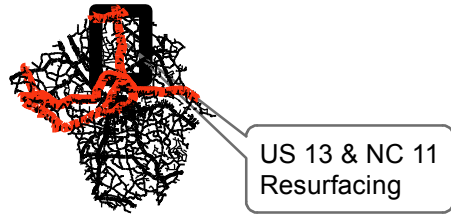


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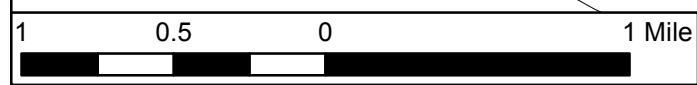
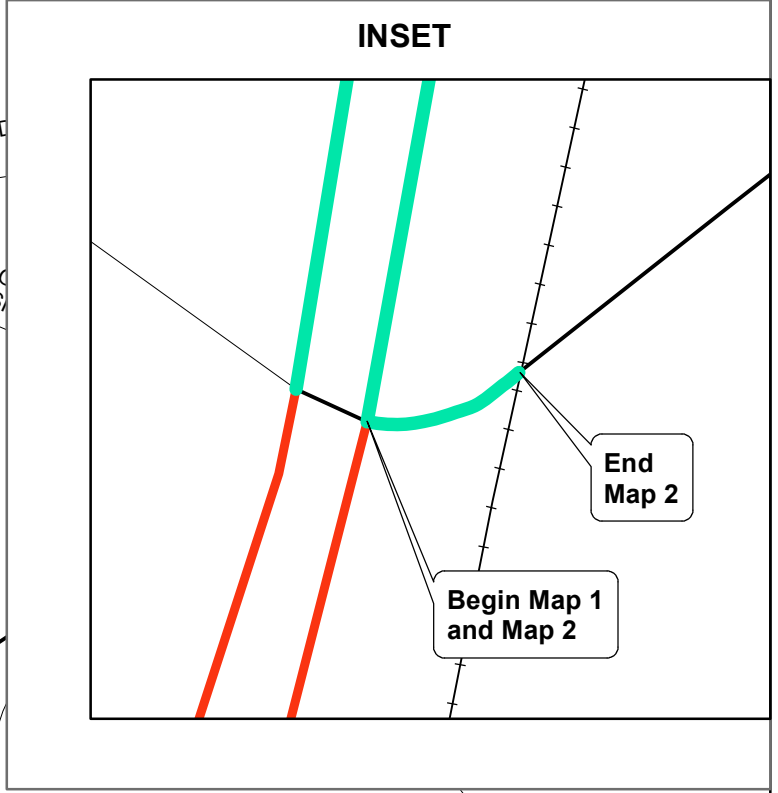
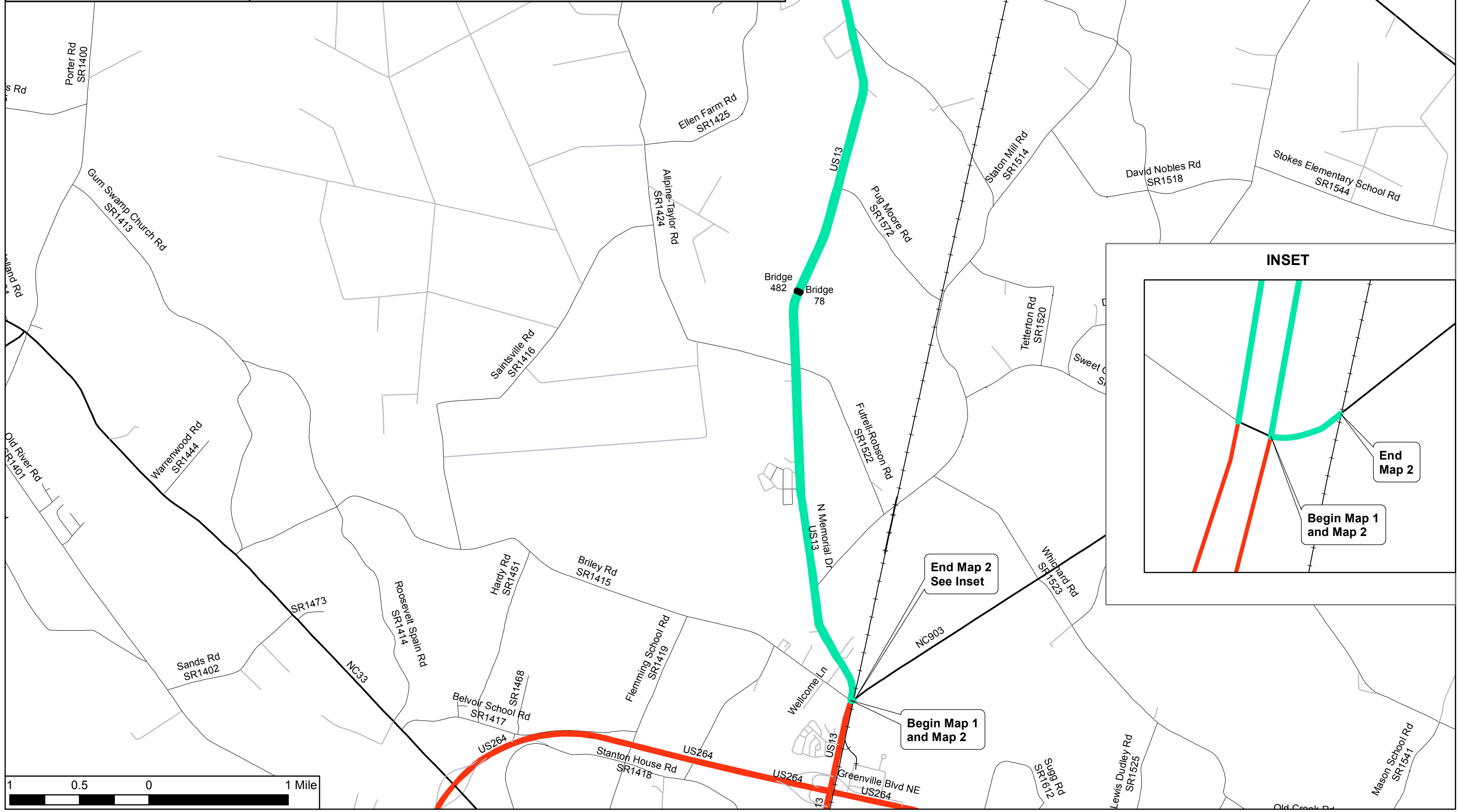
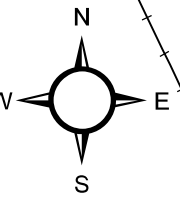
# MAP 1

WBS: 2016CPT.02.04.10741.2  
Resurface US 13 and NC 11 from  
NC 903 to SR 1512

# MAP 2

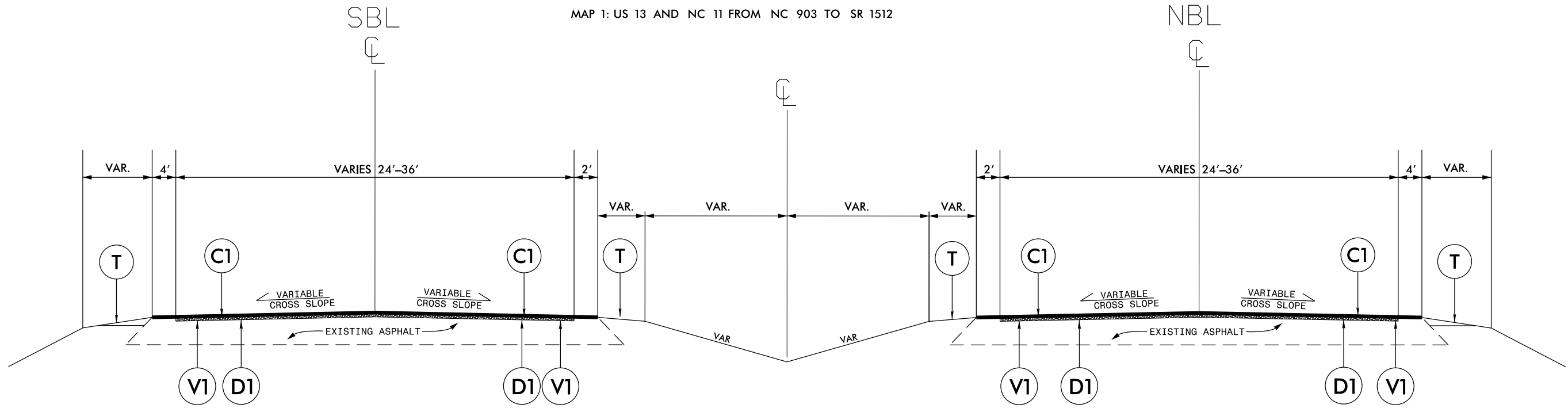
WBS: 2016CPT.02.04.10741.2  
Resurface NC 903 from US 13  
and NC 11 to Pavement Seam  
Just East of Railroad Crossing

Sheet 1



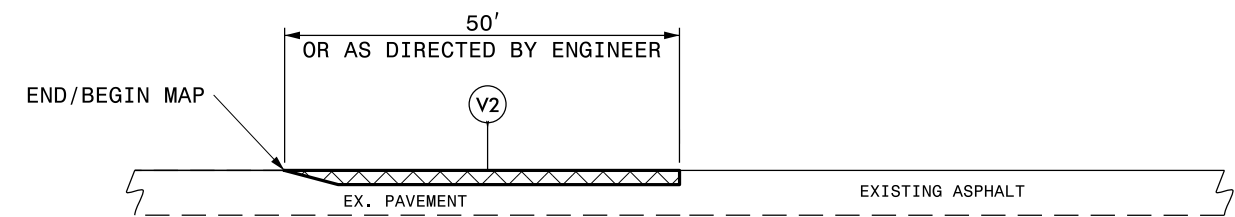
### TYPICAL SECTION NO. 1

MAP 1: US 13 AND NC 11 FROM NC 903 TO SR 1512



**NOTE:**

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE SECTIONS AND BRIDGE APPROACH TIE-INS, OR AS DIRECTED BY THE ENGINEER. SEE DETAIL 1 & 2



### DETAIL 1

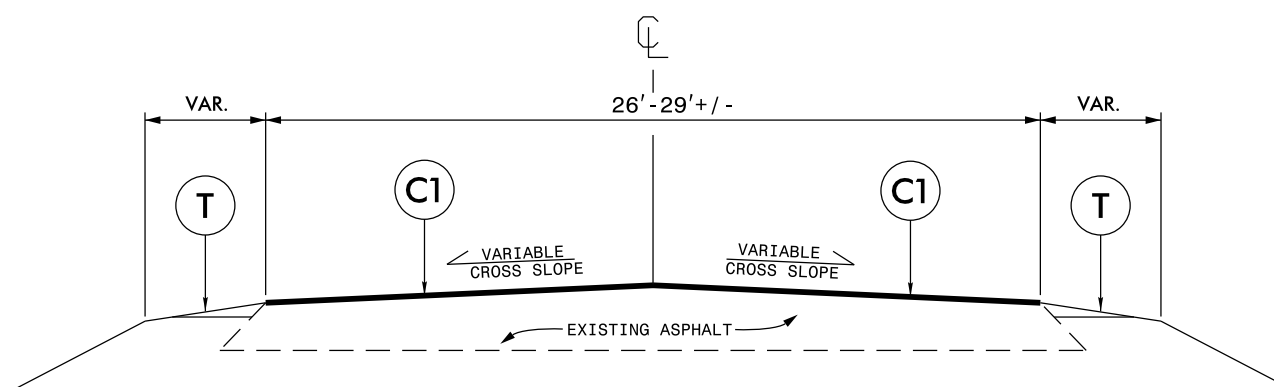
**MAIN LINE MILLING**

**NOTE:**

1. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE SECTIONS, OR AS DIRECTED BY THE ENGINEER.
2. PAVE TO THE END OF THE MILLED SURFACE TO CREATE A SMOOTH TRANSITION.

### TYPICAL SECTION NO. 2

MAP 2: NC 903 FROM US 13 AND NC 11 TO PAVEMENT SEAM AT RAILROAD CROSSING



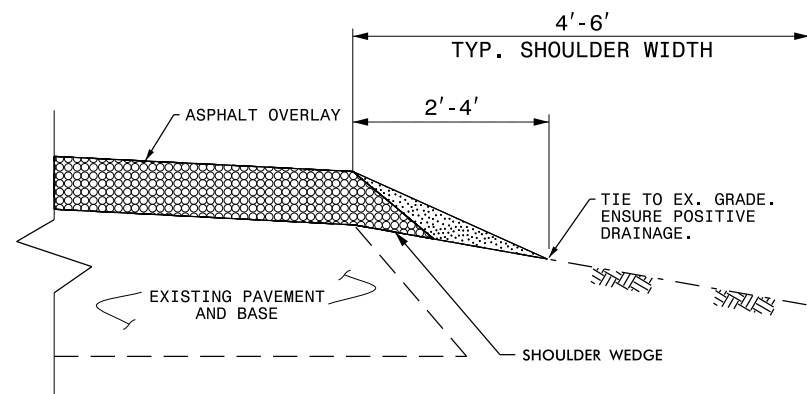
**NOTE:**

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE SECTIONS AND BRIDGE APPROACH TIE-INS, OR AS DIRECTED BY THE ENGINEER. SEE DETAIL 1 & 2

### PAVEMENT SCHEDULE

C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
D1	PROP. APPROX. 2½" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	MILLING DEPTH 2½" FOR THE WIDTH OF TRAVEL LANE.
V2	INCIDENTAL MILLING

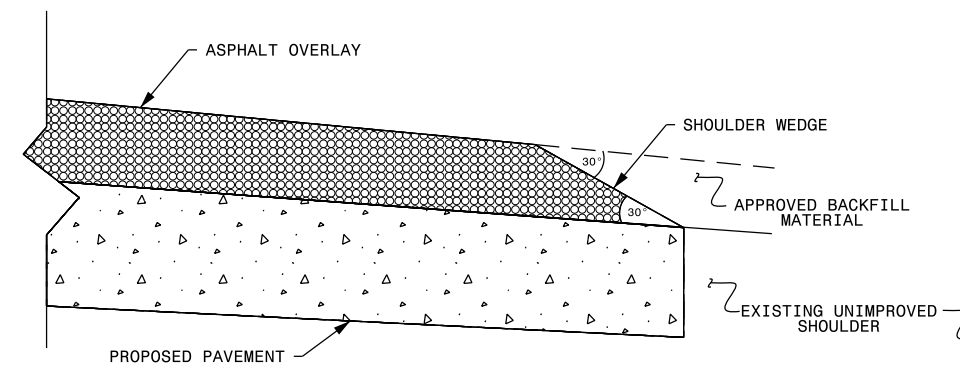
**DRAWINGS NOT TO SCALE**



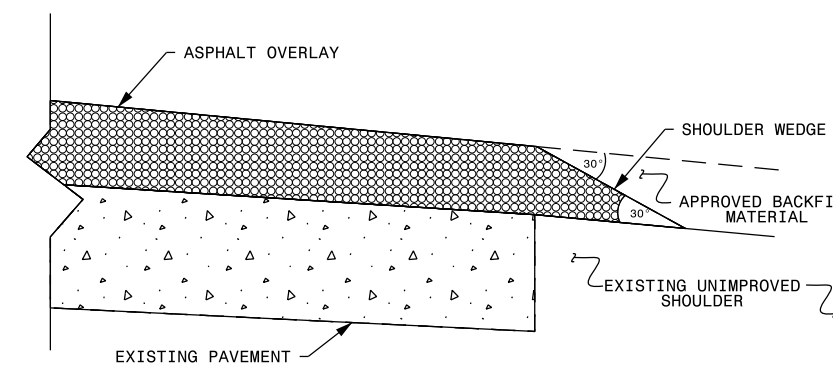
**SHOULDER RECONSTRUCTION DETAIL**

**NOTE:**

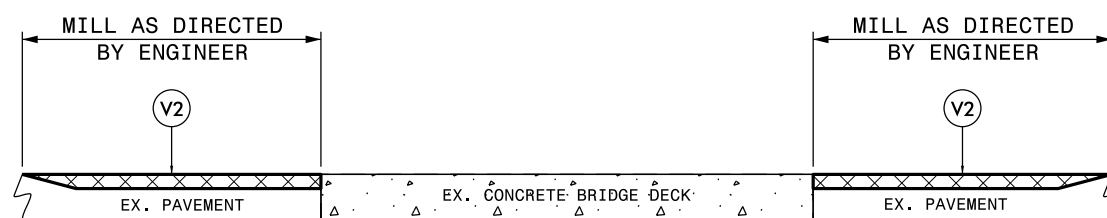
1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT APPROVED BORROW PITS OR FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.



**SHOULDER WEDGE DETAIL**  
(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



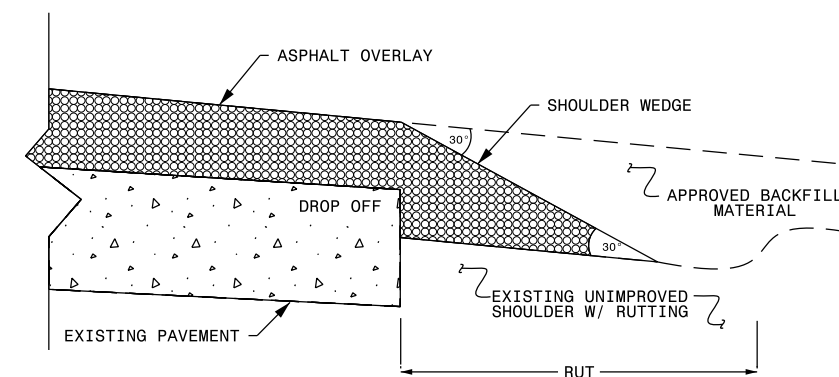
**SHOULDER WEDGE DETAIL**  
(Resurfacing Projects w/ no Widening)



**DETAIL 2**  
**BRIDGE MILLING**

**NOTE:**

1. MILLING SHALL BE PERFORMED AT BRIDGE APPROACHES AT PITT CO. BRIDGE NUMBERS 78 AND 482 AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



**SHOULDER WEDGE DETAIL**  
(Resurfacing Adjacent to Rutted Shoulder)

**DETAIL**  
**SHOULDER WEDGE DETAILS**

**NOTES:**

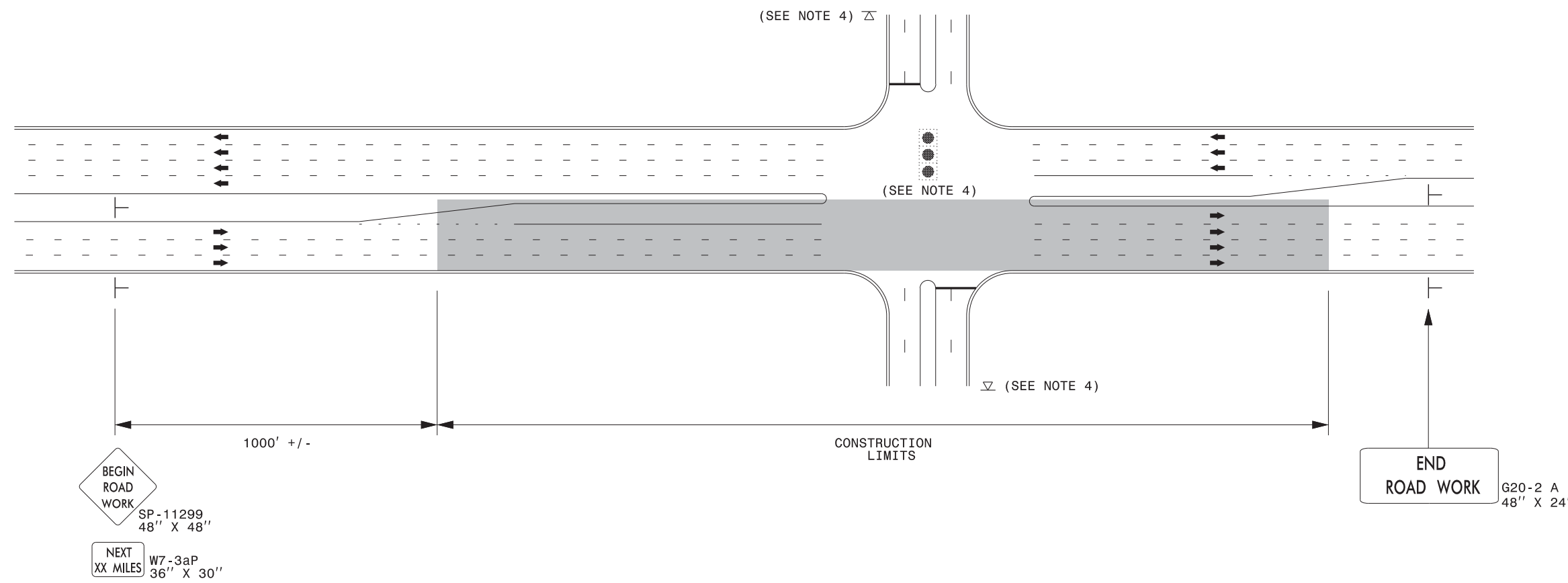
1. DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
2. BACKFILL SHOULDER WITH APPROVED MATERIAL.
3. THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.

PROJECT NO.	SHEET NO.	TOTAL NO.
2016CPT.02.04.10741.2	4	

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	2.5" MILLING SY	INCIDENTAL MILLING SY	INTER-MEDIATE COURSE, I19.0B TONS	SURFACE COURSE, S9.5B TONS	ASPHALT BINDER FOR PLANT MIX TONS	ADJ. OF METER OR VALVE BOX EA	SEED & MULCHING AC	4413000000-E	4457000000-N
																					WORK ZONE ADVANCE / GENERAL WARNING SIGNING SF	TEMP TRAFFIC CONTROL LS
2016CPT.02.04.10741.2	Pitt	1	US 13	FROM NC 903 TO SR 1512	1	2	MD	NO	NO	5.92	48	100	23.70	174,426	1,550	26,961	23,189	2,685	3	2.96	3,158	0.5
		2	NC 903	FROM US 13 TO RR CROSSING	2	2	2WU	NO	NO	0.098	60		0.09		3,468		598	36		0.05		0.5
<b>TOTAL FOR PROJ NO. 2016CPT.02.04.10741.2</b>										<b>6.018</b>		<b>100</b>	<b>23.79</b>	<b>174,426</b>	<b>5,018</b>	<b>26,961</b>	<b>23,787</b>	<b>2,721</b>	<b>3</b>	<b>3.01</b>	<b>3,158</b>	<b>1</b>
<b>GRAND TOTAL</b>										<b>6.018</b>		<b>100</b>	<b>23.79</b>	<b>174,426</b>	<b>5,018</b>	<b>26,961</b>	<b>23,787</b>	<b>2,721</b>	<b>3</b>	<b>3.01</b>	<b>3,158</b>	<b>1</b>

## URBAN / SUBURBAN WORKZONES



**NOTES:**

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND	
├	STATIONARY SIGN
➔	DIRECTION OF TRAFFIC FLOW

	<p><b>RESURFACING ADVANCE WARNING SIGNS FOR URBAN / SUBURBAN FACILITIES</b></p>
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