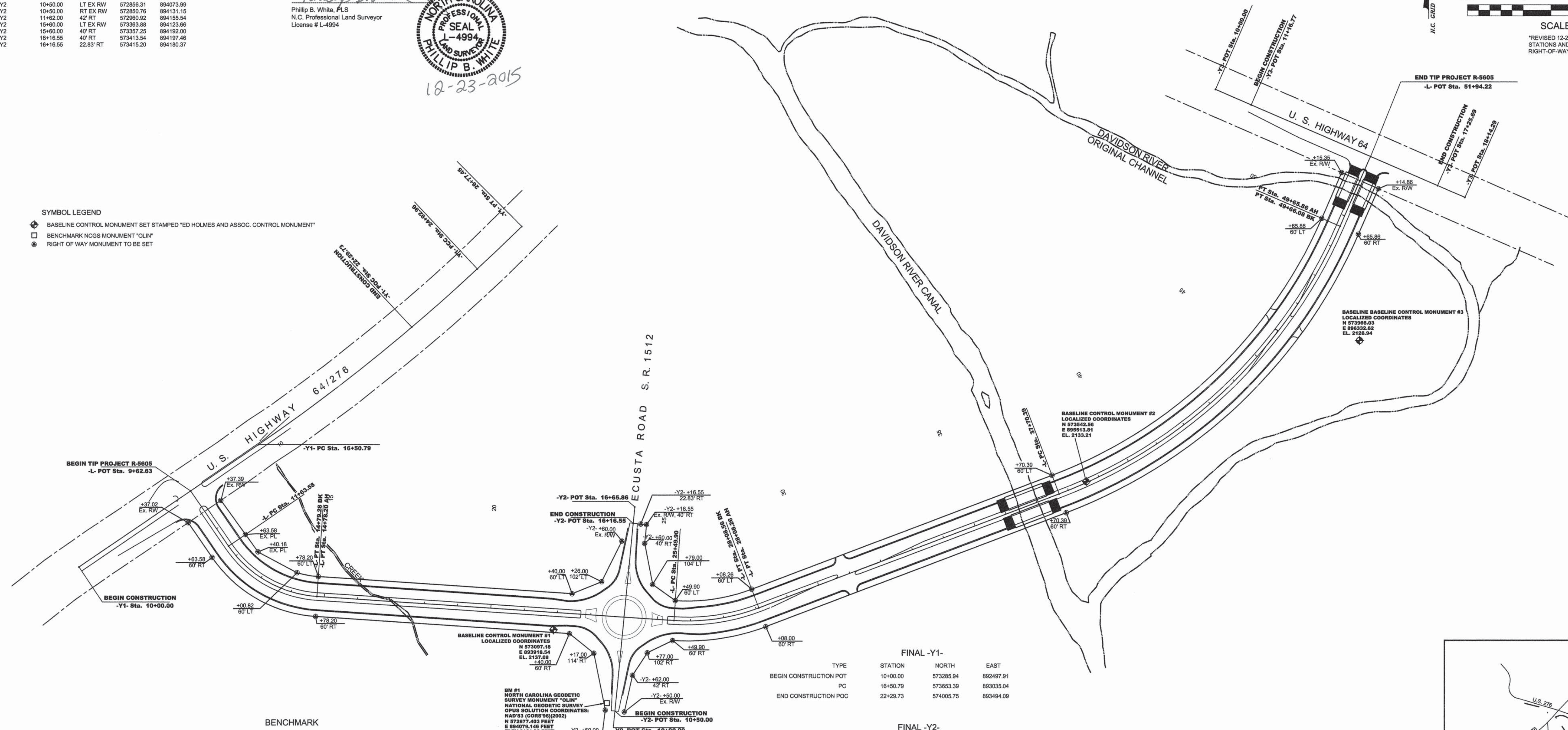


SCALE 1" = 150'

*REVISED 12-23-15 TO ADD EQUALITY TO PT STATIONS AND REVISE STATIONS OF TWO RIGHT-OF-WAY MONUMENTS

SYMBOL LEGEND

- ◆ BASELINE CONTROL MONUMENT SET STAMPED "ED HOLMES AND ASSOC. CONTROL MONUMENT"
- BENCHMARK NCGS MONUMENT "OLIN"
- RIGHT OF WAY MONUMENT TO BE SET



BENCHMARK

BM #1: NCGS MONUMENT "OLIN" N: 572877.403 E: 894079.146 ELEV: 2131.88

BASELINE CONTROL MONUMENTS

BASELINE POINT	NORTH	EAST	ELEVATION
#1	573097.18	893918.54	2137.08
#2	573542.56	895513.81	2133.21
#3	573966.03	896332.62	2126.94

FINAL -L-

TYPE	STATION	NORTH	EAST
BEGIN TIP PROJECT POT	9+62.83	573514.22	892831.61
PC	11+63.58	573348.53	892945.31
PT	14+78.20	573197.37	893210.34
PC	25+49.80	573125.24	894279.61
PT	28+08.26	573163.13	894533.45
PC	37+70.39	573505.43	895432.84
PT	49+65.86	574309.47	896274.71
END TIP PROJECT POT	51+94.22	574309.47	896274.71

FINAL -Y1-

TYPE	STATION	NORTH	EAST
BEGIN CONSTRUCTION POT	10+00.00	573285.84	892497.91
PC	16+50.79	573853.39	893035.04
END CONSTRUCTION POC	22+28.73	574005.75	893494.09

FINAL -Y2-

TYPE	STATION	NORTH	EAST
POT	10+00.00	572803.73	894098.08
BEGIN CONSTRUCTION POT	10+50.00	572853.54	894102.92
END CONSTRUCTION POT	16+18.55	573417.40	894157.65
POT	16+65.86	573466.48	894162.41

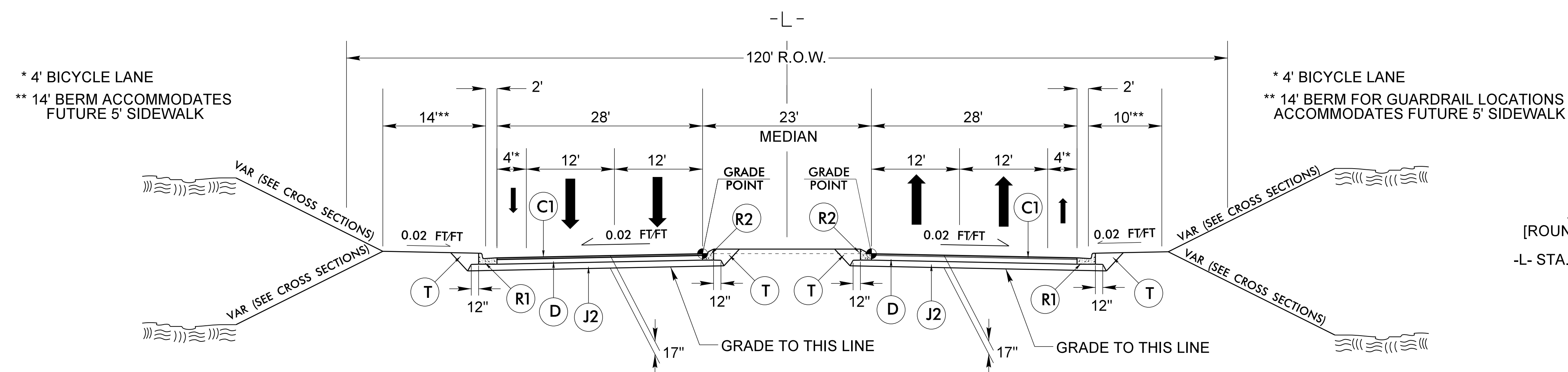
FINAL -Y3-

TYPE	STATION	NORTH	EAST
POT	10+00.00	574720.74	896901.42
BEGIN CONSTRUCTION POT	11+16.77	574674.19	896808.51
END CONSTRUCTION POT	17+25.69	574431.42	896598.94
POT	18+14.29	574396.11	896648.20

5/14/14

PROJECT REFERENCE NO. R-5605	SHEET NO. 2A-1
RW SHEET NO.	
ROADWAY DESIGN ENGINEER DESIGNED BY B. Thomas Jones III SEAL 023091 1/11/2016	PAVEMENT ENGINEER SEAL 022896 Clark Morrison

PAVEMENT SCHEDULE			
C1	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.	R1	2'-6" CONCRETE CURB AND GUTTER
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.	R2	1'-6" CONCRETE CURB AND GUTTER
J1	PROP. 8" AGGREGATE BASE COURSE.	S	CONCRETE SIDEWALK
J2	PROP. 10" AGGREGATE BASE COURSE.	T	EARTH MATERIAL
J3	PROP. 4" AGGREGATE BASE COURSE.	U	EXISTING PAVEMENT
R	8" CONCRETE APRON	NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE	



USE TYPICAL SECTION NO. 1
 -L- STA. 10+01.61 TO 36+12.17 (BEGIN BRIDGE)
 [ROUNDBOUT EXCLUDED - SEE TYPICAL SECTION NO. 4]
 -L- STA. 37+33.83 (END BRIDGE) TO 50+42.04 (BEGIN BRIDGE)
 -L- STA. 51+23.96 (END BRIDGE) TO 51+72.36

TYPICAL SECTION NO. 1

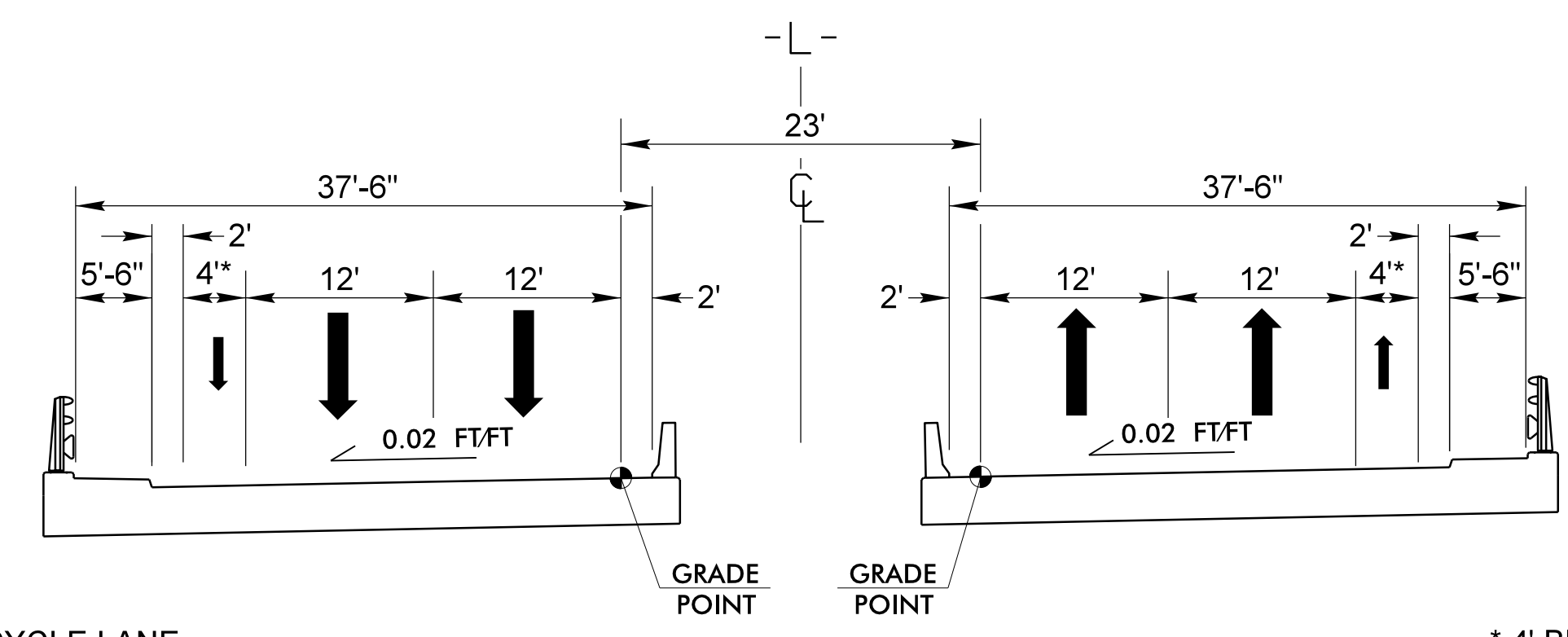
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PLANS PREPARED BY:

WGLA Engineering, PLLC
 NC License No.: P-1342
 214 N King Street Hendersonville, NC 28792
 (828) 687-7177 wglac.com

FOR
 DIVISION OF HIGHWAYS

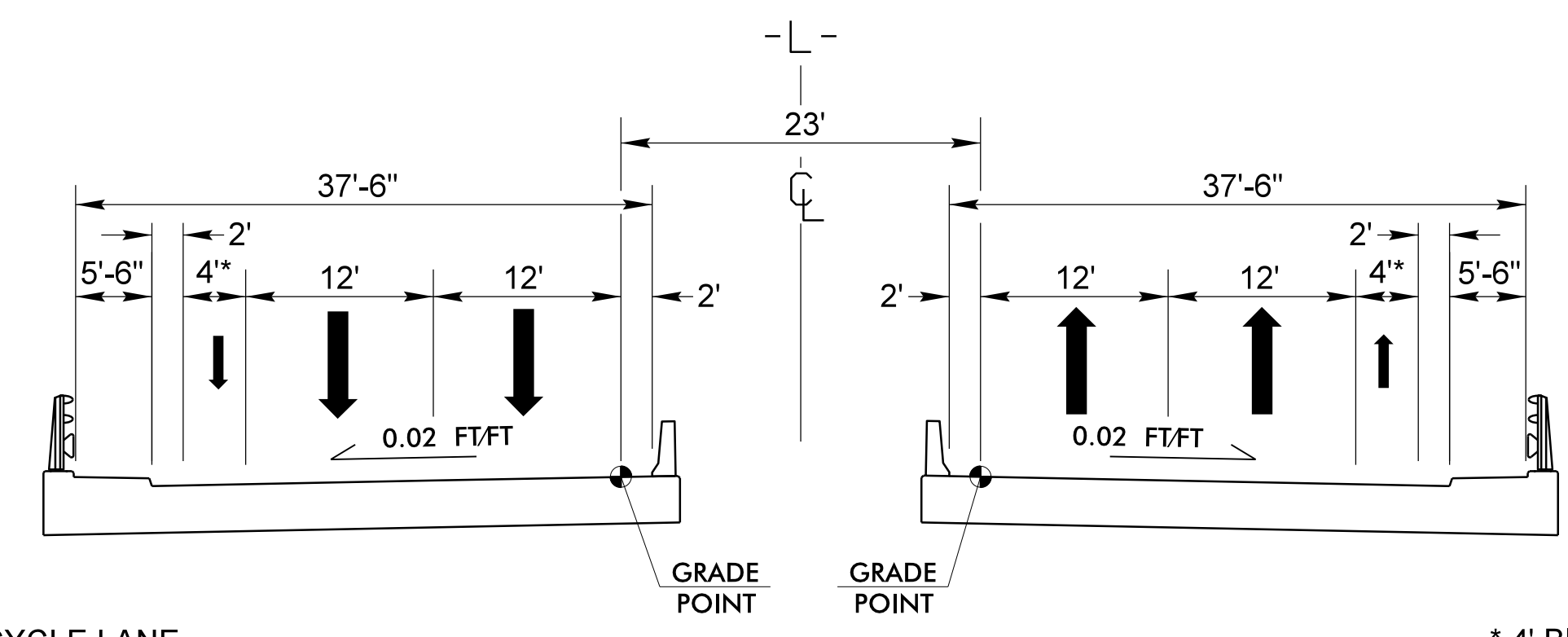
PROJECT REFERENCE NO. R-5605	SHEET NO. 2A-2
RW SHEET NO.	
ROADWAY DESIGN ENGINEER E. Thomas Jones III SEAL 023091 1/11/2016	PAVEMENT ENGINEER Clark Morrison SEAL 022896 1/11/2016



* 4' BICYCLE LANE

TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2
-L- BEGIN BRIDGE STA. 36+12.17
TO END BRIDGE STA. 37+33.83



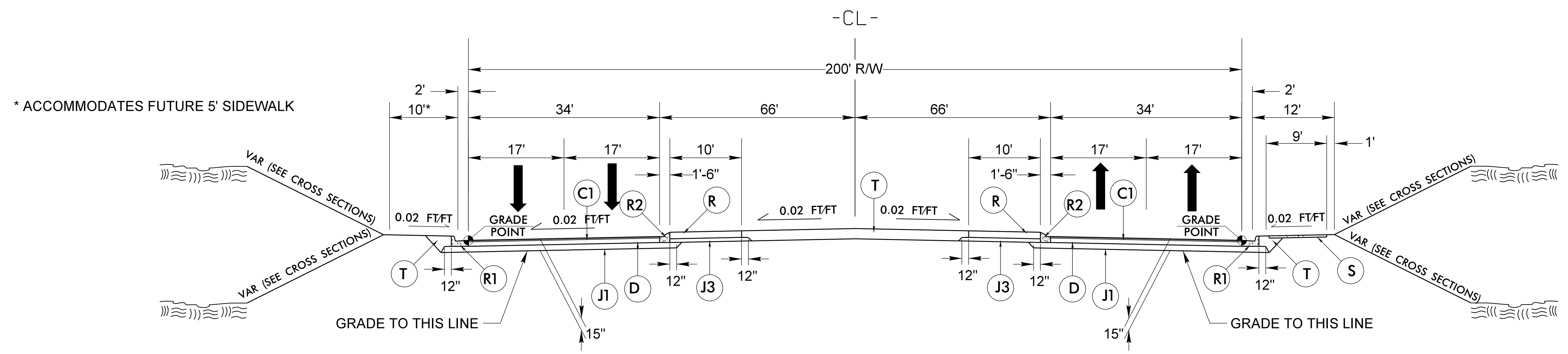
* 4' BICYCLE LANE

TYPICAL SECTION NO. 3

USE TYPICAL SECTION NO. 3
-L- BEGIN BRIDGE STA. 50+42.04
TO END BRIDGE STA. 51+23.96

PAVEMENT DESIGN	
C1	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B.
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B
J1	PROP. 8" AGGREGATE BASE COURSE.
J3	PROP. 4" AGGREGATE BASE COURSE.
R	8" CONCRETE APRON
R1	2'-6" CONCRETE CURB AND GUTTER
R2	1'-6" CONCRETE CURB AND GUTTER
S	CONCRETE SIDEWALK
T	EARTH MATERIAL

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE



* ACCOMMODATES FUTURE 5' SIDEWALK

DETAIL SHOWING ROUNDABOUT TYPICAL SECTION

(DESIGN VEHICLE = WB-67)

TYPICAL SECTION NO. 4

USE TYPICAL SECTION NO. 4
-L- STA. 23+00.00 TO 25+00.00

5/14/2014 11:44:59 AM \\P:\N\2014\DIRV\2014\Roadway\Proj\RB605_RdJ_typ.dgn

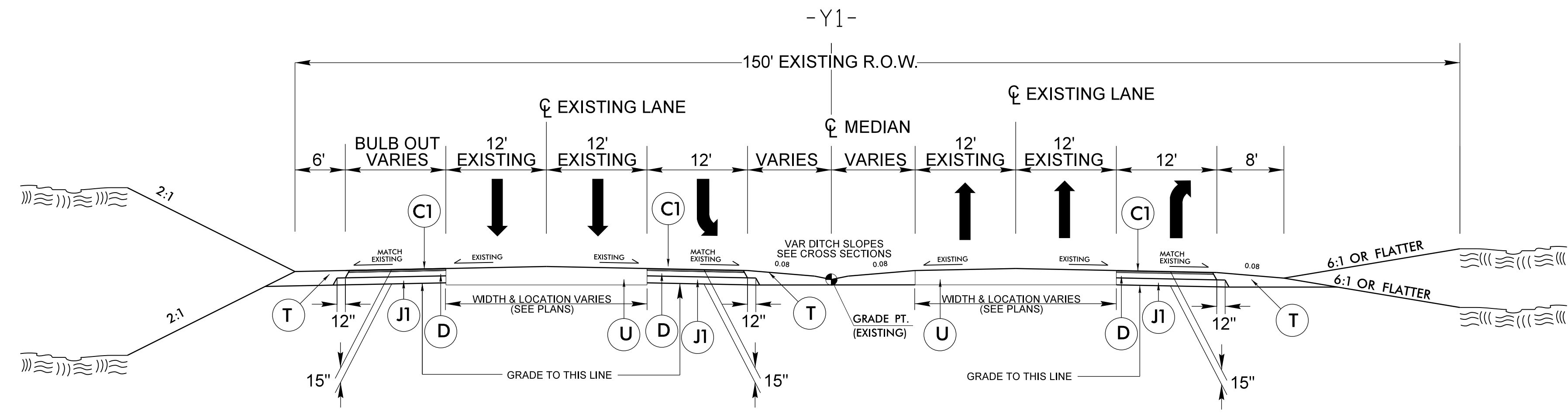
PLANS PREPARED BY:

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NC License No.: P-1342
214 N King Street Hendersonville, NC 28792
(828) 687-7177 wglacorp.com

FOR
DIVISION OF HIGHWAYS

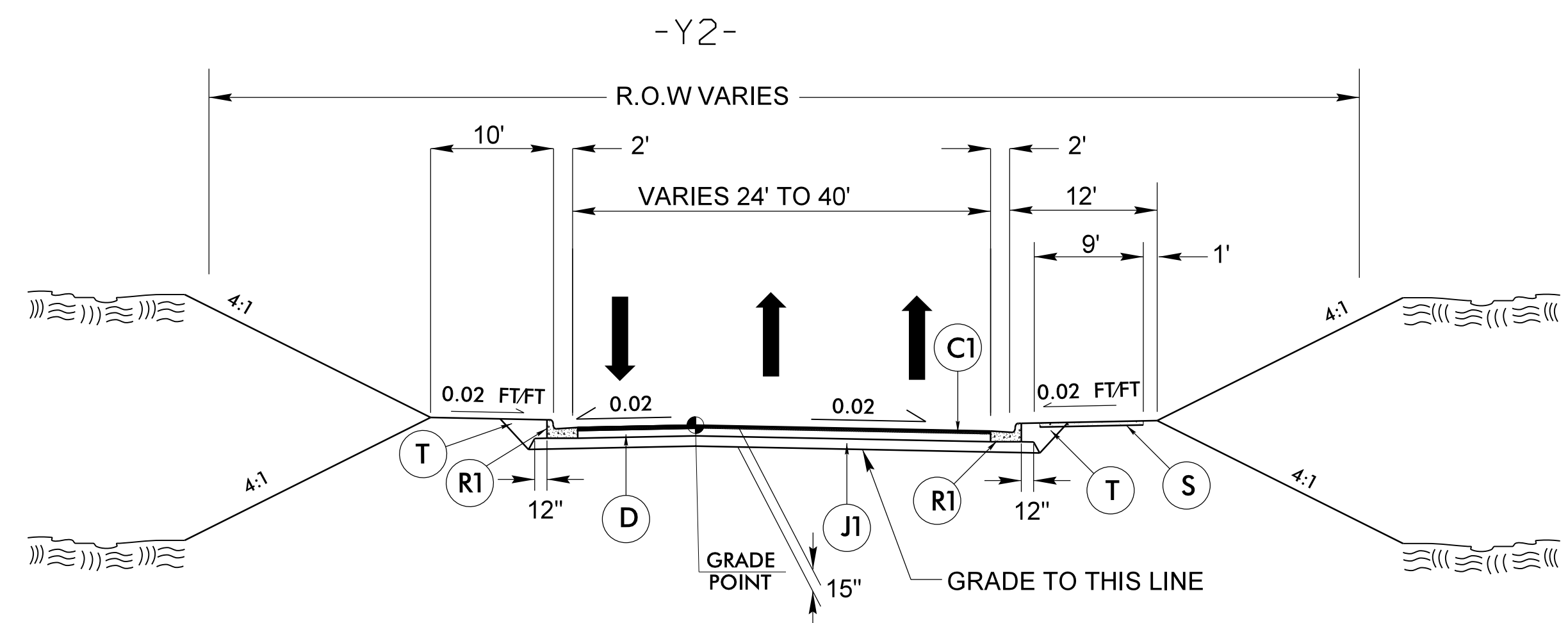
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RW SHEET NO.	
ROADWAY DESIGN ENGINEER <i>Y. Thomas Jones III</i> SEAL 023091 1/11/2016	PAVEMENT ENGINEER <i>Clark Morris</i> SEAL 022896 1/11/2016

PAVEMENT DESIGN			
C1	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B.	R1	2'-6" CONCRETE CURB AND GUTTER
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B	S	CONCRETE SIDEWALK
J1	PROP. 8" AGGREGATE BASE COURSE.	T	EARTH MATERIAL
J3	PROP. 4" AGGREGATE BASE COURSE.	U	EXISTING PAVEMENT
R	8" CONCRETE APRON	NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE	



TYPICAL SECTION NO. 5

USE TYPICAL SECTION NO. 5
-Y1- STA. 10+00 (BEGIN CONST.) TO 20+35.06 (END CONST.)




TYPICAL SECTION NO. 6

USE TYPICAL SECTION NO. 6
-Y2- STA. 10+50 (BEGIN CONST.) TO 12+33.12
14+33.12 TO 16+16.55 (END CONST.)

5/14/99

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PLANS PREPARED BY:



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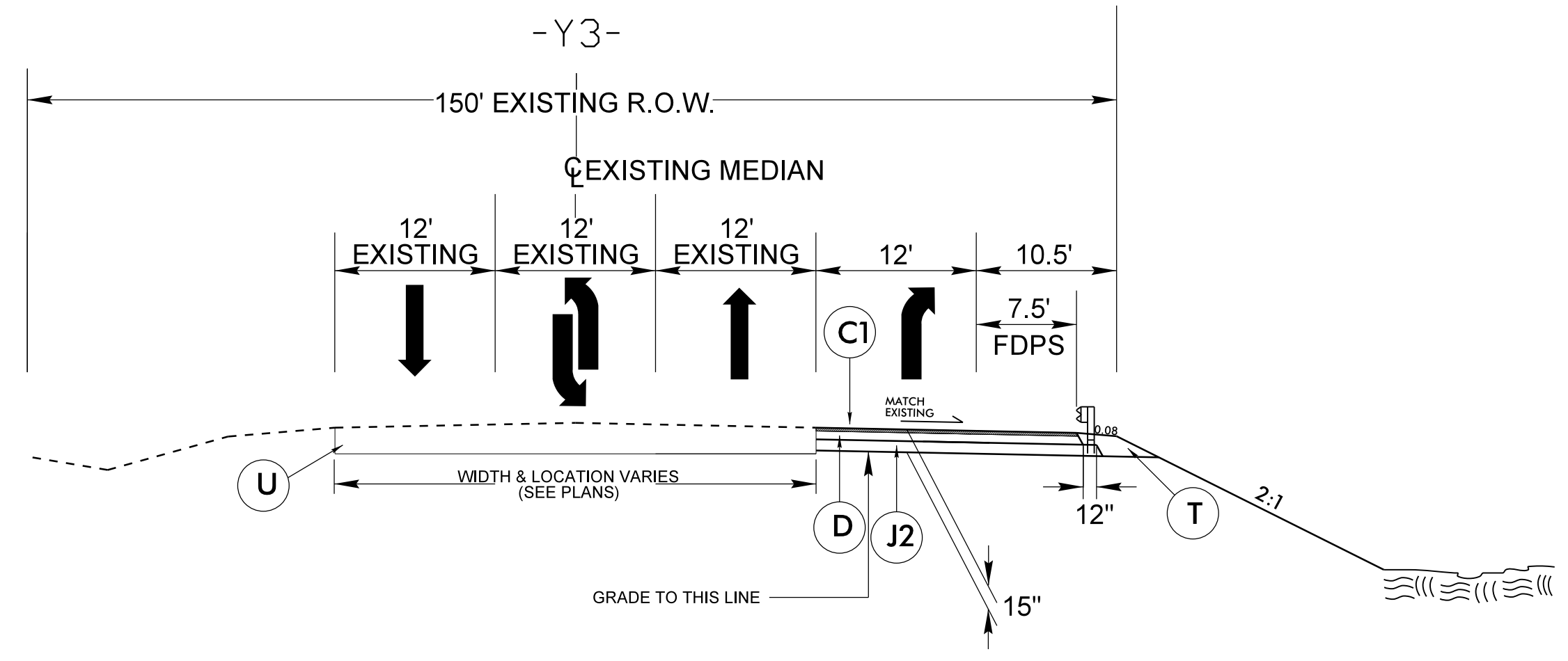
FOR
DIVISION OF HIGHWAYS

5/14/99

PAVEMENT DESIGN

C1	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B.	R1	2'-6" CONCRETE CURB AND GUTTER
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B	S	CONCRETE SIDEWALK
J1	PROP. 8" AGGREGATE BASE COURSE.	T	EARTH MATERIAL
J2	PROP. 10" AGGREGATE BASE COURSE.	U	EXISTING PAVEMENT
R	8" CONCRETE APRON	NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE	

PROJECT REFERENCE NO. <i>R-5605</i>	SHEET NO. <i>2A-4</i>
RW SHEET NO.	
ROADWAY DESIGN ENGINEER <i>W. Thomas Jones III</i> SEAL 023091 1/11/2016	PAVEMENT ENGINEER <i>Clark Morrison</i> SEAL 022896 1/11/2016




TYPICAL SECTION NO. 7

USE TYPICAL SECTION NO. 7

-Y3- STA. 11+16.77 (BEGIN CONST.) TO 17+75.00 (END CONST.)

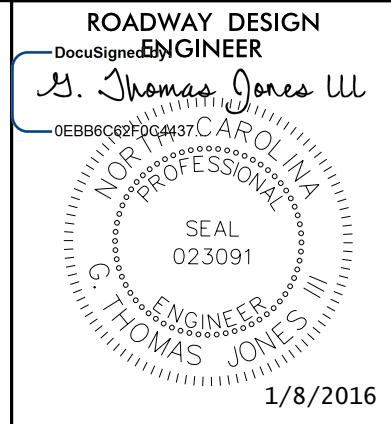
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PLANS PREPARED BY:

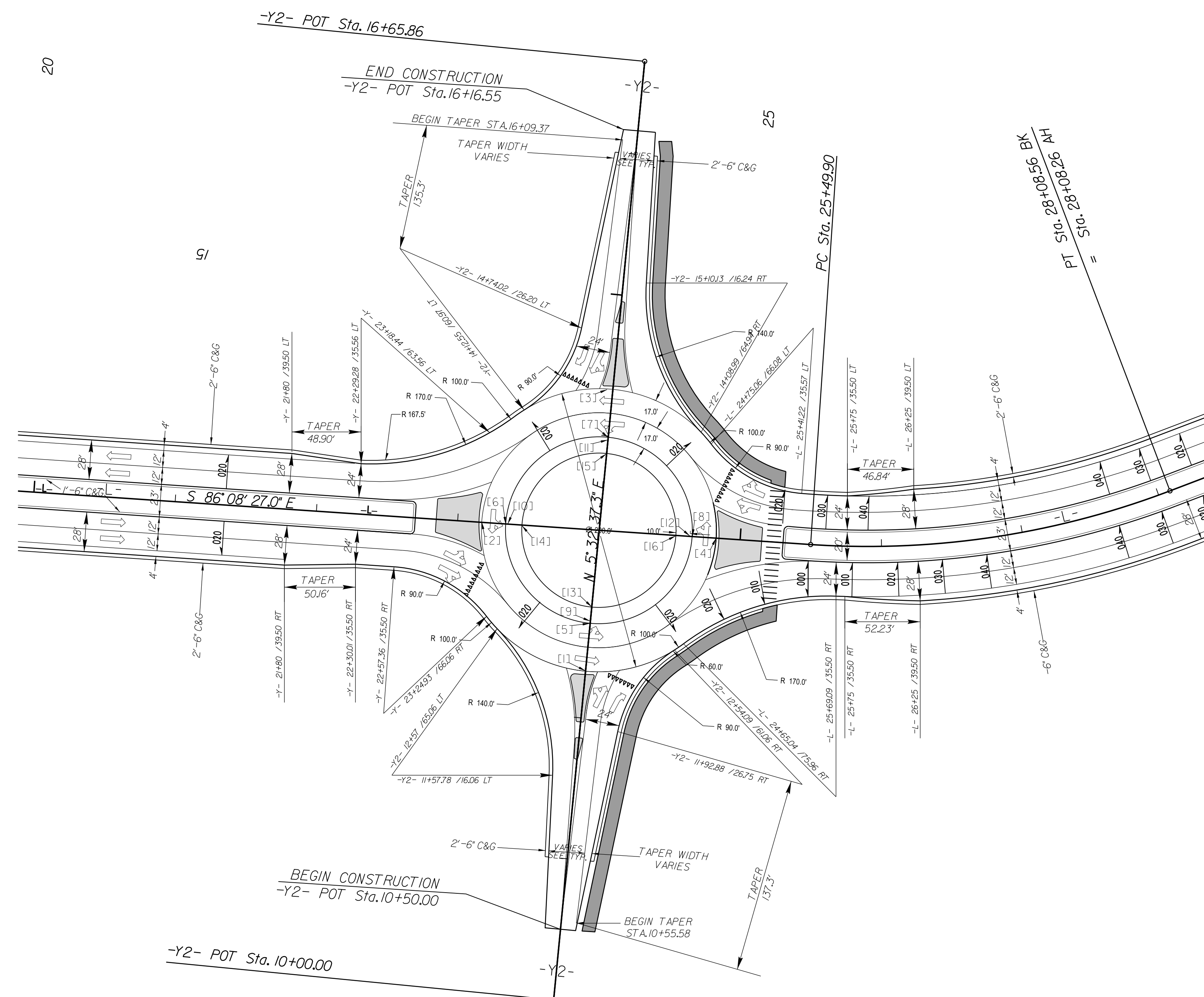
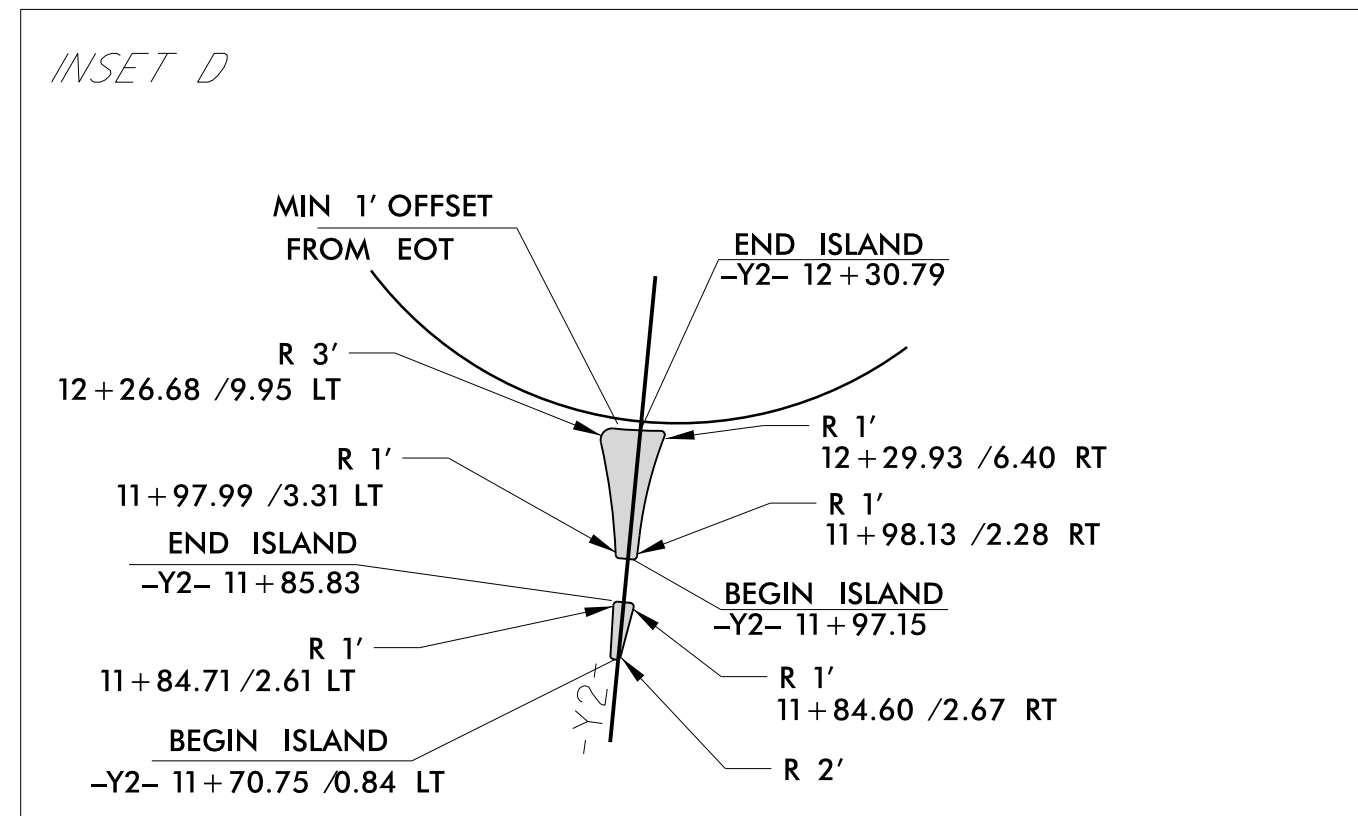
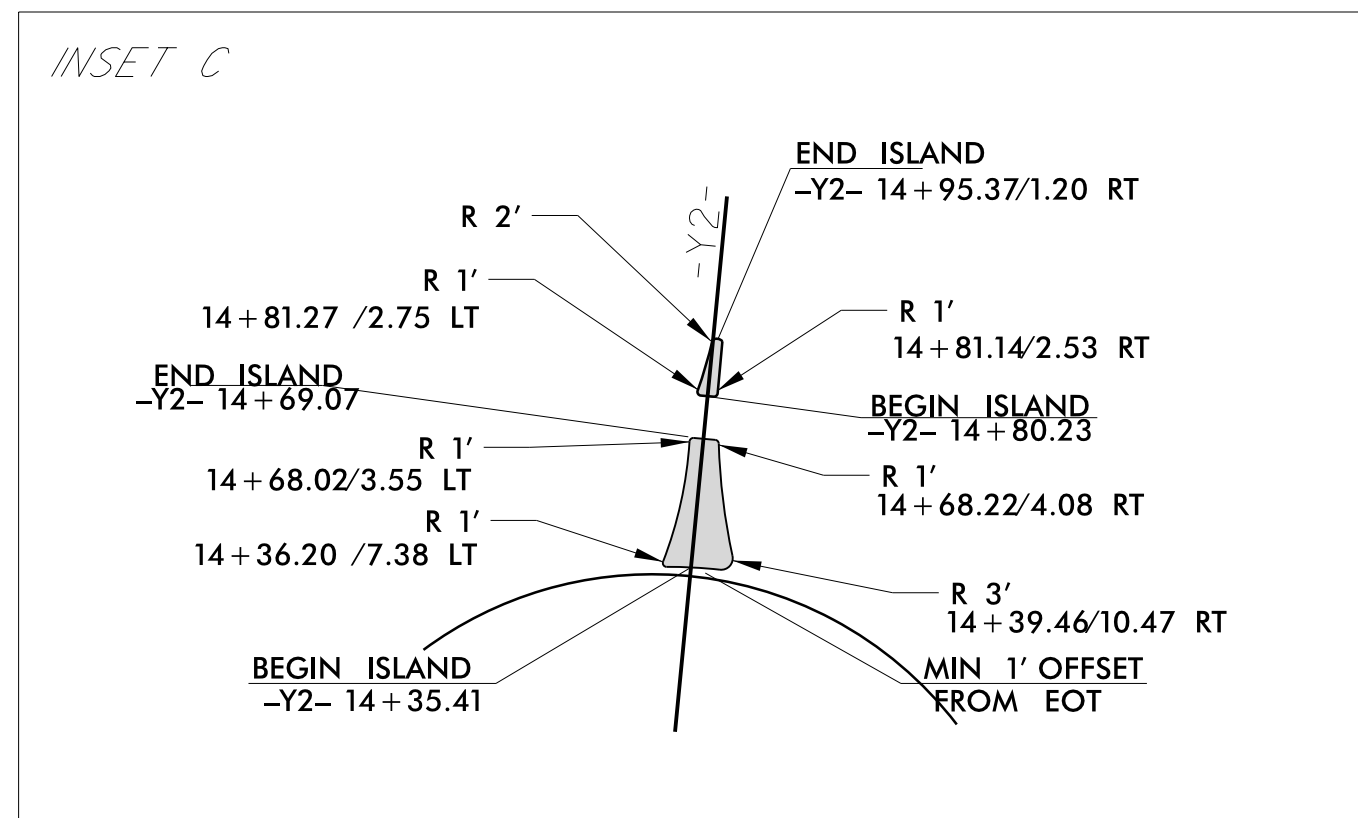
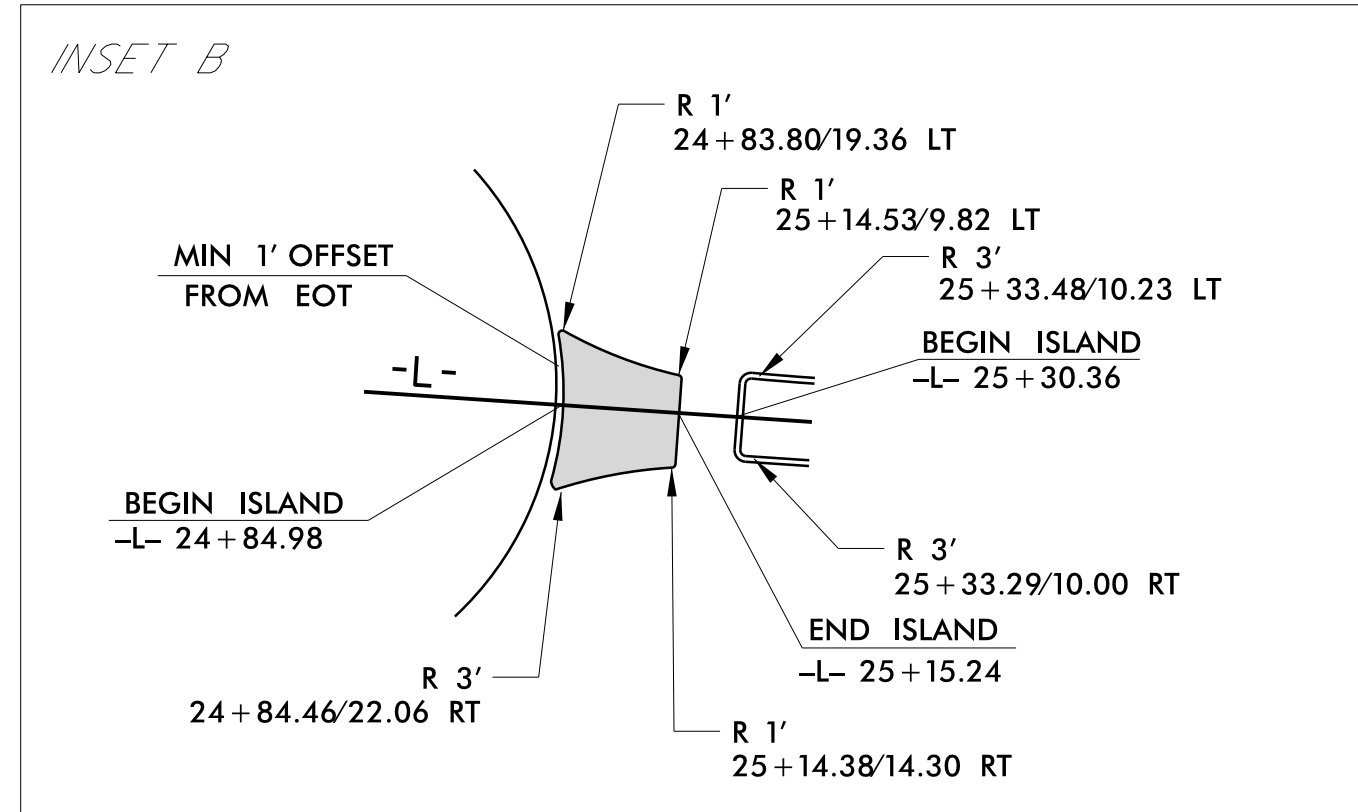
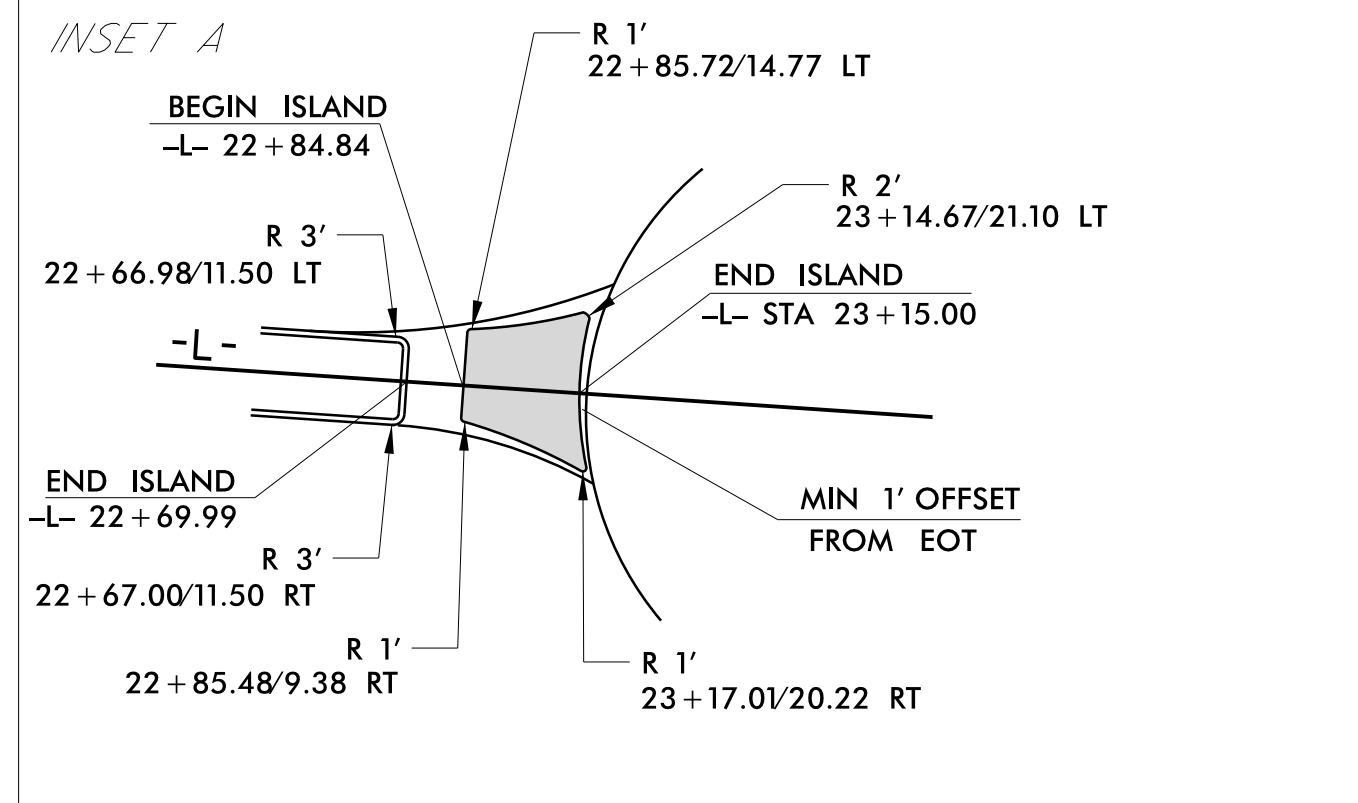


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FOR
DIVISION OF HIGHWAYS



1/8/2016



POINT NO.	ELEVATION
[1]	2132.26
[2]	2134.05
[3]	2132.26
[4]	2130.73
[5]	2132.94
[6]	2134.39
[7]	2132.94
[8]	2131.07
[9]	2133.36
[10]	2135.15
[11]	2133.36
[12]	2131.49
[13]	2133.56
[14]	2135.35
[15]	2133.56
[16]	2131.69

- LEGEND**
- CURB RAMP
 - PROPOSED CONCRETE ISLAND
 - PROPOSED CONCRETE SIDEWALK

SEE SHEET 5 FOR PLAN VIEW

PLANS PREPARED BY:

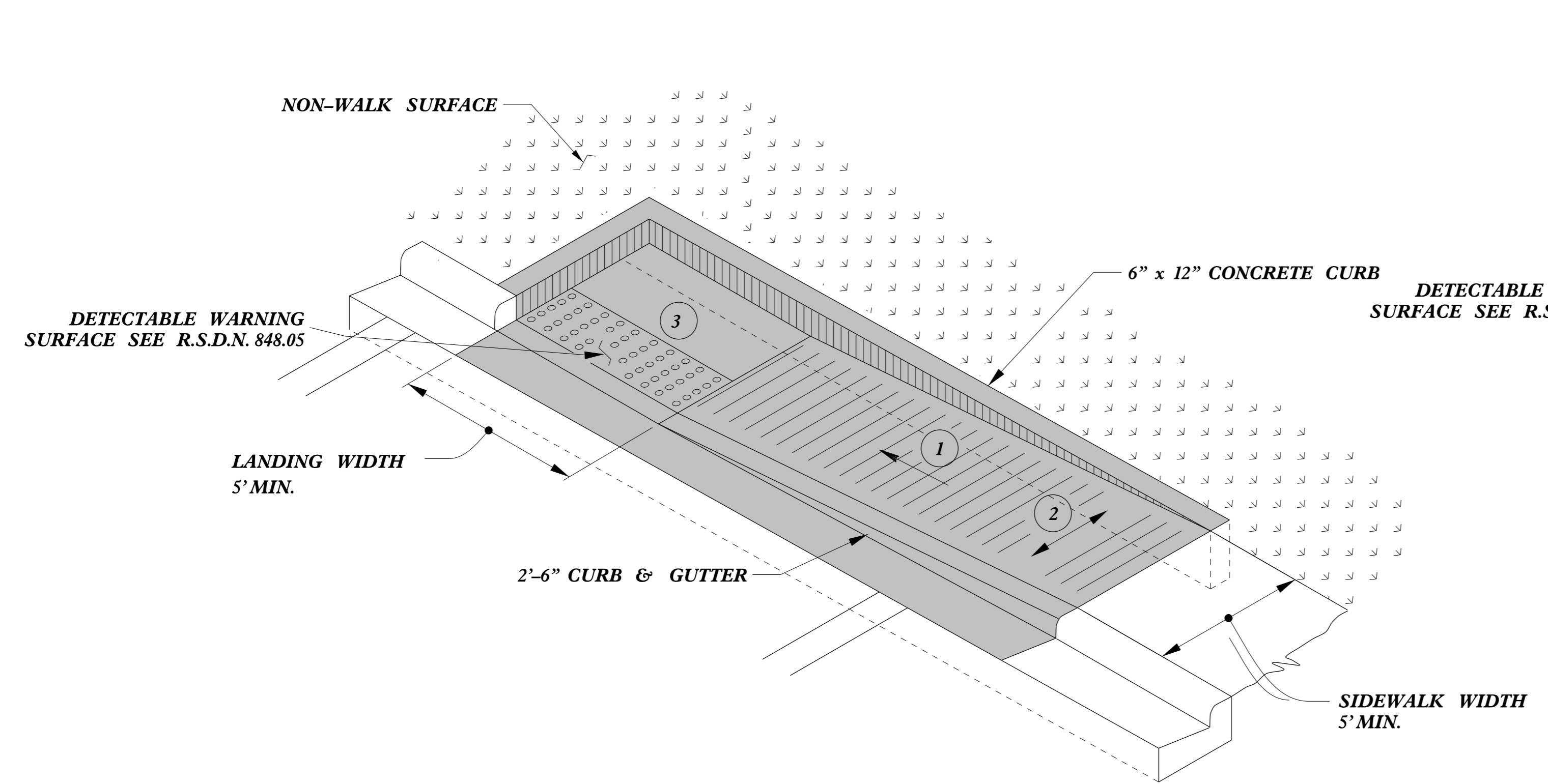


FOR DIVISION OF HIGHWAYS

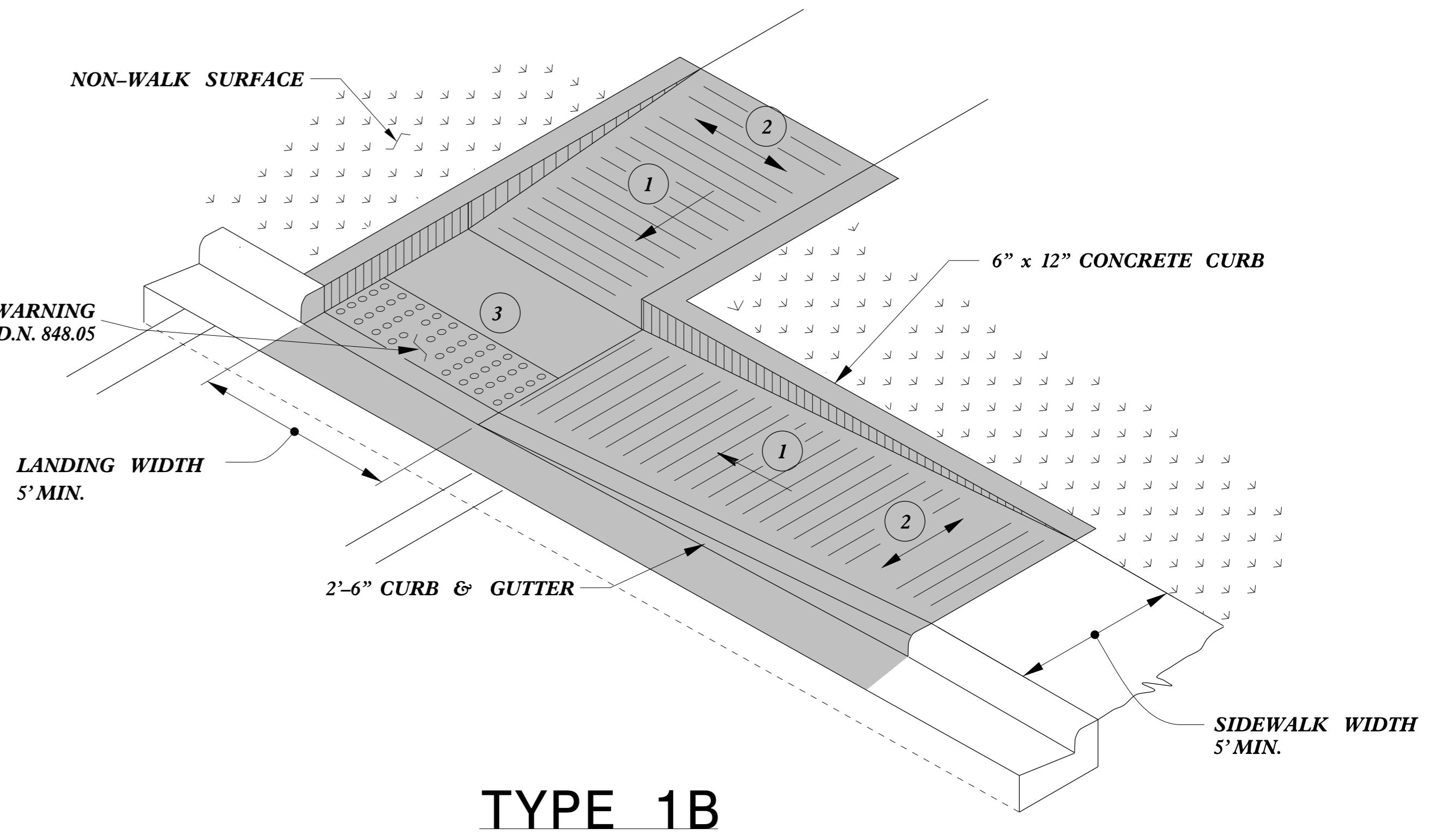
ROUNDAABOUT DETAIL

6/2/2016
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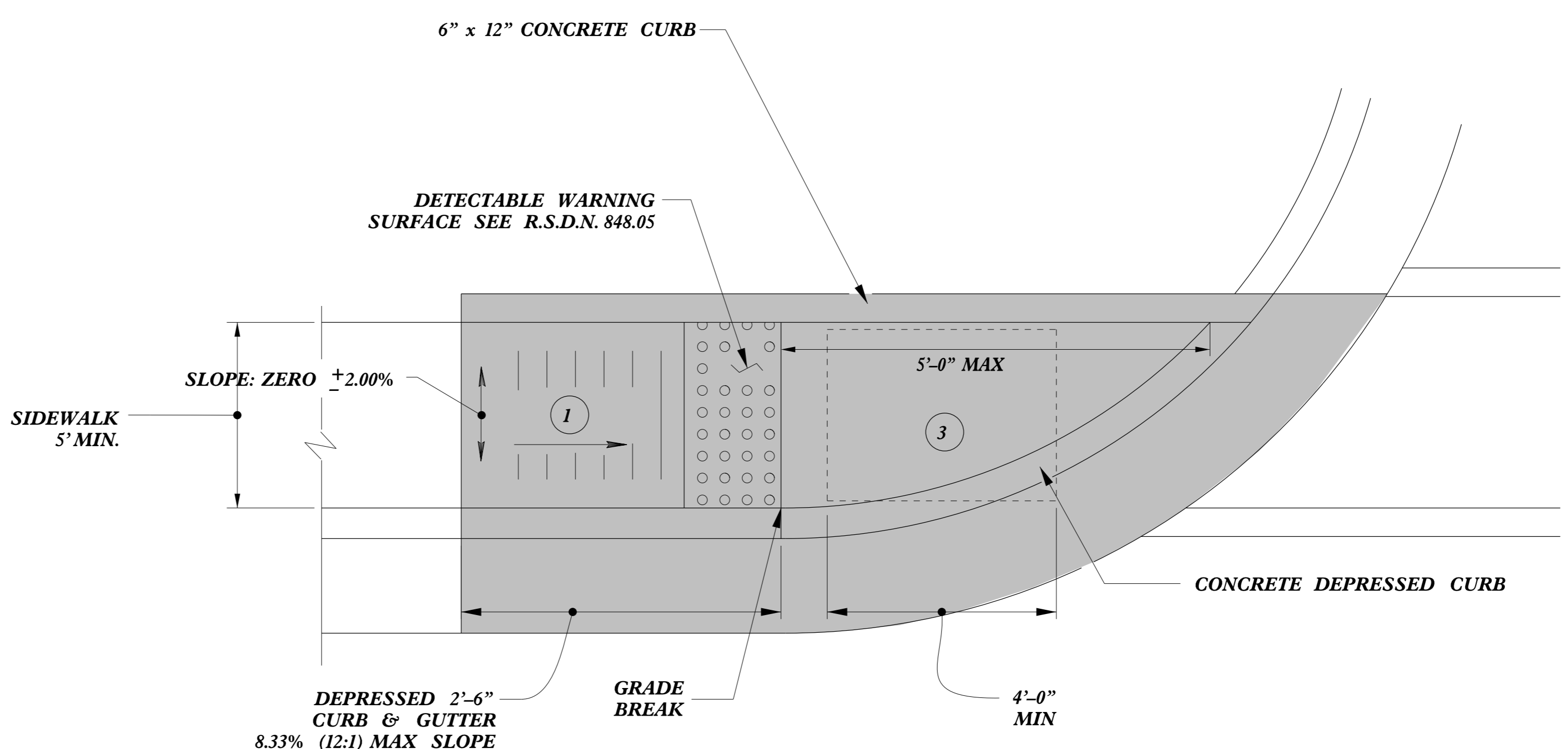
TYPE 1A



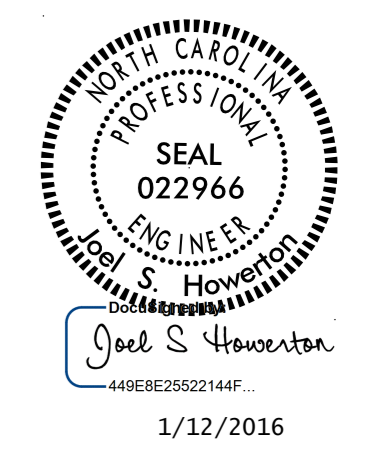
TYPE 1B

PAY LIMITS FOR 1 CURB RAMP

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



TYPE 1



DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

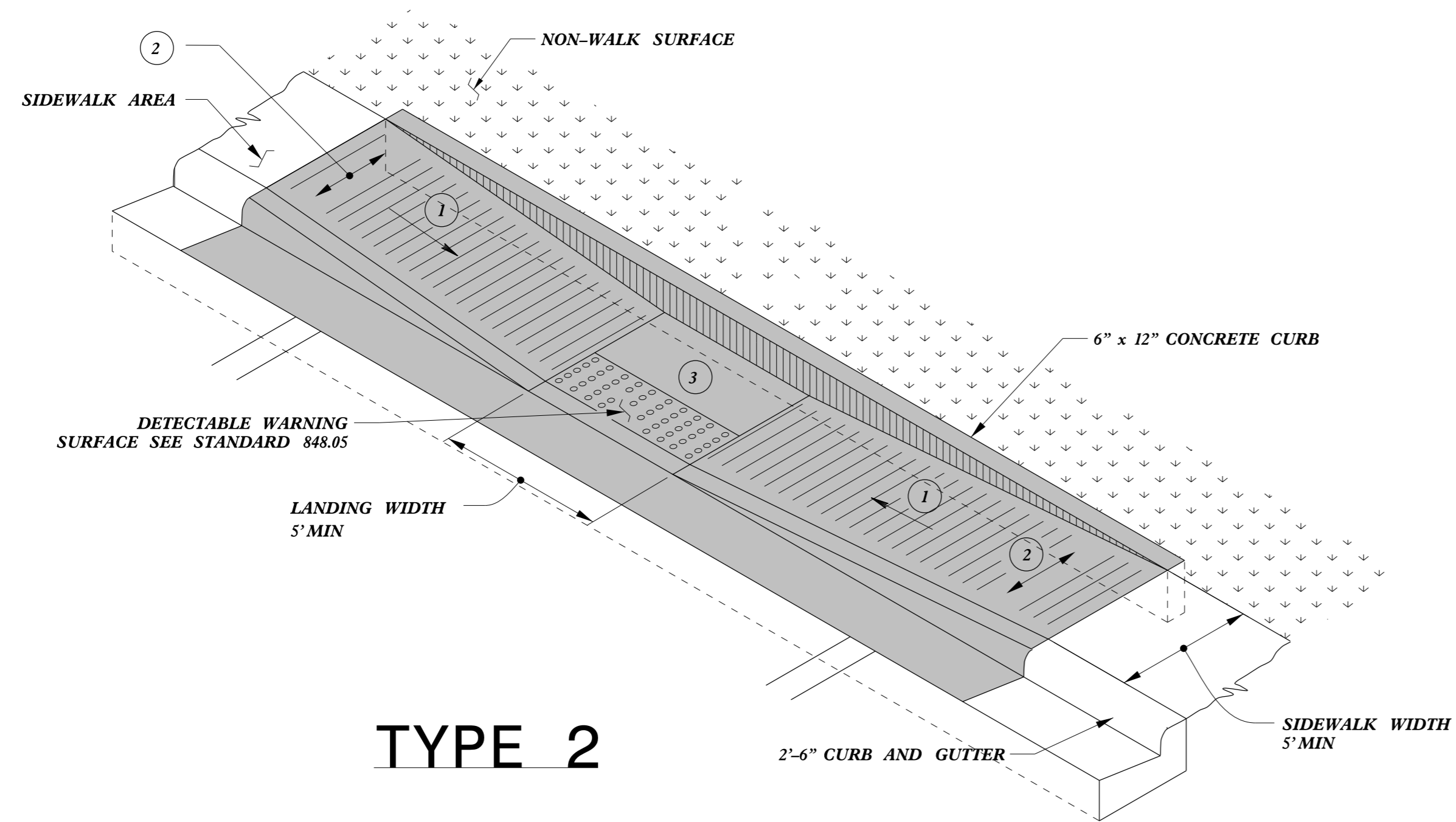
CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

CURB RAMPS
Directional Ramps

ORIGINAL BY: J.S. HOWERTON DATE: 7/7/11
 MODIFIED BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____
 FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dgn

REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

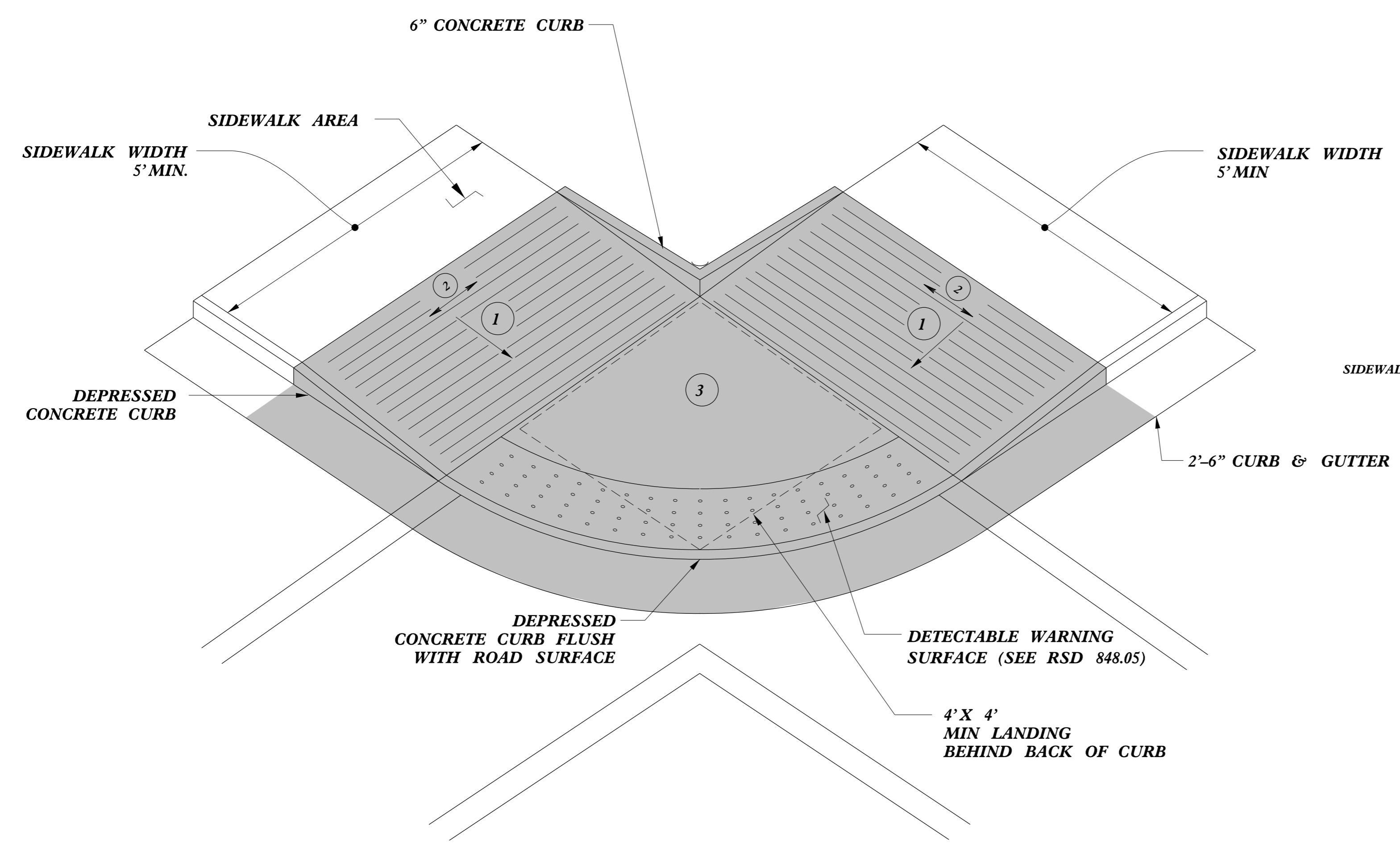
5/14/99



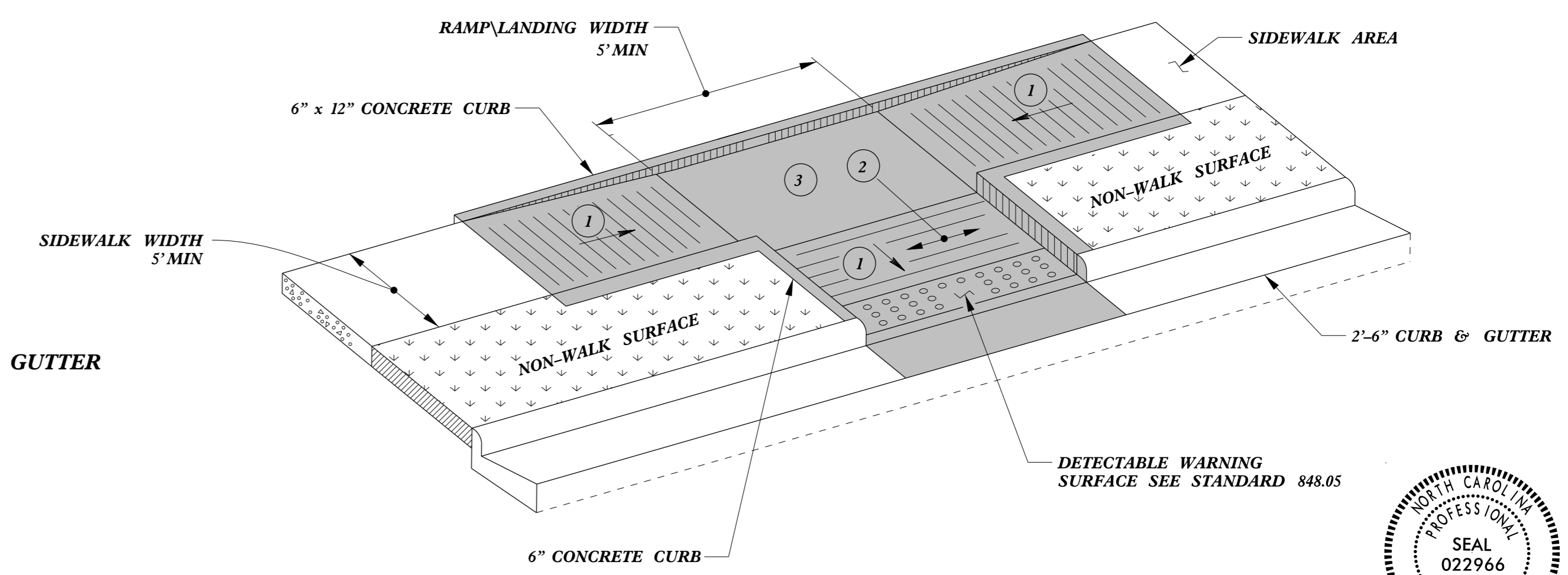
TYPE 2

PAY LIMITS FOR 1 CURB RAMP

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



TYPE 2A



TYPE 3



DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

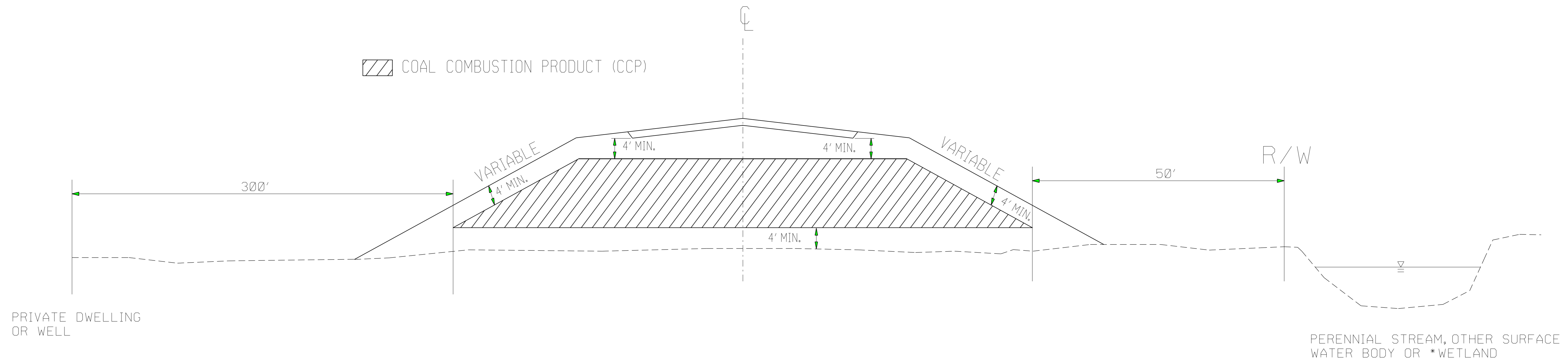
CURB RAMPS
Parallel Ramps

ORIGINAL BY: J.S. HOWERTON DATE: 7/7/11
 MODIFIED BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____
 FILE SPEC: stds\2012CurbRamp\CurbRampDetails.dgn

REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

5/14/99
C:\ME\DWG\CON\CON\USER\NAME

COAL COMBUSTION PRODUCT PLACEMENT



PLACE CCP IN HATCHED AREA IN ACCORDANCE WITH THE PROJECT SPECIAL PROVISIONS

PLACE CCP A MINIMUM OF 5' ABOVE SEASONAL HIGH GROUND WATER

PLACE AT LOCATIONS AS APPROVED BY THE ENGINEER

PLACE SOIL BORROW MATERIAL ON THE OUTSIDE OF CCP AS EACH LIFT OF CCP IS PLACED

*(OBTAIN PERMISSION FROM ARMY CORPS OF ENGINEERS)

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DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

COAL COMBUSTION PRODUCT PLACEMENT DETAIL

ORIGINAL BY: J.S.H. DATE: 3/16/15
 MODIFIED BY: DATE:
 CHECKED BY: DATE:
 FILE SPEC.: joel/coal combustion material detail.dgn

12/06/07

COMPUTED BY: GTJ DATE: 5/26/2015
 CHECKED BY: JM DATE: 5/26/2015

STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS

PROJECT REFERENCE NO. SHEET NO.
 R-5605 3B-1

SUMMARY OF EARTHWORK

STATION	STATION	UNCL. EXCAV.	UNDERCUT	EMBANK. +%	BORROW	WASTE
-Y1- STA. 10+00.00	-Y1- STA. 22+29.73	1,581 CY	-	60 CY		1,521 CY
-L- STA. 10+01.61	-L- STA. 23+17.00	5,867 CY	3,861 CY	9,270 CY	3,403 CY	3,861 CY
SUBTOTALS:		7,448 CY	3,861 CY	9,330 CY	3,403 CY	5,382 CY
-Y2- STA. 10+50.00	-Y2 STA. 12+50.00	745 CY	-	137 CY		608 CY
-Y2- STA. 14+00.00	-Y2 STA. 16+16.57	642 CY	-	205 CY		437 CY
-ROUNDABOUT EARTHWORK-		711 CY	-	431 CY		280 CY
SUBTOTALS:		2,098 CY	-	773 CY		1,325 CY
-L- STA. 24+83.00	-L- STA. 36+13.00	2,411 CY	-	18,883 CY	16,472 CY	
-L- STA. 37+33.00	-L- STA. 50+36.00	8,886 CY	-	13,606 CY	4,720 CY	
SUBTOTALS:		11,297 CY	-	32,489 CY	21,191 CY	
-L- STA. 51+00.00	-L- STA. 51+72.00	135 CY	-	846 CY	711 CY	
-Y3- STA. 11+16.77	-Y3- STA. 17+75.00	165 CY	-	1,541 CY	1,376 CY	
SUBTOTALS:		300 CY	-	2,387 CY	2,087 CY	
TOTAL:		21,143 CY	3,861 CY	44,979 CY	26,681 CY	6,707 CY
MATERIAL FOR SHOULDER CONSTRUCTION:				3,220 CY	3,220 CY	
LOSS DUE TO CLEARING AND GRUBBING:		-2,442 CY	-		2,442 CY	
ADDITIONAL UNDERCUT:			1,000 CY	1,150 CY	1,150 CY	1,000 CY
WASTE IN LIEU OF BORROW				-	-2,846 CY	-2,846 CY
PROJECT TOTAL:		18,701 CY	4,861 CY	49,348 CY	30,647 CY	4,861 CY
EST. 5% TO REPLACE TOP SOIL ON BORROW PIT:					1,532 CY	
GRAND TOTALS:		18,701 CY	4,861 CY	49,348 CY	32,179 CY	4,861 CY
SAY:		19,000 CY	5,000 CY		33,000 CY	

PAVEMENT STRUCTURE VOLUME = 6,810 CY

Note: Quantities are approximate only. The Resident Engineer will re-cross-section the work accurately when the project is staked out. These cross-section notes will be used in computing the final quantities for which the contractor will be paid.

PAVEMENT REMOVAL SUMMARY

SURVEY LINE	STATION	STATION	LOCATION LT/RT/CL	YD ³
-Y1-	14+43.14	17+01.63	CL	328
-Y1-	17+01.63	20+35.06	CL	334
-Y1-	16+64.32	17+32.42	CL	78*
-Y1-	15+72.52	17+00.91	LT	209
-Y2-	10+50.00	16+16.57	CL	1511
*EXISTING CONCRETE MEDIAN				
TOTAL:				2460 SY
SAY:				2500 SY

SUMMARY OF BREAKING EXISTING ASPHALT PAVEMENT

SURVEY LINE	STATION	STATION	LOCATION LT/RT/CL	YD ³
-L-	24+32.25	24+79.25	CL	945
TOTAL:				945 SY
SAY:				950 SY

I:\Users\gtj\Documents\Projects\DRY Connector\DRY 2014\Roadway\Pro\RS605_Rdy_summaries.dgn

12/06/07

COMPUTED BY: GTJ DATE: _____
 CHECKED BY: SC Clark DATE: _____

PROJECT NO. R-5605 SHEET NO. 3G-1

**STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS**

SUMMARY OF SUBSURFACE DRAINAGE

LINE	Station	Station	Location LT/RT/CL	Drain Type* UD/BD/SD	LF
			CONTINGENCY	SD	1000
				TOTAL LF:	1000

*UD = Underdrain
 *BD = Blind Drain
 *SD = Subsurface Drain

**SUMMARY OF
 BRIDGE WAITING PERIODS**

Bridge Description	End Bent/ Bent No.	MONTHS
Eastern Bridge over Davidson River	1/2	1
Western Bridge Over Davidson River	2	1

SUMMARY OF AGGREGATE SUBGRADE/STABILIZATION

LINE	Station	Station	Aggregate Type ASU/AST	Aggregate Thickness INCHES	Shallow Undercut CY	Class IV Subgrade Stabilization TONS	Geotextile for Soil Stabilization SY	Stabilizer Aggregate TONS	Class IV Aggregate Stabilization TONS
			CONTINGENCY	ASU	750	1500	1000		
				TOTAL CY/TONS/SY:	750	1500	1000*	0	0

ASU = Aggregate Subgrade, AST = Aggregate Stabilization

*Total square yards of Geotextile for Soil Stabilization is only the estimated quantity for ASU/AST and may only represent a portion of the geotextile quantity shown in the Item Sheets of the Proposal.

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

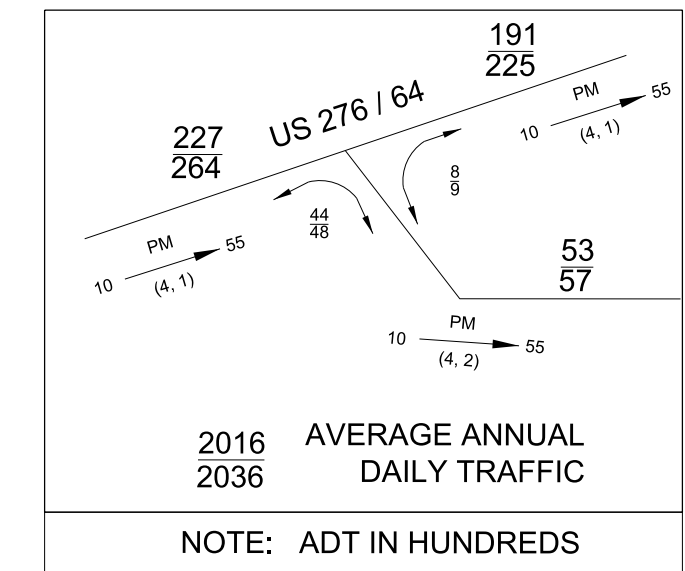
PARCEL INDEX SHEET

PARCEL No.	SHEET No.	PROPERTY OWNER NAME
1	4	DAVIDSON RIVER VILLAGE, LLC
2	4	SHEPPARD, ERNEST LEON & SANDY G
3	4	EQUITY TRUST COMPANY CUSTODIAN
4	4,5	DAVIDSON RIVER VILLAGE, LLC (SEE NOTE 1)
5	5	FORESTGATE SELF STORAGE, LLC
6	5,6	DAVIDSON RIVER VILLAGE, LLC (SEE NOTE 1)
7	6,7	DAVIDSON RIVER VILLAGE, LLC
8	7	DAVIDSON RIVER VILLAGE, LLC (SEE NOTE 1)
9	4	JORDAN, DONALD E.

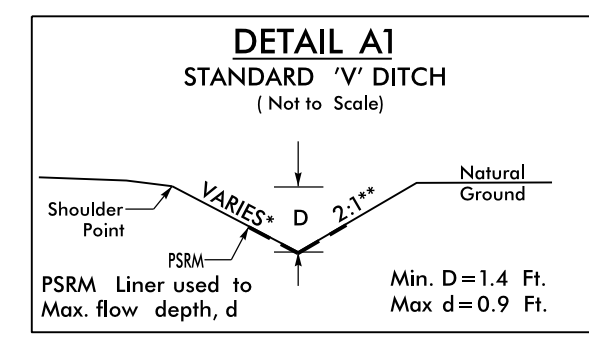
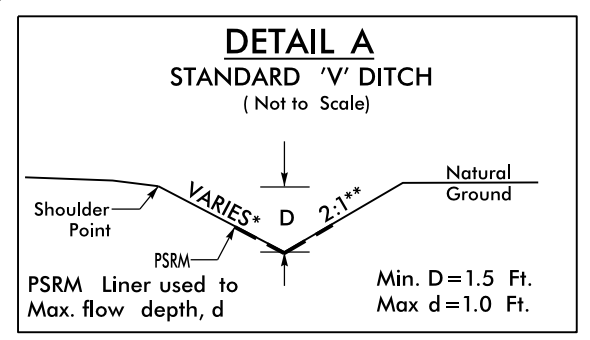
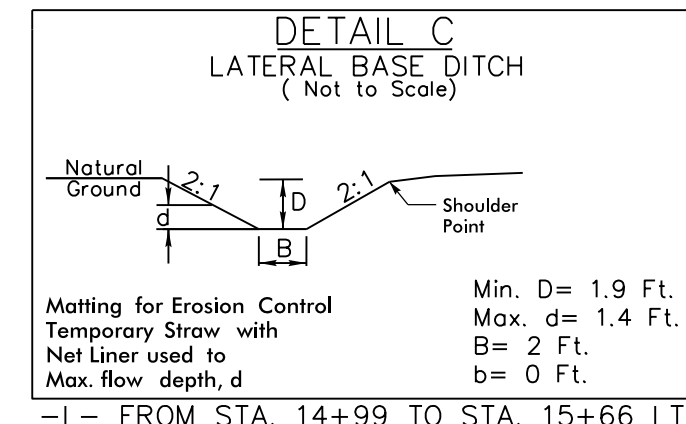
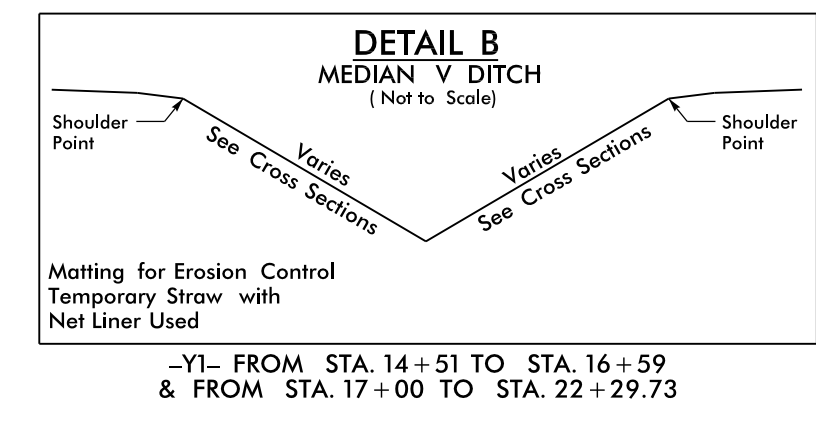
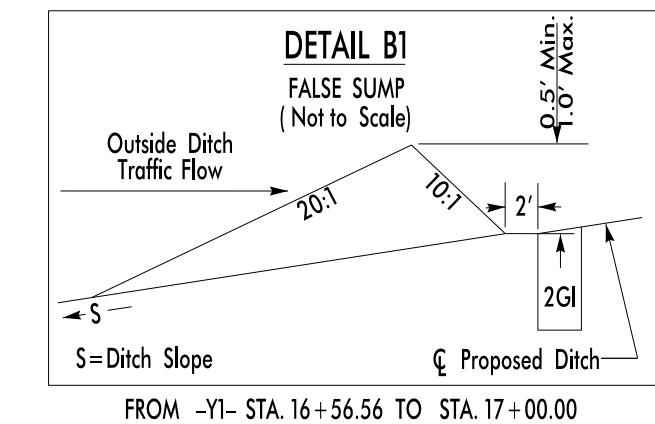
NOTE 1

The property which is the subject of this instrument is subject to the Brownfields Agreements attached as Exhibits A to the Notices of Brownfields Property recorded in the Transylvania County land records, Book 584, Page 793; Book 641, Page 590; Book 671, Page 591; and Book 730, Page 662. The property has been classified and cleaned up as a brownfields property under the Brownfields Property Reuse Act. In addition, (a) the Ecusta Real Estate 2 Tract [i.e., a portion of the property which is the subject of this instrument] was used as an unlined industrial waste landfill until it was closed under North Carolina Solid Waste Management Statutes and Rules; (b) prior to its use as a landfill, the Ecusta Real Estate 2 Tract was used as a pit-type industrial waste incinerator; and (c) its use is restricted under the Closure Plan approved by the N.C. Department of Environment and Natural Resources, Division of Waste Management (the "Division") and the Letter of Closure issued by the Division on November 18, 2005 and recorded on December 6, 2005 in Book 322, Page 261, Transylvania County Register of Deeds.

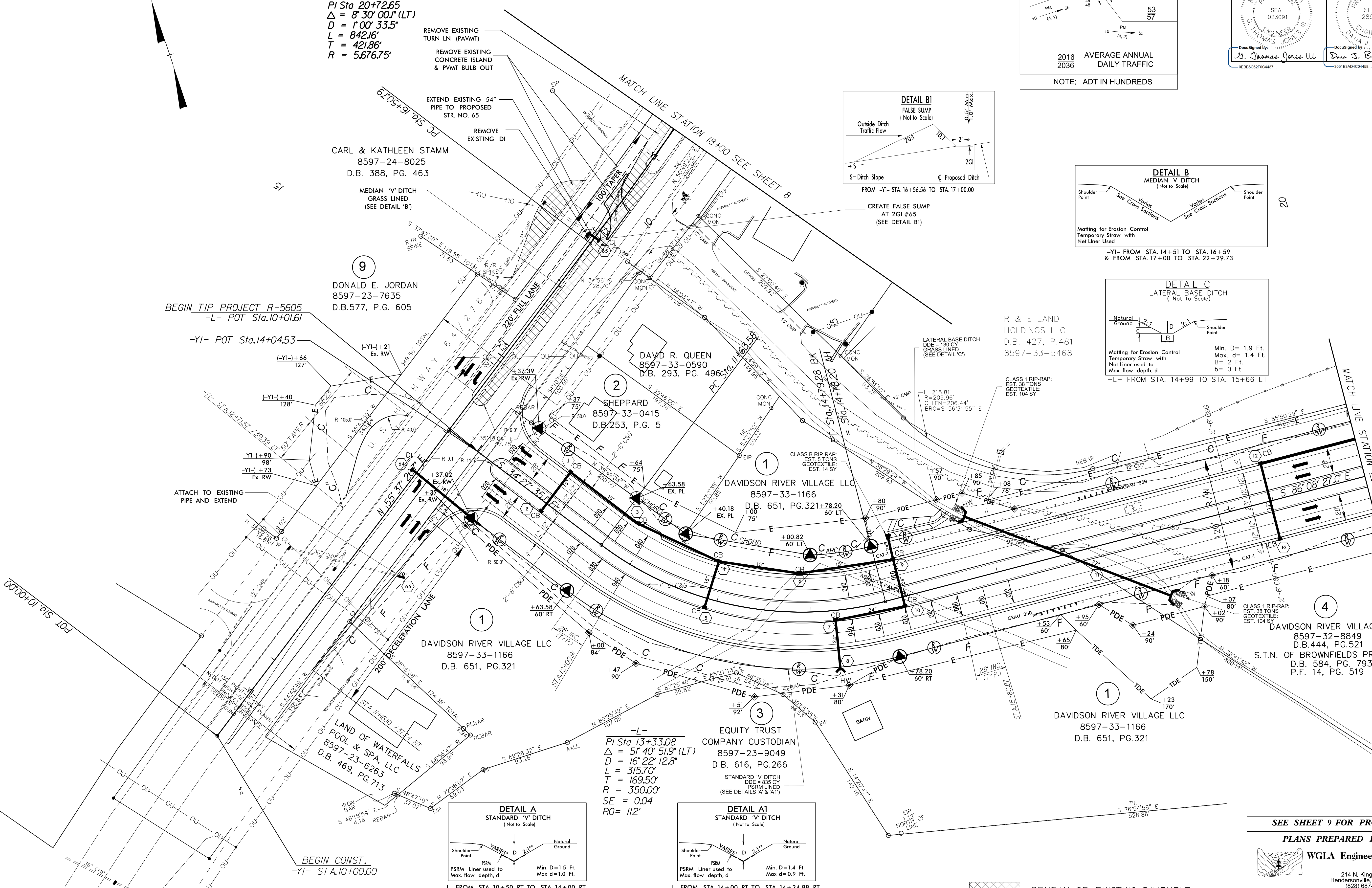
PROJECT REFERENCE NO. R-5605	SHEET NO. 4
RW SHEET NO.	
ROADWAY DESIGN ENGINEER 12/23/2015 THOMAS JONES III	HYDRAULICS ENGINEER 12/22/2015 DANA J. BOLLEN
SEAL 023091	SEAL 28945
DocuSigned by: Thomas Jones III	DocuSigned by: Dana J. Bollen
48886C2F9C4437	3051E3AD4C04458



-YI-
 PI Sta 20+72.65
 $\Delta = 8' 30'' 00.1''$ (LT)
 $D = 1' 00'' 33.5''$
 $L = 842.16'$
 $T = 421.86'$
 $R = 5,676.75'$



-L-
 PI Sta 13+33.08
 $\Delta = 5' 40'' 51.9''$ (LT)
 $D = 16' 22'' 12.8''$
 $L = 315.70'$
 $T = 169.50'$
 $R = 350.00'$
 $SE = 0.04$
 $RO = 112'$



SEE SHEET 9 FOR PROFILE

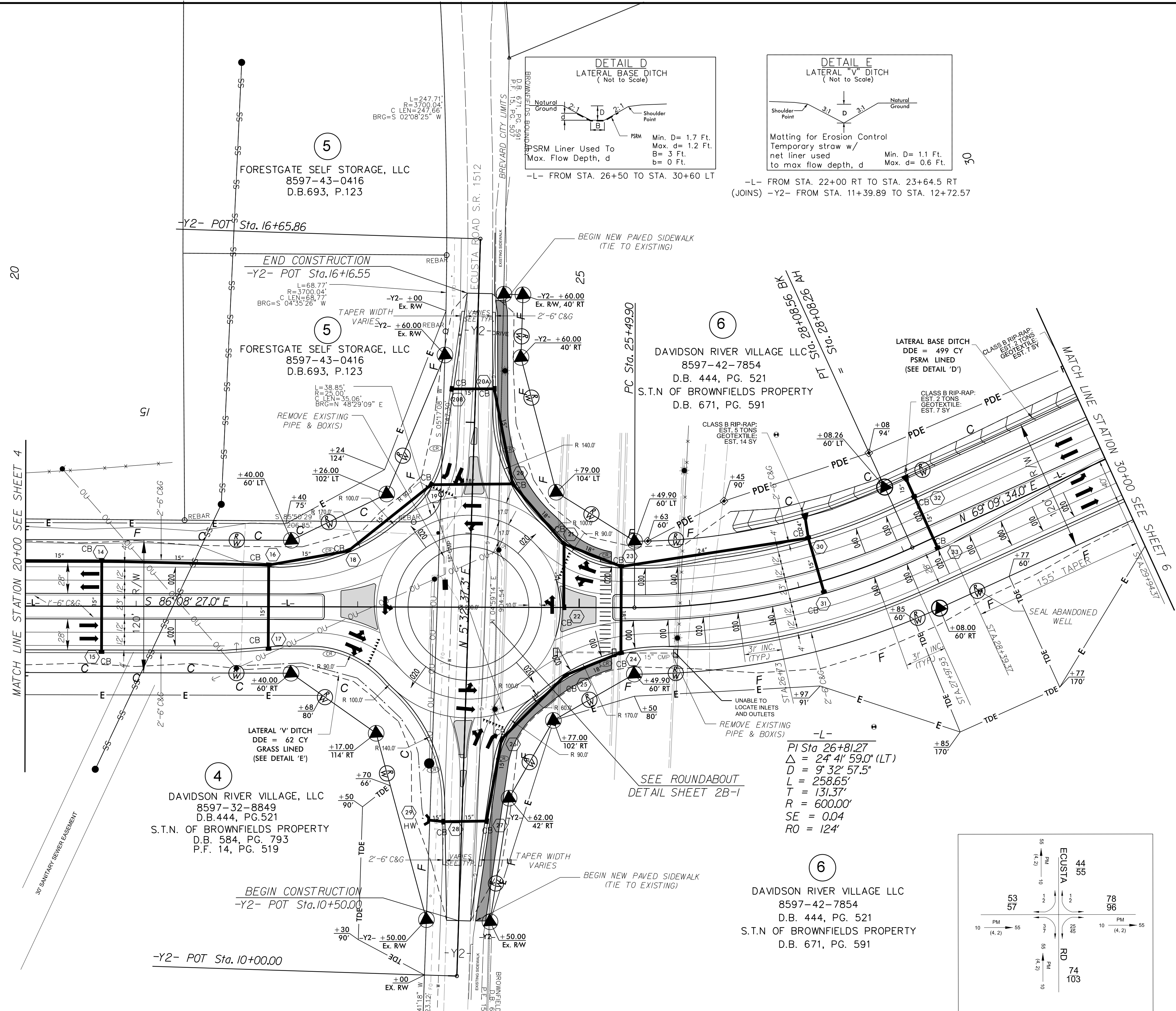
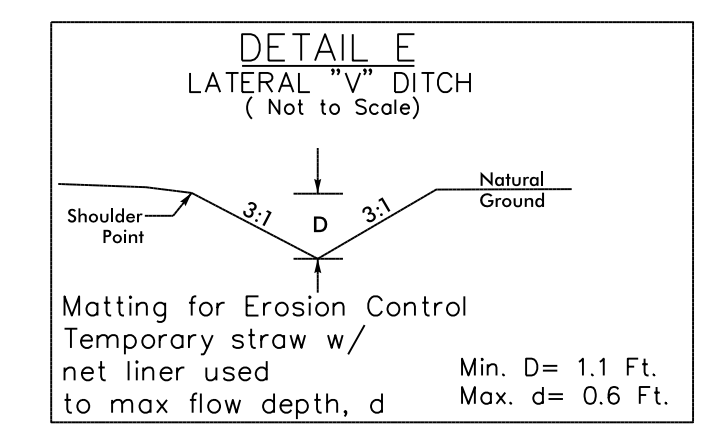
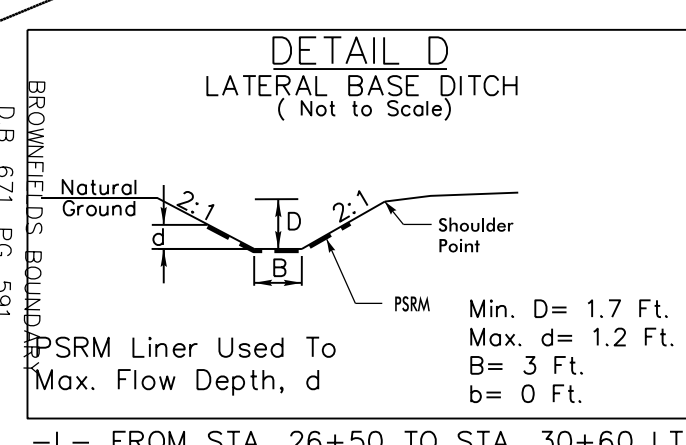
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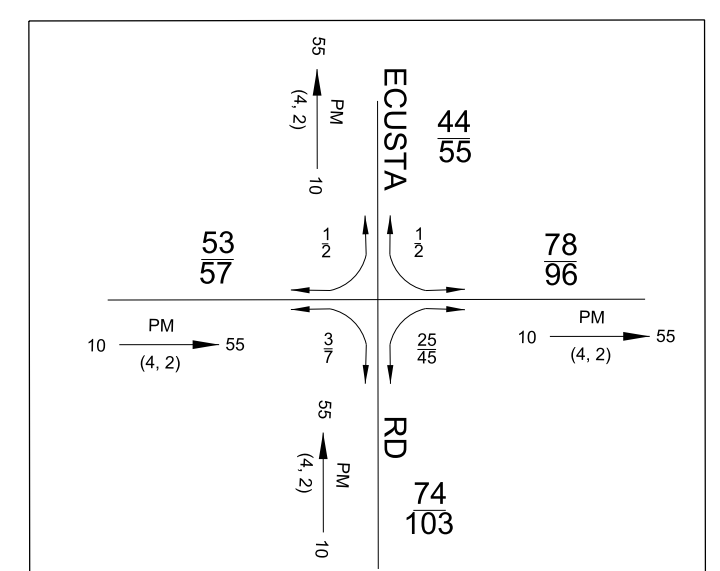
REMOVAL OF EXISTING PAVEMENT

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 02-DEC-2015 13:37 C:\DRIVE\microstation\R5605_05_Rdy_psh4.dgn
 16-SEP-2015 13:37 C:\DRIVE\microstation\R5605_05_Rdy_psh4.dgn

PROJECT REFERENCE NO. <i>R-5605</i>	SHEET NO. 5
RW SHEET NO.	
ROADWAY DESIGN ENGINEER 12/23/2015	HYDRAULICS ENGINEER 12/22/2015
DocuSigned by: <i>Thomas Jones III</i>	DocuSigned by: <i>Dana J. Bolden</i>



-L-
 PI Sta. 26+81.27
 $\Delta = 24' 41'' 59.0'' (LT)$
 $D = 9' 32'' 57.5''$
 $L = 258.65'$
 $T = 131.37'$
 $R = 600.00'$
 $SE = 0.04$
 $RO = 124'$



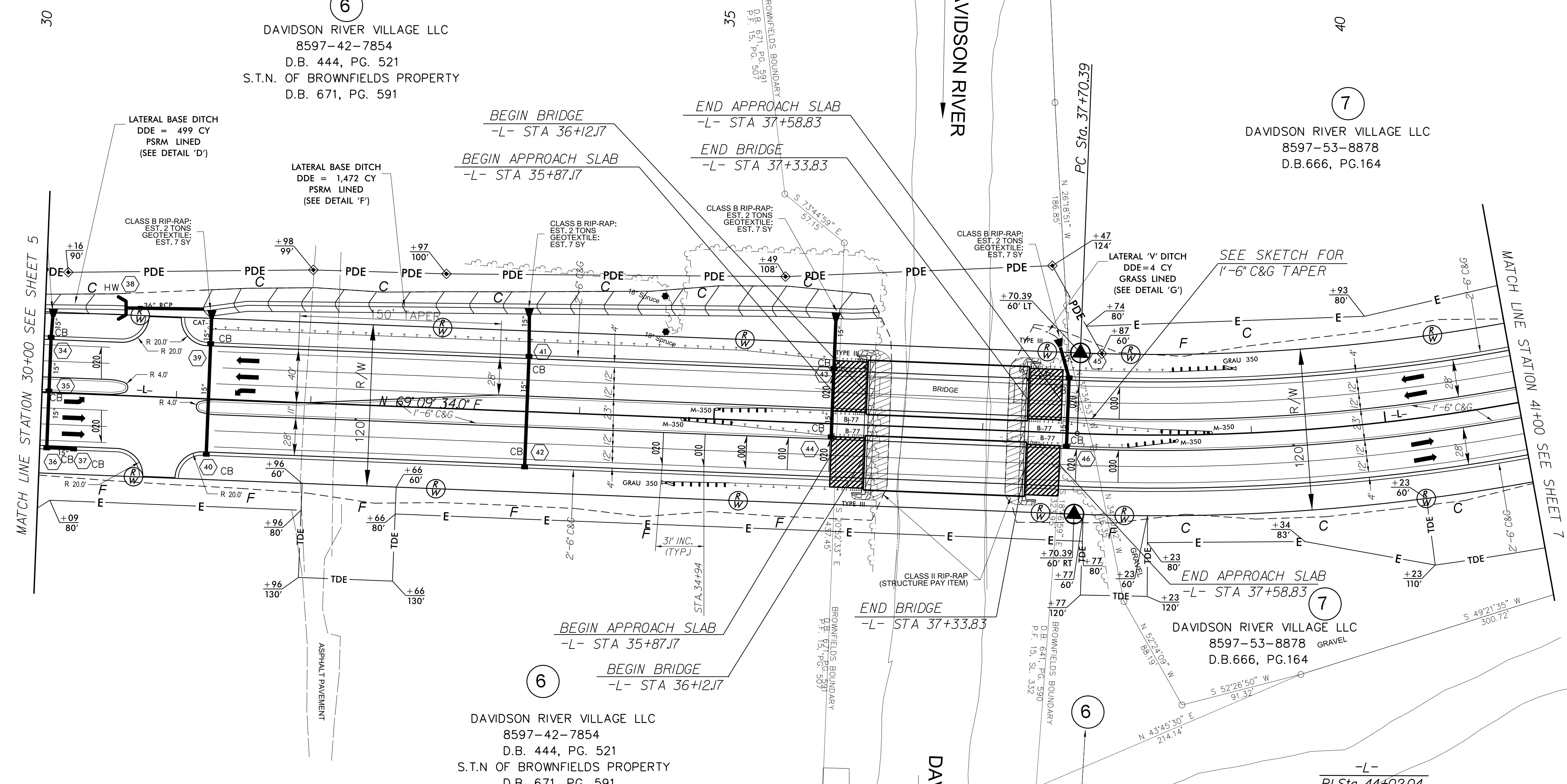
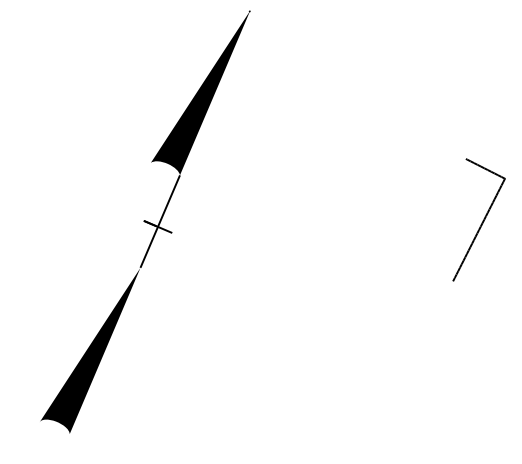
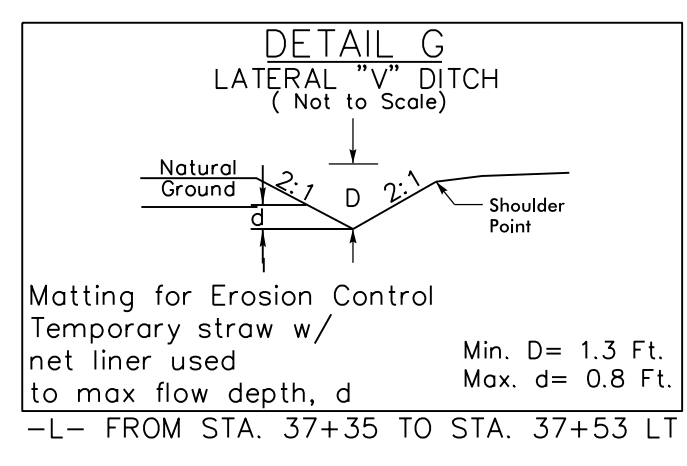
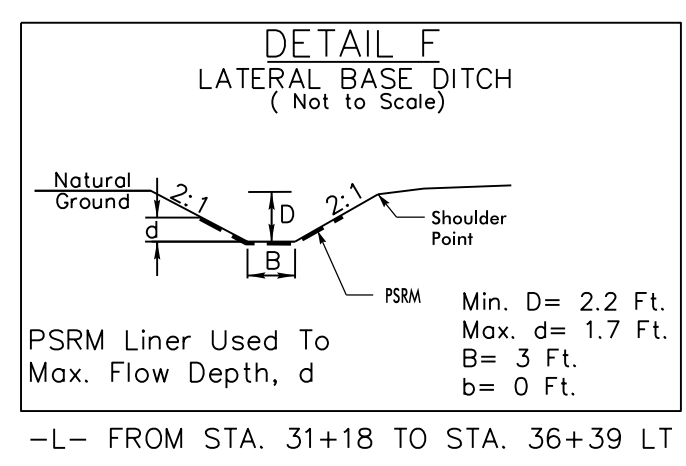
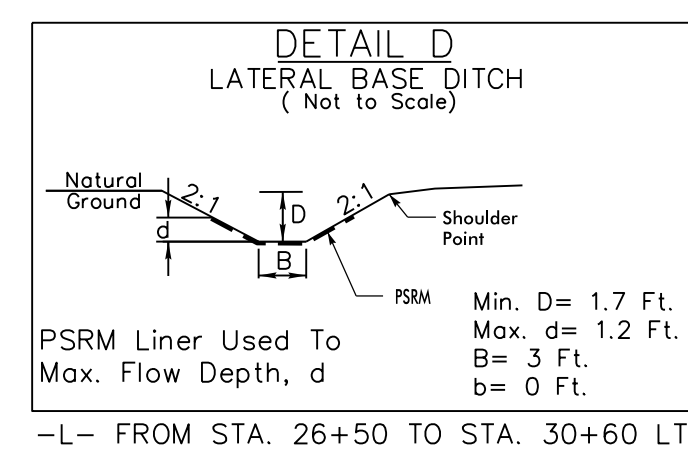
SEE SHEET 2B-1 FOR ROUNDABOUT DETAIL
 SEE SHEET 9 FOR PROFILE

PLANS PREPARED BY :

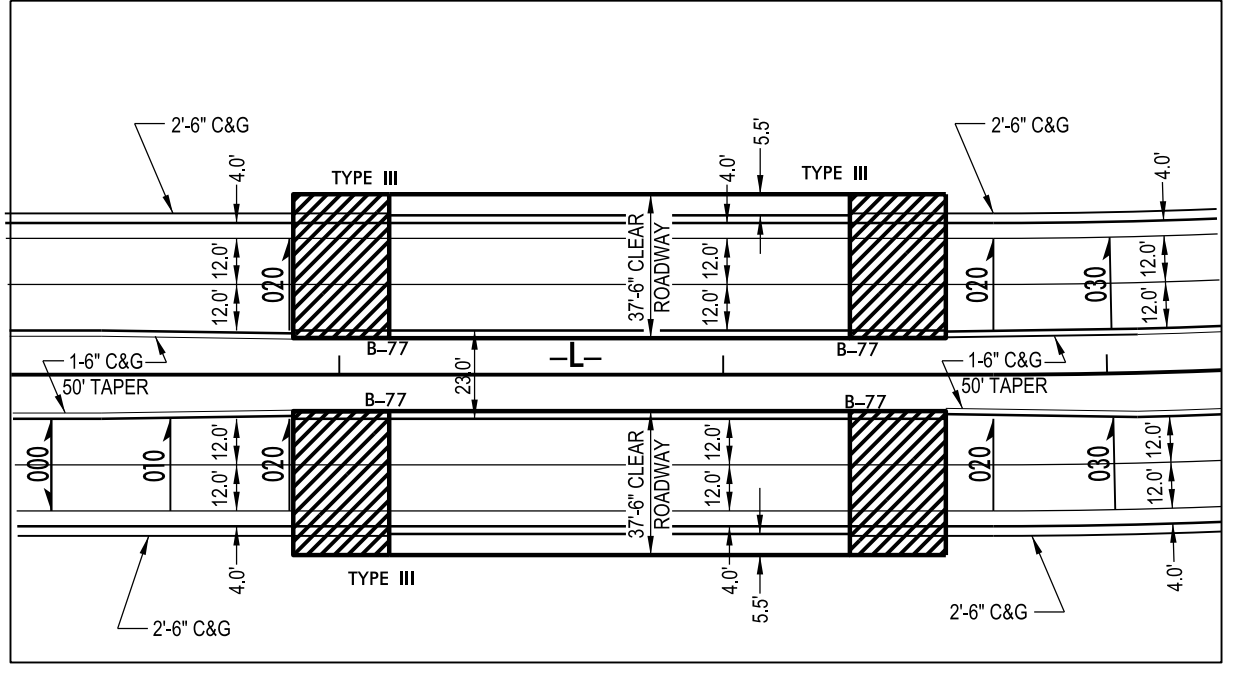
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 5/14/14/99
 20

PROJECT REFERENCE NO. R-5605	SHEET NO. 6
RW SHEET NO.	
ROADWAY DESIGN ENGINEER 12/23/2015 THOMAS JONES III	HYDRAULICS ENGINEER 12/22/2015 DANA J. BOLLEN



SKETCH SHOWING RELATIONSHIP OF BRIDGE TO ROADWAY



-L-
 PI Sta 44+02.04
 $\Delta = 45^{\circ}40'19.6''$ (LT)
 $D = 3^{\circ}49'11.0''$
 $L = 1,195.69'$
 $T = 631.65'$
 $R = 1,500.00'$
 $SE = 0.03$
 $RO = 93'$

SEE SHEET 10 FOR PROFILE

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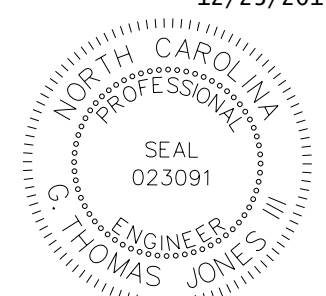
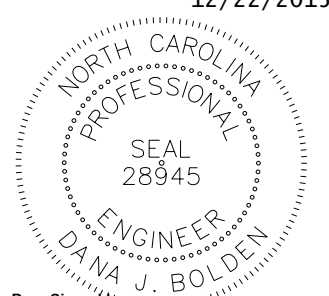
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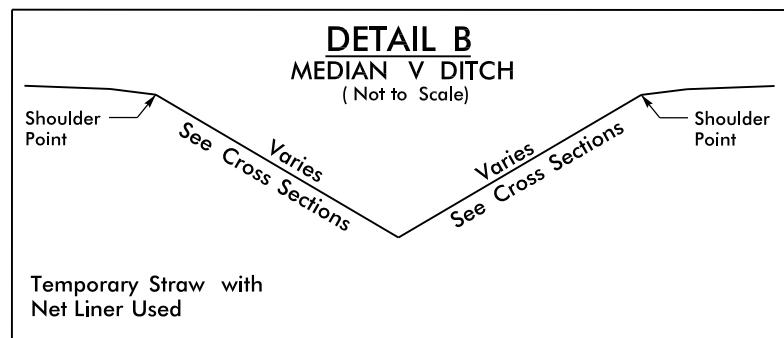
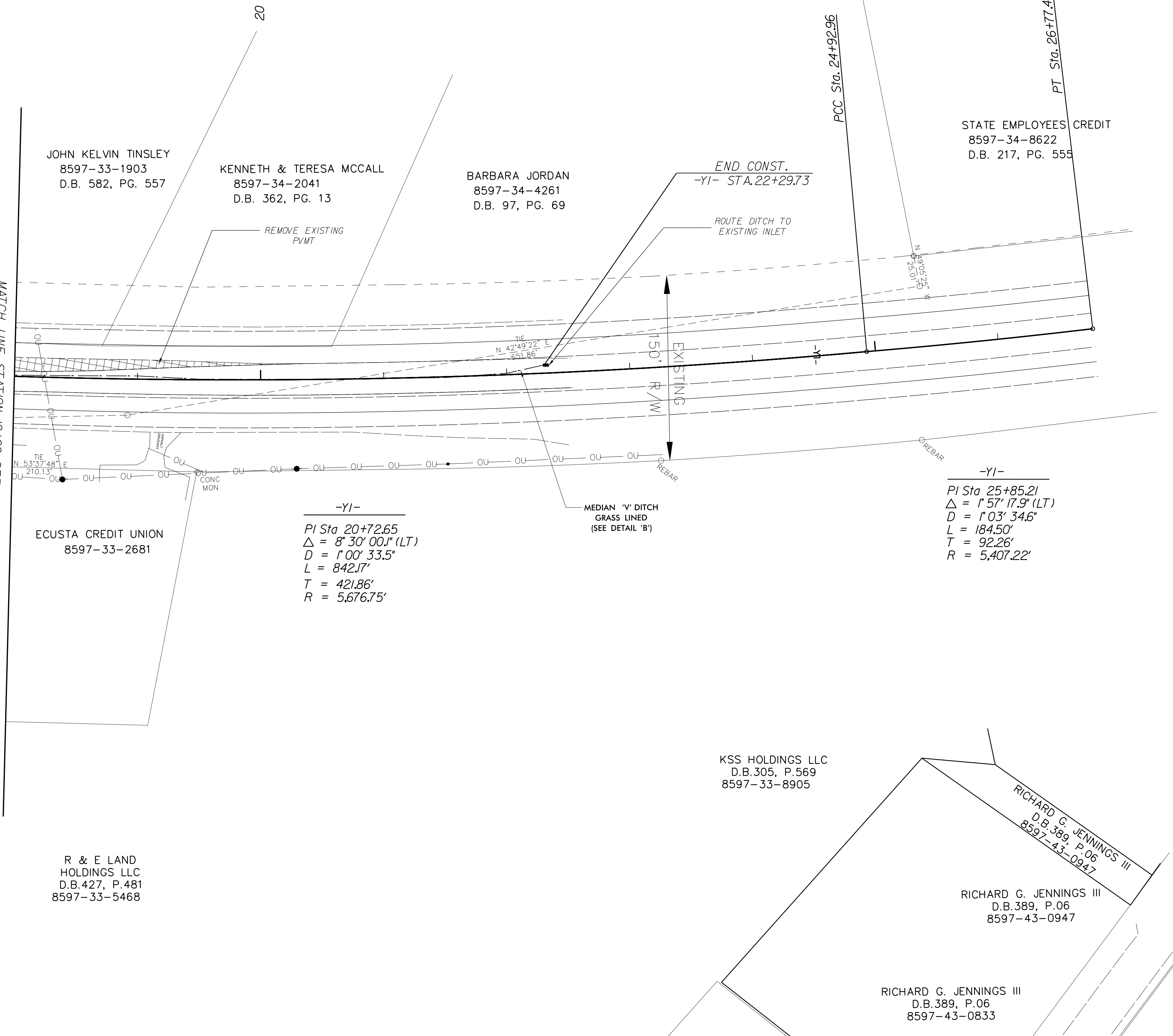
5/14/99
 52-DEC-2015 13:57 (PRV) m:\projects\105605_105605_05_Rdy_pshfb.dgn
 10/23/2015 13:57 (PRV) m:\projects\105605_105605_05_Rdy_pshfb.dgn

5/14/99

21 DEC 2015 16:59 DRV\micr\station\15605_Rdy_psh8.dgn
jared.alster

PROJECT REFERENCE NO. R-5605	SHEET NO. 8
RW SHEET NO.	
ROADWAY DESIGN ENGINEER 12/23/2015  THOMAS JONES III	HYDRAULICS ENGINEER 12/22/2015  DANA J. BOLDEN
DocuSigned by: Thomas Jones III	DocuSigned by: Dana J. Bolden

MATCH LINE STATION 18+00 SEE SHEET 4



-YI- FROM STA. 14+51 TO STA. 16+59
& FROM STA. 17+00 TO STA. 22+29.73


-YI-
PI Sta 20+72.65
 $\Delta = 8' 30' 00.1''$ (LT)
 $D = 1' 00' 33.5''$
 $L = 842.17'$
 $T = 421.86'$
 $R = 5,676.75'$

-YI-
PI Sta 25+85.21
 $\Delta = 1' 57' 17.9''$ (LT)
 $D = 1' 03' 34.6''$
 $L = 184.50'$
 $T = 92.26'$
 $R = 5,407.22'$

 REMOVAL OF EXISTING PAVEMENT

SEE SHEET 11 FOR PROFILE

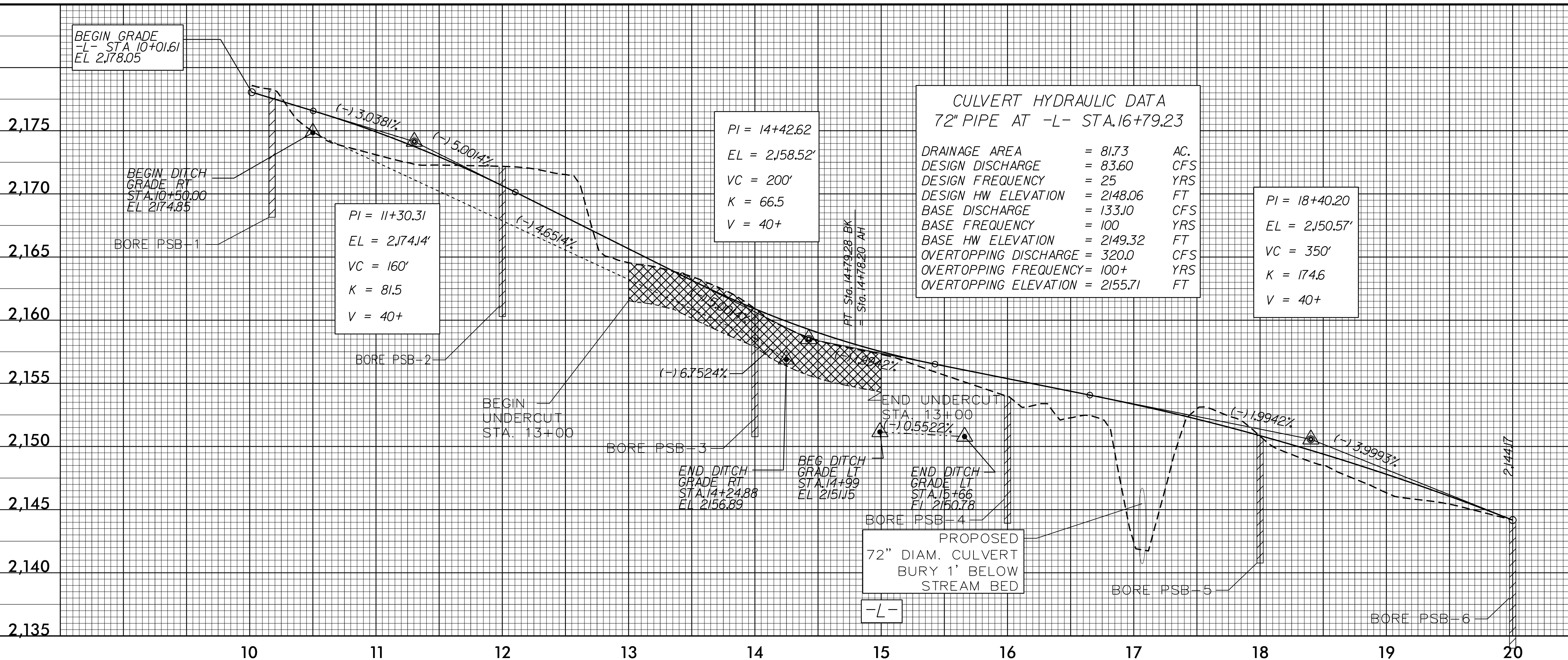
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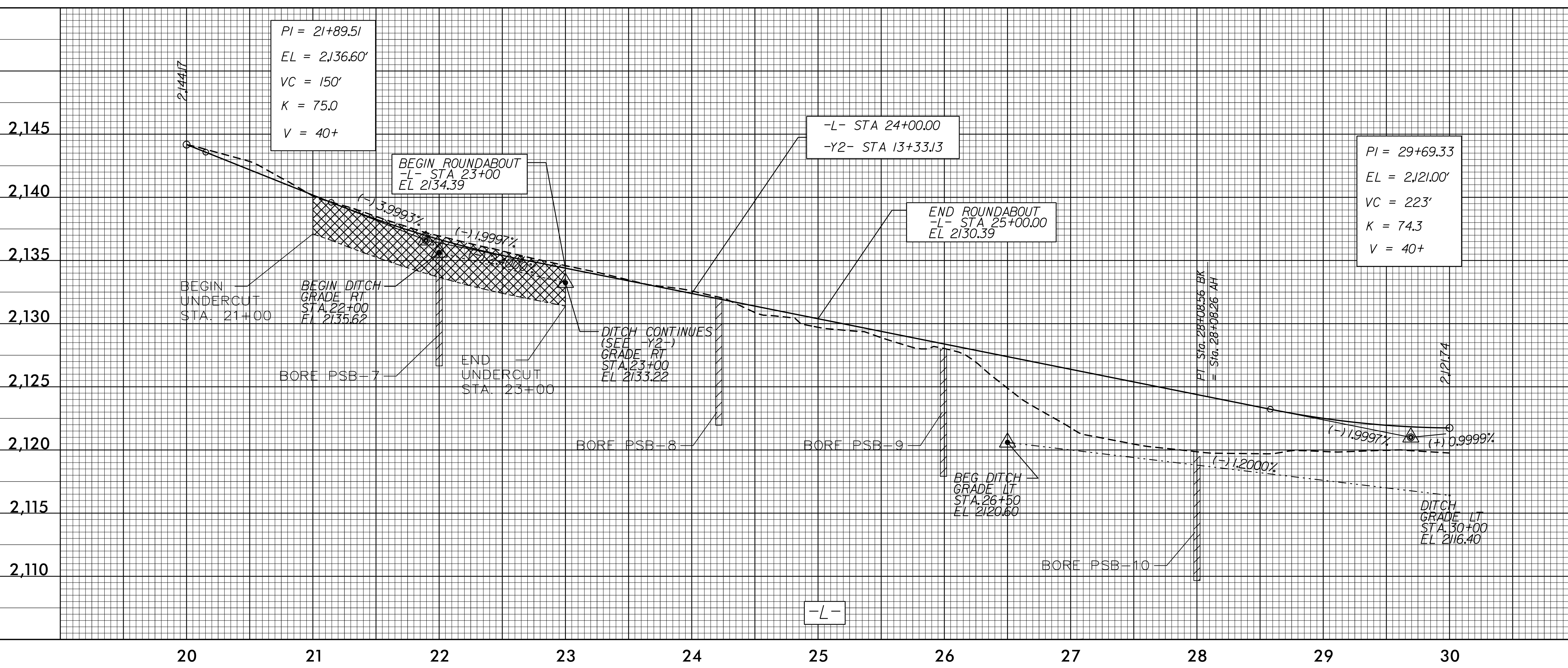
FOR
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PROJECT REFERENCE NO. R-5605	SHEET NO. 9
ROADWAY DESIGN ENGINEER 12/23/2015	HYDRAULICS ENGINEER 12/22/2015



DITCH LEGEND	
LEFT DITCH	-----
RIGHT DITCH	-----
UNDERCUT	XXXXXX

SEE SHEET 4 FOR PLAN VIEW



DITCH LEGEND	
LEFT DITCH	-----
RIGHT DITCH	-----
UNDERCUT	XXXXXX

SEE SHEET 5 FOR PLAN VIEW

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21-DEC-2015 16:30 APS\LE\A\Desk_top\DRV\microstation\R5605_Rdy.plt.dgn

5/28/99

PROJECT REFERENCE NO. R-5605	SHEET NO. 10
ROADWAY DESIGN ENGINEER 12/23/2015 	HYDRAULICS ENGINEER 12/22/2015

BRIDGE HYDRAULIC DATA

DESIGN DISCHARGE = 6,800 CFS
 DESIGN FREQUENCY = 50 YRS
 DESIGN HW ELEVATION = 2116.7 FT
 BASE DISCHARGE = 8,000 CFS
 BASE FREQUENCY = 100 YRS
 BASE HW ELEVATION = 2117.64 FT
 OVERTOPPING DISCHARGE = 20,000 CFS
 OVERTOPPING FREQUENCY = 500+ YRS
 OVERTOPPING ELEVATION = 2127.0 FT

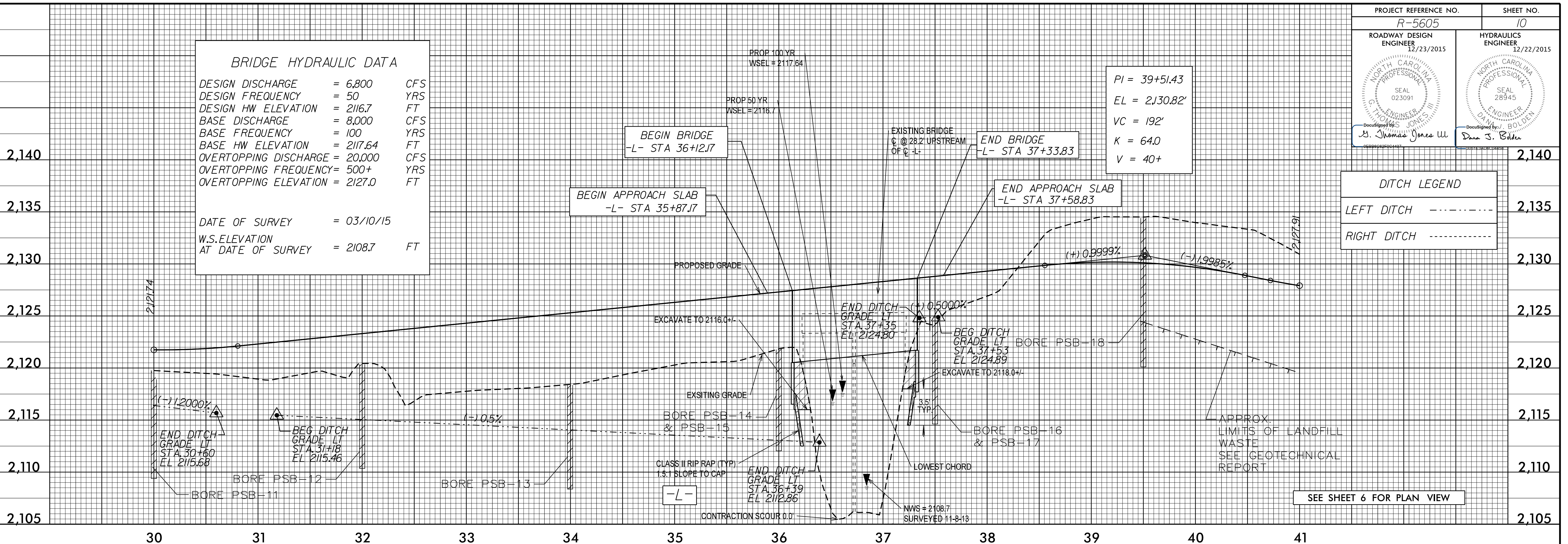
DATE OF SURVEY = 03/10/15
 W.S. ELEVATION AT DATE OF SURVEY = 2108.7 FT

PI = 39+51.43
 EL = 2130.82'
 VC = 192'
 K = 64.0
 V = 40+

DITCH LEGEND

LEFT DITCH - - - - -

RIGHT DITCH - - - - -



PIPE HYDRAULIC DATA
 42" PIPE at -L- Sta. 42+00.00

DRAINAGE AREA = 15.52 AC
 DESIGN FREQUENCY = 25 YRS
 DESIGN DISCHARGE = 56.22 CFS
 DESIGN HW ELEVATION = 2133.22 FT
 100 YEAR DISCHARGE = 84.37 CFS
 100 YEAR HW ELEVATION = 2125.69 FT
 OVERTOPPING FREQUENCY = 100+ YRS
 OVERTOPPING DISCHARGE = 111.70 CFS
 OVERTOPPING ELEVATION = 2127.01 FT

BRIDGE HYDRAULIC DATA

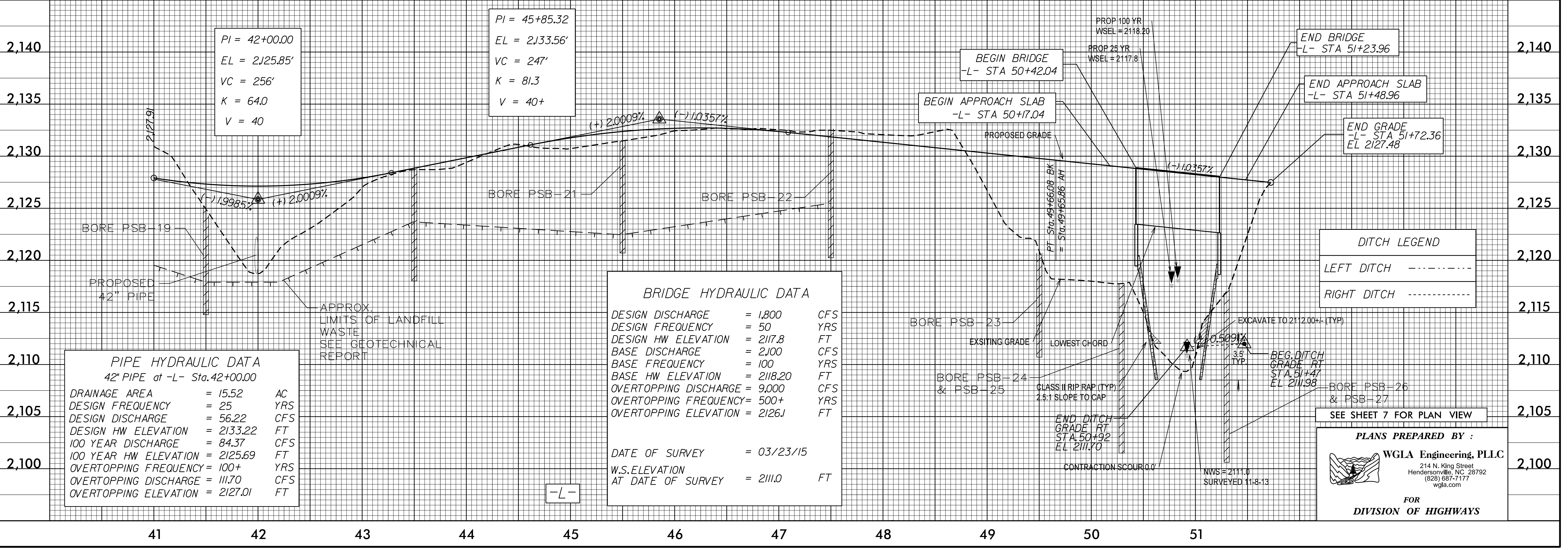
DESIGN DISCHARGE = 1,800 CFS
 DESIGN FREQUENCY = 50 YRS
 DESIGN HW ELEVATION = 2117.8 FT
 BASE DISCHARGE = 2,100 CFS
 BASE FREQUENCY = 100 YRS
 BASE HW ELEVATION = 2118.20 FT
 OVERTOPPING DISCHARGE = 9,000 CFS
 OVERTOPPING FREQUENCY = 500+ YRS
 OVERTOPPING ELEVATION = 2126.1 FT

DATE OF SURVEY = 03/23/15
 W.S. ELEVATION AT DATE OF SURVEY = 2111.0 FT

DITCH LEGEND

LEFT DITCH - - - - -

RIGHT DITCH - - - - -



PLANS PREPARED BY :

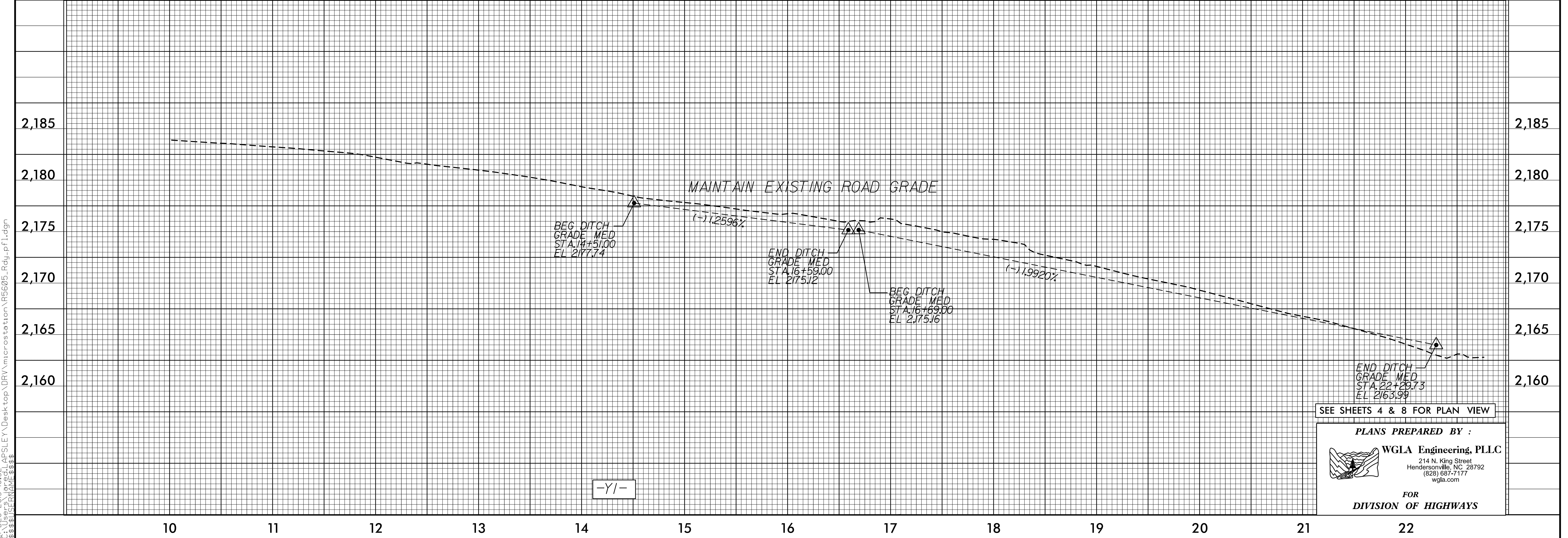
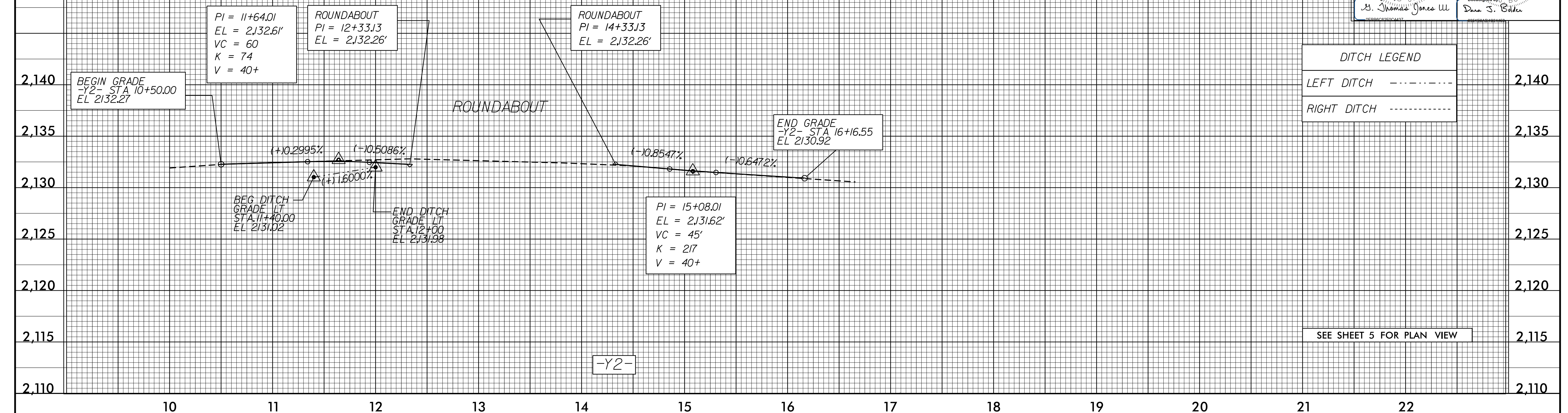
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21-DEC-2015 16:31 I:\APSL\LEV\Desk_top\DRV\micromast\station\R5605_Rdy.plt.dgn

PROJECT REFERENCE NO. R-5605	SHEET NO. 11
ROADWAY DESIGN ENGINEER 12/23/2015	HYDRAULICS ENGINEER 12/22/2015
DocuSigned by: Thomas Jones III	DocuSigned by: Dana S. Bolden

5/28/19



21 DEC 2015 16:32 APS\LE\A\Desk_top\DRV\microstation\R5605_Rdy.plt.dgn

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5/28/11

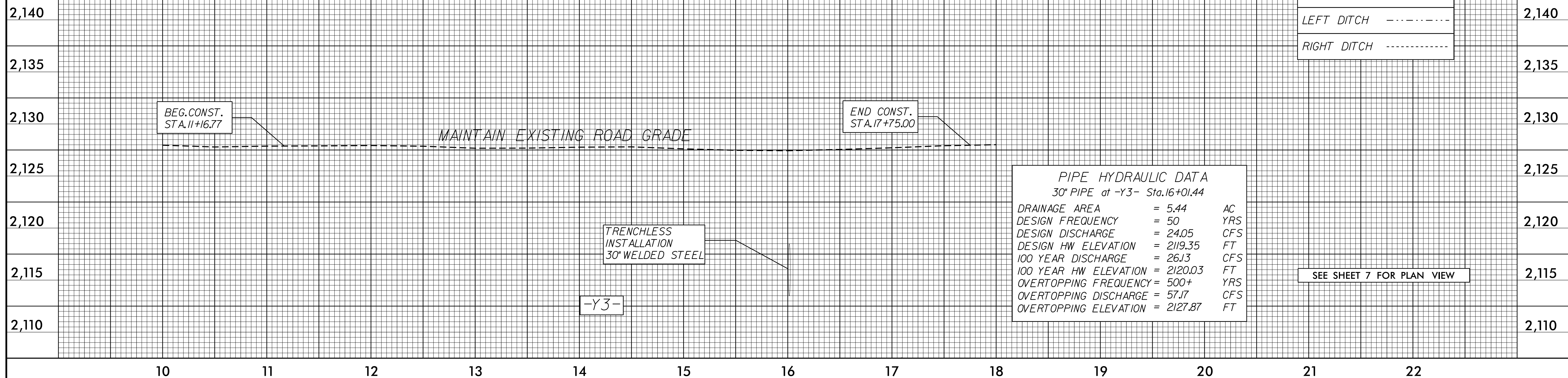
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PROJECT REFERENCE NO. R-5605	SHEET NO. 12
ROADWAY DESIGN ENGINEER 12/23/2015	HYDRAULICS ENGINEER 12/22/2015
THOMAS JONES III	DANA S. BOLDEN

DITCH LEGEND

LEFT DITCH - - - - -

RIGHT DITCH - - - - -



PIPE HYDRAULIC DATA
30" PIPE at -Y3- Sta.16+01.44

DRAINAGE AREA	= 5.44	AC
DESIGN FREQUENCY	= 50	YRS
DESIGN DISCHARGE	= 24.05	CFS
DESIGN HW ELEVATION	= 219.35	FT
100 YEAR DISCHARGE	= 26.13	CFS
100 YEAR HW ELEVATION	= 2120.03	FT
OVERTOPPING FREQUENCY	= 500+	YRS
OVERTOPPING DISCHARGE	= 57.17	CFS
OVERTOPPING ELEVATION	= 2127.87	FT

SEE SHEET 7 FOR PLAN VIEW

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