

NOTES

PROVIDE 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0" CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF "A" BARS. WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M.) @ 4'-0" CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF "A" BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

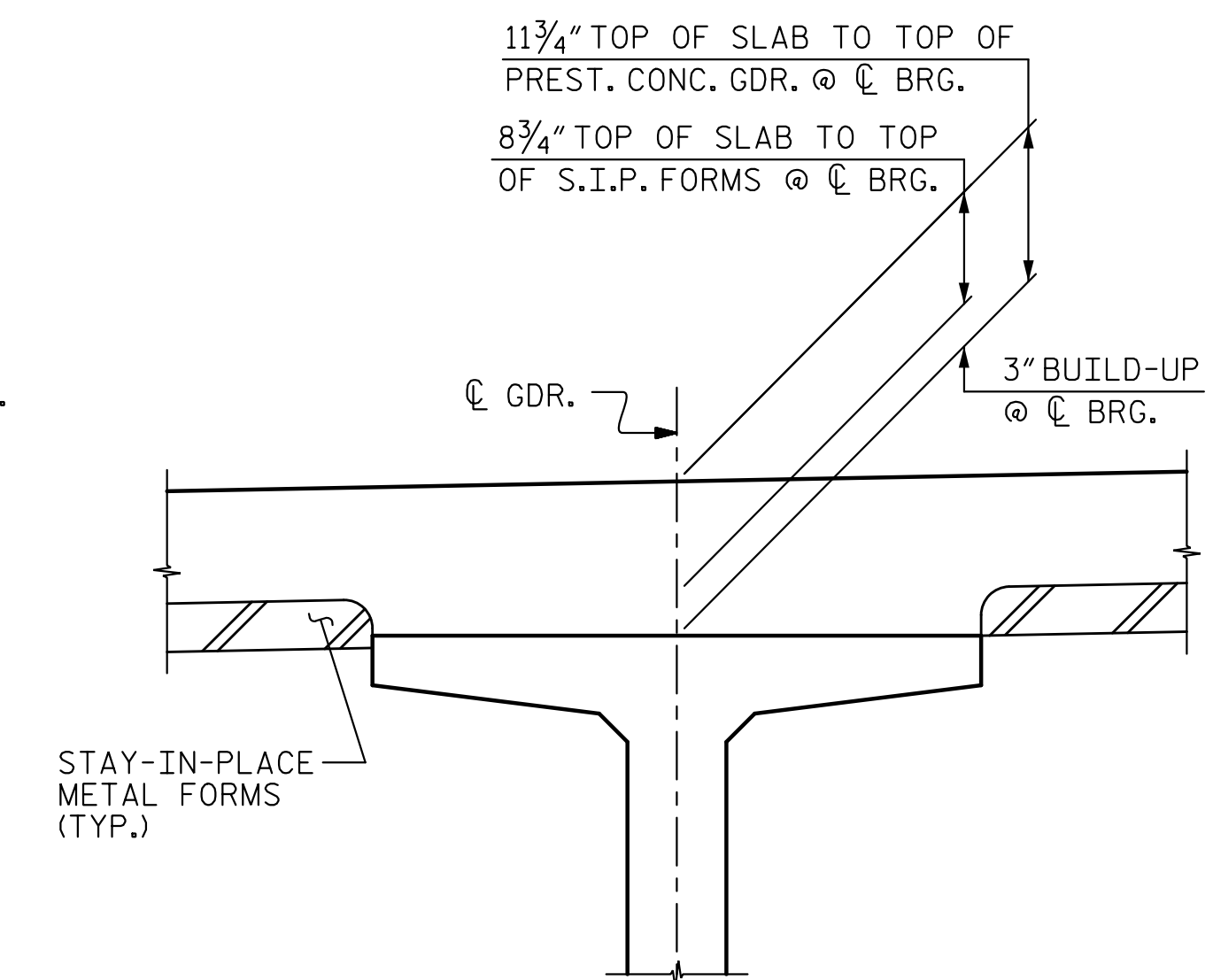
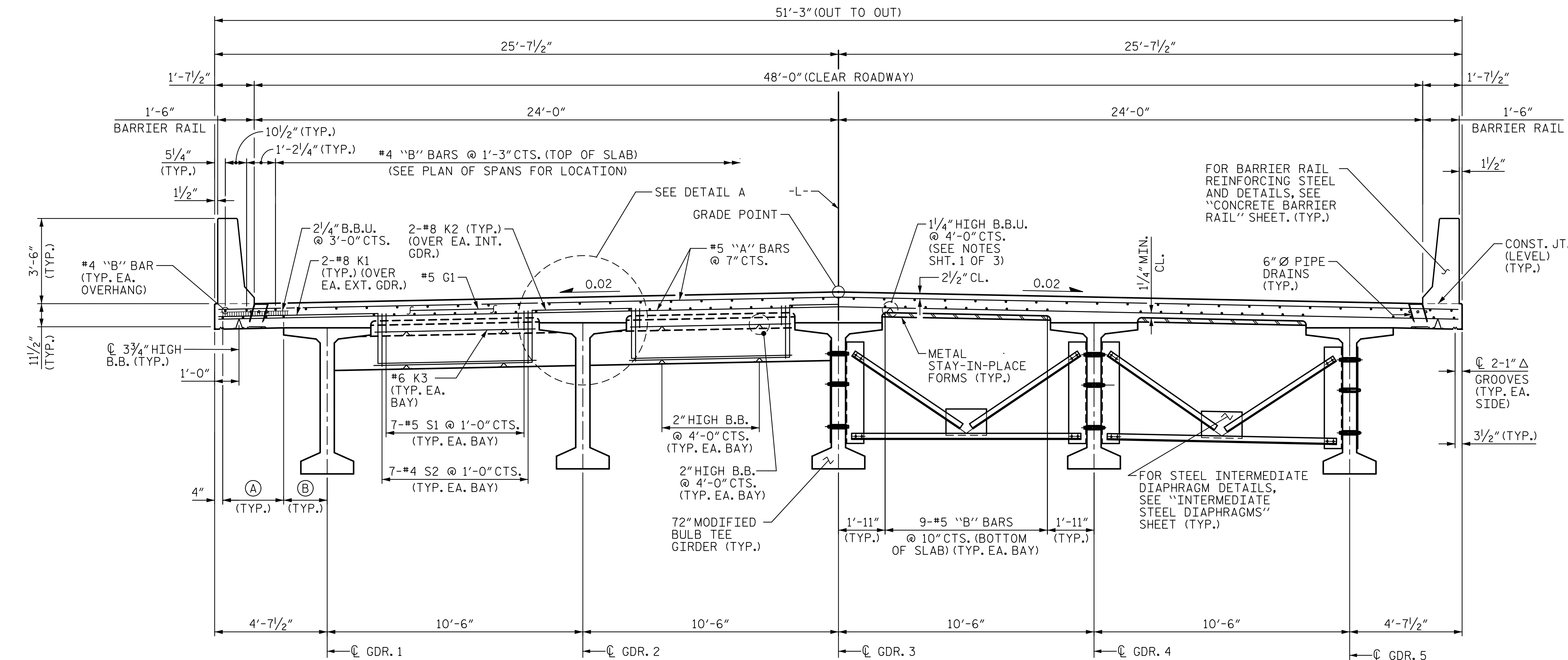
LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

PREVIOUSLY CAST CONCRETE IN A CONTINUOUS UNIT SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.

CONCRETE BARRIER RAIL IN EACH CONTINUOUS UNIT SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE IN THE UNIT HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

#5 G1 BAR MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO CLEAR REINFORCING STEEL AND STIRRUPS.

FOR 6" Ø PIPE DRAIN DETAILS, SEE SHT. 2 OF 3.



DETAIL "A"
(TYPICAL EACH GIRDER @ EACH BENT)

HALF SECTION - END BENT DIAPHRAGM HALF SECTION - INTERMEDIATE DIAPHRAGM

TYPICAL SECTION

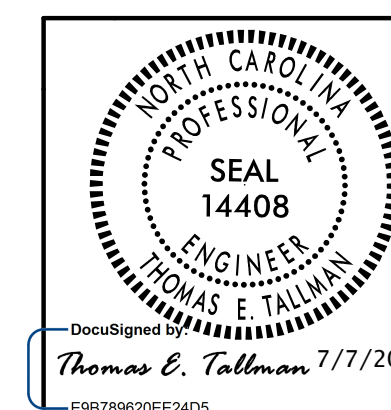
- (A) 4-#5 "B" BARS @ 10" SPA.
- (B) 1'-9 1/2"

PROJECT NO. W-5516
ROWAN COUNTY
 STATION: 61+79.40 -L-

SHEET 1 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

SUPERSTRUCTURE
 TYPICAL SECTION



REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-5
1			3			TOTAL SHEETS
2			4			41

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 TCA Engineering, Inc.

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