

**NOTES**

STIRRUPS AND "U" BARS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.

BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.

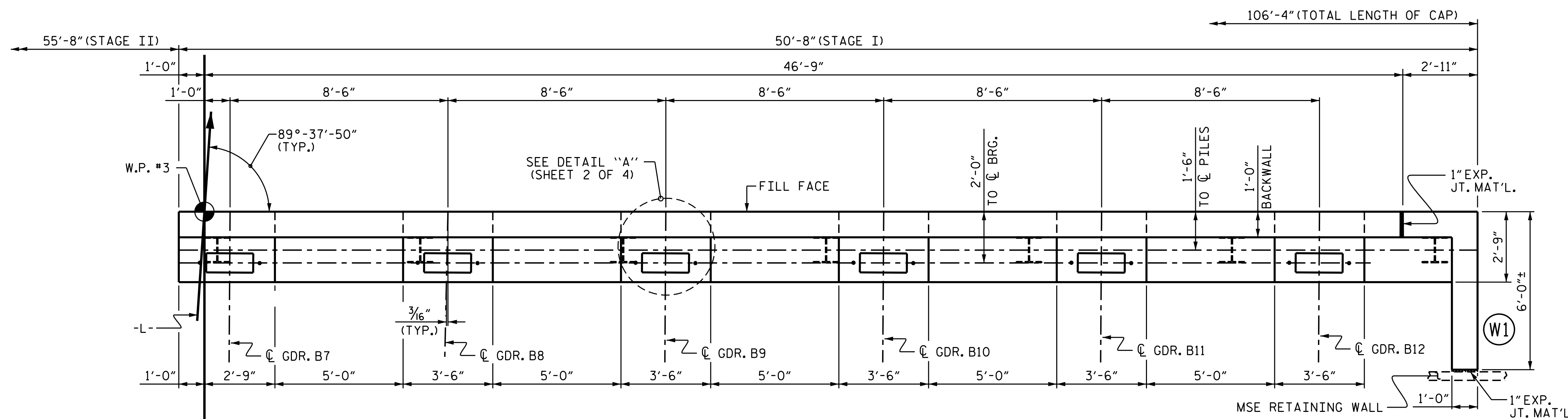
THE TOP SURFACE OF THE END BENT CAP EXCEPT THE BRIDGE SEAT BUILDUPS SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.

THE TOP SURFACE AREAS OF THE END BENT CAPS SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.

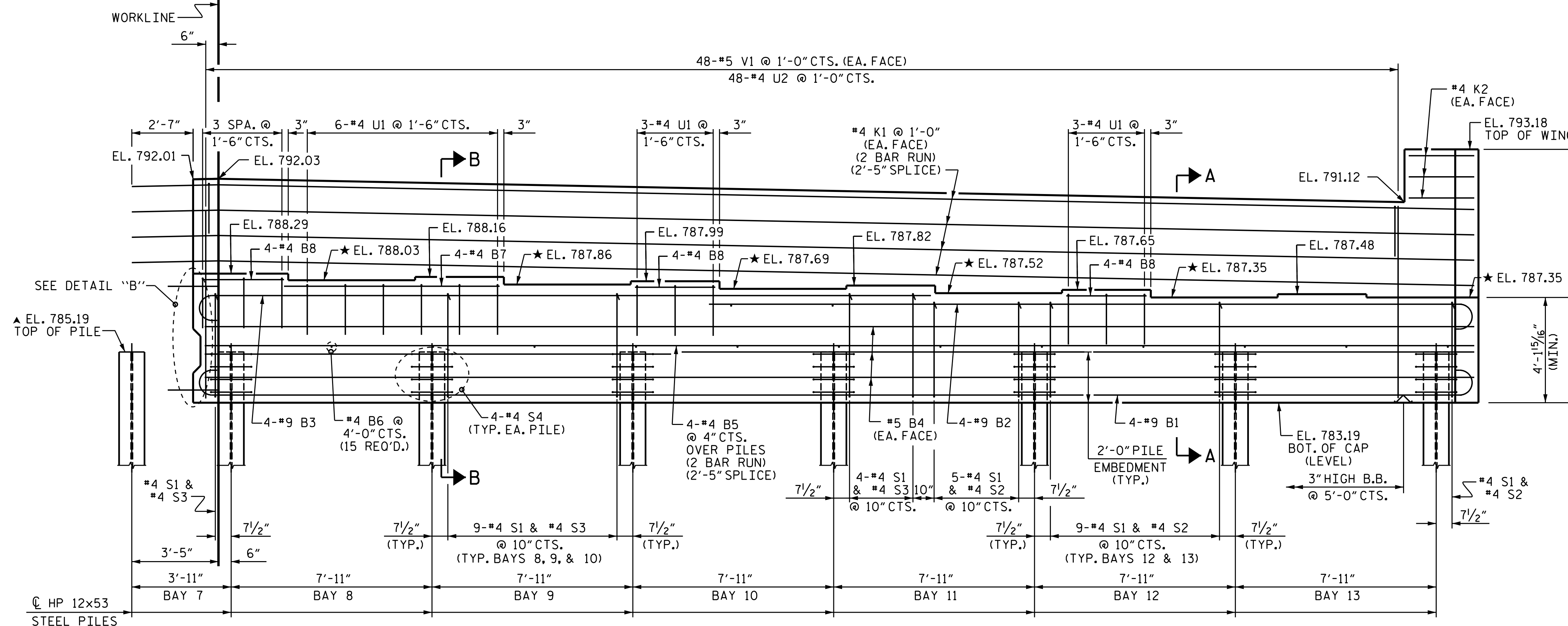
★ FOR LOCATION OF ELEVATIONS BETWEEN BUILDUPS, SEE SECTIONS ON SHEET 4 OF 4.

FOR PILE SPLICE DETAILS, SEE END BENT 1, SHEET 4 OF 4.

FOR MSE RETAINING WALLS, SEE SPECIAL PROVISIONS.

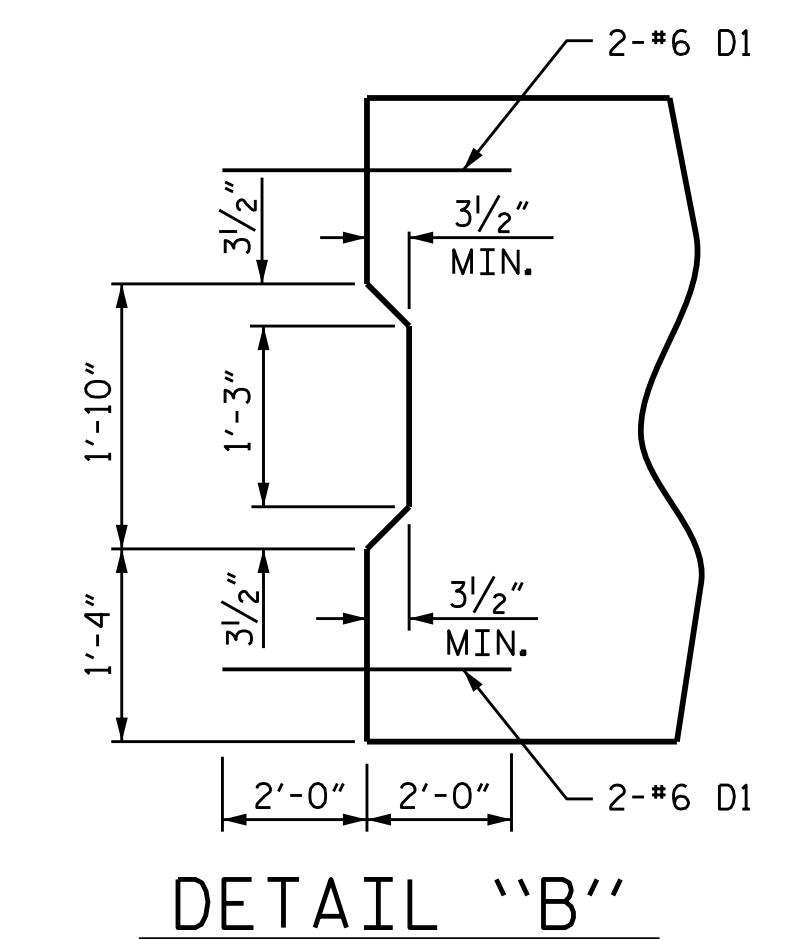


**PLAN**



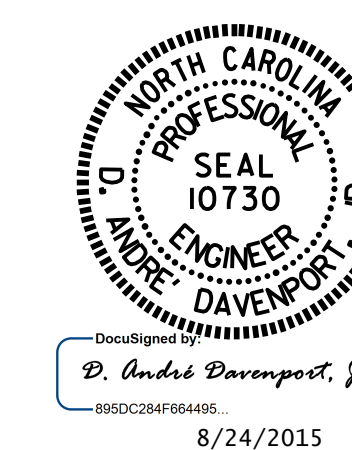
**ELEVATION**

▲ DRIVE PILE WITH STAGE I.  
FOR "TIE BACK DETAILS", FOR MSE RETAINING WALL, SEE SHEET 2 OF 4.



PROJECT NO. B-3159  
DAVIDSON COUNTY  
STATION: 13+41.72 -L-

SHEET 1 OF 4



STATE OF NORTH CAROLINA		DEPARTMENT OF TRANSPORTATION		RALEIGH	
SUBSTRUCTURE					
END BENT 2					
STAGE I					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
SHEET NO.					S-35
TOTAL SHEETS					43

DRAWN BY : K. D. LAYNE DATE : 5/15/15  
CHECKED BY : H. P. KIM DATE : 5/28/15  
DESIGN ENGINEER OF RECORD: R. L. CHESSON DATE : 6/10/15