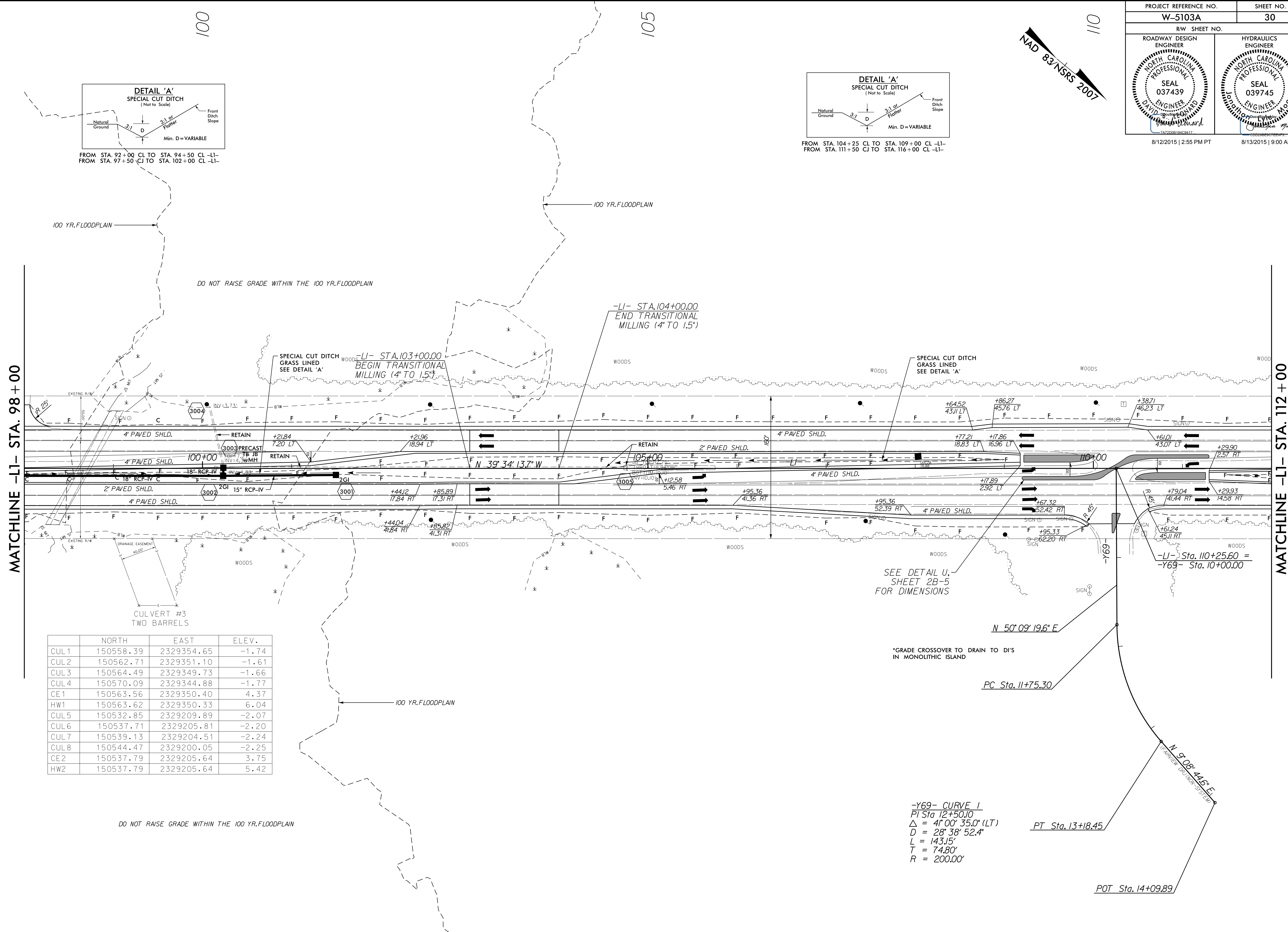
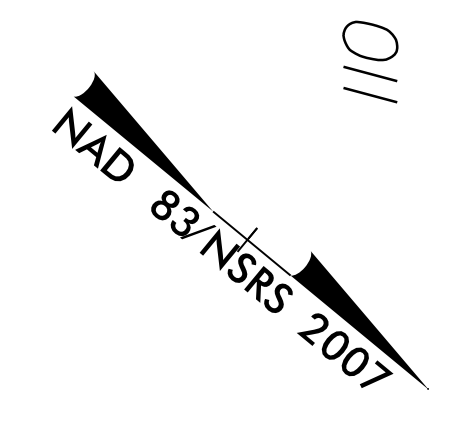
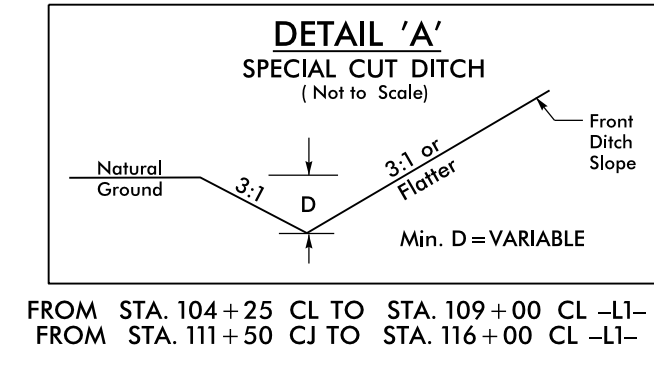
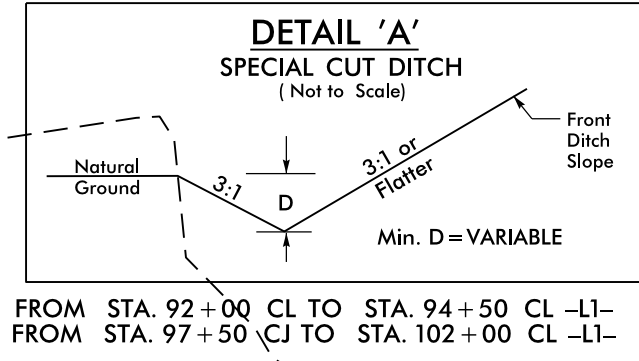


REVISIONS

PROJECT REFERENCE NO. W-5103A	SHEET NO. 30
RW SHEET NO.	
ROADWAY DESIGN ENGINEER DAVID S. LEONARD SEAL 037439	HYDRAULICS ENGINEER DAVID S. LEONARD SEAL 039745
8/12/2015 2:55 PM PT	8/13/2015 9:00 AM ET



	NORTH	EAST	ELEV.
CUL1	150558.39	2329354.65	-1.74
CUL2	150562.71	2329351.10	-1.61
CUL3	150564.49	2329349.73	-1.66
CUL4	150570.09	2329344.88	-1.77
CE1	150563.56	2329350.40	4.37
HW1	150563.62	2329350.33	6.04
CUL5	150532.85	2329209.89	-2.07
CUL6	150537.71	2329205.81	-2.20
CUL7	150539.13	2329204.51	-2.24
CUL8	150544.47	2329200.05	-2.25
CE2	150537.79	2329205.64	3.75
HW2	150537.79	2329205.64	5.42

-Y69- CURVE 1
 PI Sta. 12+50.10
 $\Delta = 41^{\circ}00'35.0"$ (LT)
 $D = 28^{\circ}38'52.4"$
 $L = 143.15'$
 $T = 74.80'$
 $R = 200.00'$

POT Sta. 14+09.89

N 50°09'19.6" E

PC Sta. 11+75.30

-LI- Sta. 110+25.60 =
 -Y69- Sta. 10+00.00

*GRADE CROSSOVER TO DRAIN TO DI'S
 IN MONOLITHIC ISLAND

DO NOT RAISE GRADE WITHIN THE 100 YR. FLOODPLAIN

DO NOT RAISE GRADE WITHIN THE 100 YR. FLOODPLAIN

-LI- STA. 104+00.00
 END TRANSITIONAL
 MILLING (4" TO 1.5")

-LI- STA. 103+00.00
 BEGIN TRANSITIONAL
 MILLING (4" TO 1.5")

MATCHLINE -LI- STA. 98+00

MATCHLINE -LI- STA. 112+00