

- NOTES:
- 1) TIE TO EXIST CURB AND GUTTER AT EXIST CONCRETE JOINT
 - 2) SAWCUT AND REMOVE OR MILL EXISTING ASPHALT PAVEMENT OR CONCRETE PAVEMENT TO TIE-IN DRIVEWAY
 - 3) TIE TO EXISTING CONCRETE SIDEWALK STRUCTURE
 - 4) REMOVE EXISTING STORM DRAINAGE
 - 5) PROP MAST ARM SIGNAL POLE (SEE SIGNAL PLANS)
 - 6) BRICK PAVEMENT SIDEWALK

Kimley»Horn
 P.O. BOX 33068 • RALEIGH, N.C. 27636-3068

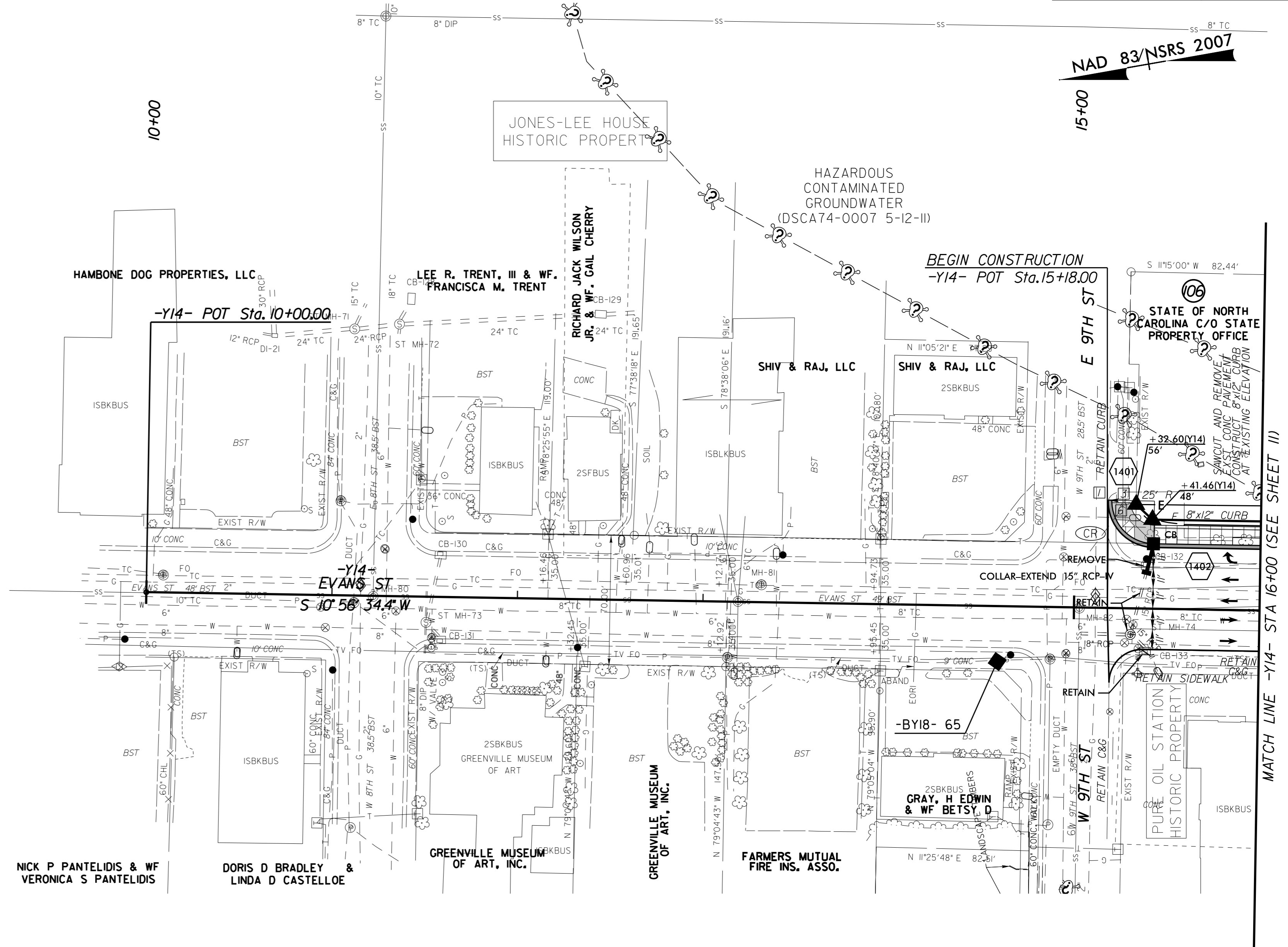
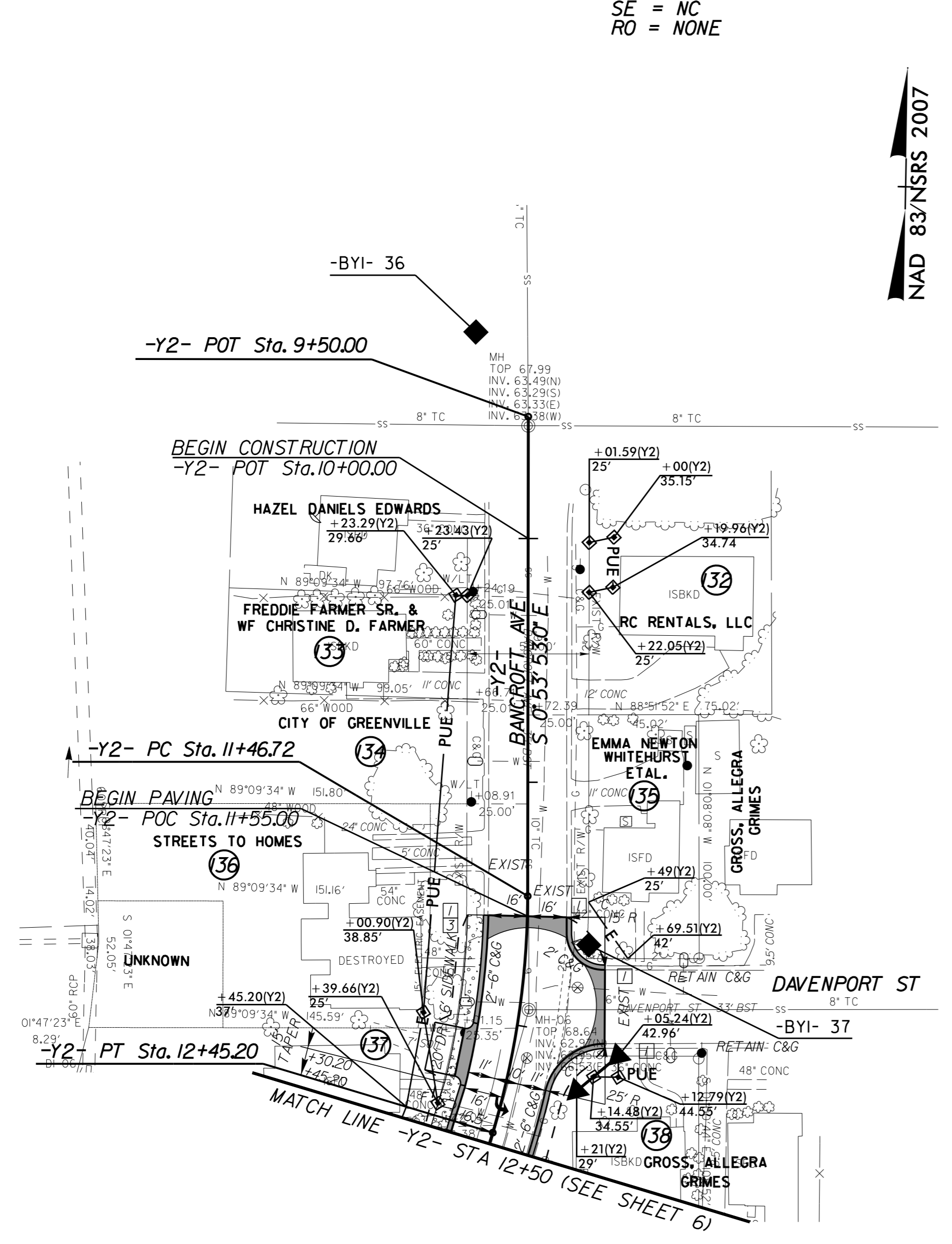
RIGHT-OF-WAY REV.
 CONST. REV.

PROJECT REFERENCE NO. U-3315	SHEET NO. 14
R/W SHEET NO.	HYDRAULICS ENGINEER
ROADWAY ENGINEER	DR. DAVID C. SMITH 20744 7/2/2015

-Y2-
 PI Sta 11+96.31
 $\Delta = 16' 35" 42.2" (RT)$
 $D = 16' 51" 06.1"$
 $L = 98.48'$
 $T = 49.59'$
 $R = 340.00'$
 $DS = 40 MPH$
 $SE = NC$
 $RO = NONE$

NAD 83/NSRS 2007

REVISIONS



NAD 83/NSRS 2007

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6/26/2015

(37) KATHERINE FIELDS LIFE EST

SEE SHEET NO. 21 FOR -Y2- PROFILE

SEE SHEET NO. 24 FOR -Y14- PROFILE