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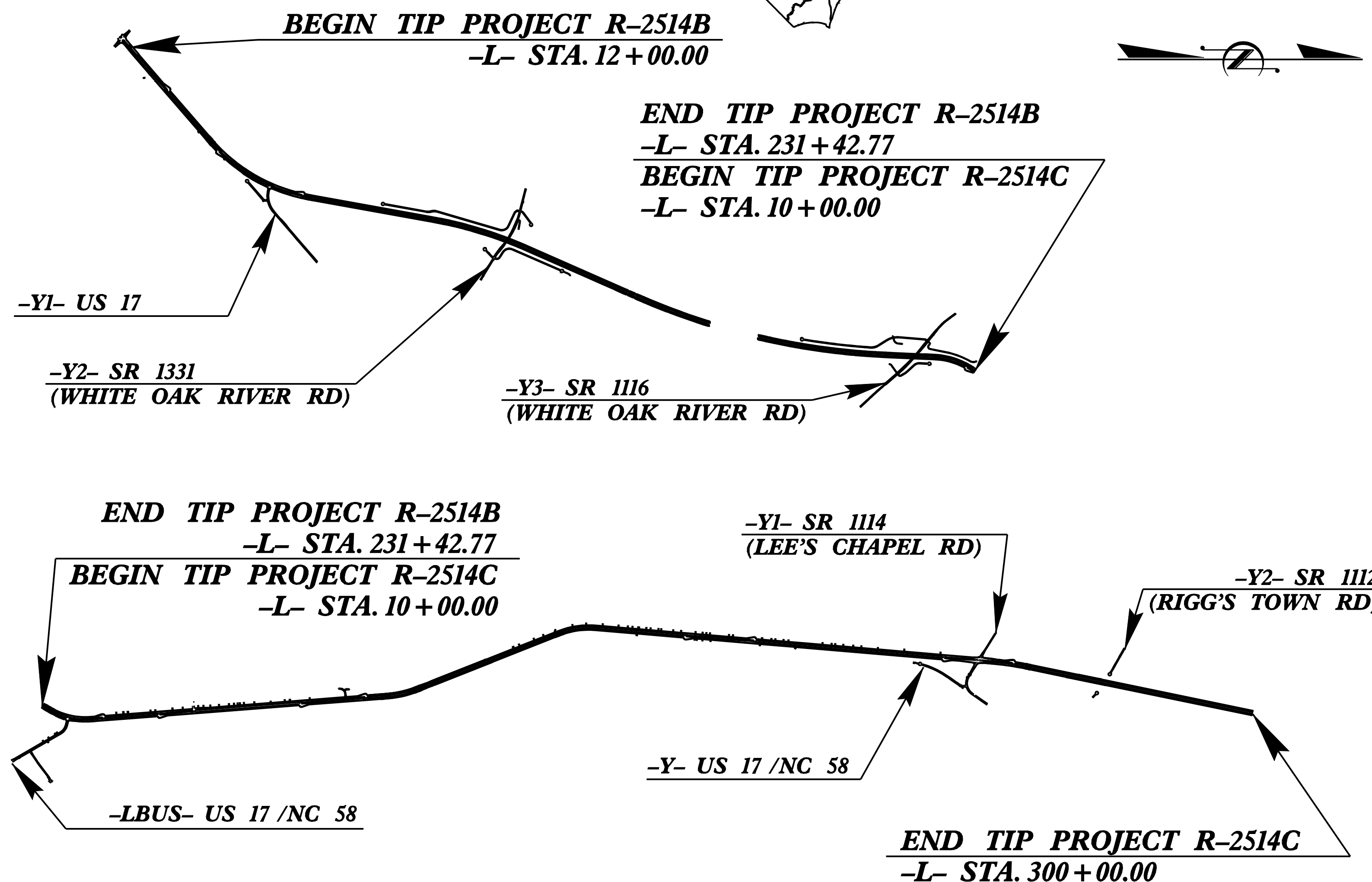
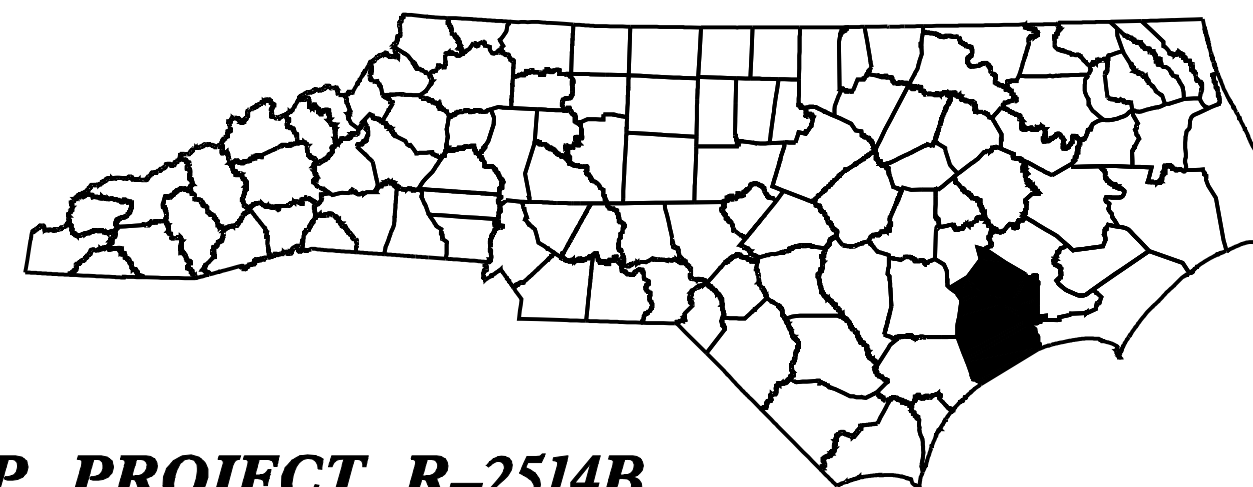
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STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

ONSLOW – JONES COUNTY

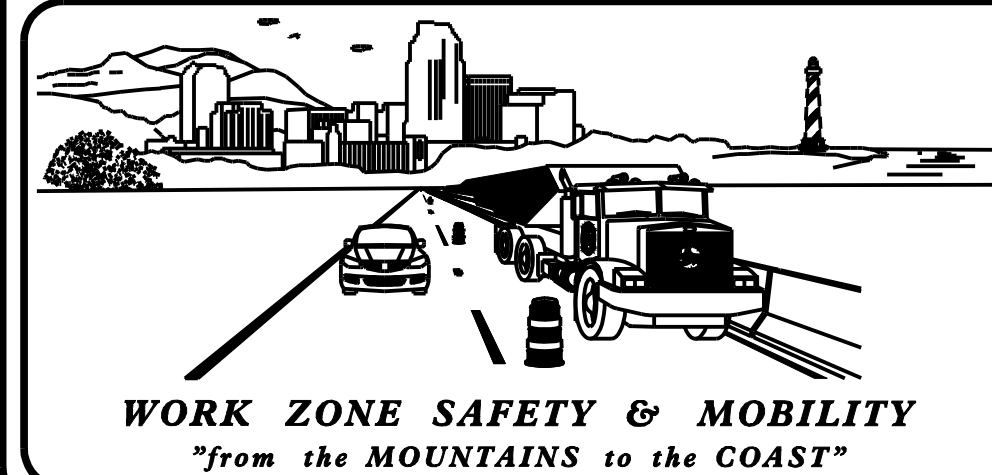


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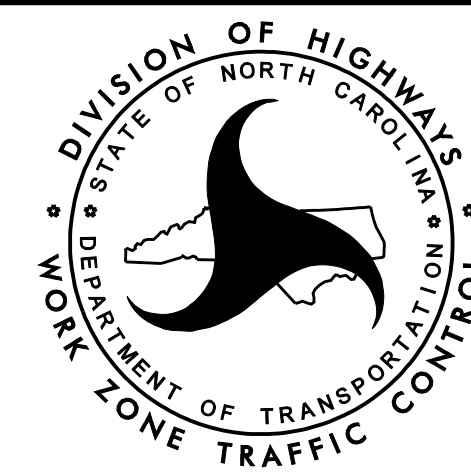
SHEET NO.
TMP-1

TIP PROJECT: R-2514B/C



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APPROVED: *Tim Arey*
DATE: 4/15/2015

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ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMPS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)
- WORK AREA
- REMOVAL
- TEMPORARY PAVEMENT
- ON-GOING CONSTRUCTION

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM SKINNY DRUM TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

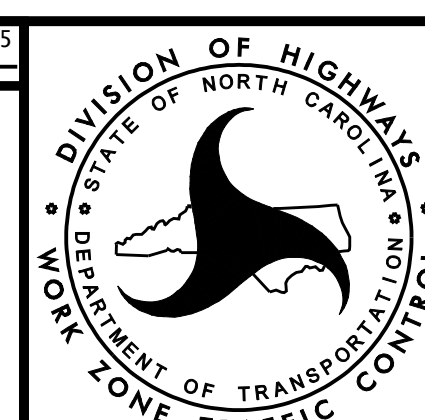
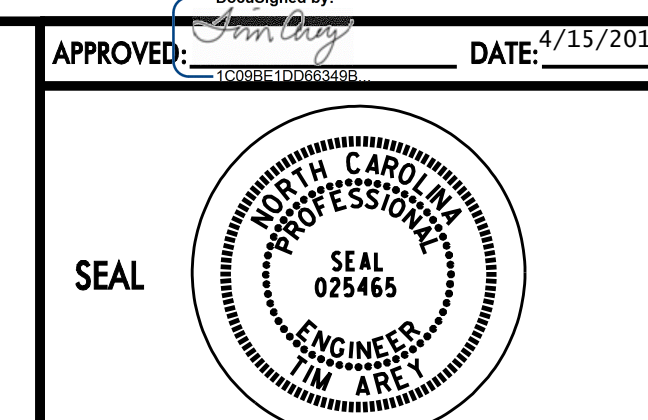
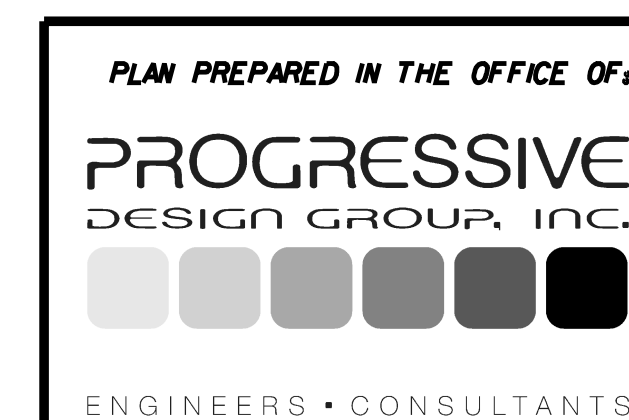
- PAVEMENT MARKING SYMBOLS

TEMPORARY PAVEMENT MARKING

PAVEMENT MARKINGS PAINT (4")	PAINT (8")
PA WHITE EDGE LINE	PV YELLOW DIAGONAL
PB YELLOW EDGE LINE	
PC 10 FT. WHITE SKIP	
PD 3 FT. - 9 FT./SP WHITE MINISKIP	
PE WHITE SOLID LANE LINE	PAINT (24")
PI YELLOW DOUBLE CENTER	P2 WHITE STOPBAR

COLD APPLIED PLASTIC-TYPE 4 (4")

- CA WHITE EDGE LINE
- CI YELLOW DOUBLE CENTERLINE



ROADWAY STANDARD
DRAWINGS & LEGEND

MANAGEMENT STRATEGIES

PROJ. REFERENCE NO. R-2514B/C	SHEET NO. TMP-1B
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THIS PROJECT UTILIZES TEMPORARY CROSSOVERS ON US 17 IN ORDER TO ALLOW US 17 TRAFFIC TO REMAIN IN A 2-LANE, 2-WAY PATTERN DURING STAGE CONSTRUCTION OF THE MAINLINE. THIS STRATEGY ALLOWS FOR MAXIMUM WORK PRODUCTIVITY AND LESSENS THE OVERALL IMPACT TO US 17 TRAFFIC. OFF SITE DETOURS WILL BE UTILIZED DURING THE CONSTRUCTION OF SOME OF THE -Y- LINES. COORDINATION WITH THE CONTRACTOR FOR THE ADJACENT R-2519D PROJECT WILL BE REQUIRED FOR A PHASE III TRAFFIC SHIFT.

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 17	MONDAY THRU FRIDAY 6:00 AM TO 9:00 AM AND 4:00 PM TO 6:00 PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
US 17

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 6:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 6:00 P.M. THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 6:00 P.M. MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 6:00 P.M. TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 6:00 P.M. MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.

F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

J) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 350 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

L) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

M) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

N) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.

COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.

O) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

P) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 350 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC BARRIER

Q) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

R) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

S) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.

T) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

U) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES DRUMS PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

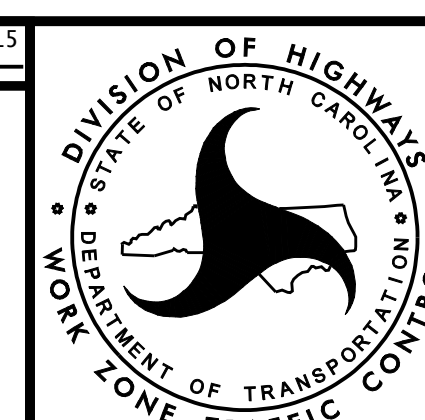
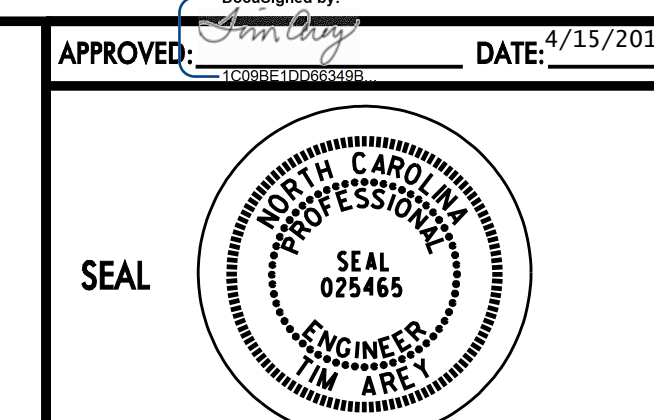
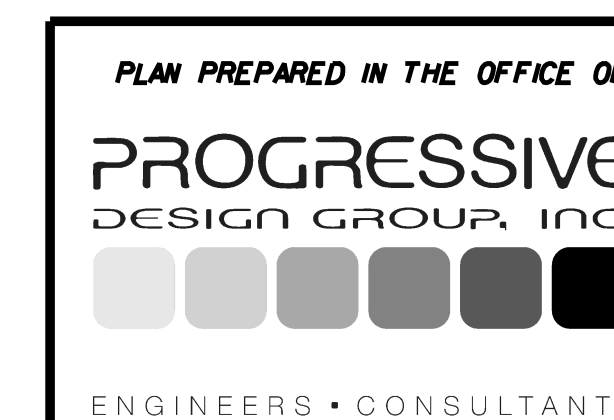
V) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL ROADS	PAINT/COLD APPLIED PLASTIC	TEMPORARY RAISED
STRUCTURES	COLD APPLIED PLASTIC	TEMPORARY RAISED

W) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

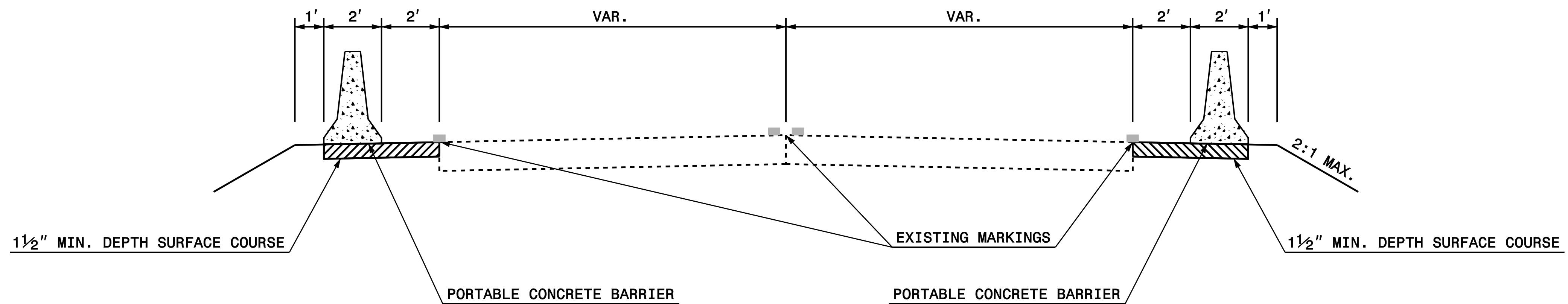
X) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

Y) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

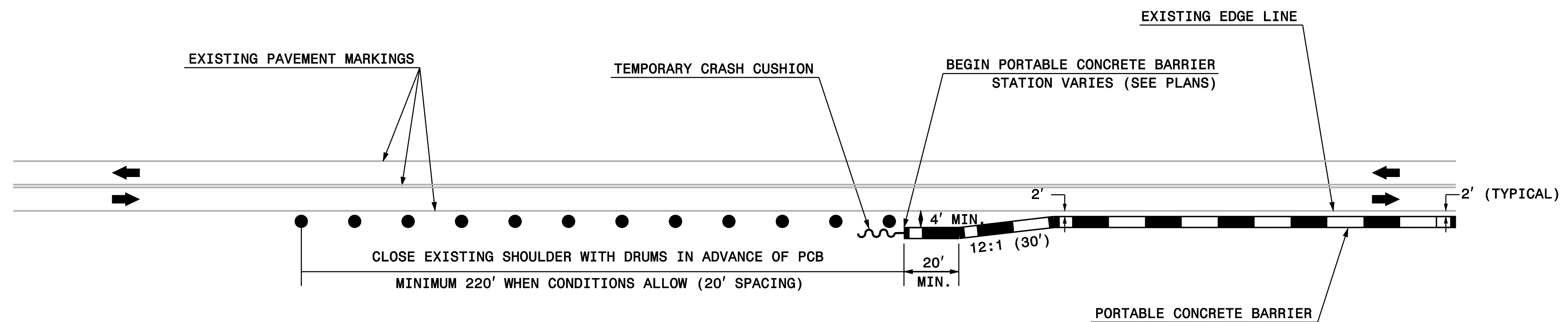


**TRANSPORTATION
OPERATIONS PLAN
GENERAL NOTES**

TYPICAL #1: PORTABLE CONCRETE BARRIER SECTION

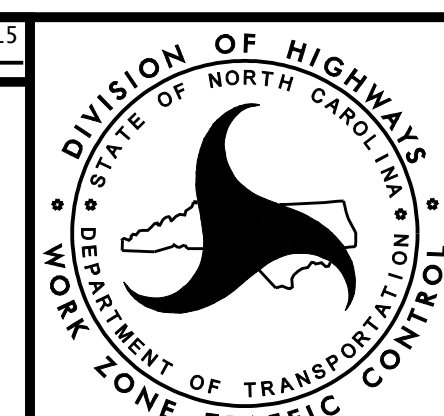


TYPICAL #2: LEADING EDGE LAYOUT FOR PORTABLE CONCRETE BARRIER



PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE
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APPROVED: *[Signature]* DATE: 4/15/2015
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ROADWAY STANDARD
DRAWINGS & LEGEND
PCB TYPICALS

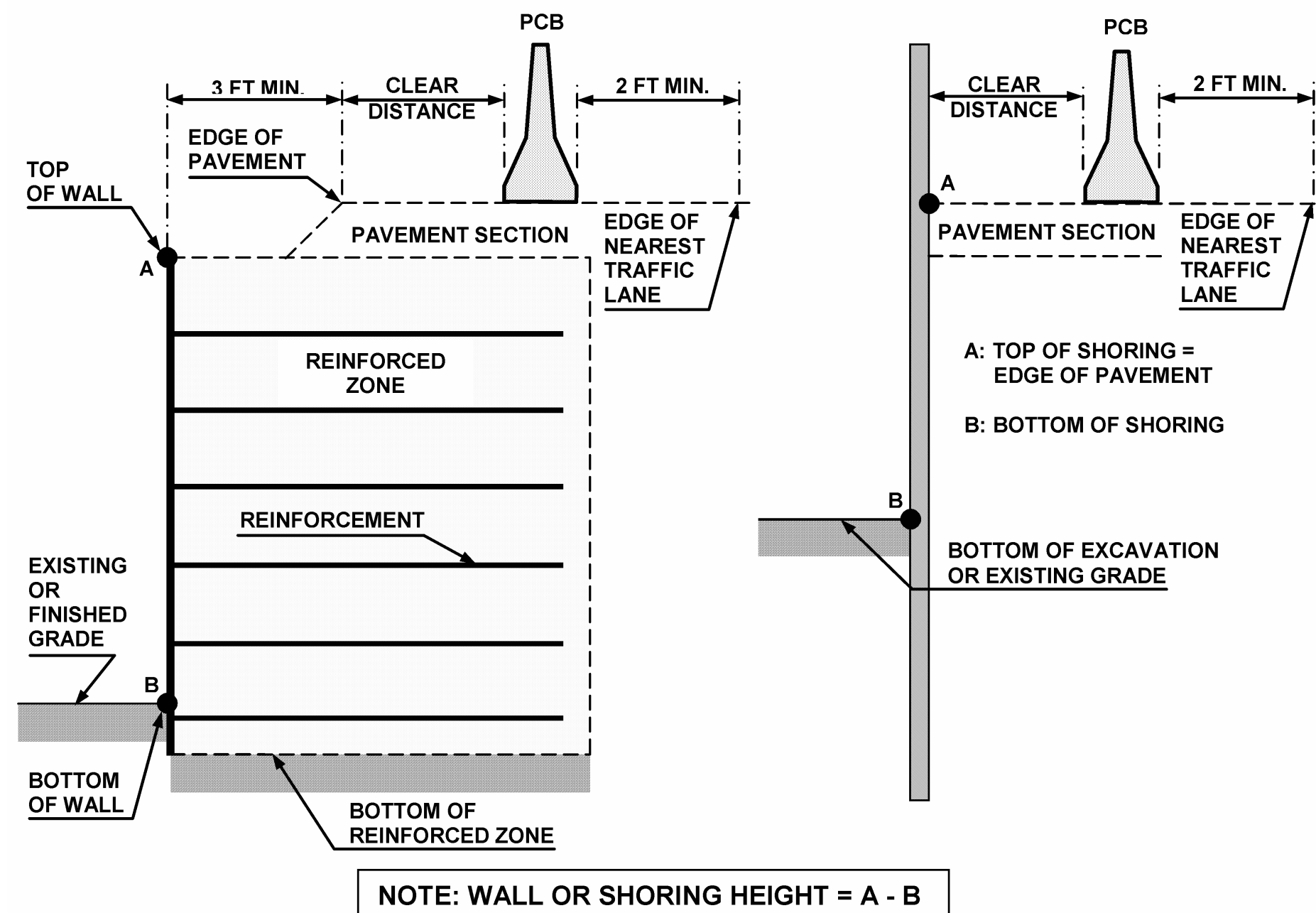


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" PROJECT SPECIAL PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT UNIT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- PCB REQUIREMENTS FOR TEMPORARY WALLS APPLY TO TEMPORARY MECHANICALLY STABILIZED EARTH (MSE) WALLS AND TEMPORARY SOIL NAIL WALLS.
- 8- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS AND OR AS APPROVED BY THE ENGINEER.
- 9- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THESE MINIMUM REQUIRED DISTANCES ARE NOT AVAILABLE, CONTACT THE ENGINEER.
- 10- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS. BARRIER DEFLECTIONS AND RESULTING MINIMUM REQUIRED CLEAR DISTANCES MIGHT VARY SIGNIFICANTLY FOR LARGER HEAVIER VEHICLES, RUNS OF BARRIER LESS THAN 200 FT IN LENGTH AND WET OR DRY PAVEMENT.

MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
		44-50	31	35	41	43	46	49
		50-56	32	36	42	44	47	50
	>56	32	36	42	45	47	51	
	Concrete	<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
		26-32	24	25	27	28	32	35
		32-38	24	26	27	30	33	36
		38-44	25	26	28	30	34	37
		44-50	26	26	28	32	35	37
50-56		26	26	28	32	35	38	
>56	26	27	29	32	36	38		
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
Anchored PCB	Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds					

* See Figure Below

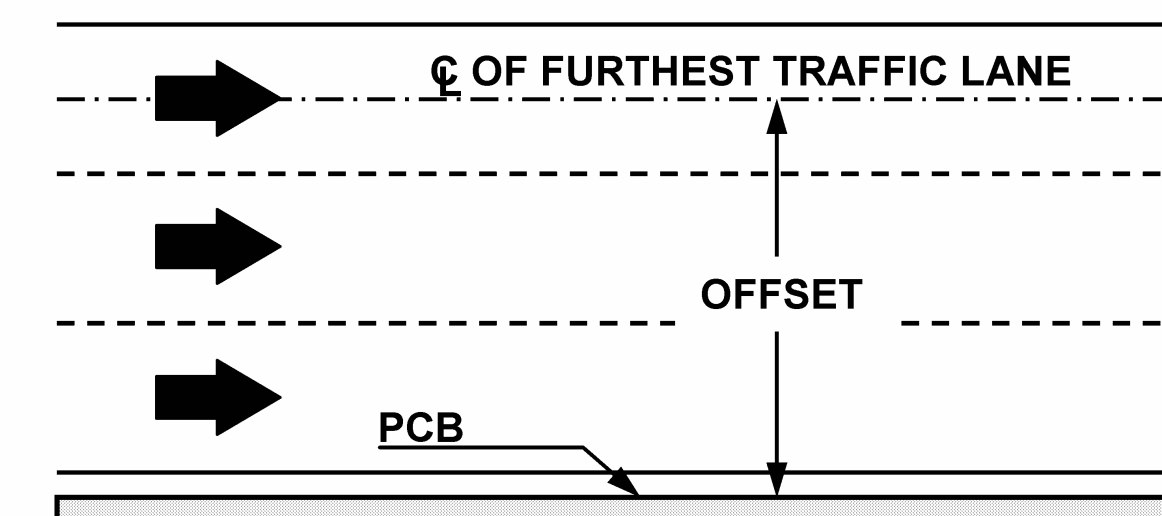


FIGURE B

Shoring Location No. 1

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DESIGN TEMPORARY SHORING FROM STATION 100+40± -L-, 2 FT RIGHT, TO STATION 101+60 ± -L-, 2 FT RIGHT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUND WATER ELEVATION:

UNIT WEIGHT (γ) = 120 LB/CF
 FRICTION ANGLE (ϕ) = 30 DEGREES
 COHESION (c) = 0 LB/SF
 GROUND WATER ELEVATION = 36 FT ±

DO NOT USE CANTILEVER, BRACED AND/OR ANCHORED SHORING FOR TEMPORARY SHORING FROM STATION 100+40± -L-, 2 FT RIGHT, TO STATION 101+60 ± -L-, 2 FT RIGHT.

AT THE CONTRACTOR'S OPTION, USE A STANDARD TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION 100+40± -L-, 2 FT RIGHT, TO STATION 101+60 ± -L-, 2 FT RIGHT. SEE STANDARD DRAWING NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

WHEN BACKFILL FOR BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

Shoring Location No. 2

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

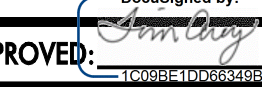
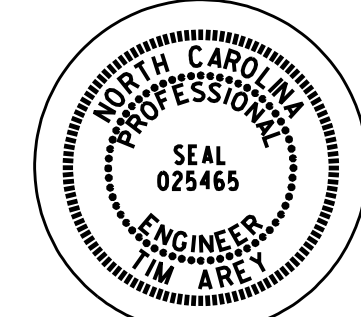

DESIGN TEMPORARY SHORING FROM STATION 102+40± -L-, 2 FT RIGHT, TO STATION 103+60 ± -L-, 2 FT RIGHT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUND WATER ELEVATION:

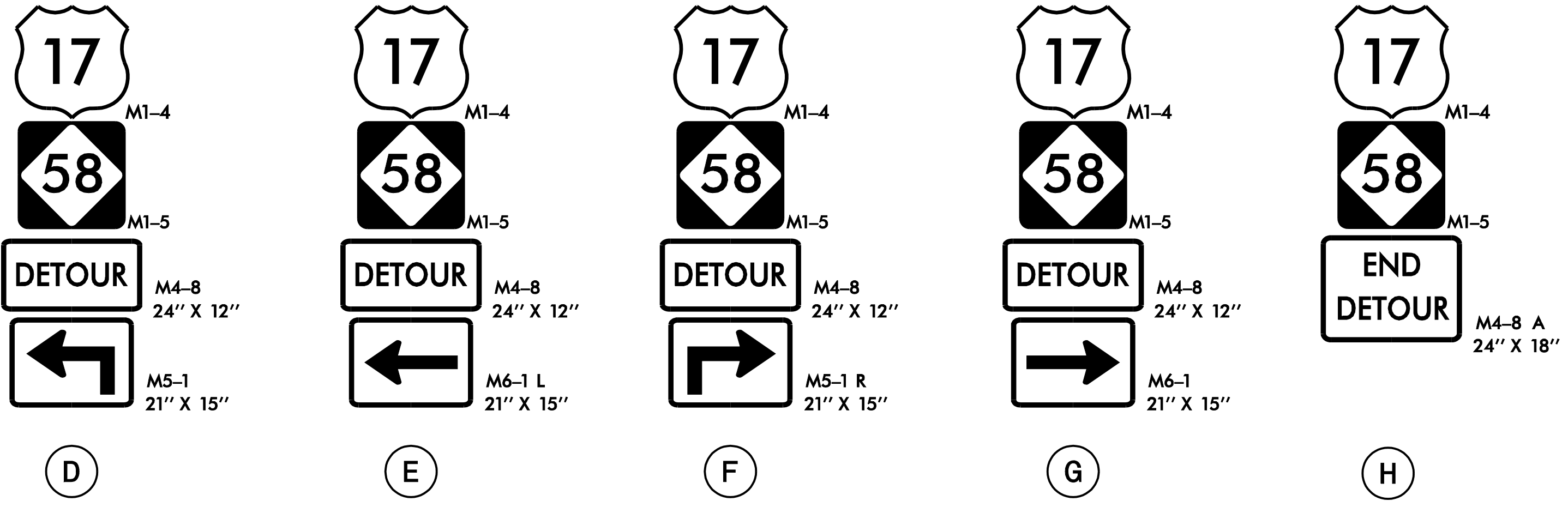
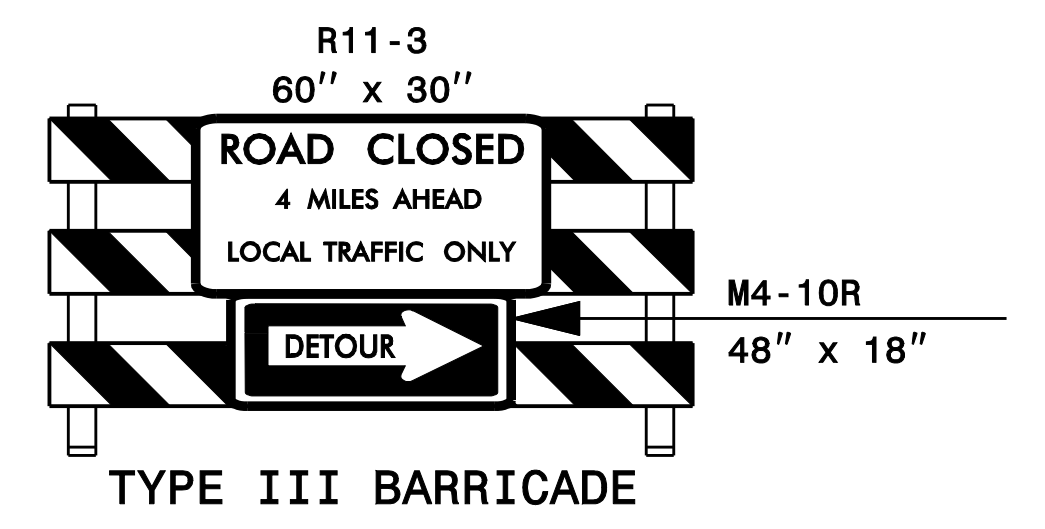
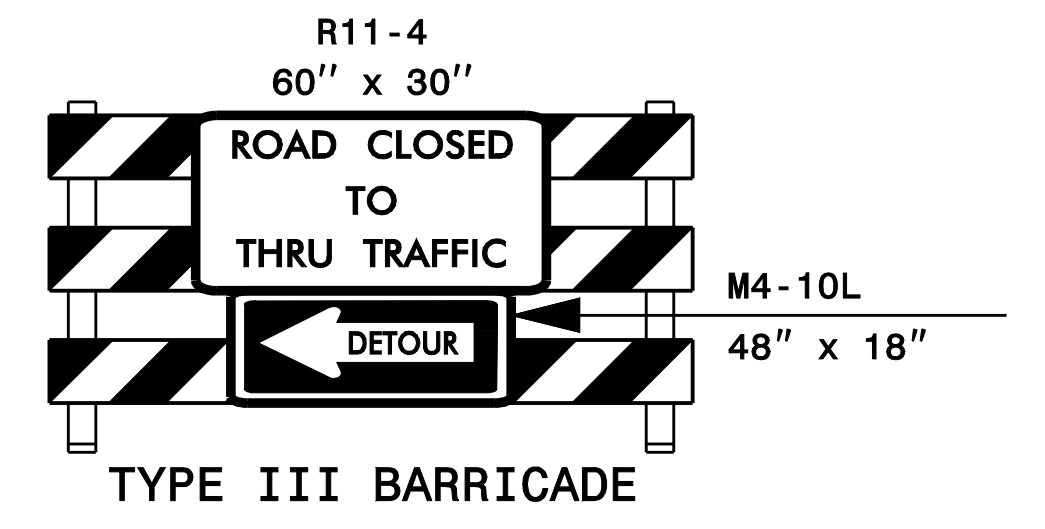
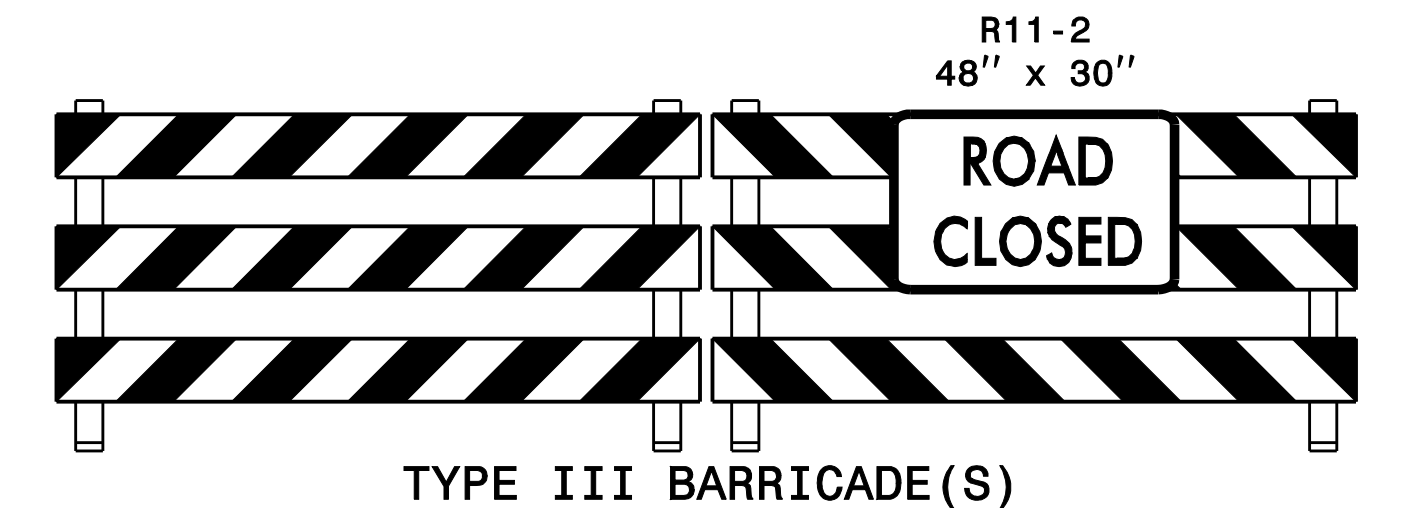
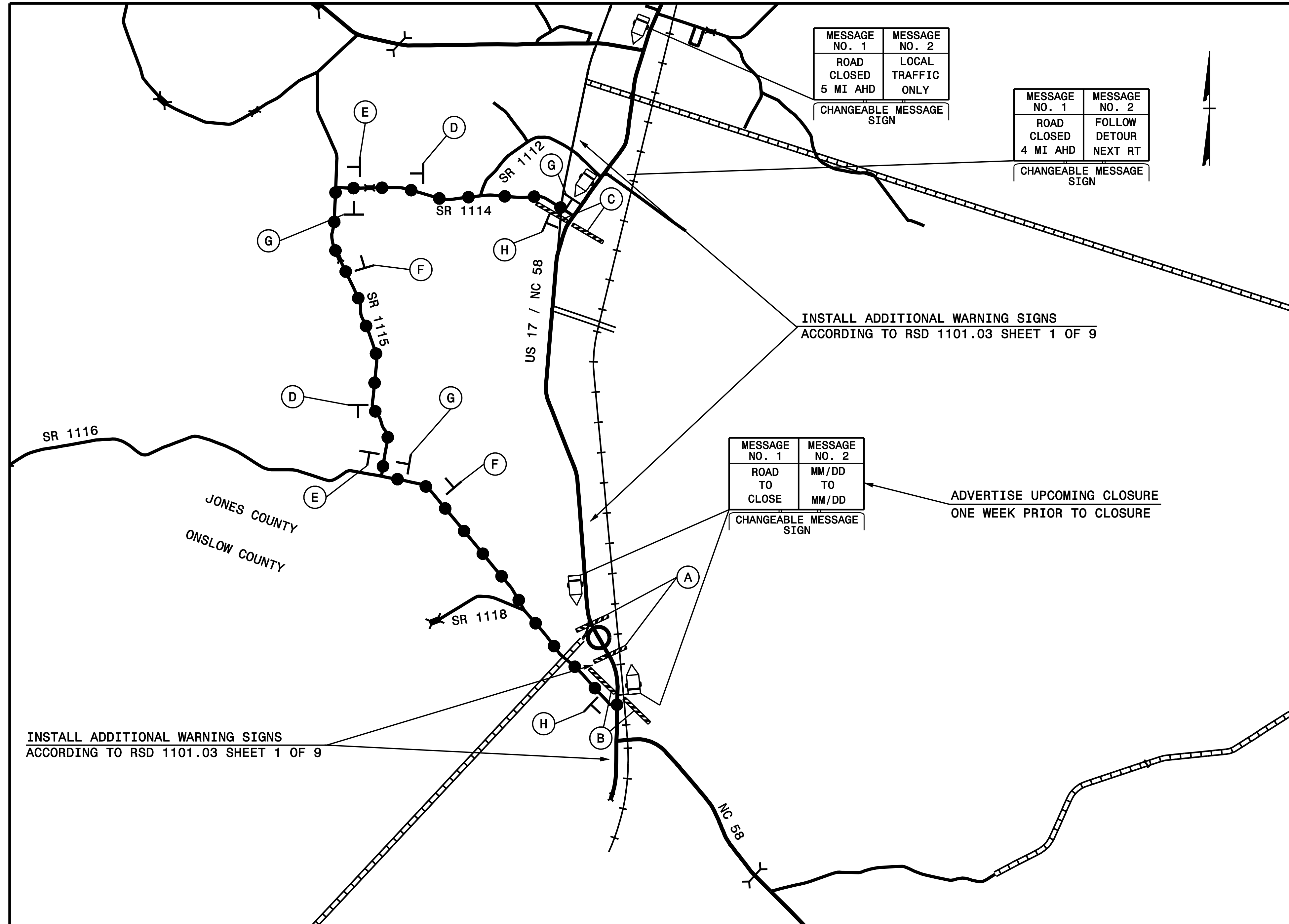
UNIT WEIGHT (γ) = 120 LB/CF
 FRICTION ANGLE (ϕ) = 30 DEGREES
 COHESION (c) = 0 LB/SF
 GROUND WATER ELEVATION = 36 FT ±

DO NOT USE CANTILEVER, BRACED AND/OR ANCHORED SHORING FOR TEMPORARY SHORING FROM STATION 102+40± -L-, 2 FT RIGHT, TO STATION 103+60 ± -L-, 2 FT RIGHT.

AT THE CONTRACTOR'S OPTION, USE A STANDARD TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION 102+40± -L-, 2 FT RIGHT, TO STATION 103+60 ± -L-, 2 FT RIGHT. SEE STANDARD DRAWING NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

WHEN BACKFILL FOR BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

DocuSigned by: APPROVED:  DATE: 4/15/2015 SEAL 		TRANSPORTATION OPERATIONS PLAN TEMPORARY SHORING DATA
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PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE DESIGN GROUP, INC.

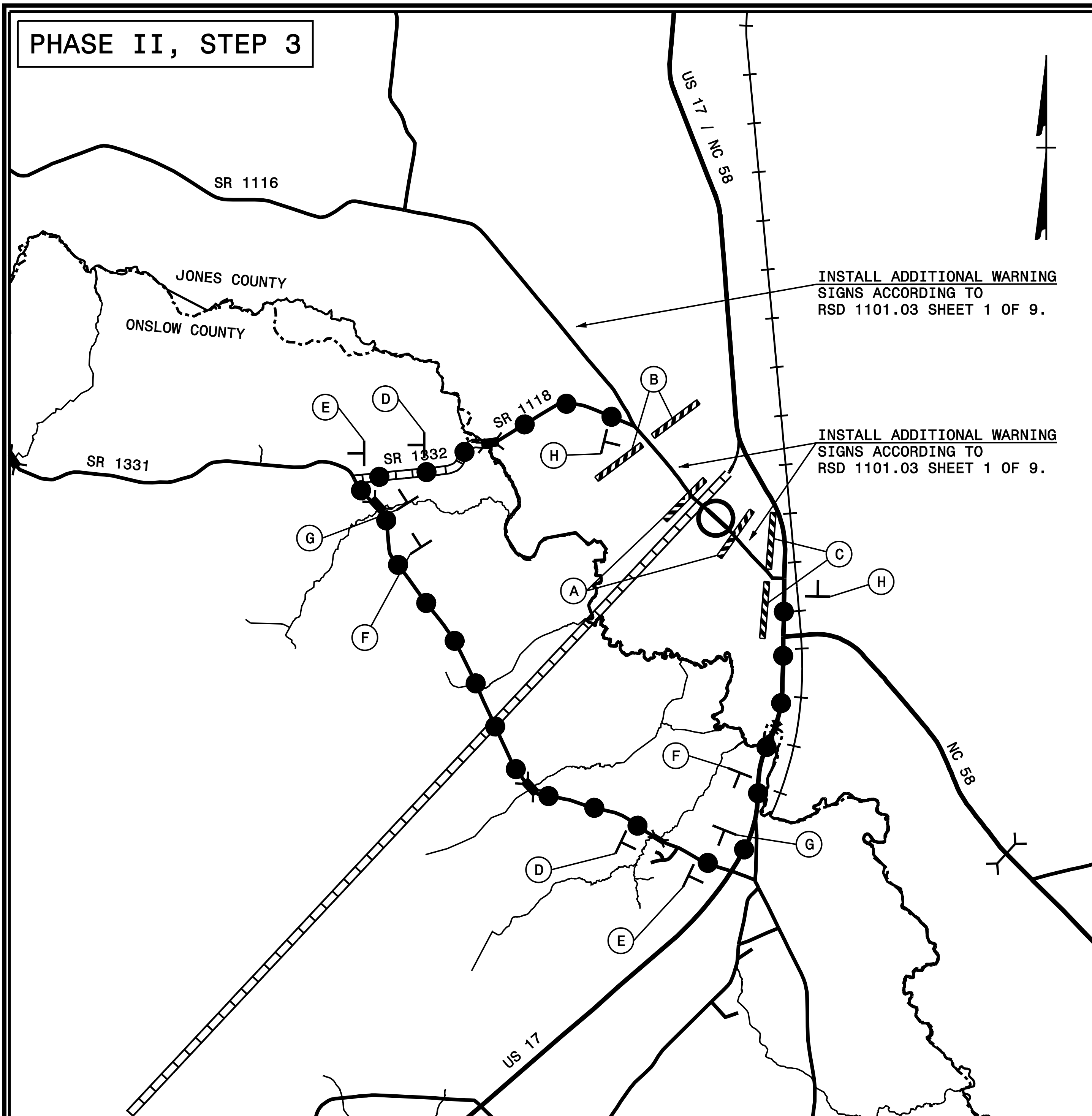
ENGINEERS • CONSULTANTS

APPROVED: DATE: 4/15/2015

SEAL:

TRANSPORTATION MANAGEMENT PLAN
OFF SITE DETOUR PLAN
-LBUS- (US 17)

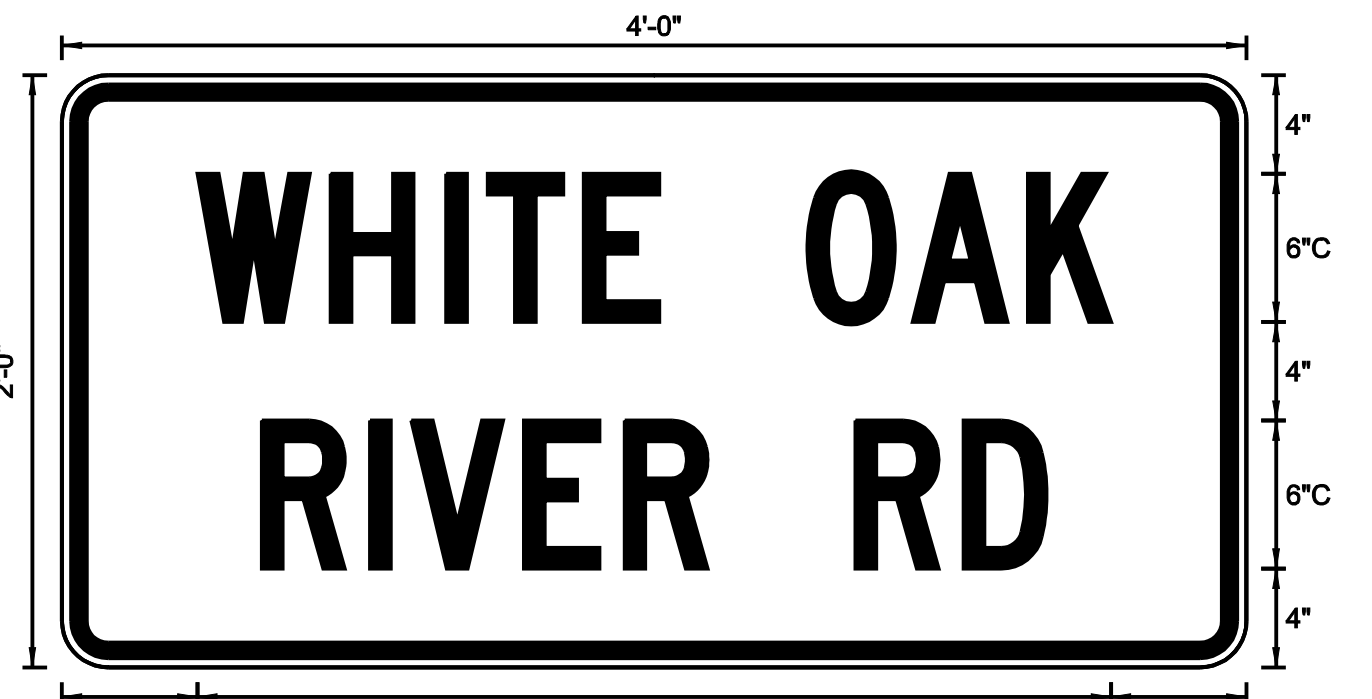
PHASE II, STEP 3



SIGN NUMBER: name
TYPE: STATIONARY
QUANTITY: SEE PLANS
SIGN WIDTH: 4'-0"
HEIGHT: 2'-0"
TOTAL AREA: 8.0 Sq.Ft.
BORDER TYPE: FLUSH
RECESS: 0.38"
WIDTH: 0.63"
RADII: 1.5"
NO. Z BARS:
LENGTH:

SYMBOL	X	Y	WID	HT

DESIGN BY: none
PROJECT ID: R-2514B/C
CHECKED BY:
DIV: 2/3
DATE: 4/15/2015



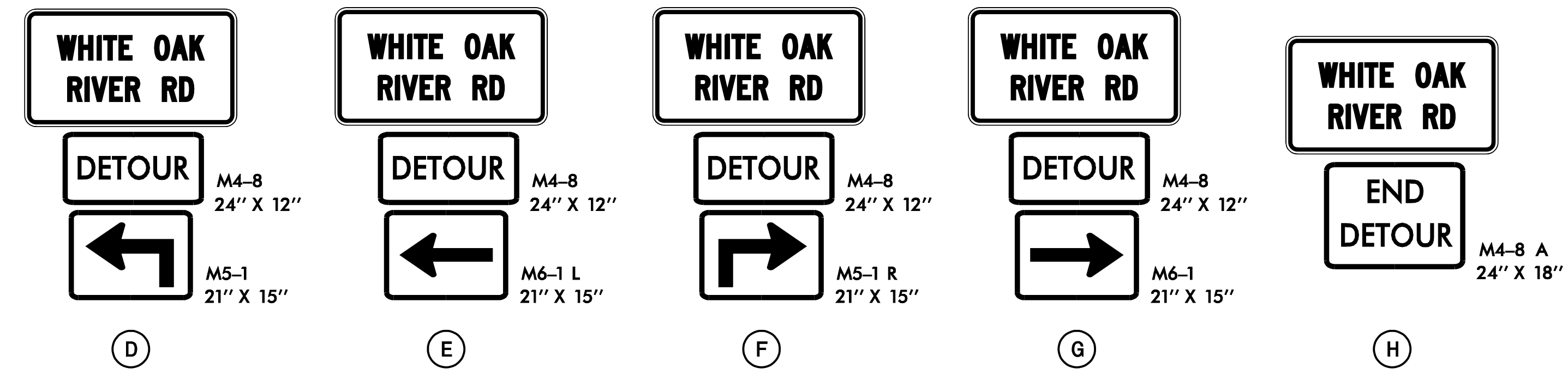
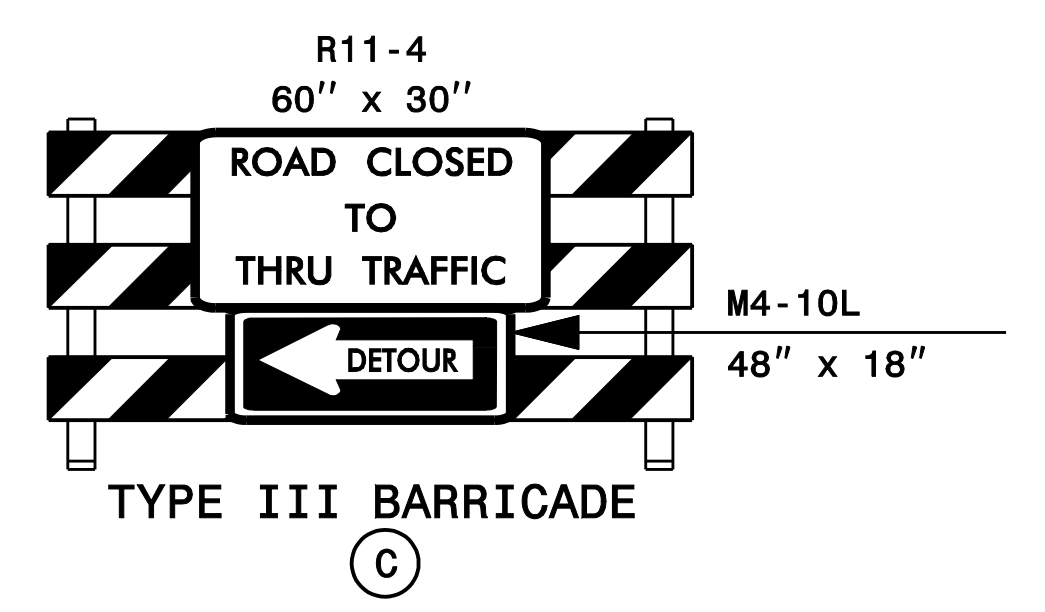
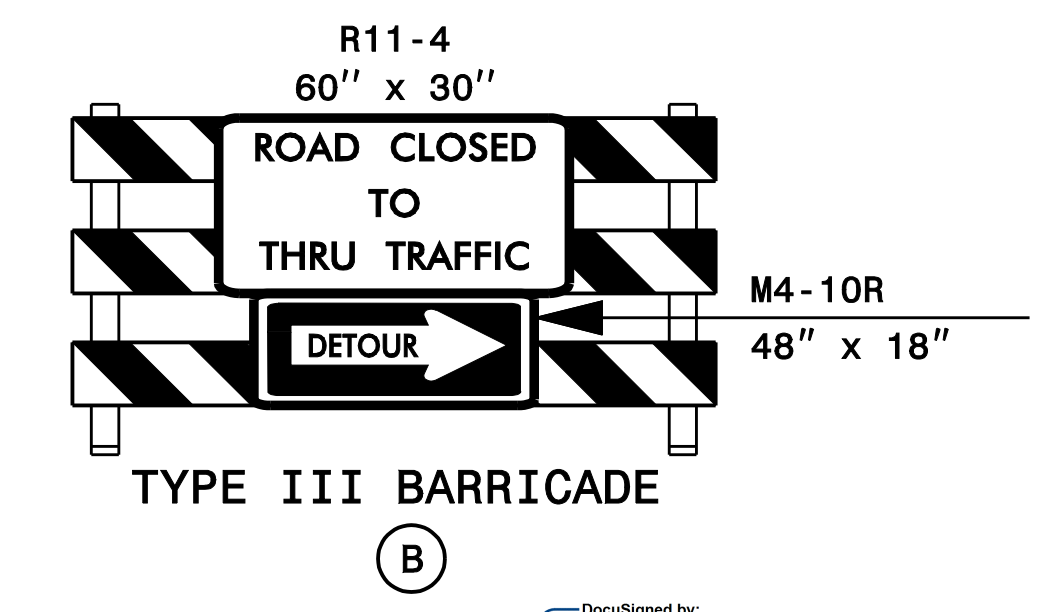
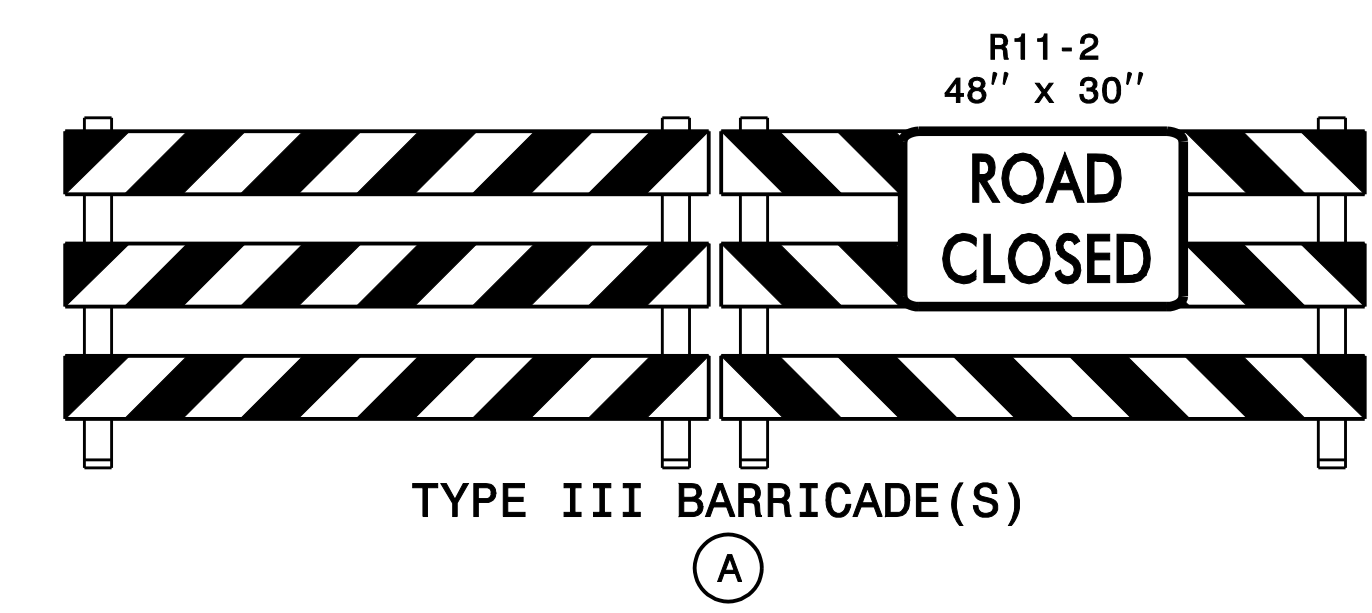
- USE NOTES: 1,2
- Legend and border shall be direct applied black non-reflective sheeting.
 - Background shall be NC Grade B fluorescent orange retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter

Letter	W	H	I	T	E	O	A	K	Series/Size	Text Length
W	5.5	5.4	4.7	1.7	3.9	3.1	6	4.3	C 2000	37
H									C 2000	31.8
I										
T										
E										
O										
A										
K										
R	8.1	4.4	1.7	4.6	4.1	3.4	6	4.4	C 2000	31.8
I										
V										
E										
R										
D										

FILENAME: detour sign designs
NORTH CAROLINA D.O.T. SIGN DETAIL



PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE DESIGN GROUP, INC.

ENGINEERS • CONSULTANTS

APPROVED: [Signature] DATE: 4/15/2015

SEAL: [Professional Engineer Seal]

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

TRANSPORTATION MANAGEMENT PLAN
OFF SITE DETOUR PLAN
-Y3- (WHITE OAK RIVER RD)

PROJECT PHASING

PHASE I

- STEP 1: INSTALL WORK ZONE ADVANCE WARNING SIGNS ACCORDING TO ROADWAY STANDARD DRAWING NO. 1101.01.
- STEP 2: BEGIN CONSTRUCTION ON THE FOLLOWING UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE: (USE ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15 AS DEEMED NECESSARY BY THE CONDITIONS DESCRIBED IN GENERAL NOTES F AND G ON SHEET TMP-1B)
- -L-: IN THE LOCATIONS SHOWN ON SHEETS TMP-4 THRU TMP-19 INCLUDING THE PROPOSED NORTHBOUND AND SOUTHBOUND -L- BRIDGES AT STA. 148+00+/- AND STA. 173+00+/- AND JUST THE PROPOSED NORTHBOUND -L- BRIDGE AT STA. 102+00+/-.
 - FOR THE -L- BRIDGE AT STA. 102+00+/-:
 - INSTALL TEMPORARY SHORING AS THE PROPOSED FILL HEIGHT INCREASES AS SHOWN ON SHEET TMP-9.
 - FOR -L-(RIGHT) CONSTRUCTION (STA. 78+00+/- TO 209+30+/-):
 - INSTALL TEMPORARY GUARDRAIL ALONG EXISTING NORTHBOUND -L- IN THE LOCATION SHOWN ON SHEET TMP-9 PRIOR TO BEGINNING MAJOR EARTHWORK OPERATIONS IN THIS LOCATION. STAGE CONSTRUCT THE PROPOSED STORMWATER INSTALLATIONS IN THIS AREA AS SHOWN ON SHEETS TMP-9 THRU TMP-11.
 - -LBUS-: IN THE LOCATIONS SHOWN ON SHEETS TMP-8 AND TMP-18.
 - -Y1-(US 17): IN THE LOCATIONS SHOWN ON SHEETS TMP-4 AND TMP-17.
 - -DET1-: IN THE LOCATIONS SHOWN ON SHEETS TMP-4 AND TMP-15.
 - -DET2-: IN THE LOCATIONS SHOWN ON SHEETS TMP-4 AND TMP-16.
 - -DET3-: IN THE LOCATIONS SHOWN ON SHEETS TMP-8 AND TMP-18.
 - -DET4-: IN THE LOCATIONS SHOWN ON SHEETS TMP-11 AND TMP-19.
 - -DET5-: IN THE LOCATIONS SHOWN ON SHEETS TMP-11 AND TMP-19.
 - -Y2A-, -Y2B-, -Y2C- AND -Y2D-: IN THE LOCATIONS SHOWN ON SHEETS TMP-5, TMP-6, AND TMP-14.
 - -Y3A-, -Y3B-, -Y3C-, -Y3D-, -Y3E-: IN THE LOCATIONS SHOWN ON SHEET TMP-7.
 - TEMPORARY PAVEMENT LOCATIONS:
 - STA. 56+20+/- -L- TO STA. 58+50+/- -L- AS SHOWN ON SHEETS TMP-4 AND TMP-17.
 - STA. 13+90+/- -L- TO STA. 16+40+/- -L- AS SHOWN ON SHEETS TMP-8 AND TMP-18.

INTERMEDIATE CONTRACT TIME: COMPLETE THE WORK REQUIRED OF PHASE I, STEP 3 IN 60 CONSECUTIVE HOURS BEGINNING ON A FRIDAY AT 6:00PM AND COMPLETING BY THE FOLLOWING MONDAY AT 6:00AM. THE 60 CONSECUTIVE HOUR PERIOD SHALL NOT OCCUR AT ANY TIME BETWEEN THE MONTHS OF MAY TO SEPTEMBER. (SEE SPECIAL PROVISIONS)

- STEP 3: CLOSE -LBUS- (US 17) TO THRU TRAFFIC, AND PLACE -LBUS- (US 17) TRAFFIC ON THE OFF SITE DETOUR SHOWN ON SHEET TMP-2B. INSTALL PROPOSED 48" RCP AT STA. 21+00+/- -LBUS- (US 17) AS SHOWN ON SHEET TMP-8 AND REOPEN -LBUS- (US 17) TO TRAFFIC.

INTERMEDIATE CONTRACT TIME: COMPLETE THE WORK REQUIRED OF PHASE I, STEP 4 IN 280 CONSECUTIVE CALENDAR DAYS. (SEE SPECIAL PROVISIONS)

- STEP 4: CLOSE -Y2- (WHITE OAK RIVER ROAD) TO THRU TRAFFIC, AND PLACE -Y2- (WHITE OAK RIVER ROAD) ON THE OFF SITE DETOUR SHOWN ON SHEET TMP-2C. CONSTRUCT PROPOSED -Y2- (WHITE OAK RIVER ROAD) INCLUDING THE PROPOSED -Y2- BRIDGE OVER -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE IN THE LOCATIONS SHOWN ON SHEETS TMP-5 AND TMP-6. COMPLETE CONSTRUCTION ON -Y2A-, -Y2B-, -Y2C- AND -Y2D- UP THRU ONE LAYER OF SURFACE COURSE. INSTALL TEMPORARY PAINT PAVEMENT MARKINGS ON -Y2- (WHITE OAK RIVER ROAD) AS SHOWN ON SHEET TMP-21 AND OPEN -Y2- (WHITE OAK RIVER ROAD), -Y2A-, -Y2B-, -Y2C- AND -Y2D- UP TO THE FINAL TRAFFIC PATTERN.

PHASE II

- STEP 1: COMPLETE THE ROADWAY WORK (FROM PHASE I, STEP 2) REQUIRED TO SHIFT -L- TRAFFIC INTO THE TRAFFIC PATTERN SHOWN ON SHEET TMP-20.
- USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15, AS NEEDED, PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS ON -L-, -DET1-, AND -DET2- IN THE LOCATIONS SHOWN ON SHEETS TMP-28 THRU TMP-30 AND SHIFT -L- TRAFFIC INTO THE PATTERN SHOWN ON SHEETS TMP-28 THRU TMP-30. BEGIN CONSTRUCTION ON -L- IN THE LOCATIONS SHOWN ON SHEETS TMP-28 THRU TMP-30 UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.
- STEP 2: COMPLETE THE ROADWAY WORK (FROM PHASE I, STEP 2) REQUIRED TO SHIFT -L- TRAFFIC INTO THE TRAFFIC PATTERN SHOWN ON SHEETS TMP-23 THRU TMP-26. INSTALL PORTABLE CONCRETE BARRIER AND TEMPORARY CRASH CUSHIONS ALONG -L- (RIGHT) IN THE LOCATIONS SHOWN ON SHEETS TMP-24 AND TMP-25.
- USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15, AS NEEDED, PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS ON -L-, -DET3-, AND -DET4- IN THE LOCATIONS SHOWN ON SHEETS TMP-23 THRU TMP-26 AND TMP-31 AND TMP-32 AND SHIFT -L- TRAFFIC INTO THE PATTERN SHOWN ON SHEETS TMP-23 THRU TMP-26 AND TMP-31 AND TMP-32. BEGIN CONSTRUCTION ON -L- AND -LBUS- IN THE LOCATIONS SHOWN ON SHEETS TMP-23 THRU TMP-26 AND TMP-31 AND TMP-32 UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.

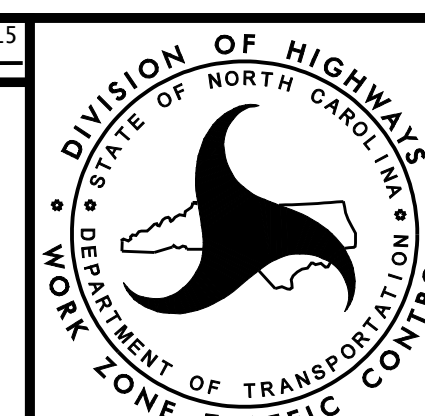
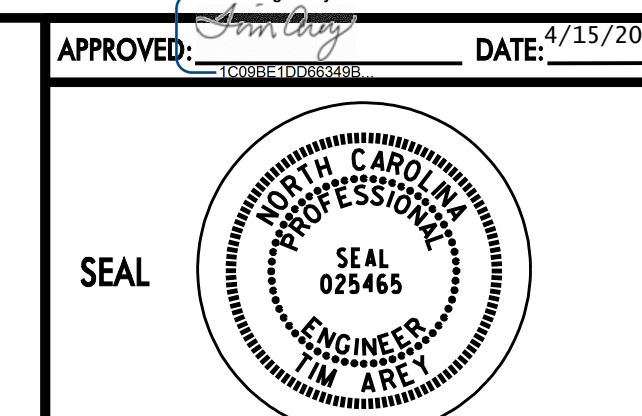
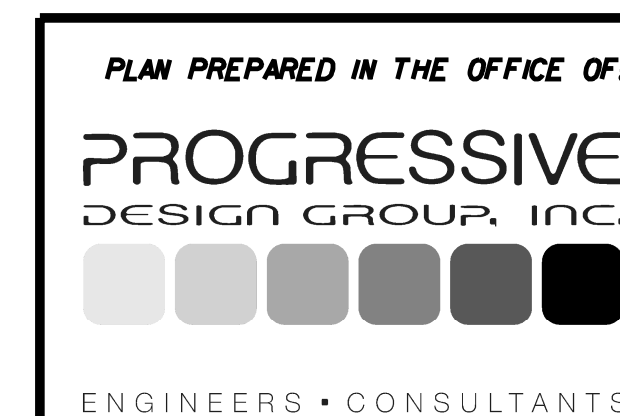
INTERMEDIATE CONTRACT TIME: COMPLETE THE WORK REQUIRED OF PHASE II, STEP 3 IN 280 CONSECUTIVE CALENDAR DAYS. NOTE THAT THE WORK REQUIRED OF THIS INTERMEDIATE CONTRACT TIME CAN NOT BEGIN UNTIL -Y2- (WHITE OAK RIVER ROAD) HAS BEEN RE-OPENED TO TRAFFIC AS DESCRIBED IN PHASE I, STEP 4. (SEE SPECIAL PROVISIONS)

- STEP 3: CLOSE -Y3- (WHITE OAK RIVER ROAD) TO THRU TRAFFIC, AND PLACE -Y3- (WHITE OAK RIVER ROAD) TRAFFIC ON THE OFF SITE DETOUR SHOWN ON SHEET TMP-2D. CONSTRUCT PROPOSED -Y3- (WHITE OAK RIVER ROAD) INCLUDING THE PROPOSED -Y2- BRIDGE OVER -L-, -Y3A-, -Y3B-, -Y3C-, AND -Y3D- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE IN THE LOCATIONS SHOWN ON SHEET TMP-22. INSTALL TEMPORARY PAINT PAVEMENT MARKINGS ON -Y3- (WHITE OAK RIVER ROAD) AND OPEN -Y3- (WHITE OAK RIVER ROAD), -Y3A-, -Y3B-, -Y3C-, AND -Y3D- UP TO THE FINAL TRAFFIC PATTERN.

INTERMEDIATE CONTRACT TIME: COMPLETE THE WORK REQUIRED OF PHASE II, STEP 4 IN 60 CONSECUTIVE CALENDAR DAYS. (SEE SPECIAL PROVISIONS)

- STEP 4: CLOSE -Y1- (LEE'S CHAPEL ROAD) TO THRU TRAFFIC, AND PLACE -Y1- (LEE'S CHAPEL ROAD) TRAFFIC ON THE OFF SITE DETOUR SHOWN ON SHEET TMP-2E. CONSTRUCT PROPOSED -Y1-/Y1A- (LEE'S CHAPEL ROAD) AND THE STUB PORTION OF PROPOSED -YA- SHOWN ON SHEET TMP-27 UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE IN THE LOCATIONS SHOWN ON SHEET TMP-27. CONSTRUCT A TEMPORARY PAVEMENT TIE IN FROM EXISTING -Y1- TO PROPOSED -Y1A- IN THE LOCATION SHOWN ON SHEET TMP-27. INSTALL TEMPORARY PAINT PAVEMENT MARKINGS ON -L- AND -Y1- AS SHOWN ON SHEET TMP-34 ACCORDING TO THE PATTERNS LAID OUT IN THE FINAL PAVEMENT MARKING PLAN UNLESS OTHERWISE LABELED ON SHEET TMP-34. INSTALL THE PROPOSED SIGNS SHOWN ON SHEET TMP-34 ALONG -L- AND -Y1-. SEE THE PROPOSED SIGNING PLANS FOR MORE DETAILS. OPEN -L- AND -Y1- (LEE'S CHAPEL ROAD) TO THE TRAFFIC PATTERN SHOWN ON SHEET TMP-34.

- STEP 5: CONSTRUCT THE PROPOSED CUL-DE-SACS ALONG EXISTING -Y2- (RIGG'S TOWN ROAD) AS SHOWN IN THE ROADWAY DESIGN PLANS AND PERMANENTLY CLOSE -Y2- (RIGG'S TOWN ROAD) ON BOTH SIDES OF -L-. CONSTRUCT THE REMAINING SECTIONS OF -L- IN THE VICINITY OF -Y2- (RIGG'S TOWN ROAD).



TRANSPORTATION
OPERATIONS PLAN
TRAFFIC CONTROL PHASING

PROJECT PHASING

PHASE III

STEP 1: COMPLETE CONSTRUCTION UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE ON ALL ROADWAYS REQUIRED TO SHIFT TRAFFIC INTO THE PATTERN SHOWN ON SHEETS TMP-33 THRU 46.

AWAY FROM TRAFFIC, PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS AND ALL TRAFFIC CONTROL DEVICES THAT WILL NOT CONFLICT WITH EXISTING TRAFFIC PATTERNS ON -L-, -Y1- (US 17), -LBUS- (US 17), AND -DET5- AS SHOWN ON SHEETS TMP-33 THRU 46. ENSURE THAT THE R-2514D CONTRACTOR HAS COMPLETED THE PAVEMENT MARKING REVISIONS REQUIRED WITHIN THE R-2514D PROJECT LIMITS FOR THE TRAFFIC PATTERN SHOWN ON SHEETS TMP-44 THRU TMP-46.

STEP 3:

AREA 2: COMPLETE THE FOLLOWING:

- A) REMOVE THE TRAFFIC CONTROL DEVICES ON NORTHBOUND -L- THAT CURRENTLY REQUIRE NORTHBOUND -L- TRAFFIC TO U-TURN AT STA. 243+00 AND OPEN NORTHBOUND -L- FROM -Y1- (LEE'S CHAPEL ROAD) TO THE NC 58 INTERCHANGE INTO THE PATTERN SHOWN ON SHEETS TMP-43 THRU TMP-47.
- COMPLETE THE FOLLOWING: USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15, AND ALTERNATING LANE CLOSURES:
- B) PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS FOR ONLY SOUTHBOUND -L- TRAFFIC IN THE TIE IN AREAS OF -LBUS- AS SHOWN ON SHEET TMP-41 AND IN THE TIE IN AREAS OF -Y1A- (LEE'S CHAPEL ROAD) TO EXISTING US 17 AS SHOWN ON SHEET TMP-43. OPEN UP PROPOSED SOUTHBOUND -L- FROM THE -L-/-Y1- INTERSECTION (PREVIOUSLY OPENED IN AREA 1A) ALL THE WAY TO THE US 58 INTERCHANGE AND SWITCH SOUTHBOUND -L- TRAFFIC ONTO -Y1A- (LEE'S CHAPEL ROAD) AND ONTO -LBUS-. ANY VEHICLES HEADING SOUTHBOUND ON -DET3- (SHEET TMP-41) SHOULD BE STOPPED AT THE -LBUS-/-DET3- INTERSECTION AND PROVIDED ASSISTANCE GETTING ONTO -LBUS-.
- C) PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS FOR NORTHBOUND -L- TRAFFIC IN THE TIE IN AREAS OF -LBUS- AS SHOWN ON SHEET TMP-41 AND IN THE TIE IN AREAS OF -Y1A- (LEE'S CHAPEL ROAD) TO -Y- (US 17) AS SHOWN ON SHEET TMP-43. OPEN UP PROPOSED NORTHBOUND -L- FROM THE -L-/-Y1- INTERSECTION (PREVIOUSLY OPENED IN AREA 1B) ALL THE WAY TO THE US 58 INTERCHANGE AND SWITCH NORTHBOUND -L- TRAFFIC ONTO -Y1A- (LEE'S CHAPEL ROAD) AND ONTO -LBUS-. ANY VEHICLES HEADING NORTHBOUND ON -YA- (SHEET TMP-43) SHOULD BE STOPPED AT THE -Y1A-/-YA- INTERSECTION AND PROVIDED ASSISTANCE GETTING ONTO THE -Y1A- ROADWAY.
- D) PLACE TYPE III BARRICADES AND DRUMS TO CLOSE ACCESS TO ALL THE PHASE II ROADWAYS THAT ARE NOW CLOSED AND COMPLETE THE INSTALLATION OF ANY REMAINING PAVEMENT MARKINGS AND THE OBLITERATION OF ANY CONFLICTING PAVEMENT MARKINGS.

STEP 4: CONSTRUCT THE REMAINING SECTIONS OF -L- AND -YA- (US 17) IN THE LOCATIONS SHOWN ON SHEETS TMP-35 THRU TMP-42 UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.

PHASE IV

STEP 1: USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 AND 3 OF 15, PLACE THE FINAL LAYER OF SURFACE COURSE, FINAL PAVEMENT MARKINGS AND MARKERS ON ALL ROADWAYS WITHIN THE PROJECT LIMITS AND OPEN ALL ROADWAYS TO THE FINAL TRAFFIC PATTERN. LEAVE THE TRAFFIC CONTROL DEVICES IN PLACE THAT ARE REQUIRED FOR THE -L- NORTHBOUND "ALL TRAFFIC EXIT" PATTERN THAT WAS PUT IN PLACE IN PHASE III, STEP 3. NOTE: WHEN INSTALLING FINAL SIGNS, COVER THE 70mph SPEED LIMIT SIGNS UNTIL PROJECT R-2519D IS OPENED TO THE FINAL TRAFFIC PATTERN AS DIRECTED BY THE ENGINEER.

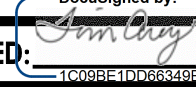

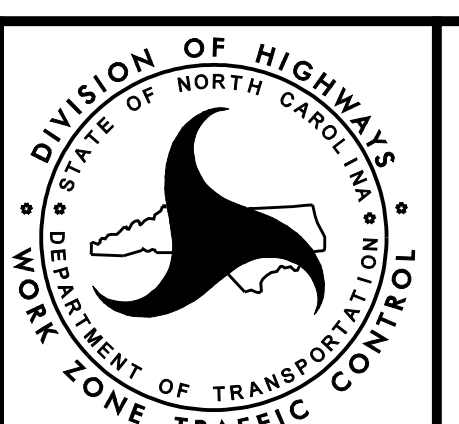
STEP 2: REMOVE ALL OTHER TRAFFIC CONTROL DEVICES. (SEE SPECIAL PROVISIONS)

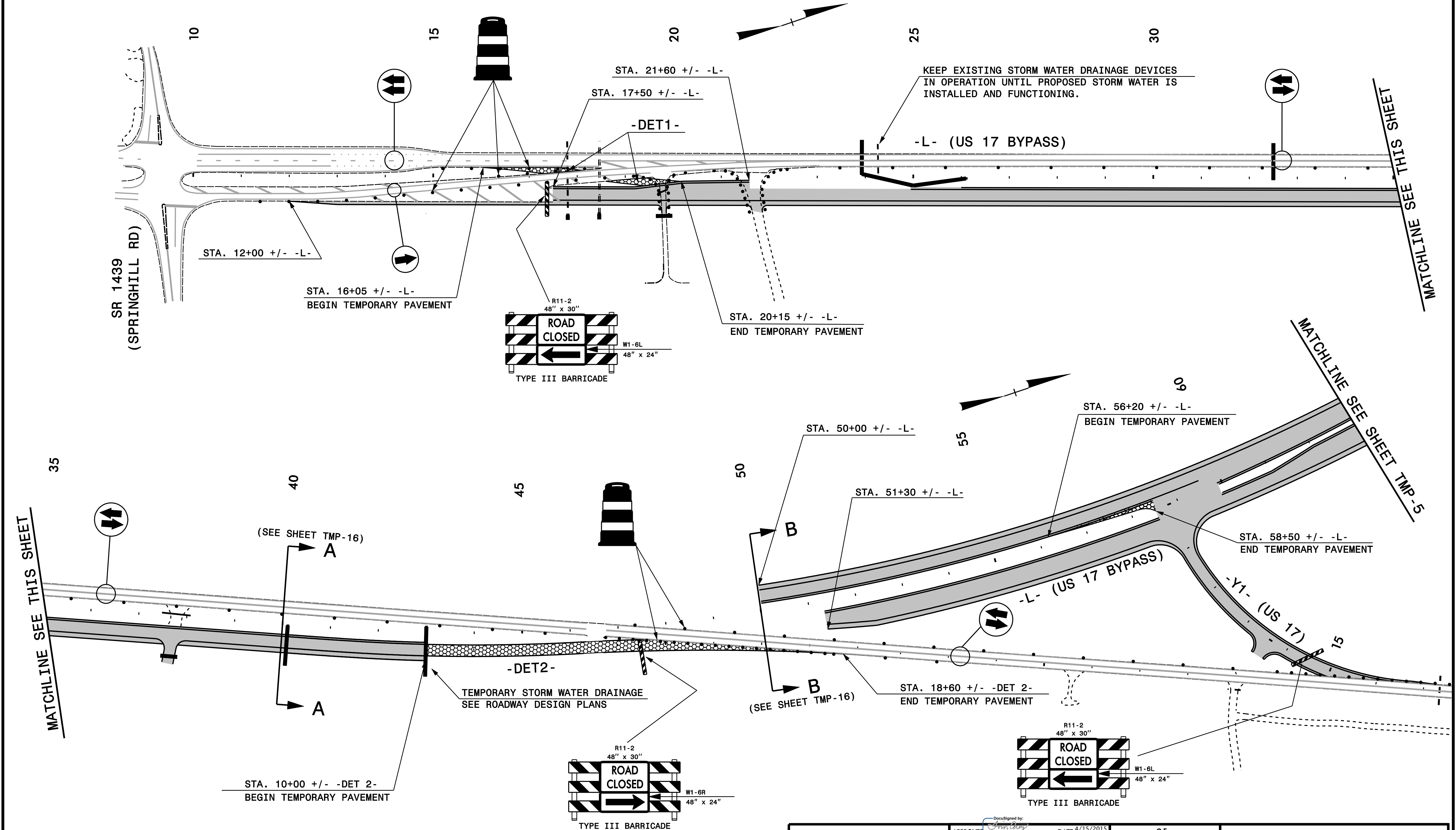
NOTES
<p>DESCRIPTION AND REQUIREMENTS OF THE PHASE III MAIN LINE TRAFFIC SHIFT DETAILED IN PHASE III STEPS 2 AND 3.</p> <p>DESCRIPTION:</p> <p>THE PHASE III MAIN LINE TRAFFIC SHIFT IS SHOWN ON SHEETS TMP-35 THRU TMP-46. THIS TRAFFIC SHIFT PLACES THE ENTIRE -L- LINE FROM SR 1439 (SPRINGHILL RD) TO THE PROPOSED NC 58 INTERCHANGE INTO A NEW TRAFFIC PATTERN. THE -L- LINE LIMITS OF THIS TRAFFIC SHIFT IS BROKEN DOWN INTO TWO MAIN AREAS AS FOLLOWS:</p> <p>AREA 1: FROM SR 1439 (SPRINGHILL RD) TO -Y1- (US 17) AS SHOWN ON SHEETS TMP-38 THRU TMP-40.</p> <p>AREA 2: FROM -LBUS- (US 17) TO THE PROPOSED NC 58 INTERCHANGE AS SHOWN ON SHEETS TMP-41 THRU TMP-48 INCLUDING THE -Y1- (LEE'S CHAPEL ROAD) TIE IN TO EXISTING -L- SHOWN ON SHEET TMP-43.</p> <p>REQUIREMENTS:</p> <ol style="list-style-type: none"> 1) TWO WEEK ADVANCE NOTIFICATION OF THE NEW TRAFFIC PATTERN DISTRIBUTED TO ALL RESIDENCES WITH DRIVEWAY ACCESS TO THE LEFT OF -L- FROM STA. 25+00+/- -L- TO STA. 226+00+/- -L-. AT A MINIMUM, THE ADVANCE NOTIFICATION SHOULD GIVE PROJECTED DATES OF THE TRAFFIC SWITCH WITH A DESCRIPTION/DRAWING, AS DIRECTED BY THE ENGINEER, THAT EXPLICITLY INDICATES A REQUIREMENT TO STOP AND LOOK BEFORE ENTERING THE ROADWAY (LEFT SIDE -L-). 2) UTILIZATION OF POLICE VEHICLES AND OFFICERS. POLICE VEHICLES WITH OFFICER GUIDANCE ASSISTANCE SHALL BE UTILIZED AT THE -L-/-Y1- INTERSECTION (SHEET TMP-36) AND AT THE -L-/-LBUS- INTERSECTION (SHEET TMP-37) UNTIL THE TRAFFIC SHIFT DESCRIBED IN PHASE III, STEPS 2 AND 3 IS COMPLETED. 3) COMPLETE THE TRAFFIC SHIFT DESCRIBED IN PHASE III, STEPS 2 AND 3 IN 60 CONSECUTIVE HOURS BEGINNING ON A FRIDAY AT 6:00PM AND COMPLETING BY THE FOLLOWING MONDAY AT 6:00AM. 4) COORDINATE THE WORK REQUIRED OF PHASE III, STEPS 2 AND 3 WITH THE R-2514D CONTRACTOR.

STEP 2:

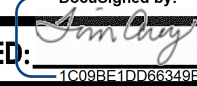

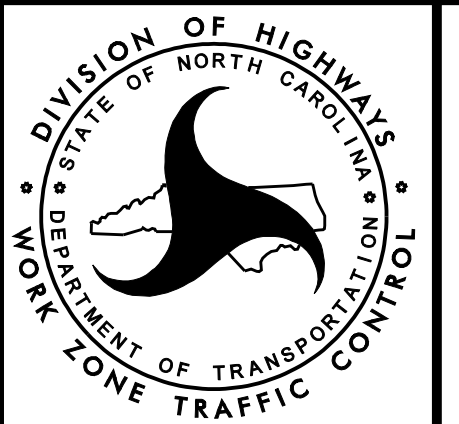
AREA 1: COMPLETE THE FOLLOWING USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15, AND ALTERNATING LANE CLOSURES:

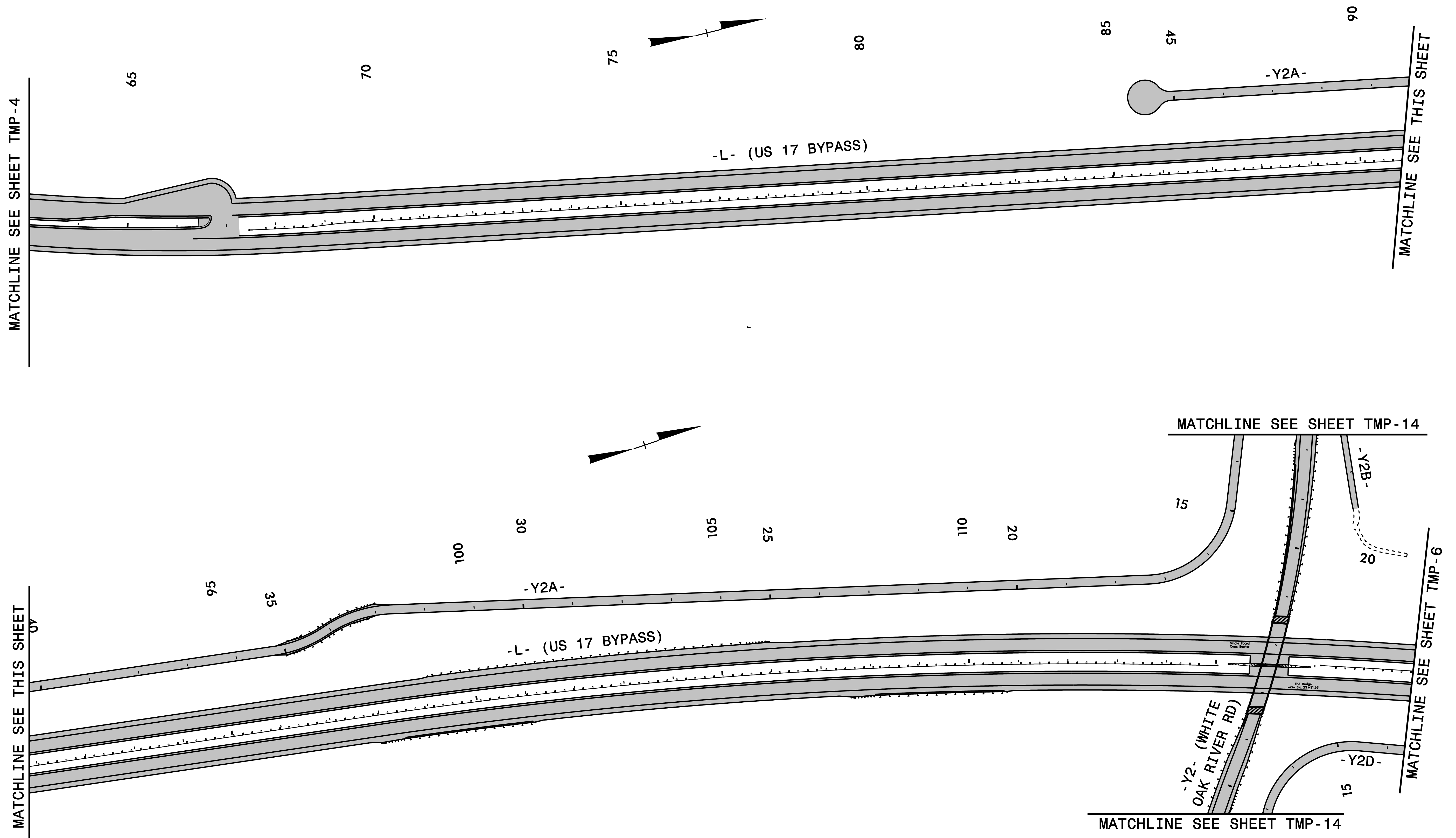
- A) PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS FOR ONLY SOUTHBOUND US 17 TRAFFIC IN THE TIE IN AREAS OF -L- AS SHOWN ON SHEET TMP-38 AND IN THE TIE IN AREAS OF -L- TO -Y1- (US 17) AS SHOWN ON SHEET TMP-40. SHIFT SOUTHBOUND -L- TRAFFIC INTO THE PATTERN SHOWN ON SHEETS TMP-38 THRU TMP-40. UTILIZE POLICE CONTROL AND FROM ROADWAY STANDARD DRAWING NO. 1101.03, SHEET 2 OF 9, THE TYPE III BARRICADE AND THE 'ROAD CLOSED' AND 'NO RIGHT TURN' SIGN ASSEMBLY TO FORCE SOUTHBOUND -L- TRAFFIC TO TEMPORARILY TURN LEFT AT THE -L-/-Y1- INTERSECTION UNTIL AREA 2 HAS BEEN OPENED TO TRAFFIC.
- B) PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS FOR NORTHBOUND US 17 TRAFFIC IN THE TIE IN AREAS OF -L- AS SHOWN ON SHEET TMP-38 AND IN THE TIE IN AREAS OF -L- TO -Y1- (US 17) AS SHOWN ON SHEET TMP-40. SHIFT NORTHBOUND -L- TRAFFIC INTO THE PATTERN SHOWN ON SHEETS TMP-38 THRU TMP-40. UTILIZE POLICE CONTROL AND FROM ROADWAY STANDARD DRAWING NO. 1101.03, SHEET 2 OF 9, THE TYPE III BARRICADE AND THE 'ROAD CLOSED AHEAD', 'DETOUR AHEAD', AND 'ROAD CLOSED 1000 FT' SIGN ASSEMBLIES TO FORCE NORTHBOUND -L- TRAFFIC TO TEMPORARILY TURN RIGHT AT THE -L-/-Y1- INTERSECTION UNTIL AREA 2 HAS BEEN OPENED TO TRAFFIC.

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> 		<p>TRANSPORTATION OPERATIONS PLAN TRAFFIC CONTROL PHASING</p>
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ALL PAVEMENT MARKINGS ARE EXISTING MARKINGS UNLESS OTHERWISE NOTED

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS	APPROVED:  DATE: 4/15/2015 SEAL: 		TRANSPORTATION MANAGEMENT PLAN PHASE I OVERVIEWS
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ALL PAVEMENT MARKINGS ARE EXISTING
MARKINGS UNLESS OTHERWISE NOTED

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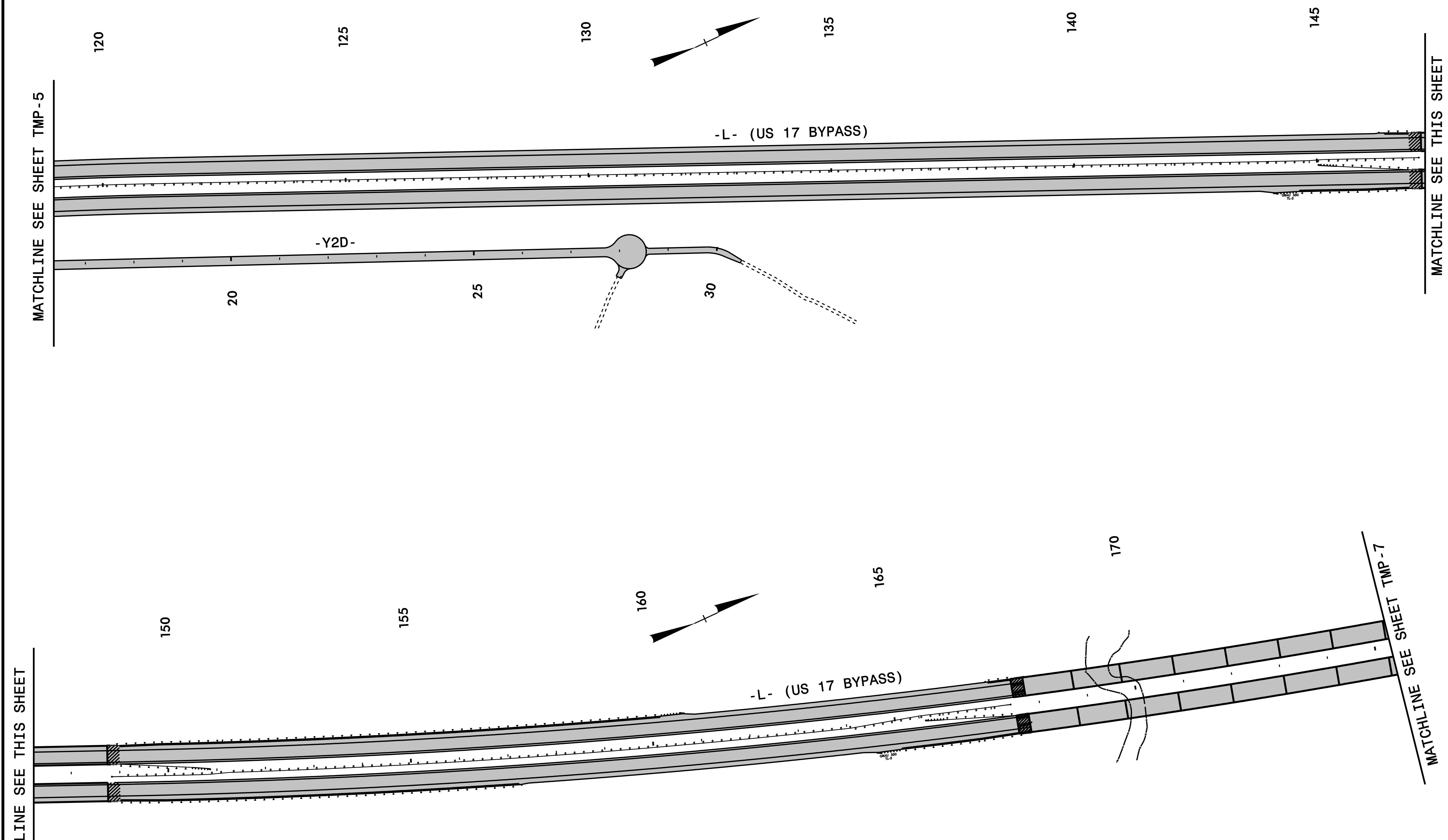
APPROVED: *[Signature]* DATE: 4/15/2015

SEAL

PROFESSIONAL ENGINEER
TIM ARY
SEAL 025465

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

TRANSPORTATION
MANAGEMENT PLAN
PHASE I OVERVIEWS



MATCHLINE SEE THIS SHEET

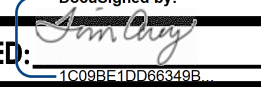
MATCHLINE SEE THIS SHEET

MATCHLINE SEE SHEET TMP-7

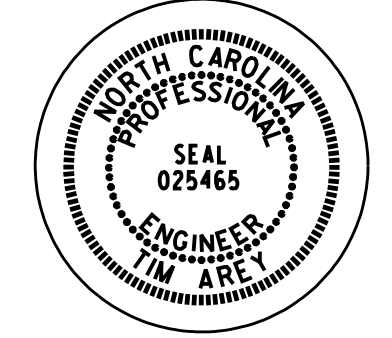
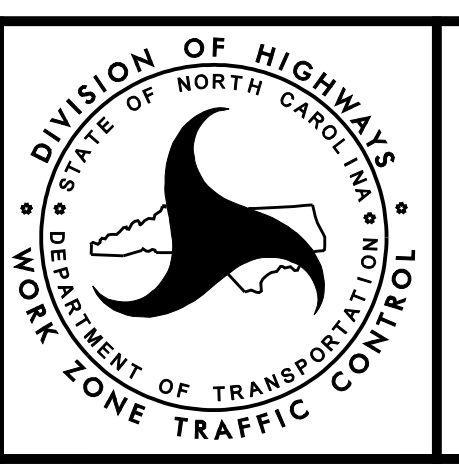
PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE
DESIGN GROUP, INC.

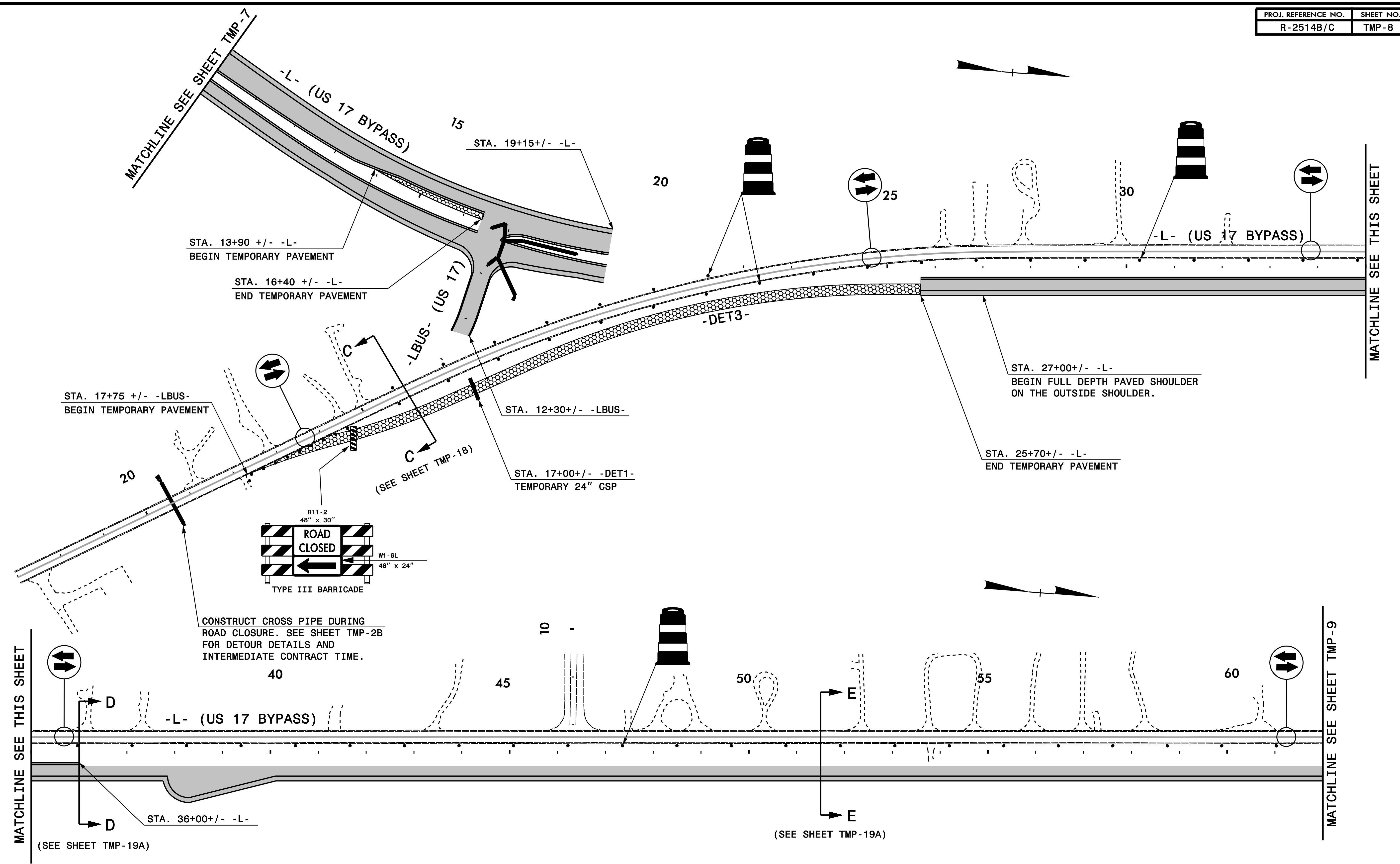
ENGINEERS • CONSULTANTS

APPROVED:  DATE: 4/15/2015

SEAL

TRANSPORTATION
MANAGEMENT PLAN
PHASE I OVERVIEWS

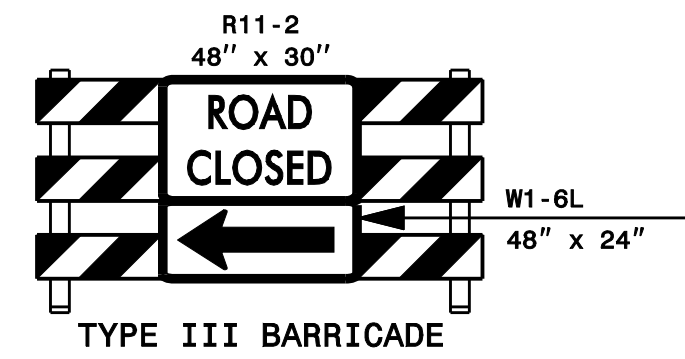


MATCHLINE SEE THIS SHEET

MATCHLINE SEE THIS SHEET

MATCHLINE SEE SHEET TMP-9

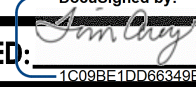
CONSTRUCT CROSS PIPE DURING ROAD CLOSURE. SEE SHEET TMP-2B FOR DETOUR DETAILS AND INTERMEDIATE CONTRACT TIME.

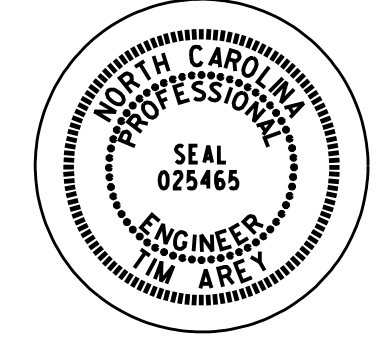


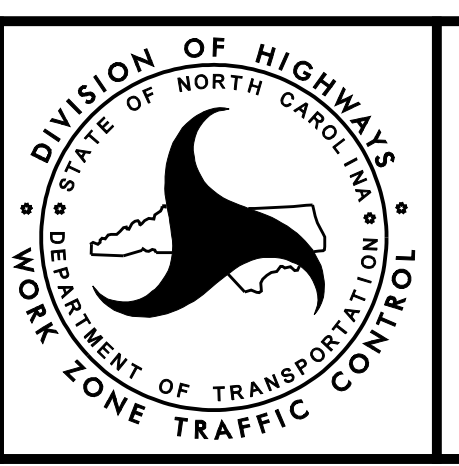
PLAN PREPARED IN THE OFFICE OF:

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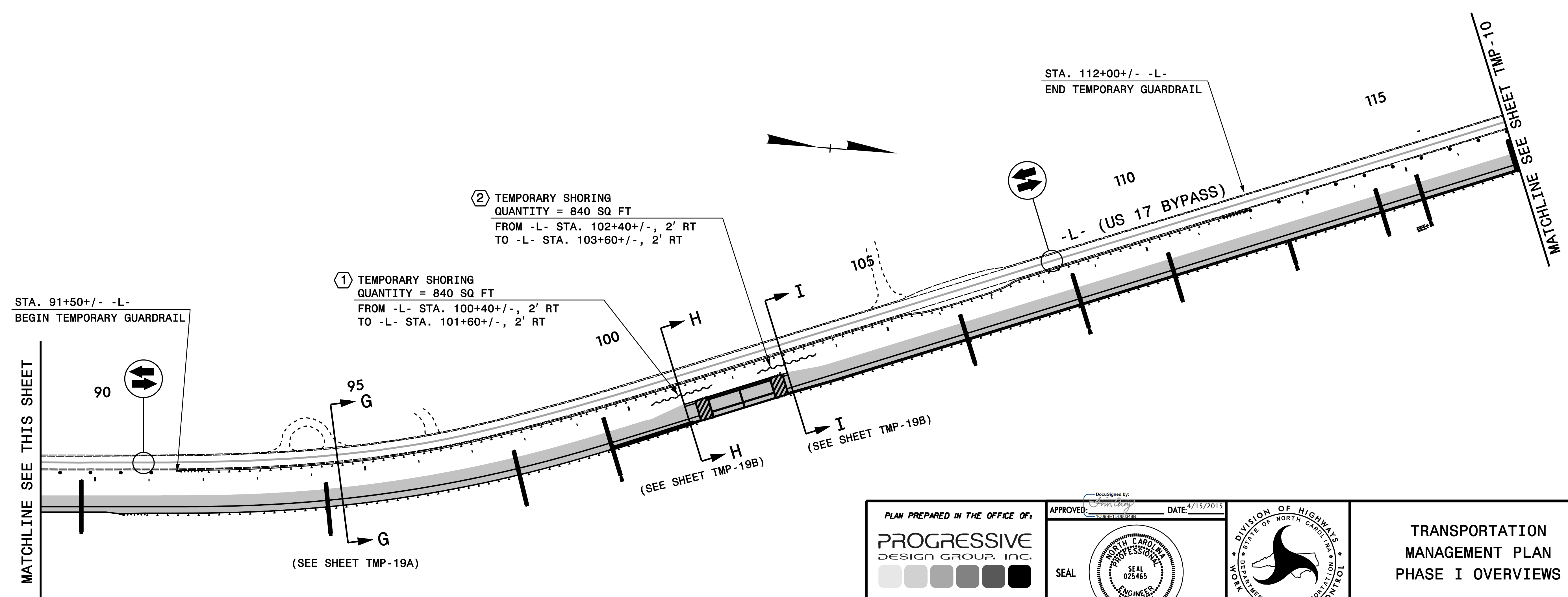
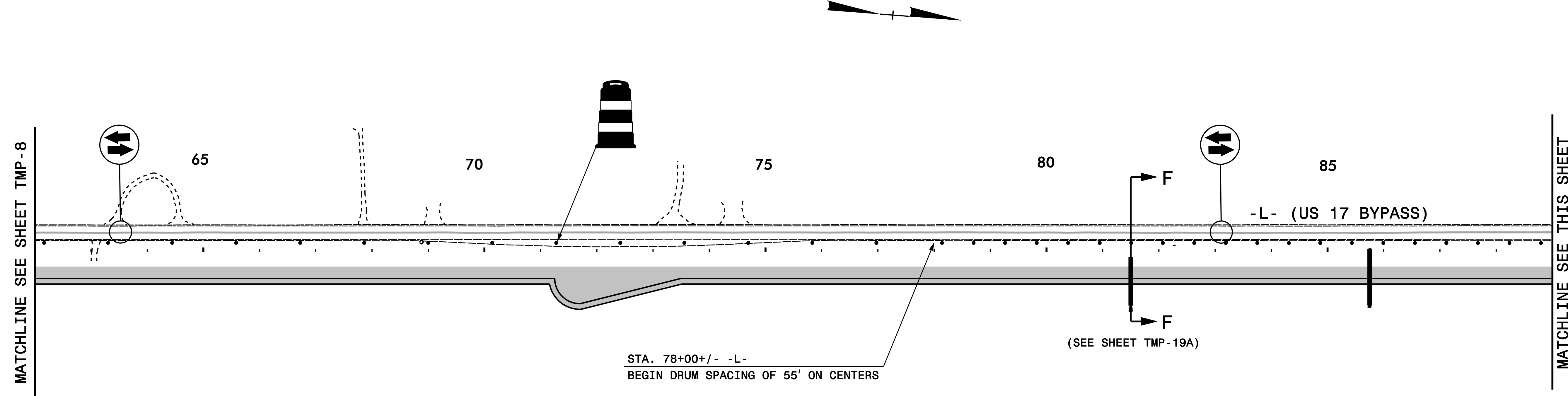
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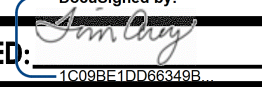
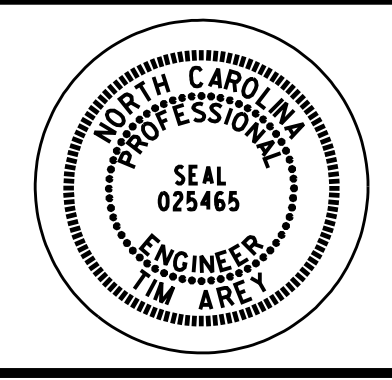
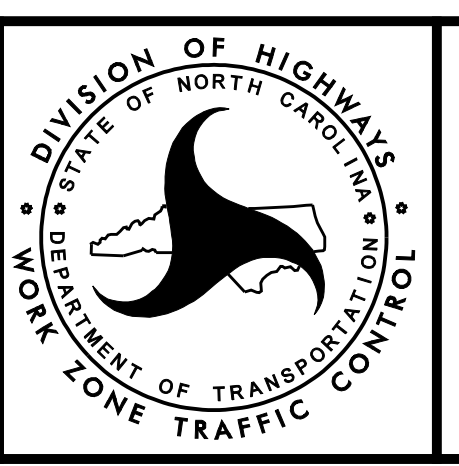
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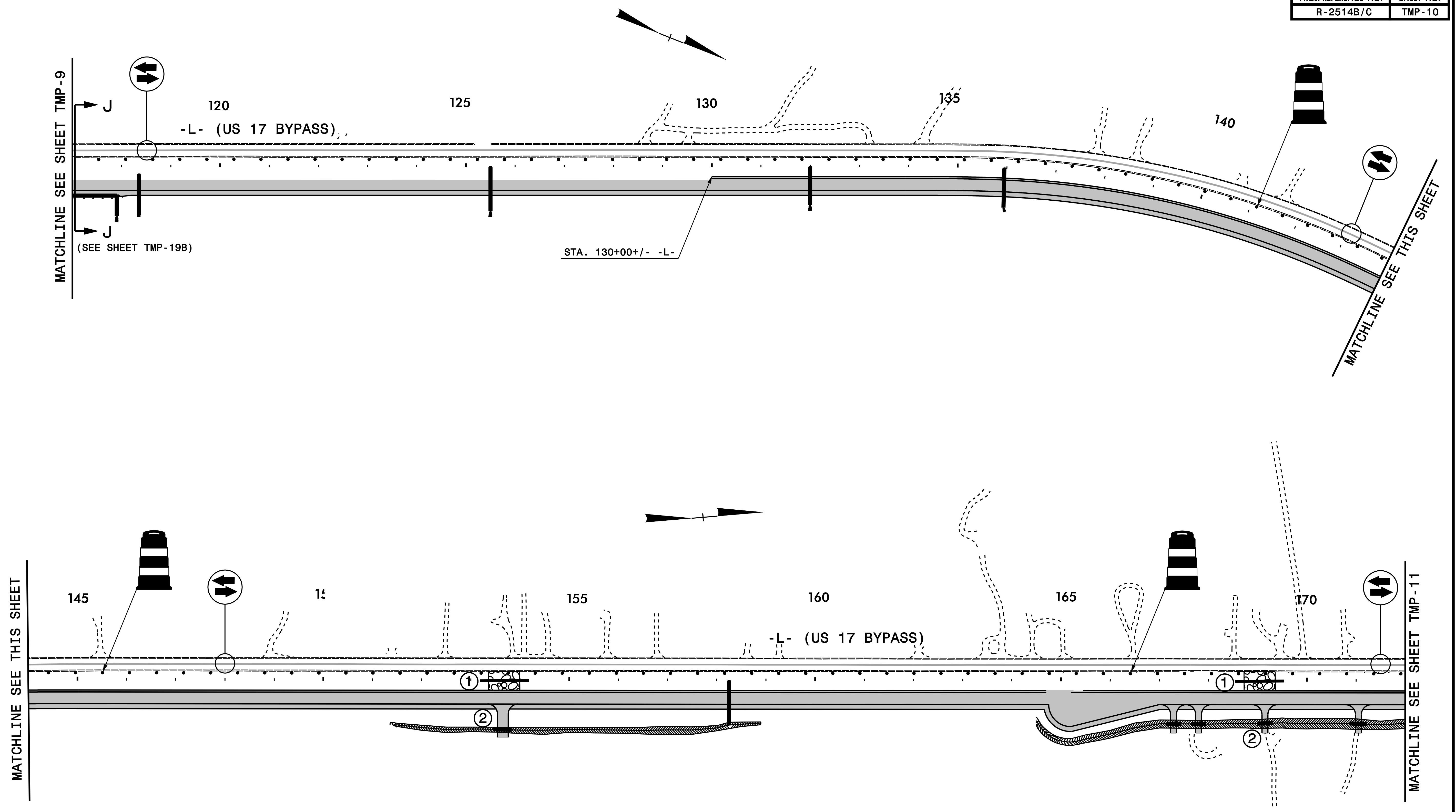


DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

TRANSPORTATION
MANAGEMENT PLAN
PHASE I OVERVIEWS



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NOTES:
 1. INSTALL TEMPORARY 18" CSP TO MAINTAIN DRAINAGE.
 2. INSTALL INCIDENTAL STONE TO MAINTAIN ACCESS TO ADJACENT PARCELS.

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 DESIGN GROUP, INC.

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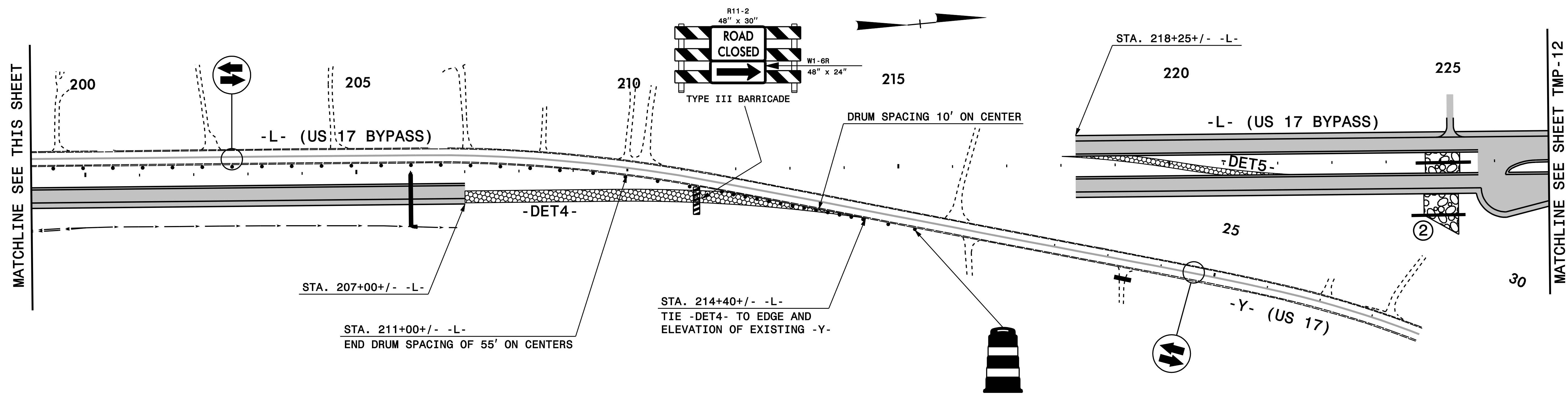
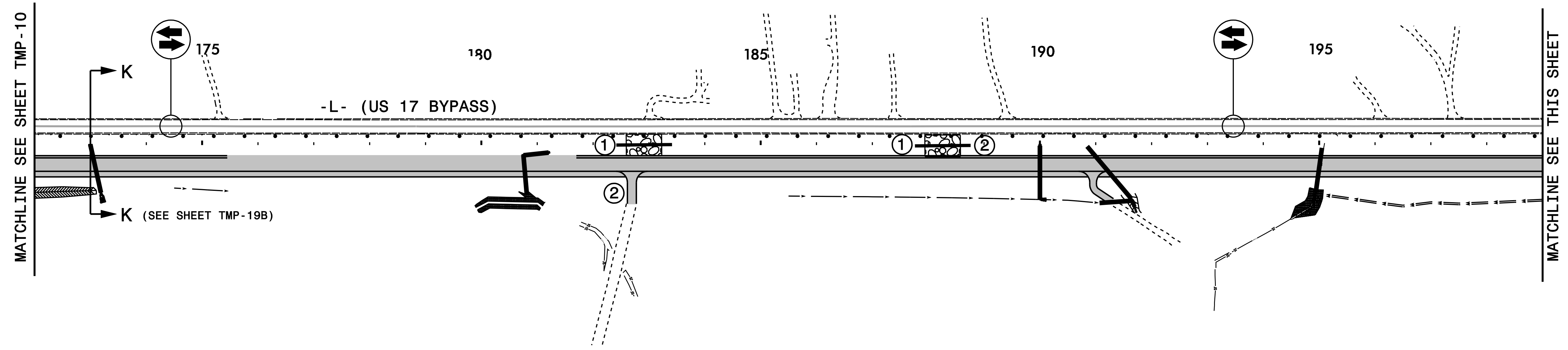
APPROVED: *[Signature]* DATE: 4/15/2015

SEAL

PROFESSIONAL ENGINEER
 SEAL 025465
 TIM ARY

DIVISION OF HIGHWAYS
 NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 WORK ZONE TRAFFIC CONTROL

**TRANSPORTATION
 MANAGEMENT PLAN
 PHASE I OVERVIEWS**



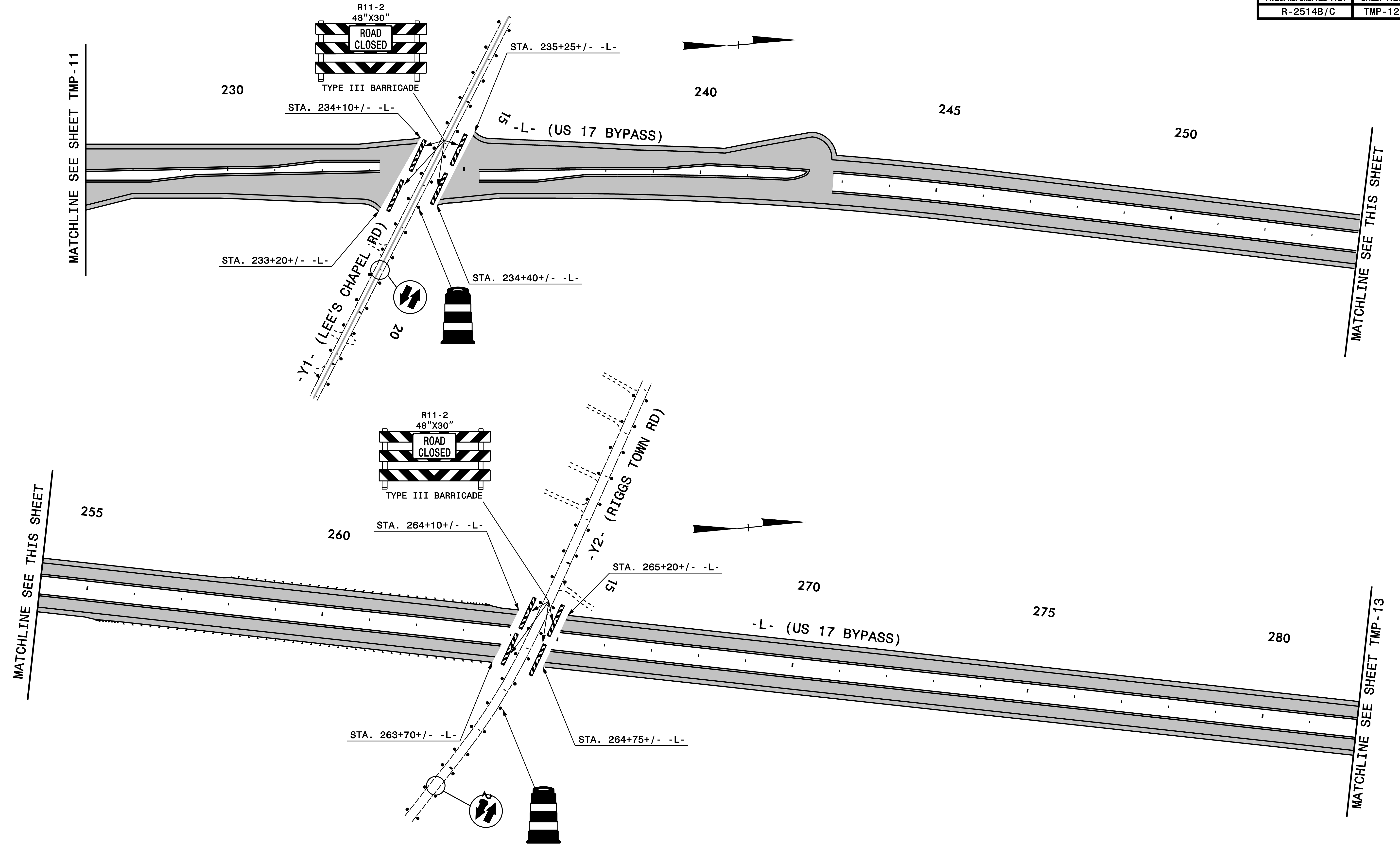
NOTES:
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 2. INSTALL INCIDENTAL STONE TO MAINTAIN ACCESS TO ADJACENT PARCELS.

PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE
 DESIGN GROUP, INC.
 ENGINEERS • CONSULTANTS

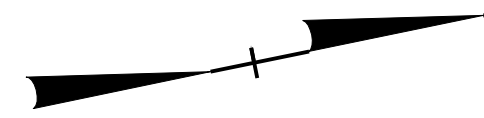
APPROVED: *[Signature]* DATE: 4/15/2015
 SEAL
 NORTH CAROLINA PROFESSIONAL ENGINEER
 TIM ARY
 025465

DIVISION OF HIGHWAYS
 STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 WORK ZONE TRAFFIC CONTROL

TRANSPORTATION
 MANAGEMENT PLAN
 PHASE I OVERVIEWS



<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>NORTH CAROLINA PROFESSIONAL ENGINEER TIM ARLEY</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE I OVERVIEWS</p>
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MATCHLINE SEE SHEET TMP-12

285

290

295

300

30

-L- (US 17 BYPASS)

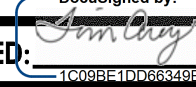


STA. 300+00+/- -L-

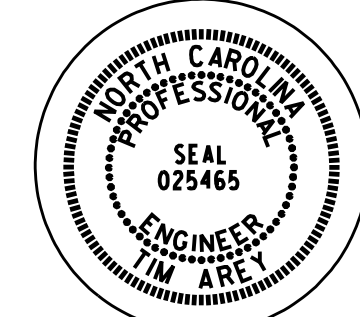
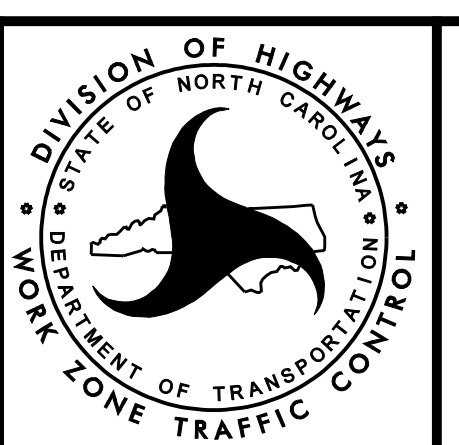
PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE
DESIGN GROUP, INC.

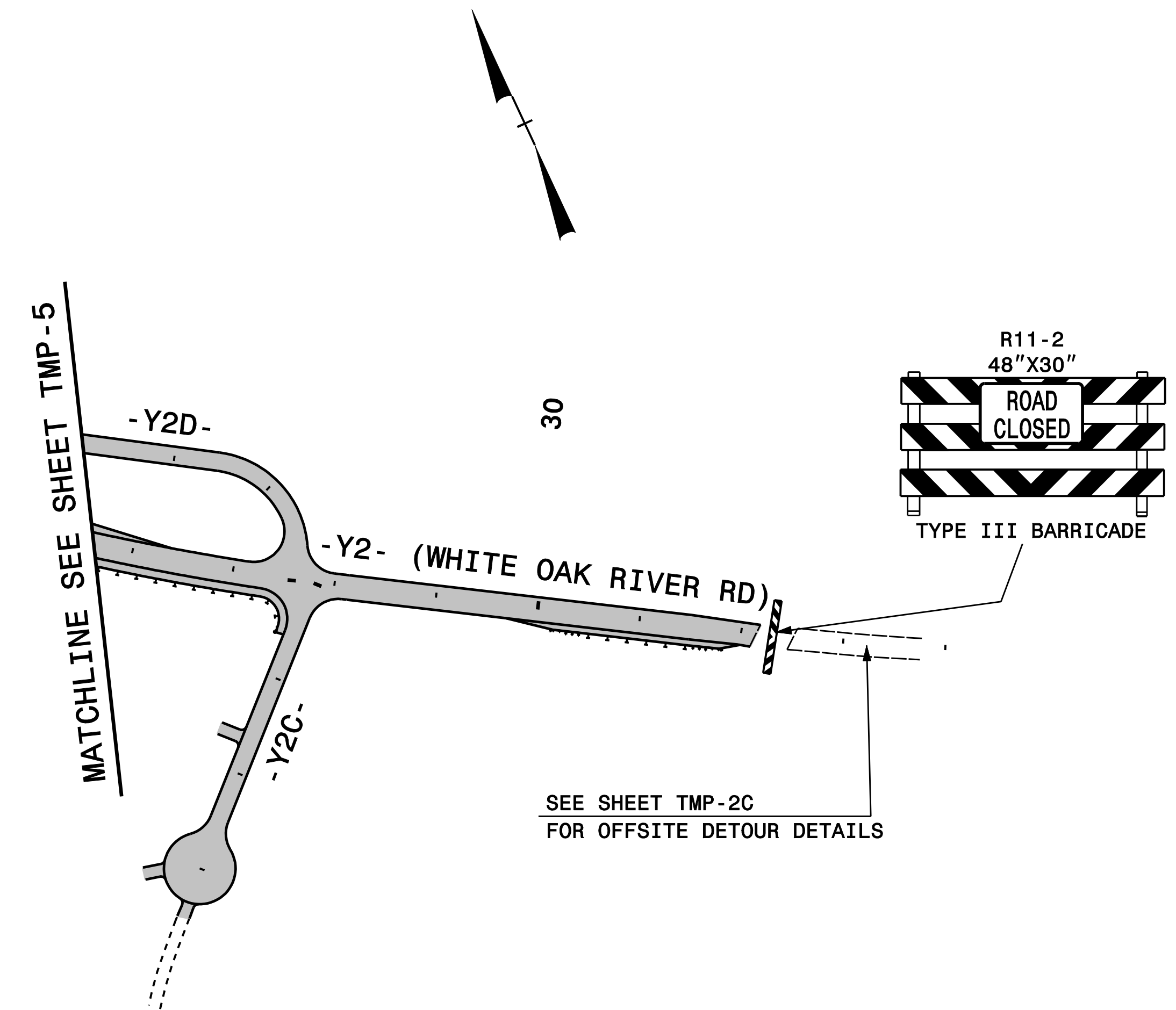
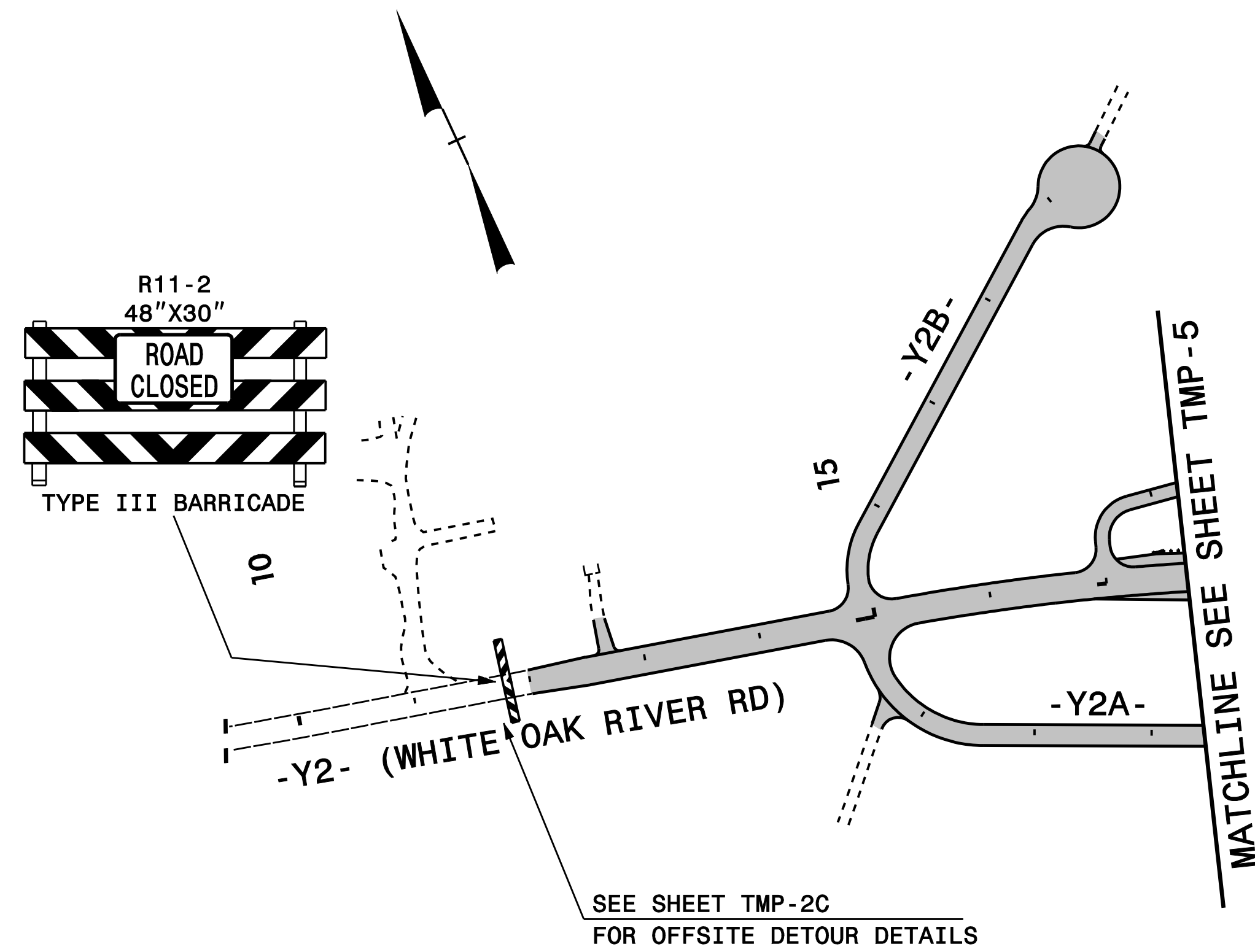
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APPROVED:  DATE: 4/15/2015

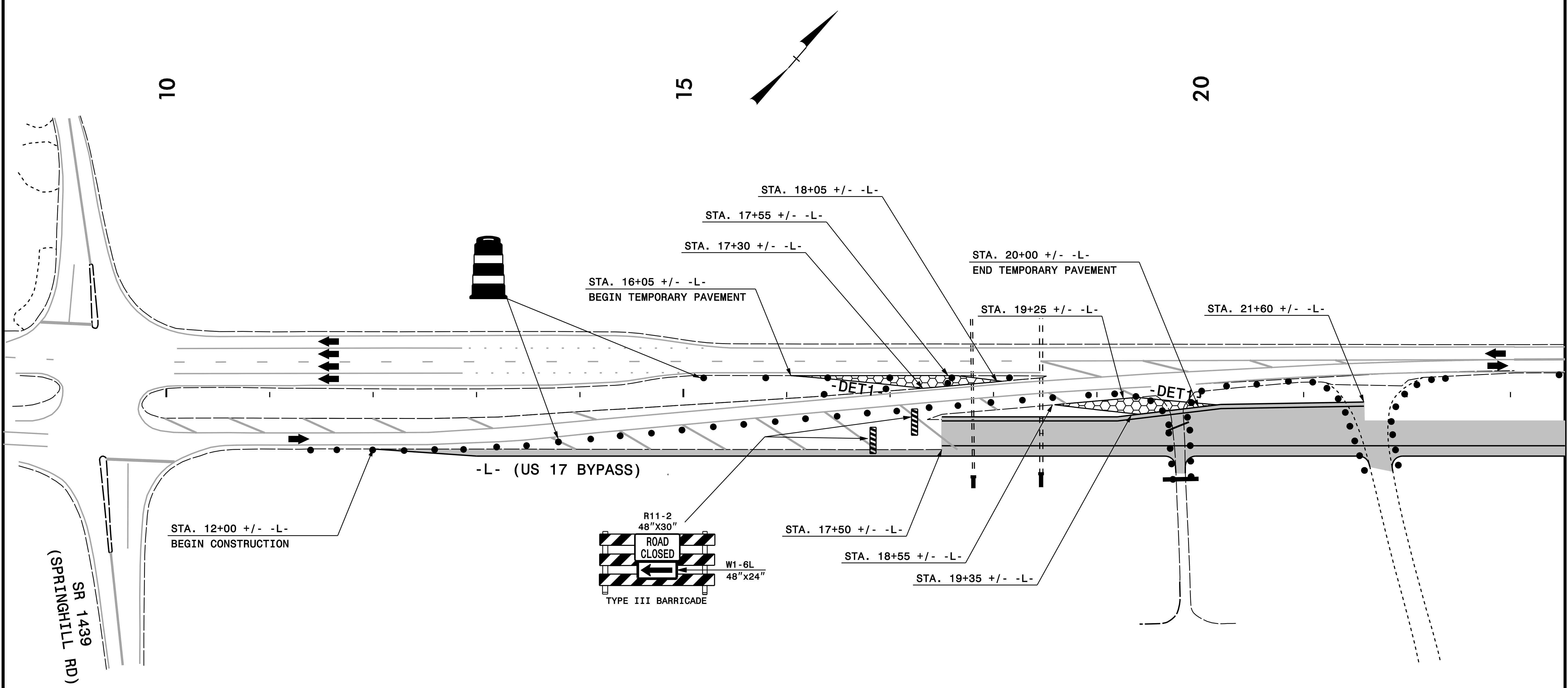
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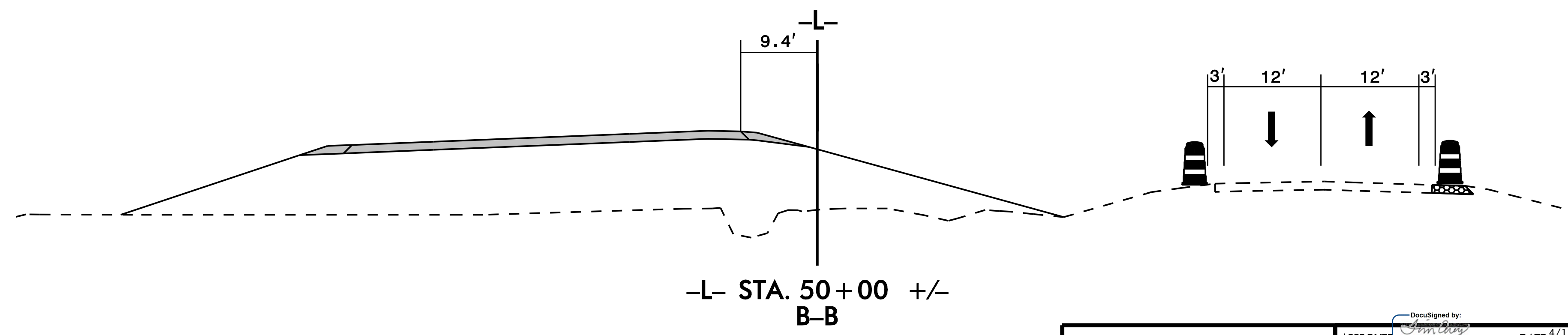
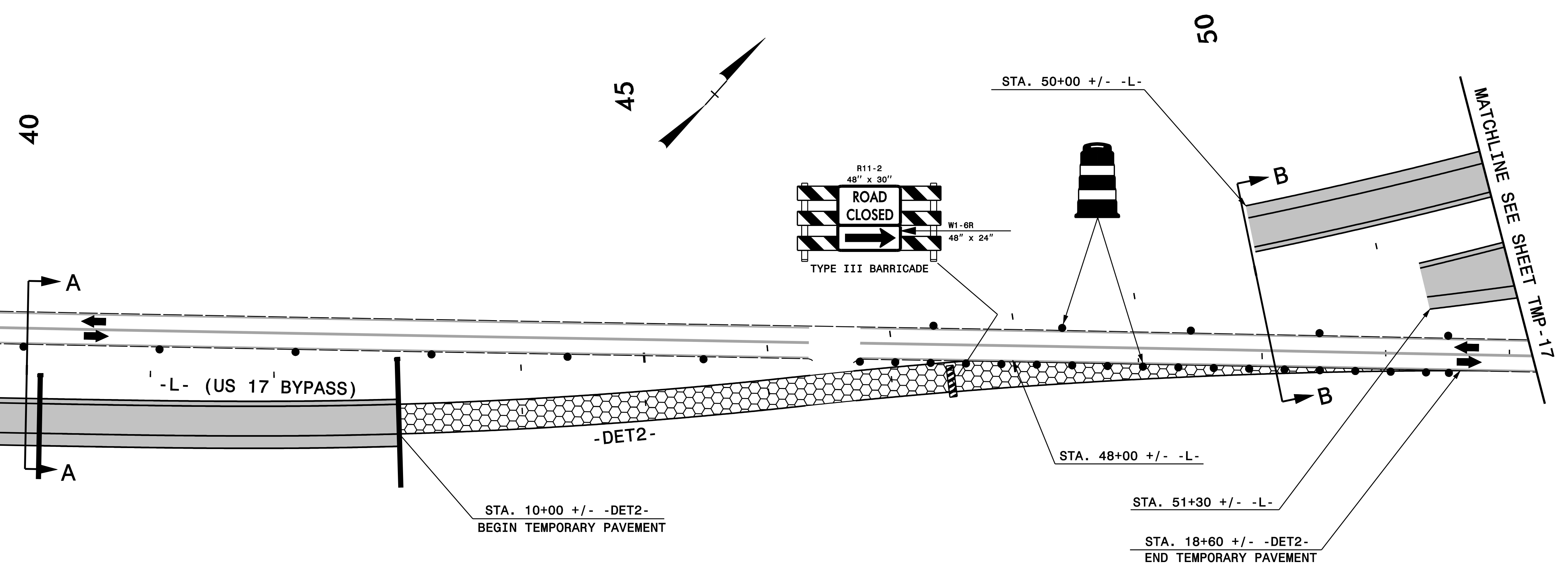
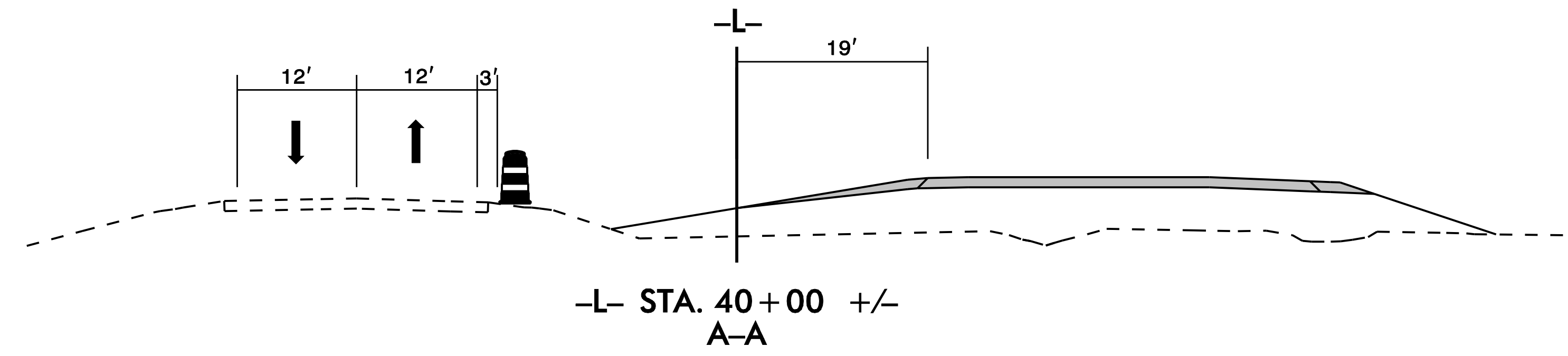
TRANSPORTATION
MANAGEMENT PLAN
PHASE I OVERVIEWS



<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>PROFESSIONAL ENGINEER TIM ARY SEAL 025465</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE I OVERVIEWS</p>
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<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>PROFESSIONAL ENGINEER TIM ARY</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE I DETAILS</p>
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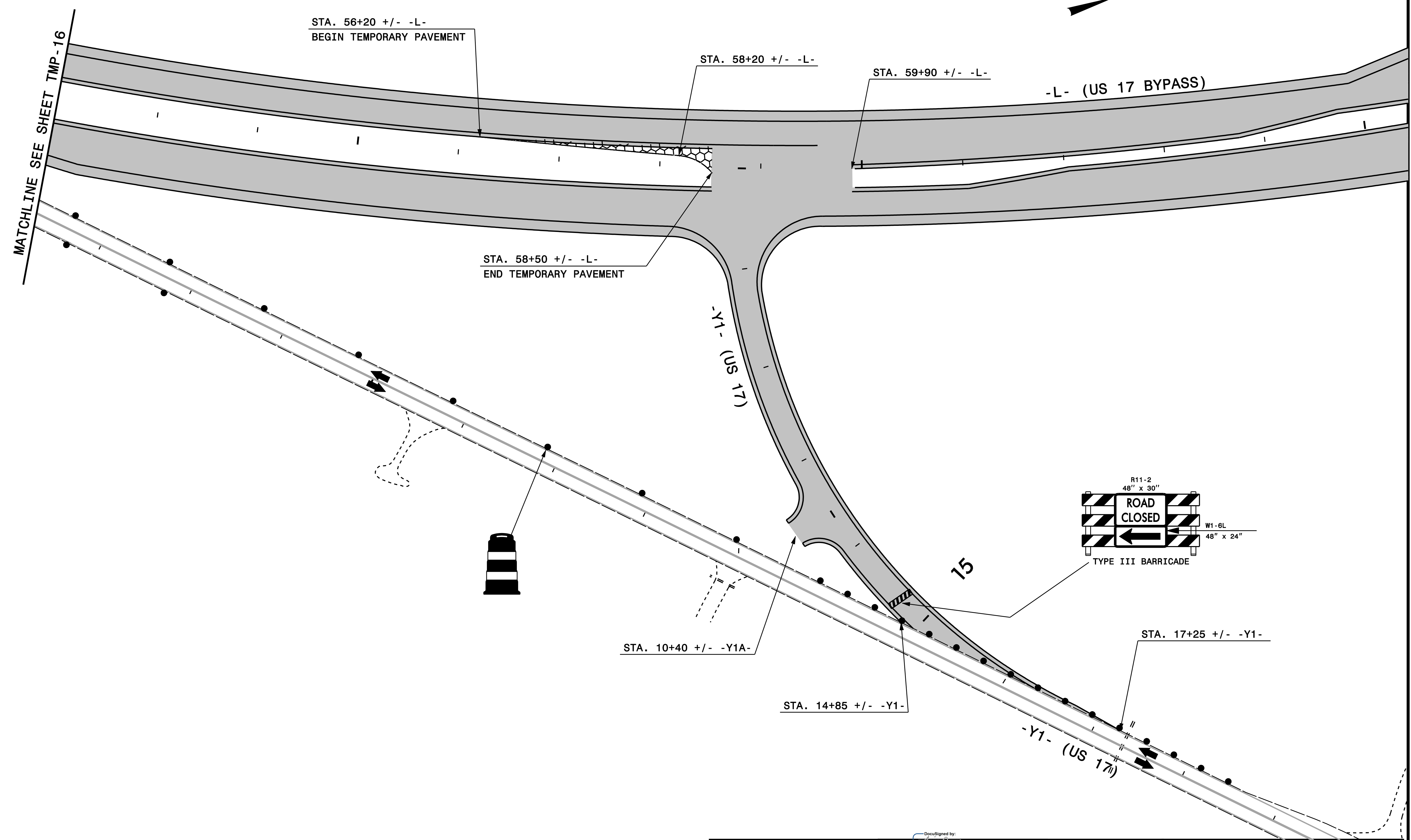
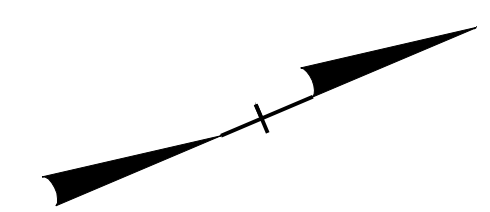


<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>PROFESSIONAL ENGINEER TIM ARY SEAL 025465</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE I DETAILS</p>
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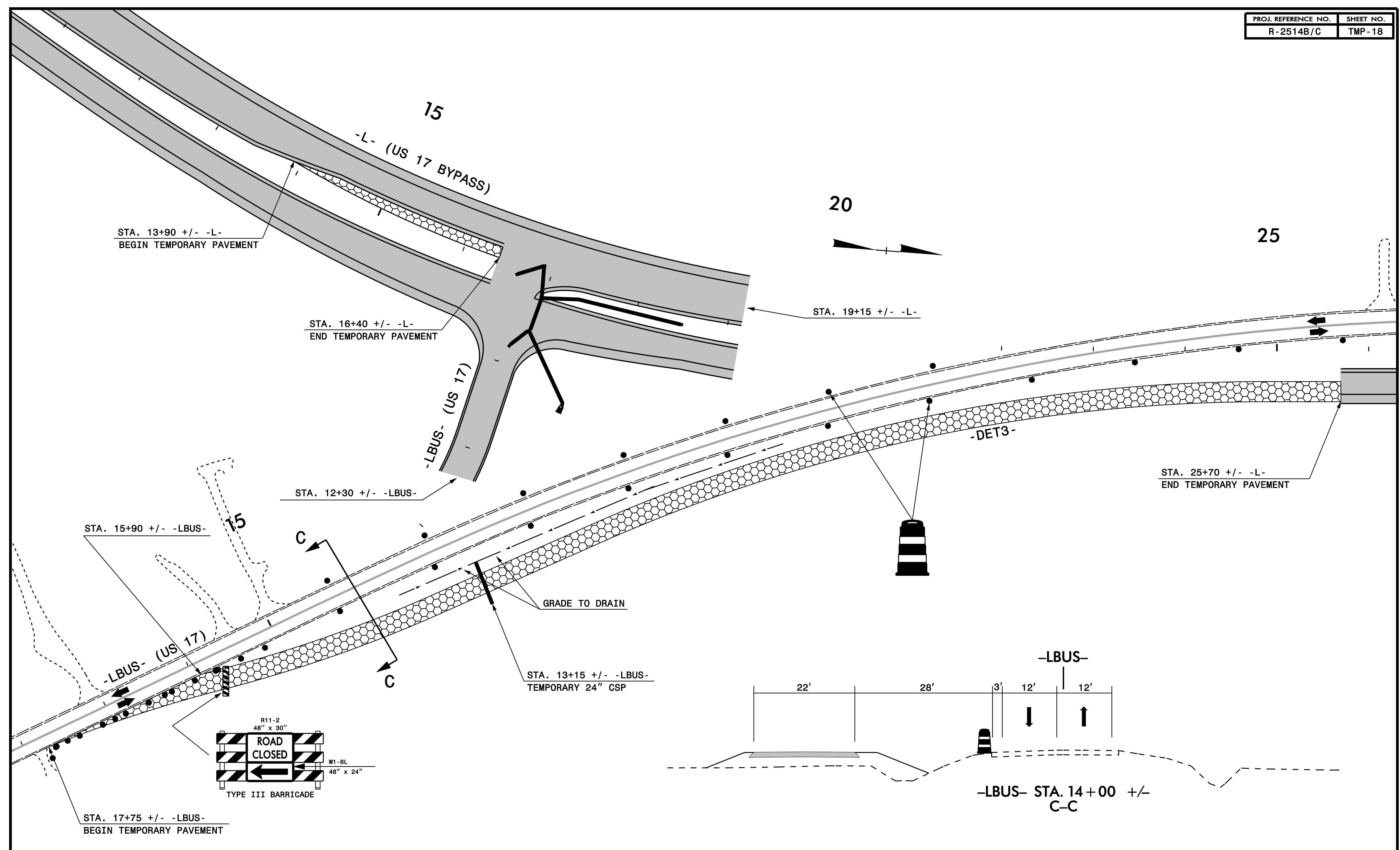
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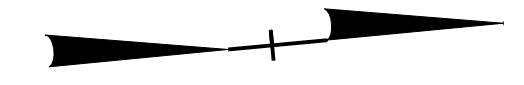
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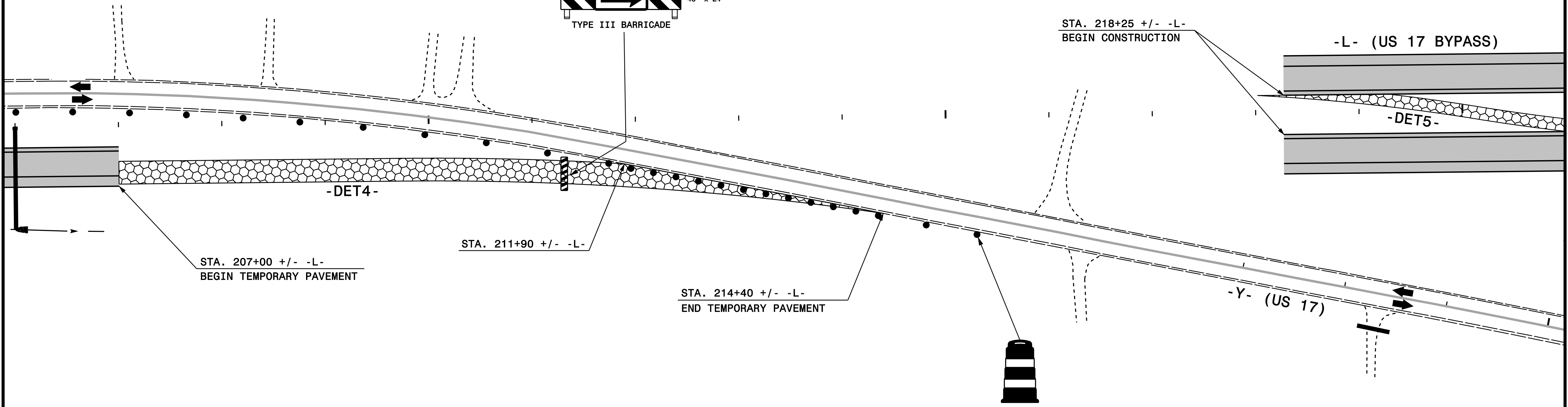
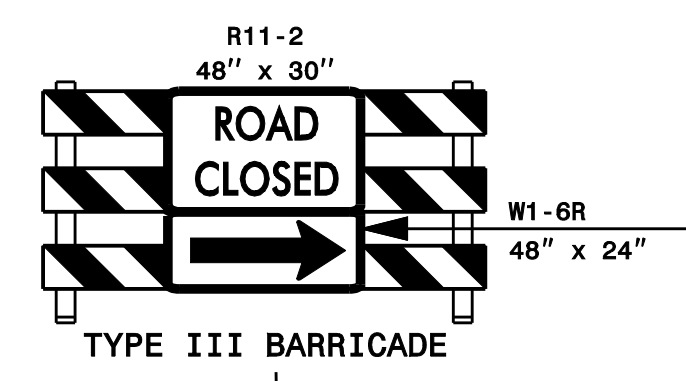
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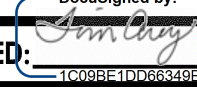
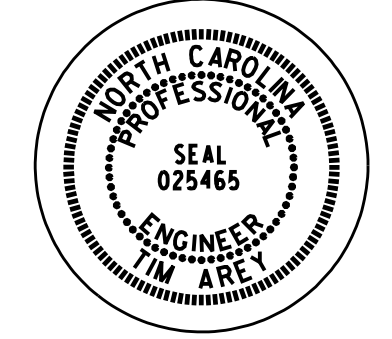
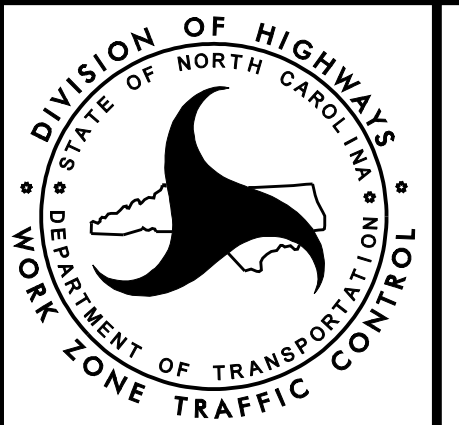


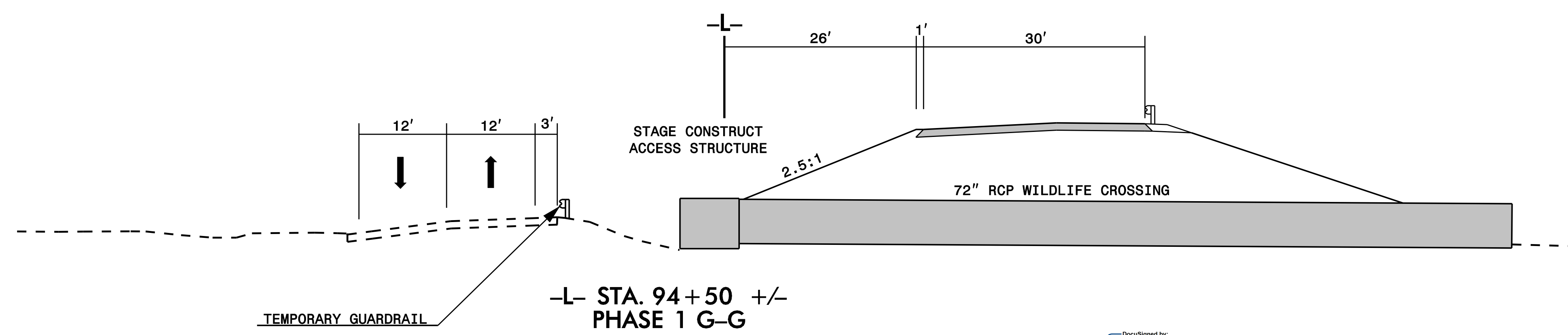
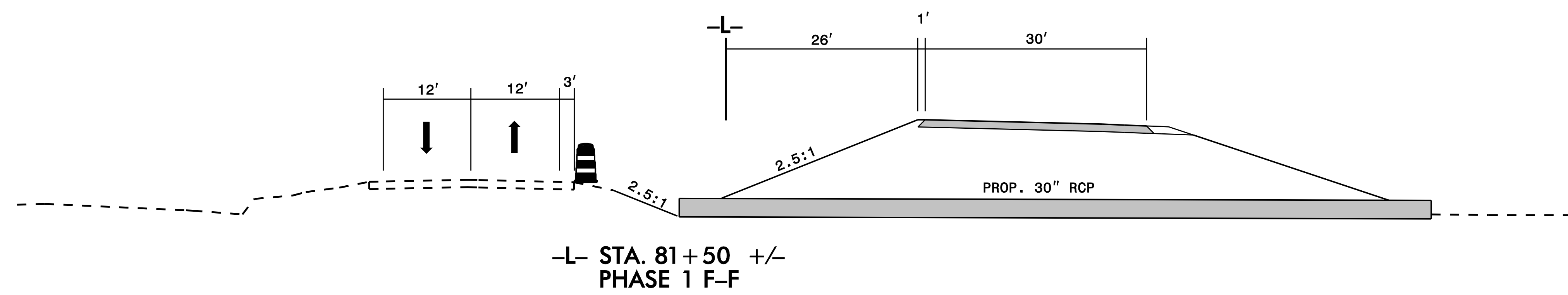
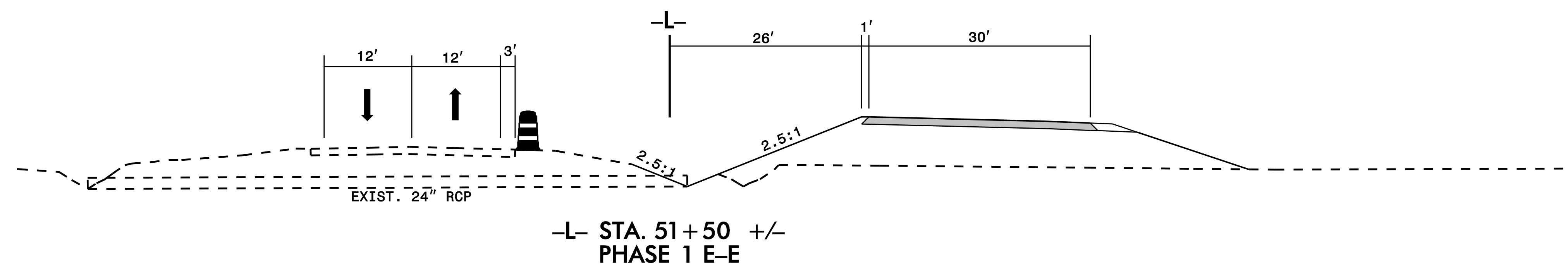
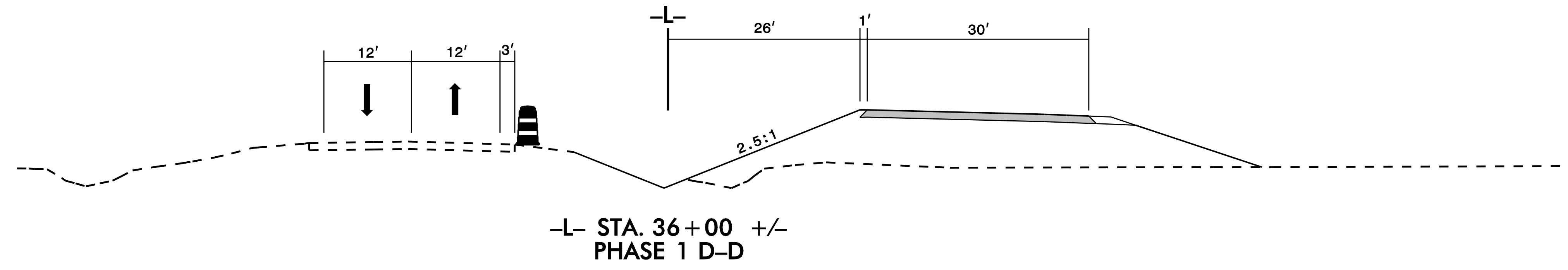
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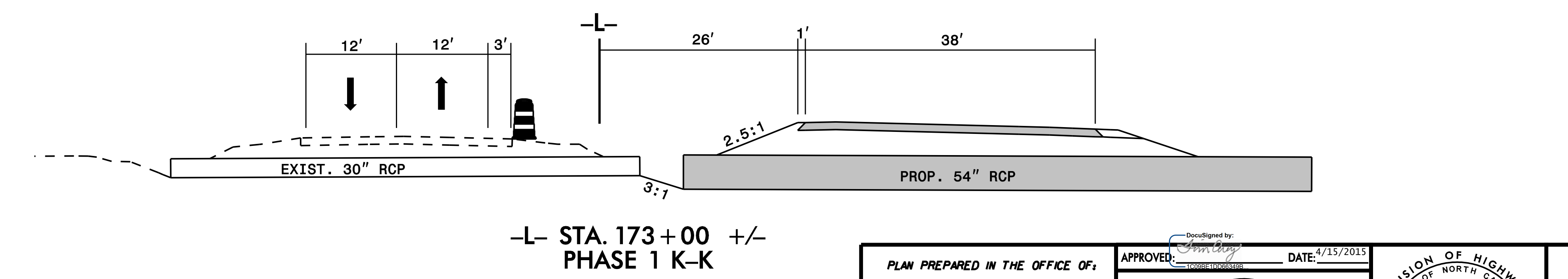
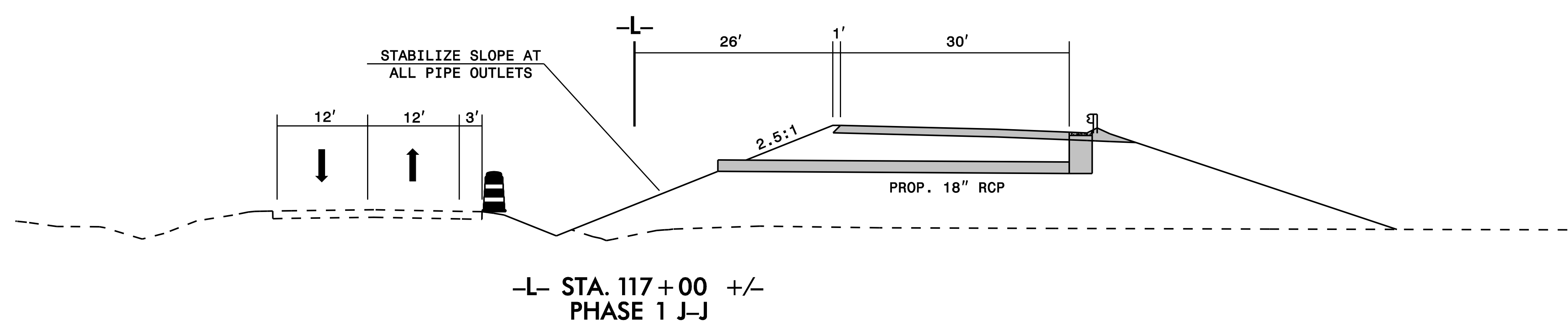
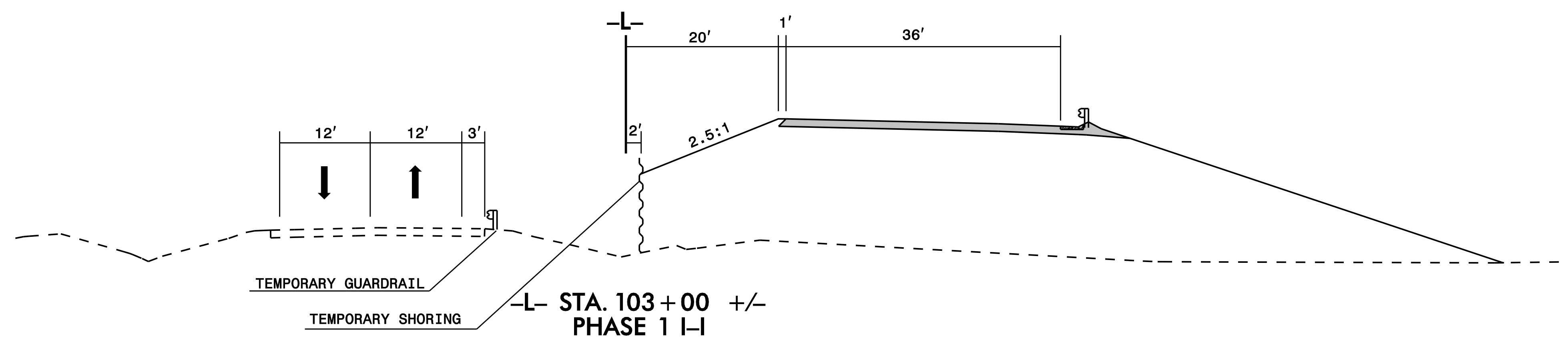
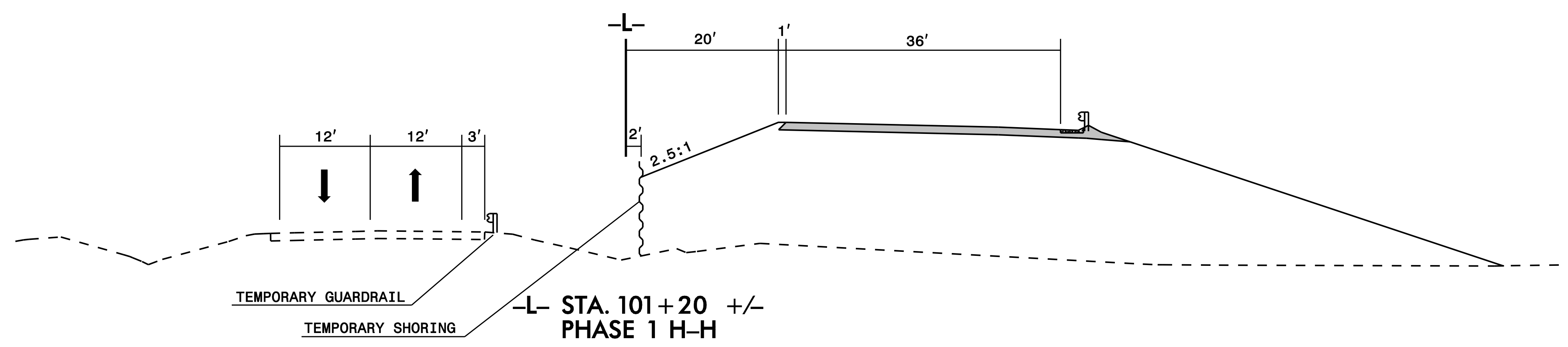
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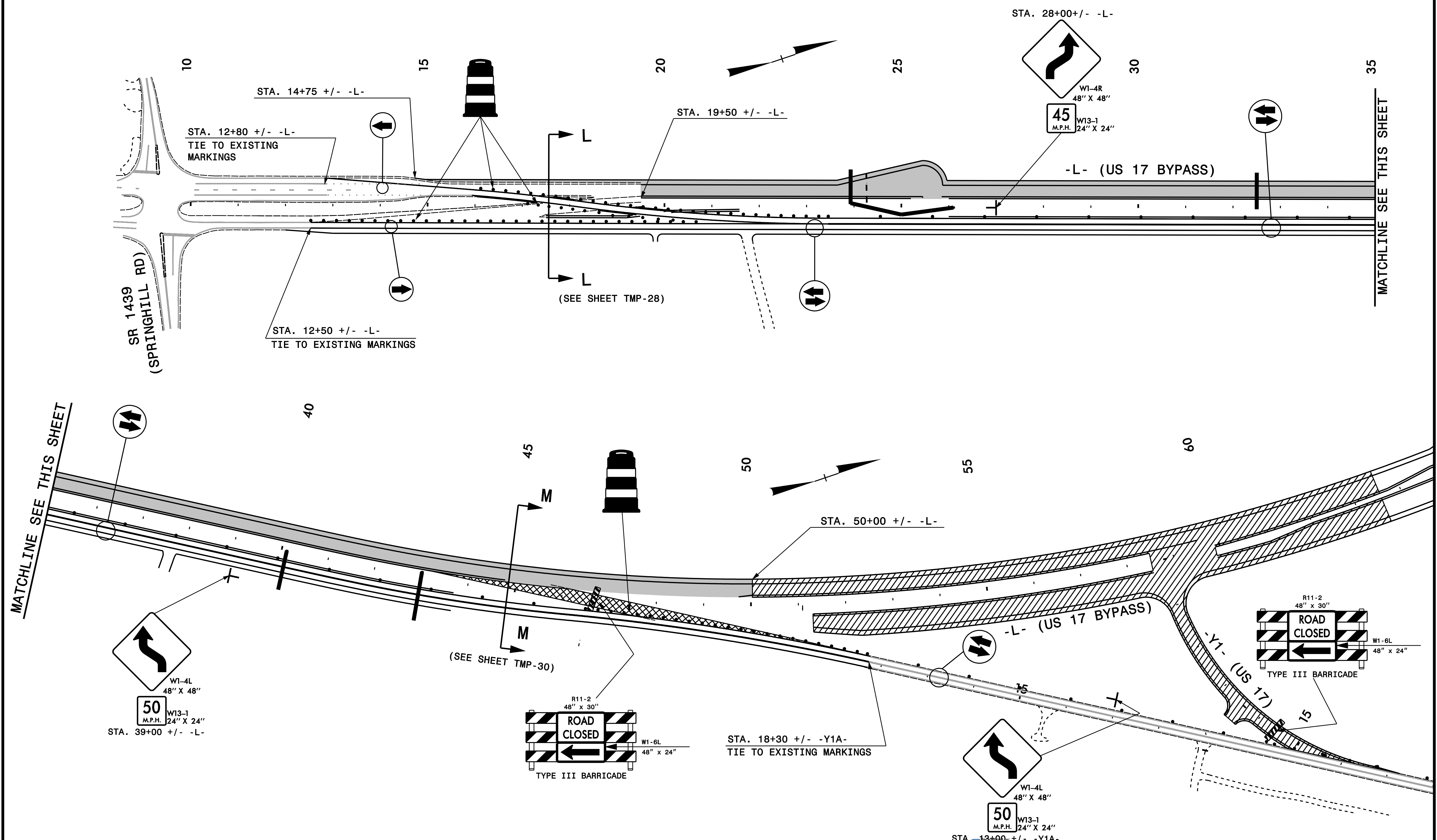
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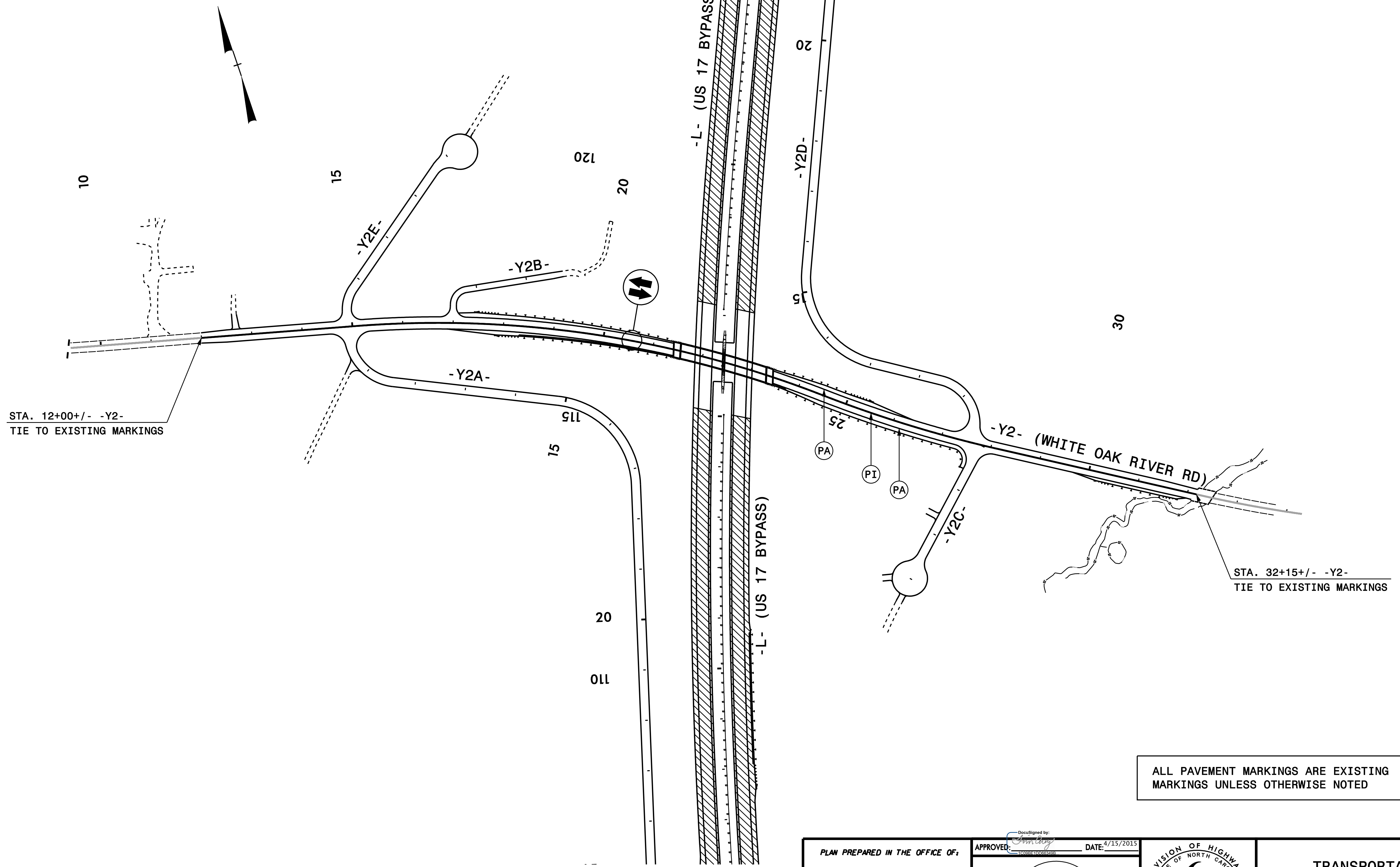
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<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>NORTH CAROLINA PROFESSIONAL ENGINEER TIM ARY</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN CROSS SECTIONS</p>
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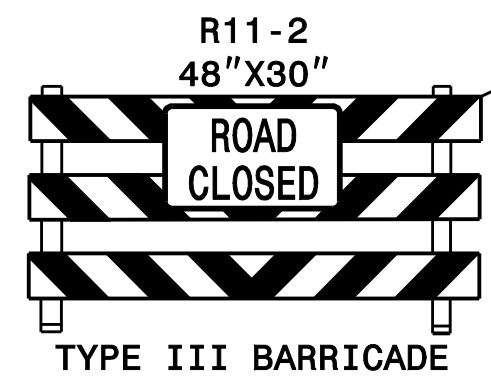
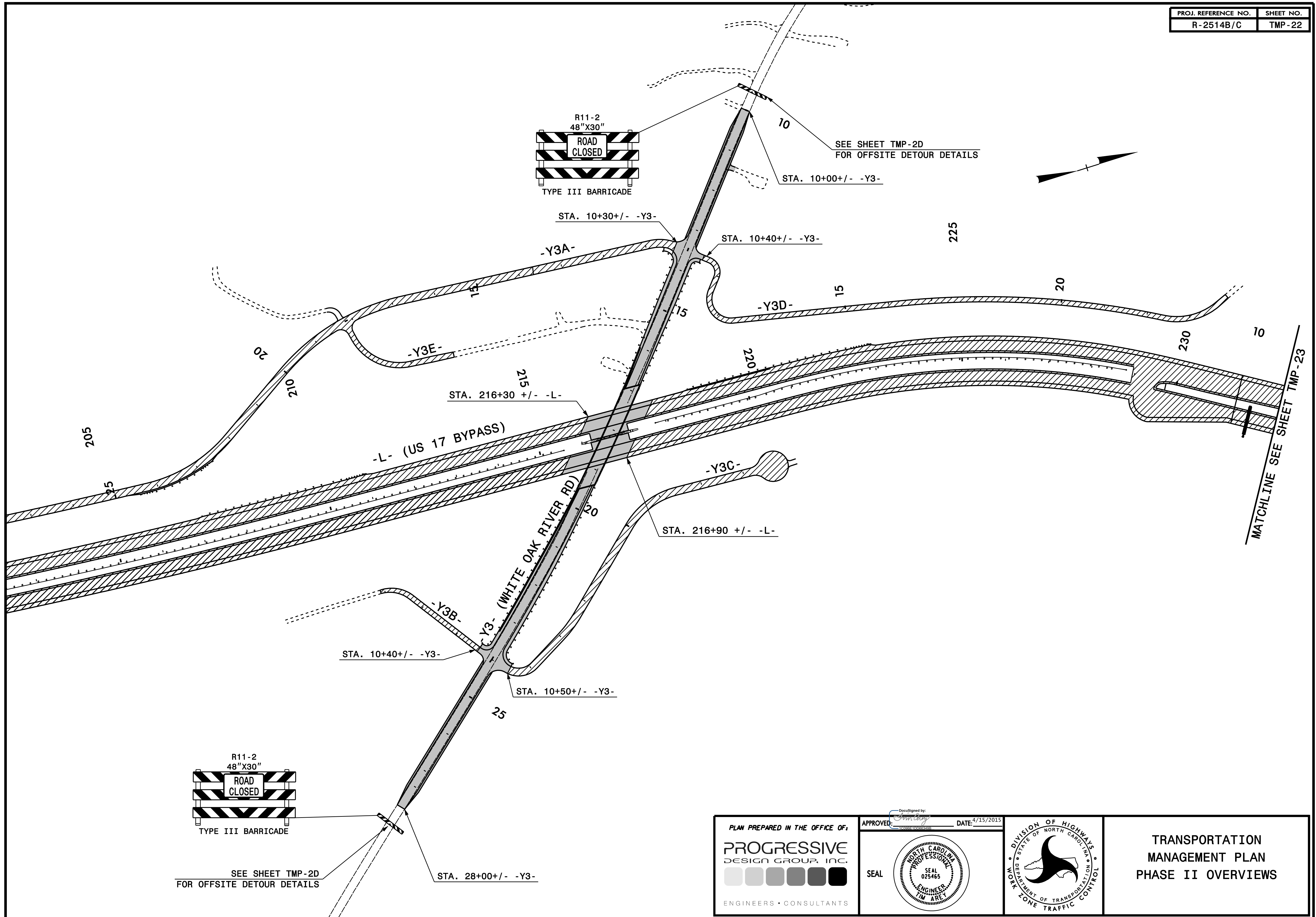


<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>PROFESSIONAL ENGINEER SEAL 025465 TIM ARTHUR</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE II OVERVIEWS</p>
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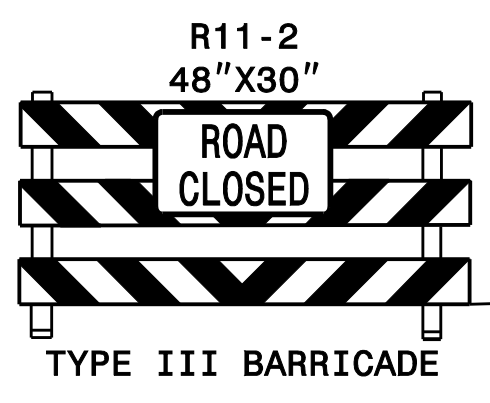


ALL PAVEMENT MARKINGS ARE EXISTING
MARKINGS UNLESS OTHERWISE NOTED

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>NORTH CAROLINA PROFESSIONAL ENGINEER TIM AREY</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE II OVERVIEWS</p>
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SEE SHEET TMP-2D
FOR OFFSITE DETOUR DETAILS

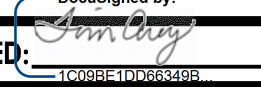


SEE SHEET TMP-2D
FOR OFFSITE DETOUR DETAILS


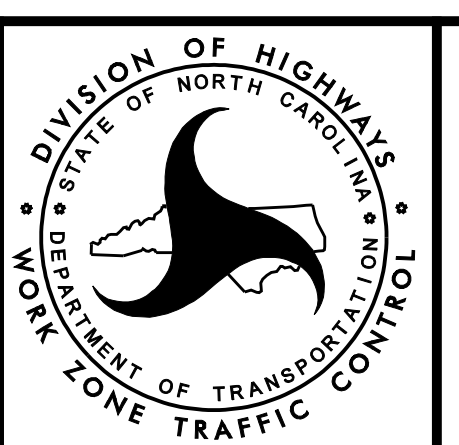
PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE
DESIGN GROUP, INC.

ENGINEERS • CONSULTANTS

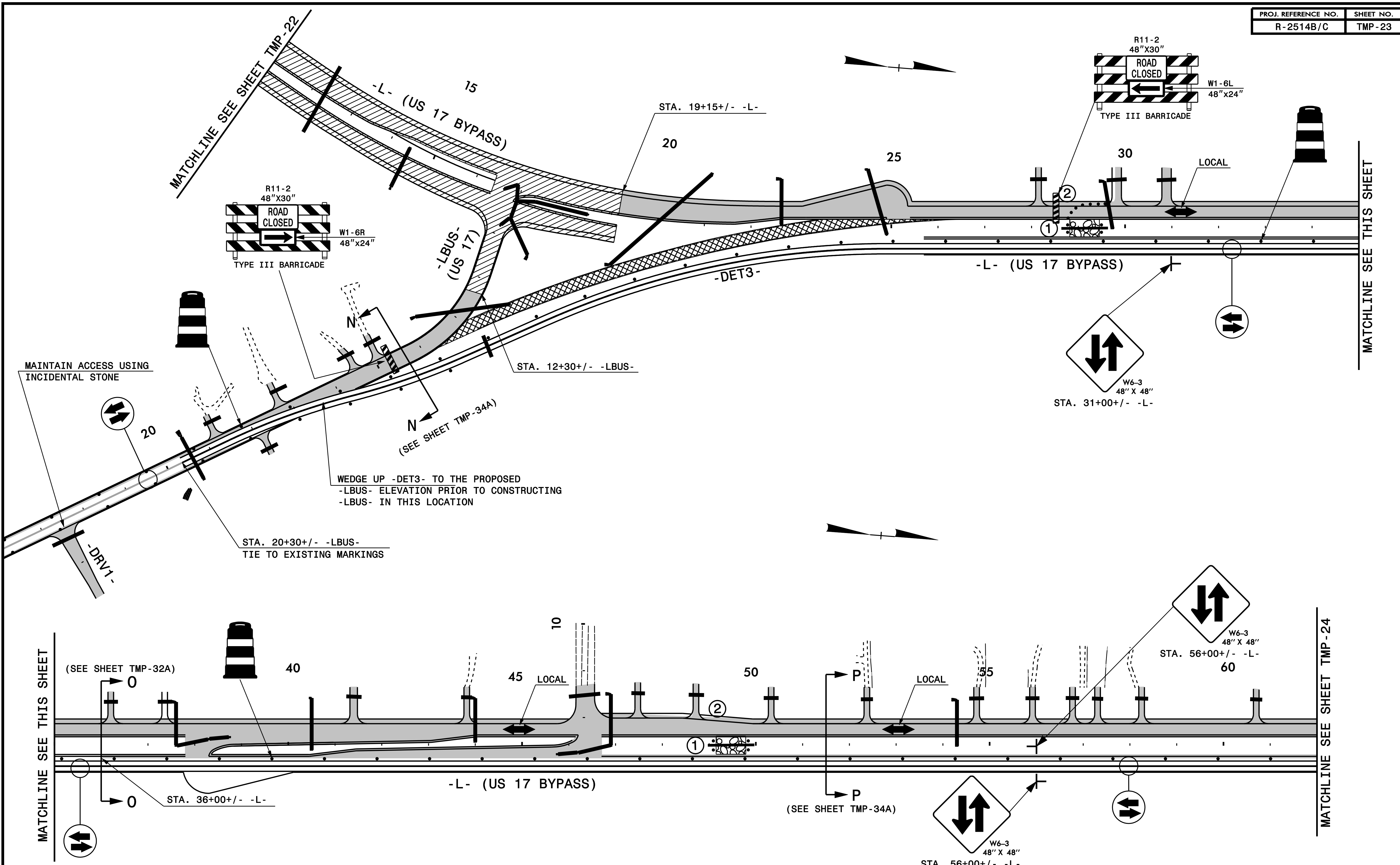
APPROVED:  DATE: 4/15/2015

SEAL

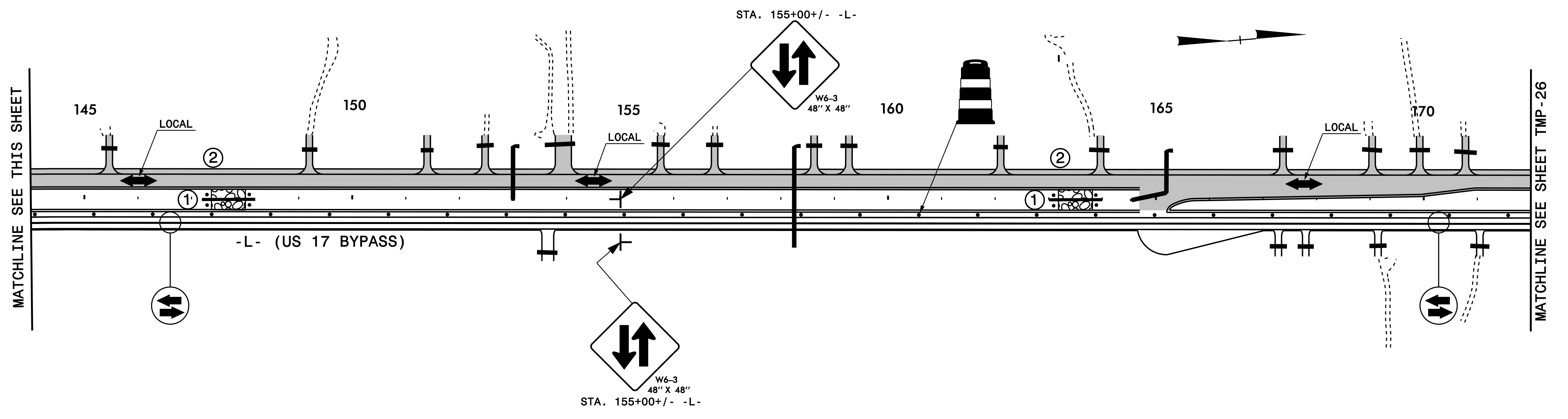
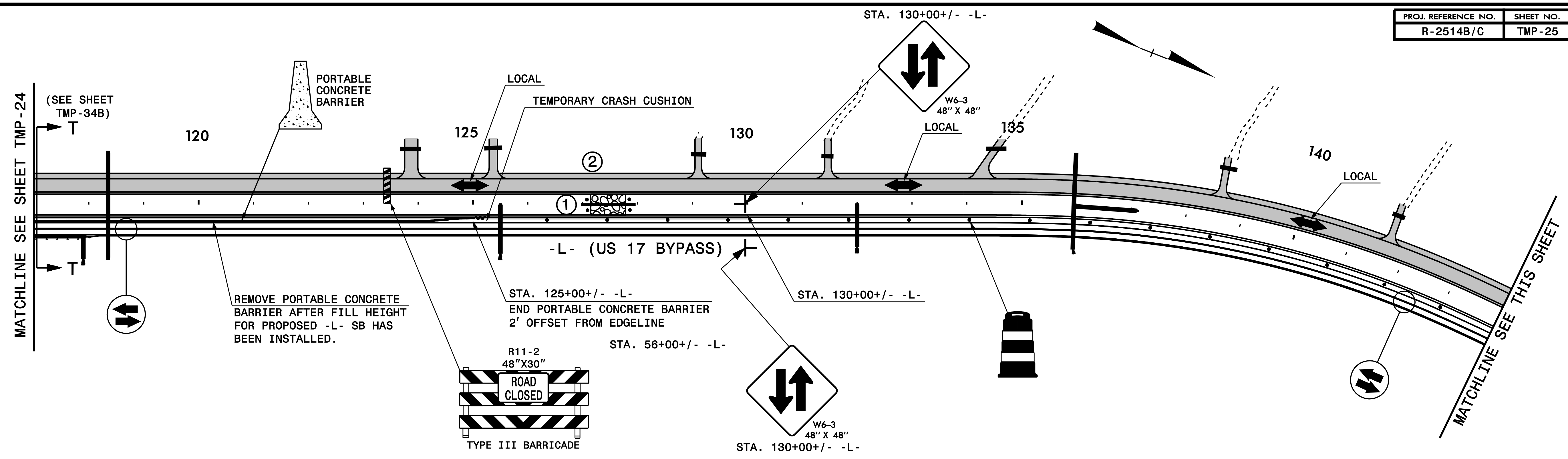
TRANSPORTATION
MANAGEMENT PLAN
PHASE II OVERVIEWS

MATCHLINE SEE SHEET TMP-23



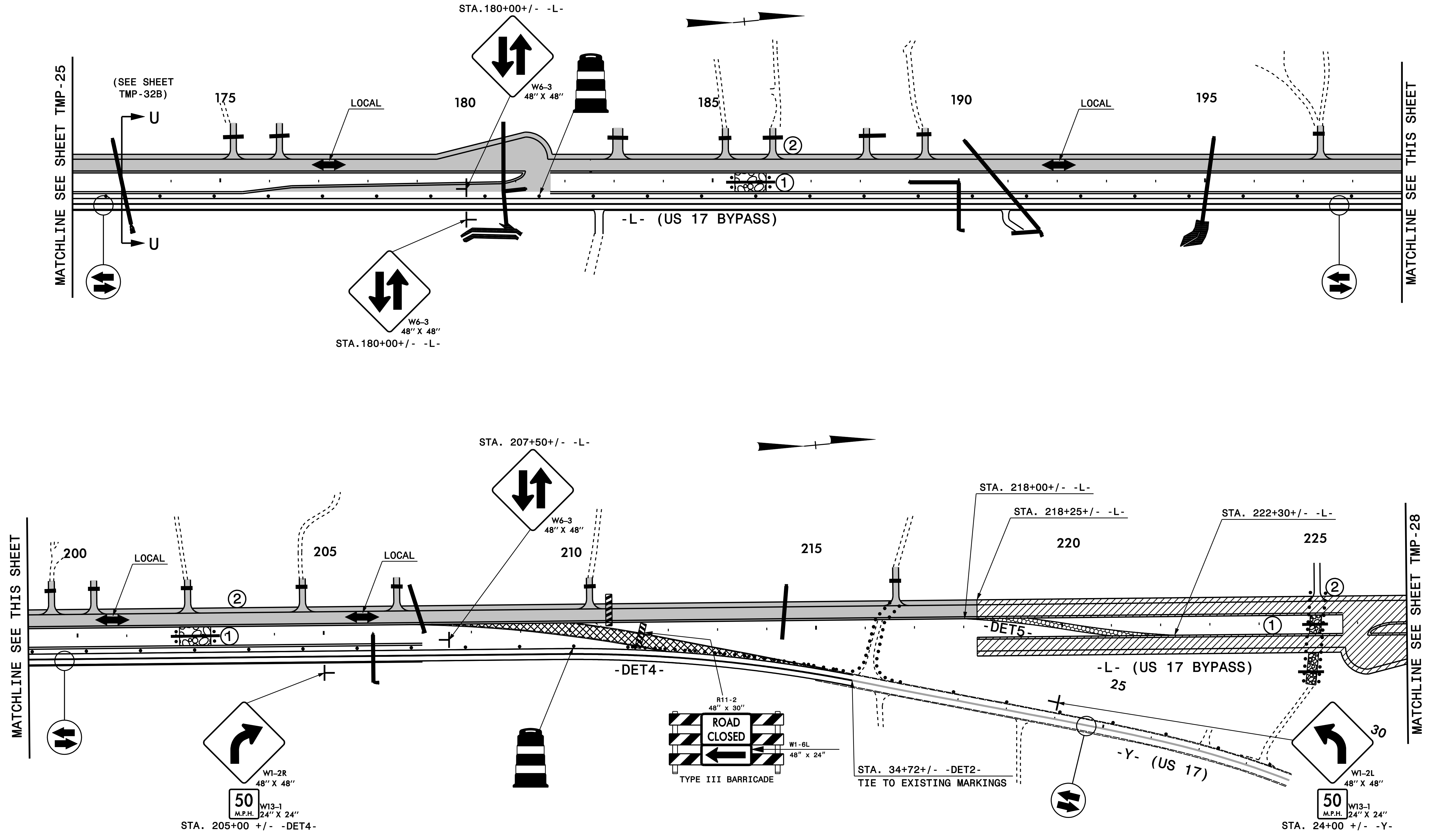
NOTES:
 1. INSTALL TEMPORARY 18" CSP TO MAINTAIN DRAINAGE.
 2. INSTALL INCIDENTAL STONE TO MAINTAIN ACCESS TO ADJACENT PARCELS.

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS	APPROVED: <i>[Signature]</i> DATE: 4/15/2015 SEAL: <i>[Professional Seal]</i> NORTH CAROLINA PROFESSIONAL ENGINEER IM. ARE. 025465	DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL	TRANSPORTATION MANAGEMENT PLAN PHASE II OVERVIEWS
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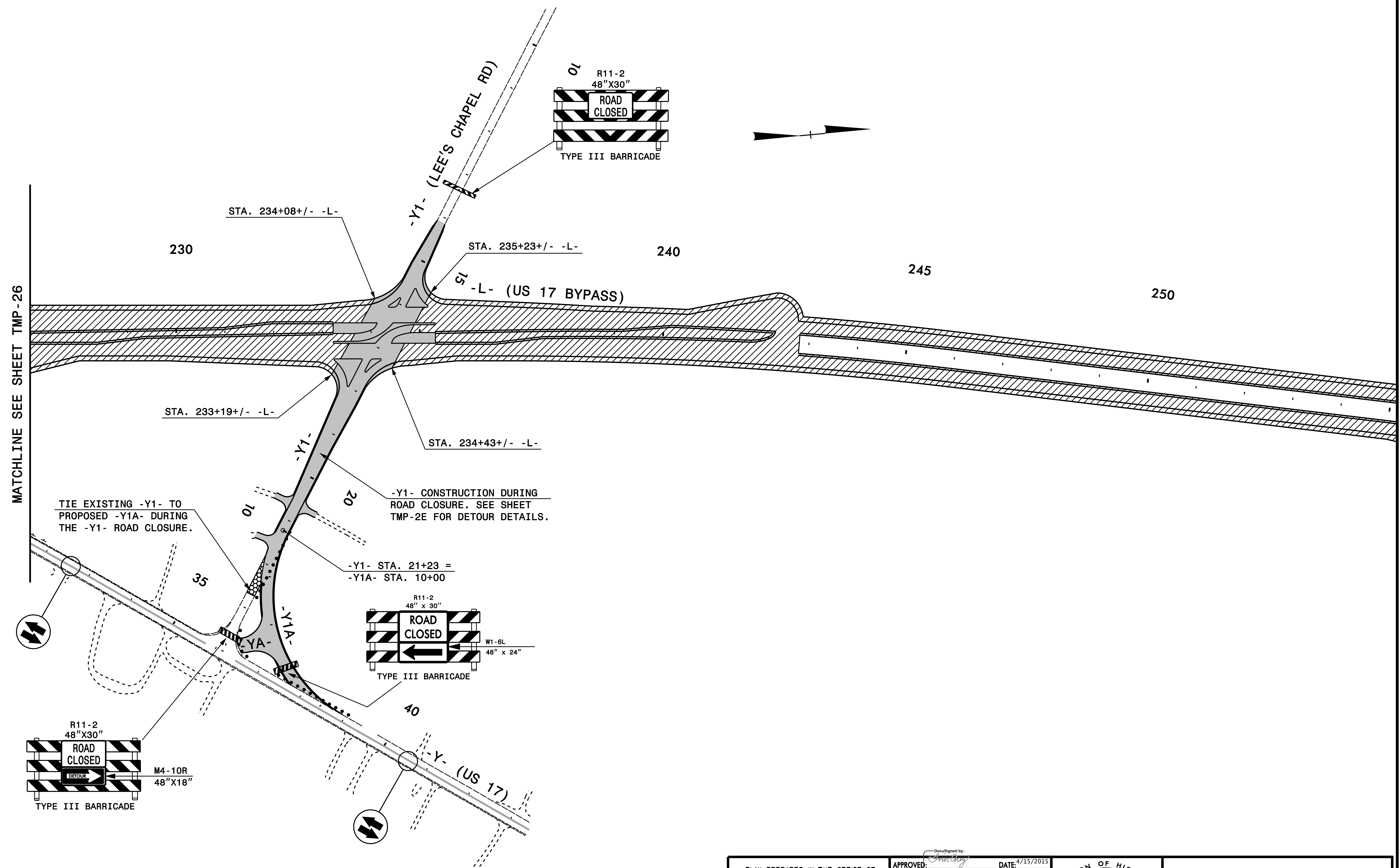
NOTES:
 1. INSTALL TEMPORARY 18" CSP TO MAINTAIN DRAINAGE.
 2. INSTALL INCIDENTAL STONE TO MAINTAIN ACCESS TO ADJACENT PARCELS.

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS	APPROVED: <i>[Signature]</i> DATE: 4/15/2015 SEAL: <i>[Professional Seal]</i>	DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL	TRANSPORTATION MANAGEMENT PLAN PHASE II OVERVIEWS
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NOTES:
 1. INSTALL TEMPORARY 18" CSP TO MAINTAIN DRAINAGE.
 2. INSTALL INCIDENTAL STONE TO MAINTAIN ACCESS TO ADJACENT PARCELS.

<p>PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS</p>	<p>APPROVED: DATE: 4/15/2015 SEAL</p>		<p>TRANSPORTATION MANAGEMENT PLAN PHASE II OVERVIEWS</p>
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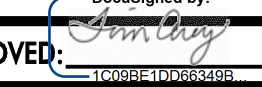
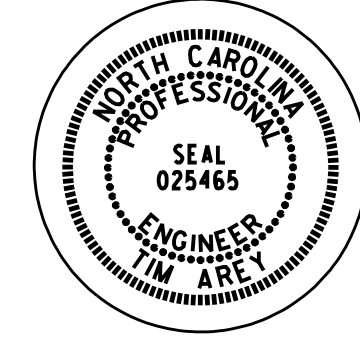
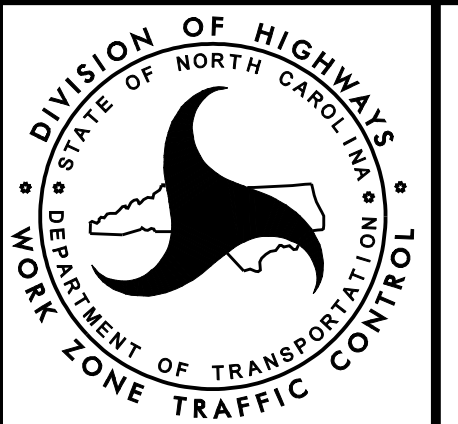


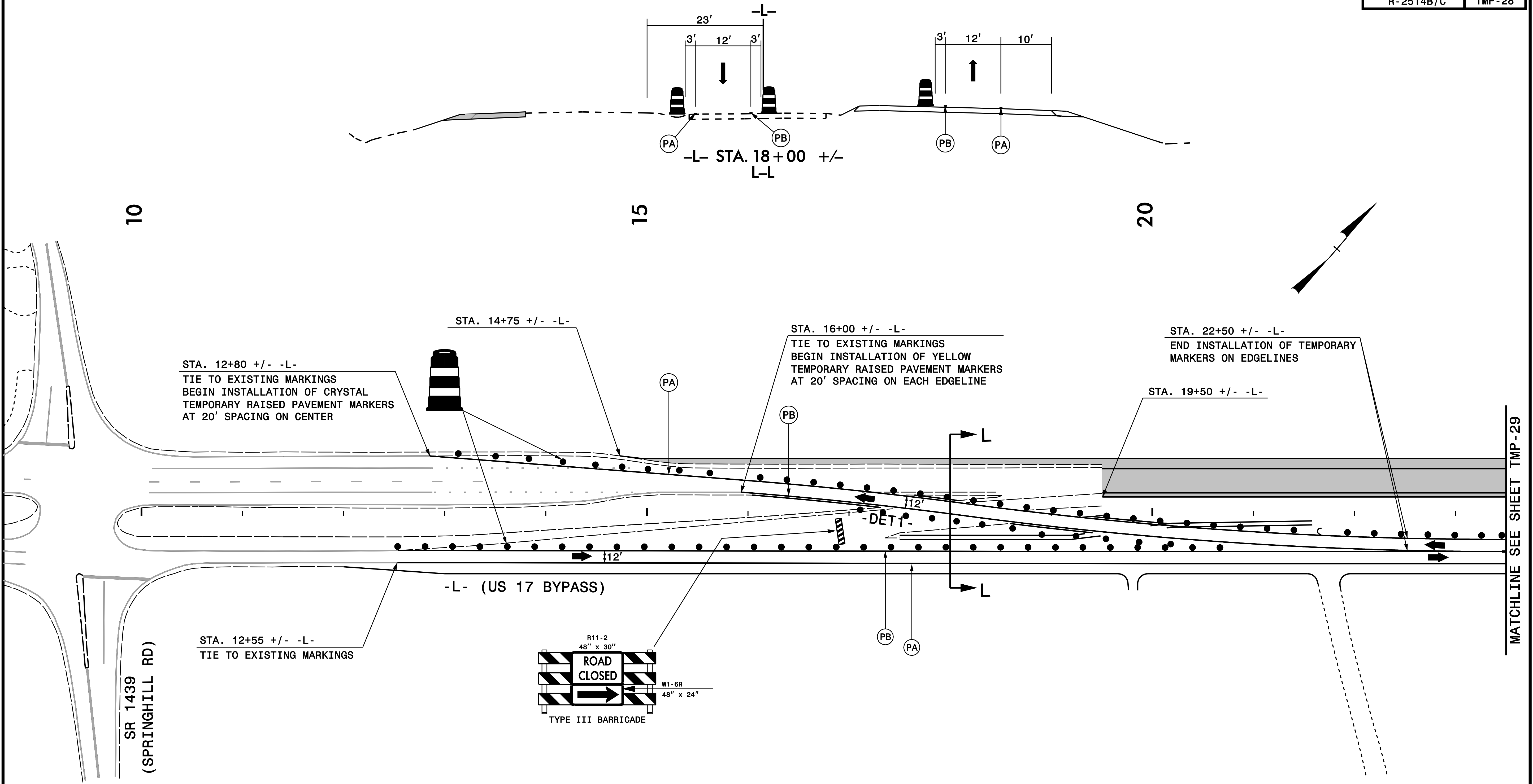
MATCHLINE SEE SHEET TMP-26

TIE EXISTING -Y1- TO PROPOSED -Y1A- DURING THE -Y1- ROAD CLOSURE.

-Y1- CONSTRUCTION DURING ROAD CLOSURE. SEE SHEET TMP-2E FOR DETOUR DETAILS.

-Y1- STA. 21+23 =
-Y1A- STA. 10+00

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> 		<p>TRANSPORTATION MANAGEMENT PLAN PHASE II OVERVIEWS</p>
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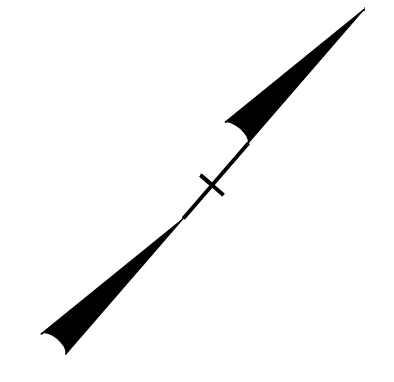
NOTE:
INSTALL TEMPORARY RAISED PAVEMENT MARKERS
ALONG THE PHASE II TRAFFIC PATTERN IN ACCORDANCE
WITH ROADWAY STANDARD DRAWINGS #1250.01 & #1251.01
FROM STA. 12+55 +/- -L- TO STA. 215+75 +/- -L-.

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS	APPROVED: DATE: 4/15/2015			TRANSPORTATION MANAGEMENT PLAN PHASE II DETAILS
	SEAL			

25

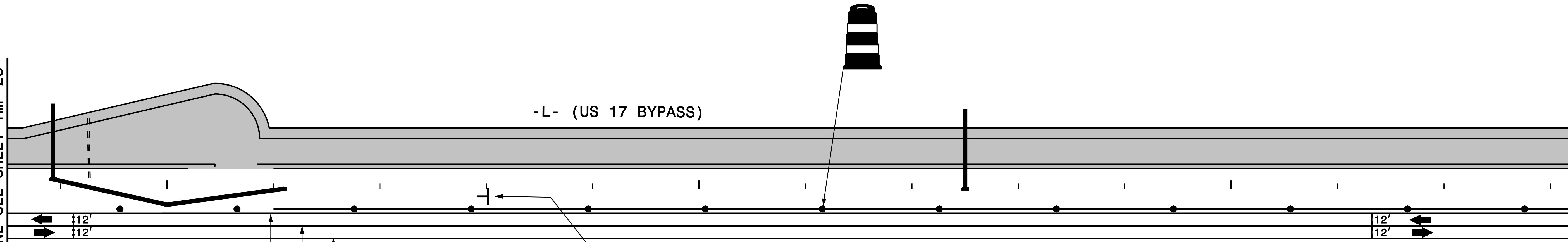
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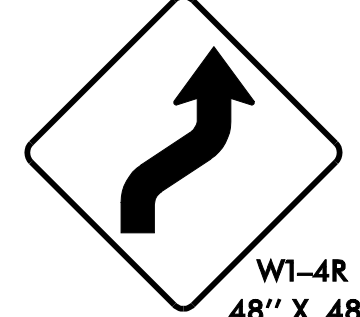
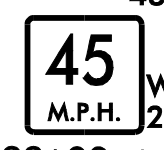
MATCHLINE SEE SHEET TMP-28

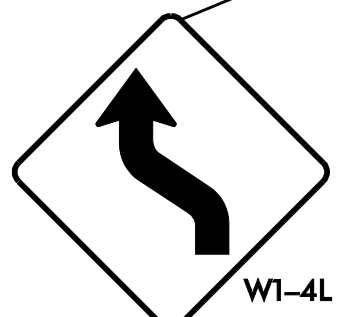
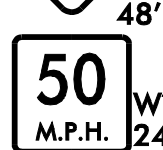
MATCHLINE SEE SHEET TMP-30



-L- (US 17 BYPASS)

PA
PI
PA

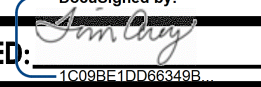

 W1-4R
 48" X 48"

 W13-1
 24" X 24"
 STA. 28+00 +/- -L-


 W1-4L
 48" X 48"

 W13-1
 24" X 24"
 STA. 38+00 +/- -L-


PLAN PREPARED IN THE OFFICE OF:

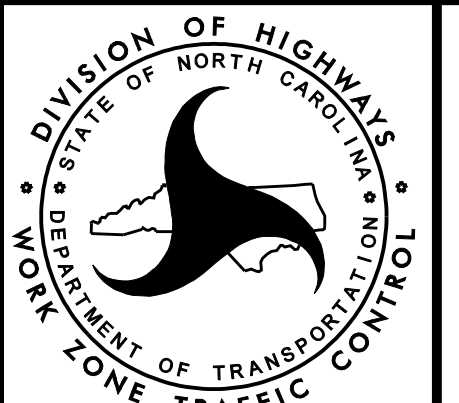
PROGRESSIVE
DESIGN GROUP, INC.

ENGINEERS • CONSULTANTS

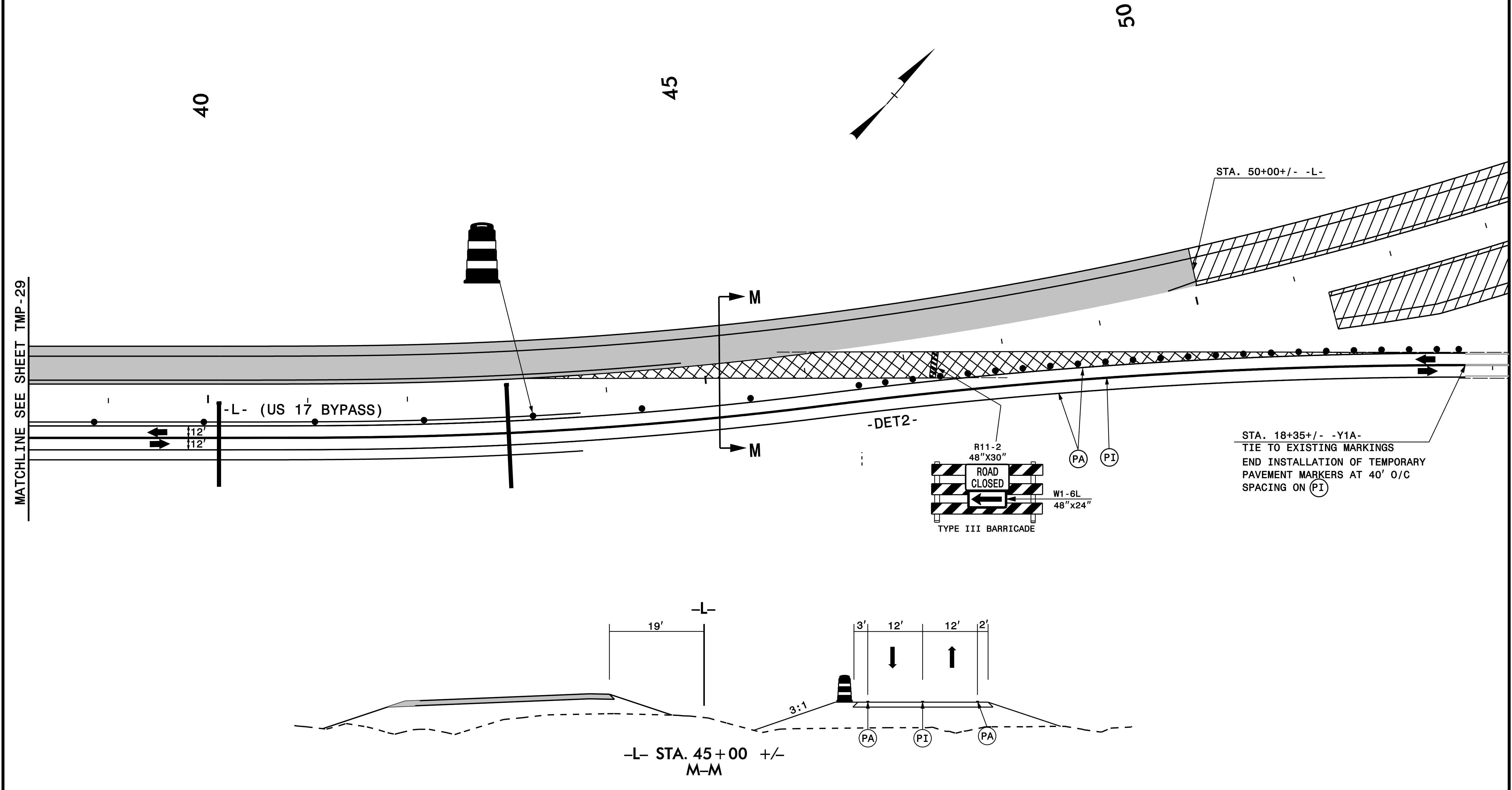
APPROVED:  DATE: 4/15/2015

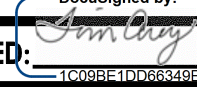

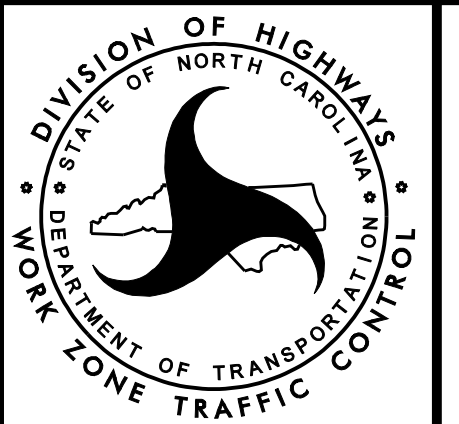
SEAL

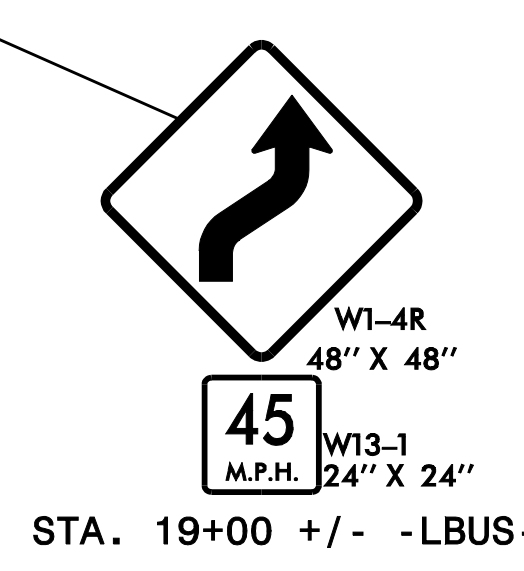
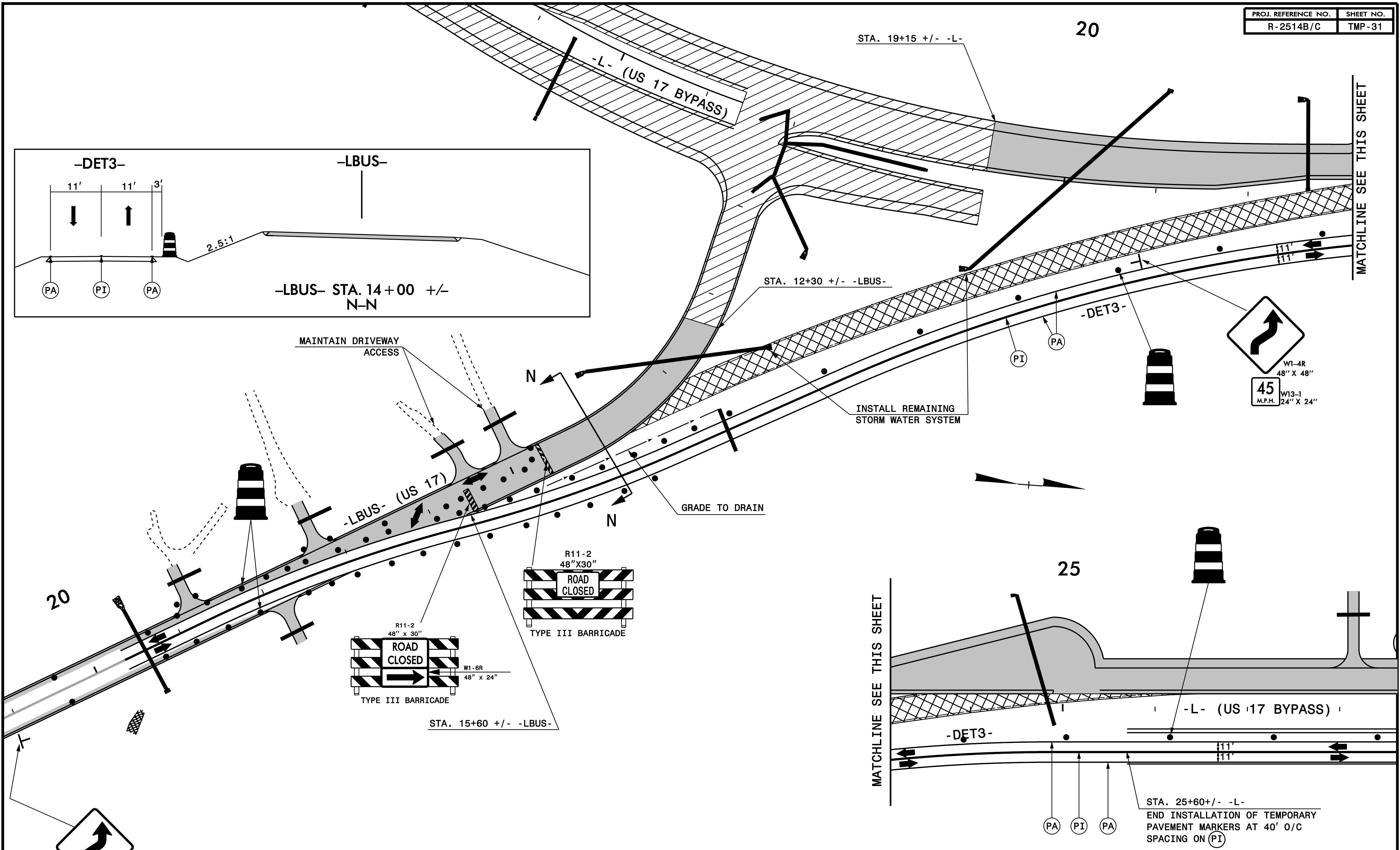
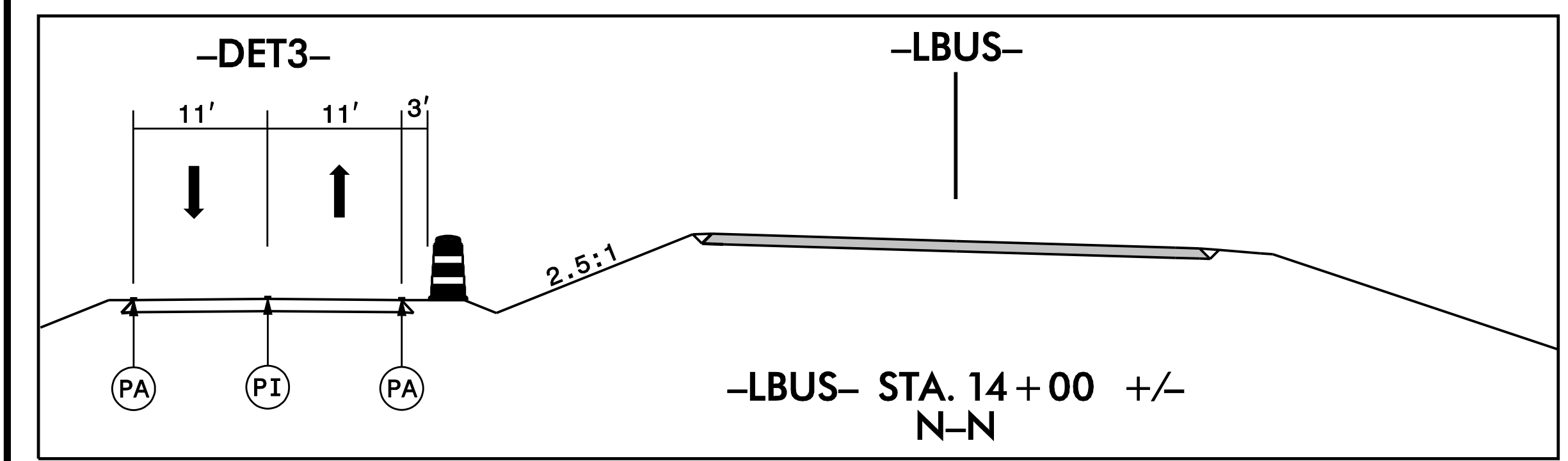




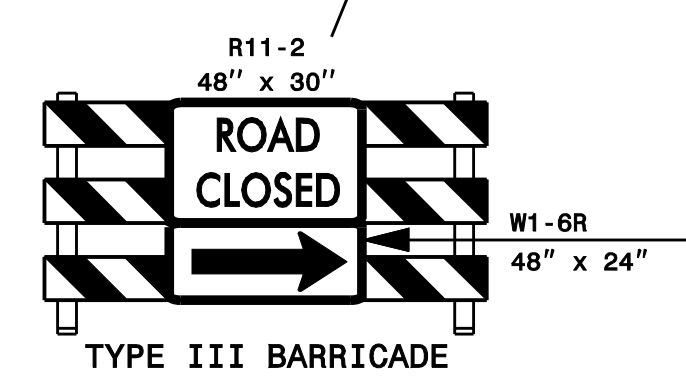
TRANSPORTATION
MANAGEMENT PLAN
PHASE II DETAILS



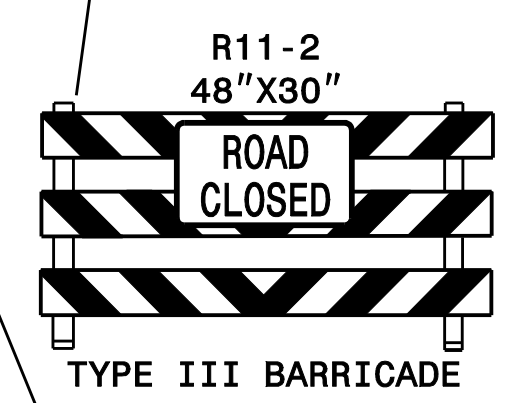
<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> <p></p>	<p></p> <p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE II DETAILS</p>
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STA. 19+00 +/- -LBUS-

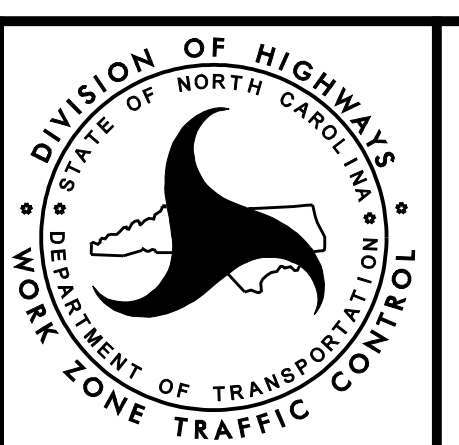


STA. 15+60 +/- -LBUS-



PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE DESIGN GROUP, INC.
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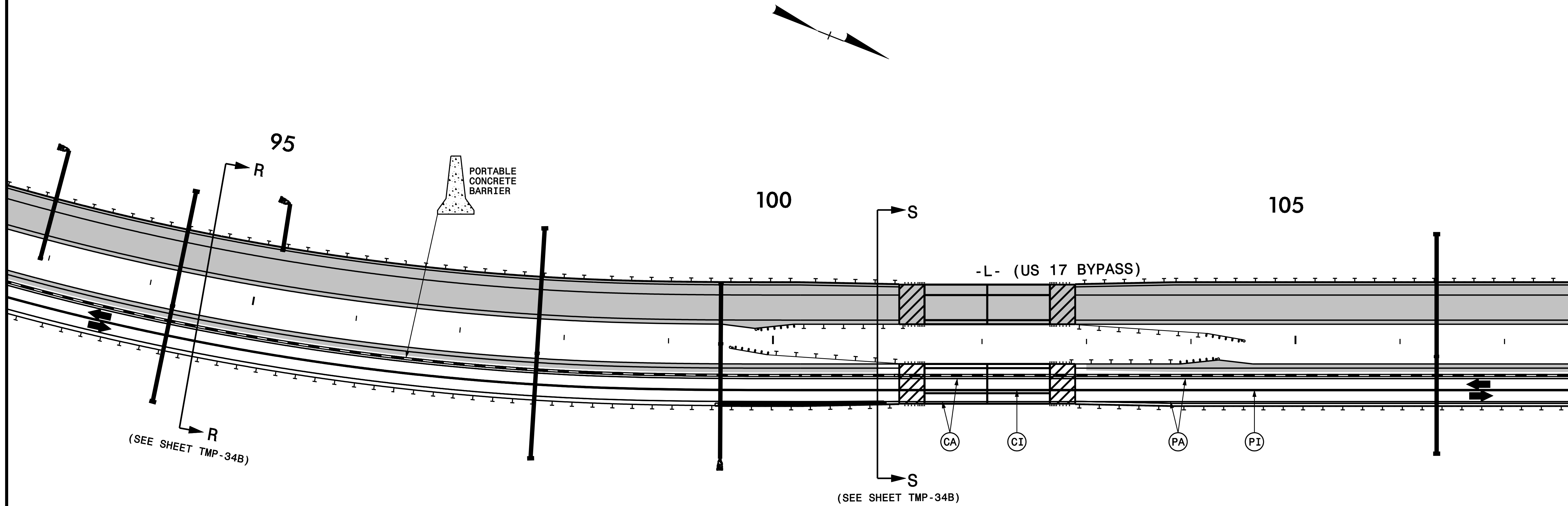
APPROVED: [Signature] DATE: 4/15/2015
SEAL
NORTH CAROLINA PROFESSIONAL ENGINEER
TIM ARY



**TRANSPORTATION MANAGEMENT PLAN
PHASE II DETAILS**

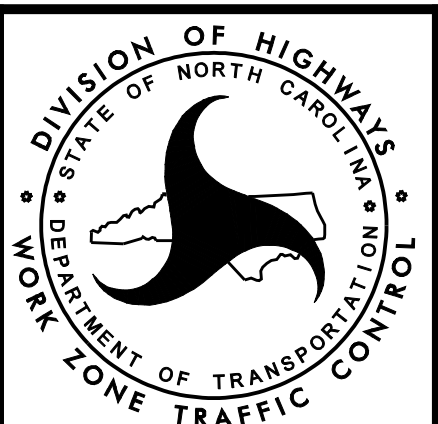
MATCHLINE SEE THIS SHEET

MATCHLINE SEE THIS SHEET

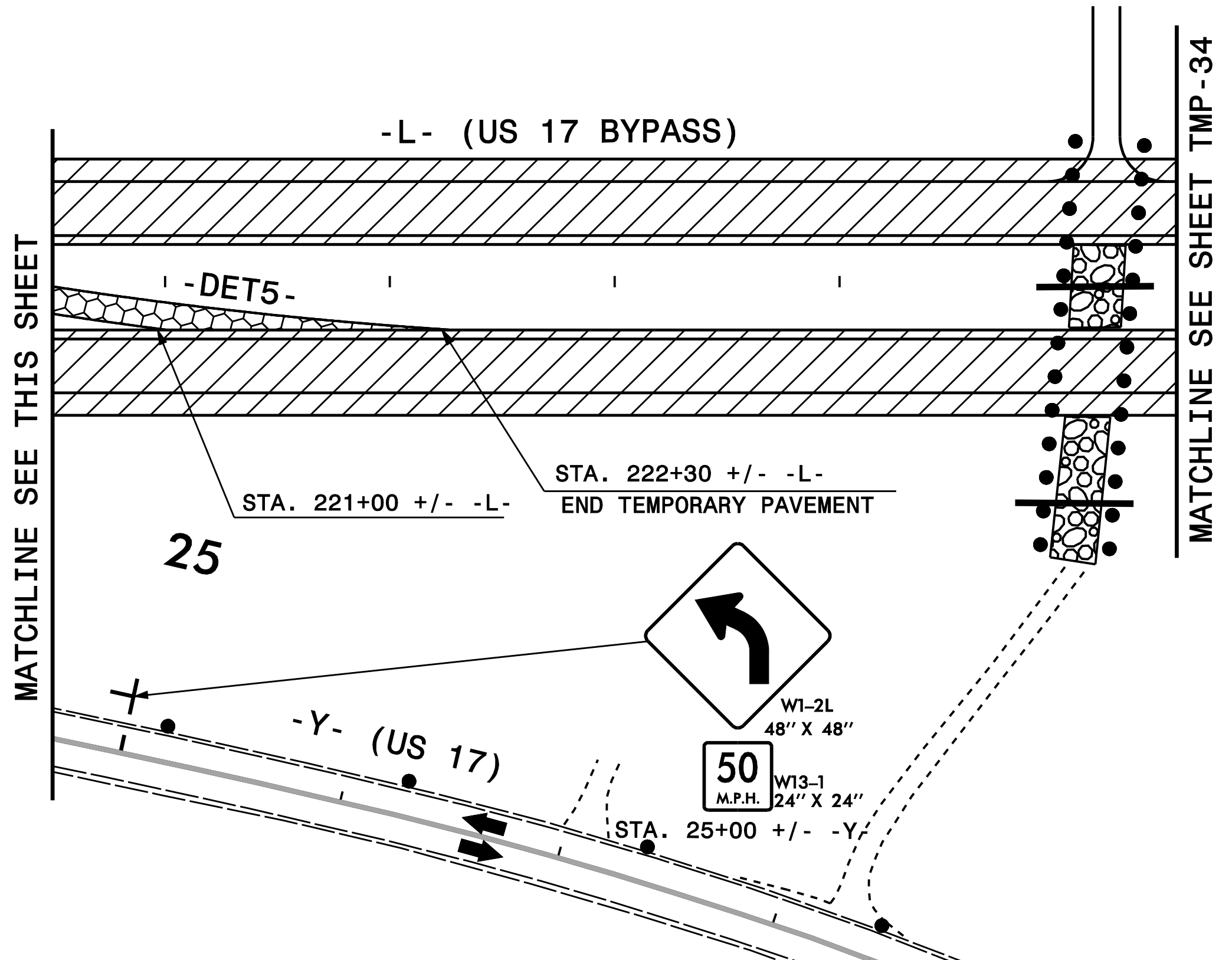
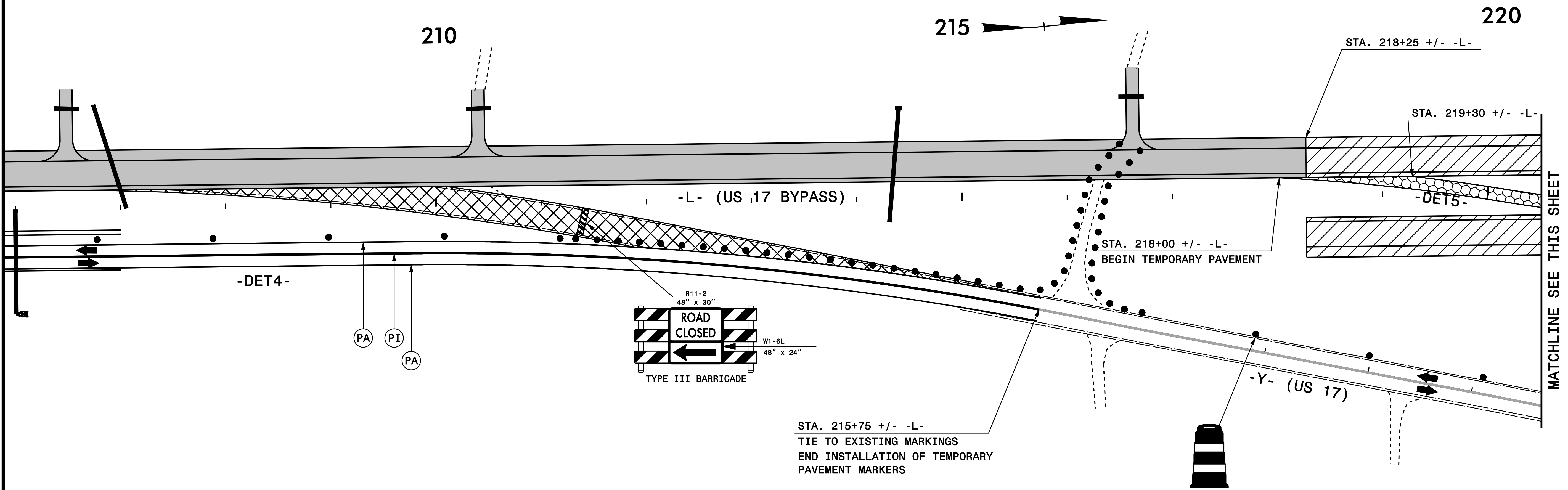


PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE
 DESIGN GROUP, INC.
 ENGINEERS • CONSULTANTS

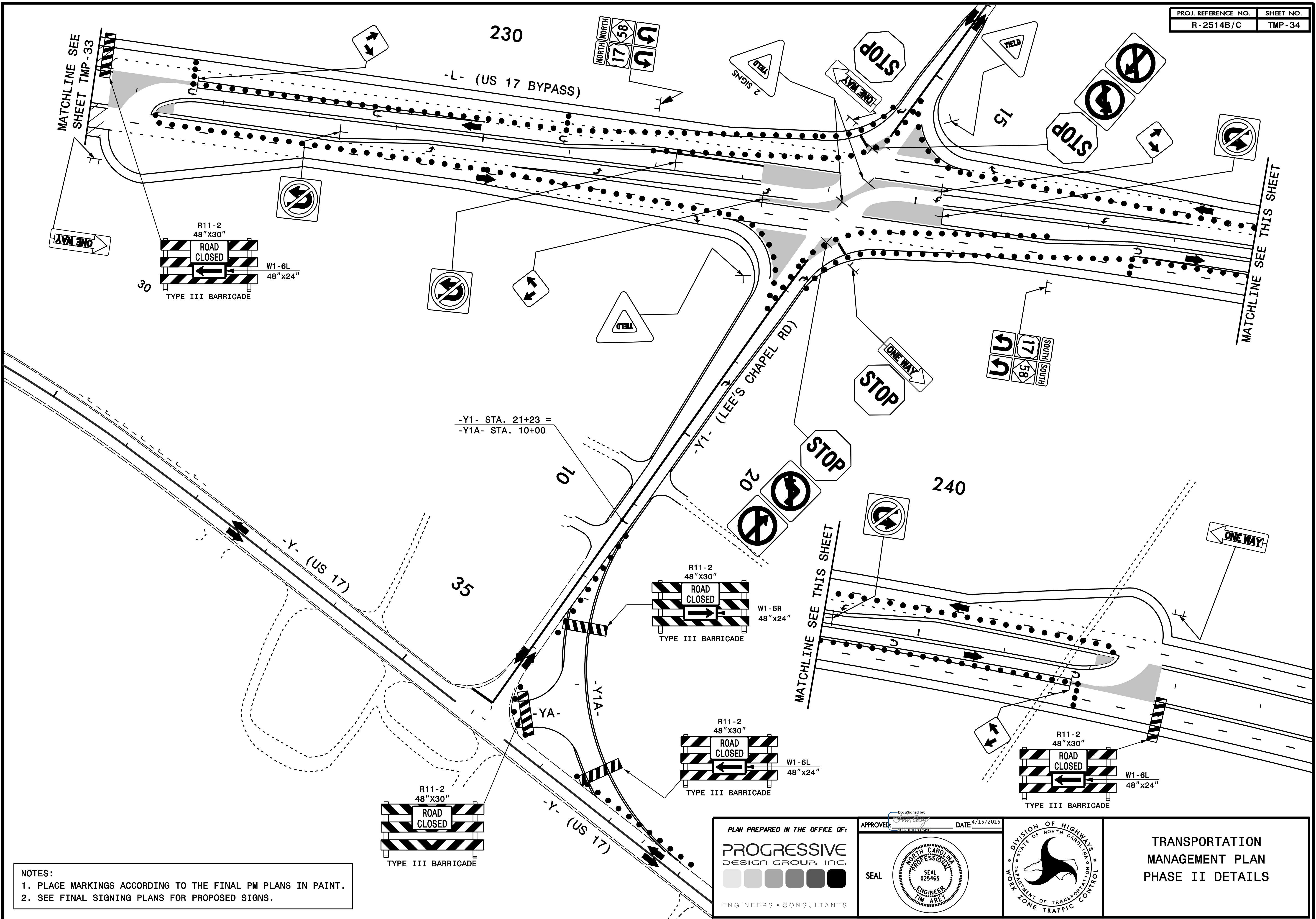
APPROVED: *[Signature]* DATE: 4/15/2015
 SEAL
 NORTH CAROLINA
 PROFESSIONAL
 ENGINEER
 TIM ARRY



TRANSPORTATION
 MANAGEMENT PLAN
 PHASE II DETAILS

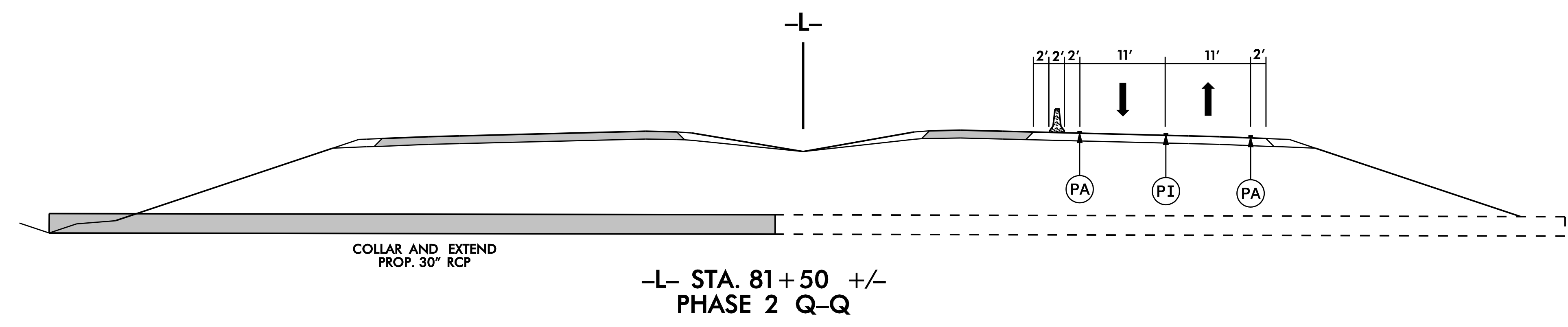
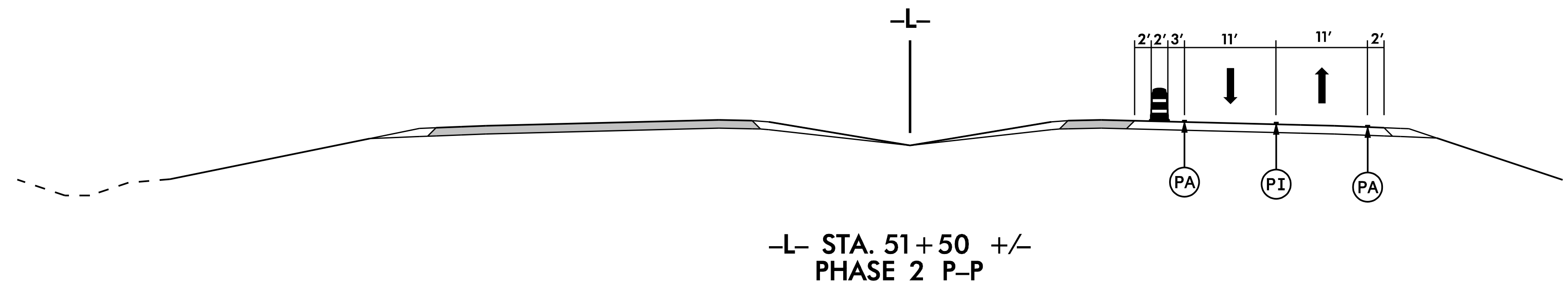
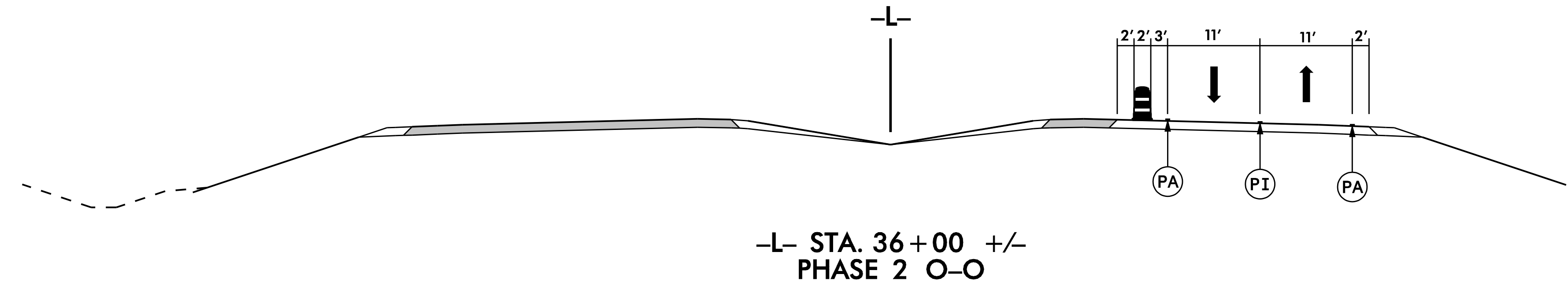


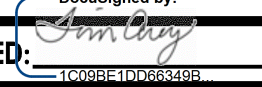
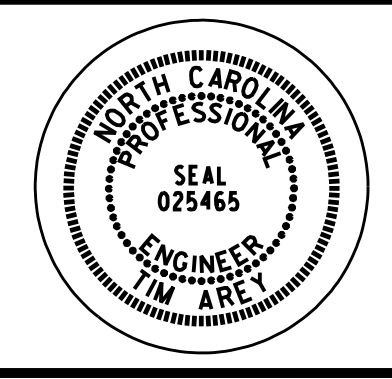

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS	APPROVED: <i>[Signature]</i> DATE: 4/15/2015	DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL	TRANSPORTATION MANAGEMENT PLAN PHASE II DETAILS
	SEAL: <i>[Professional Seal]</i>		

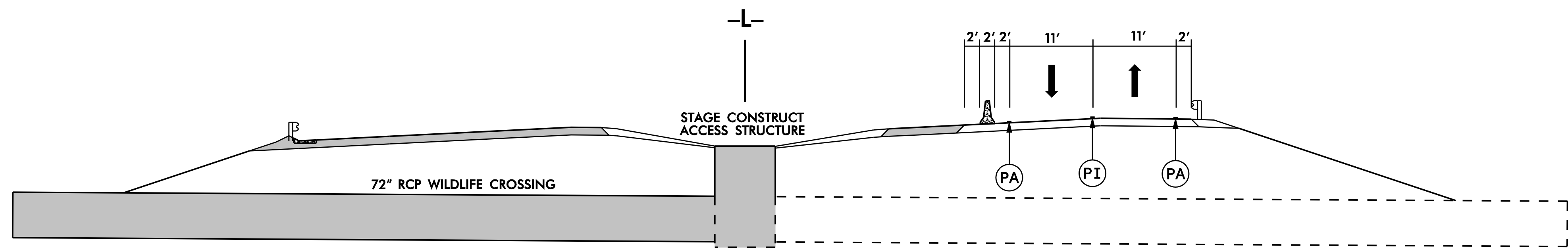


NOTES:
 1. PLACE MARKINGS ACCORDING TO THE FINAL PM PLANS IN PAINT.
 2. SEE FINAL SIGNING PLANS FOR PROPOSED SIGNS.

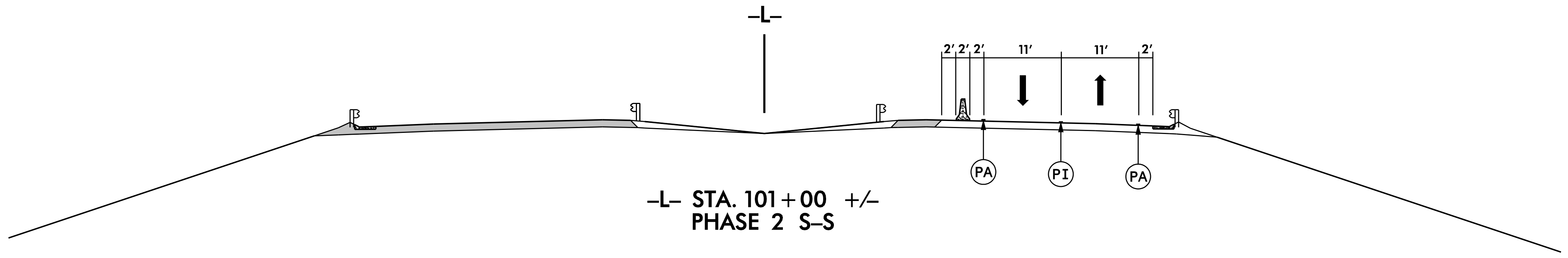
<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>NORTH CAROLINA PROFESSIONAL ENGINEER TIM ARY</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE II DETAILS</p>
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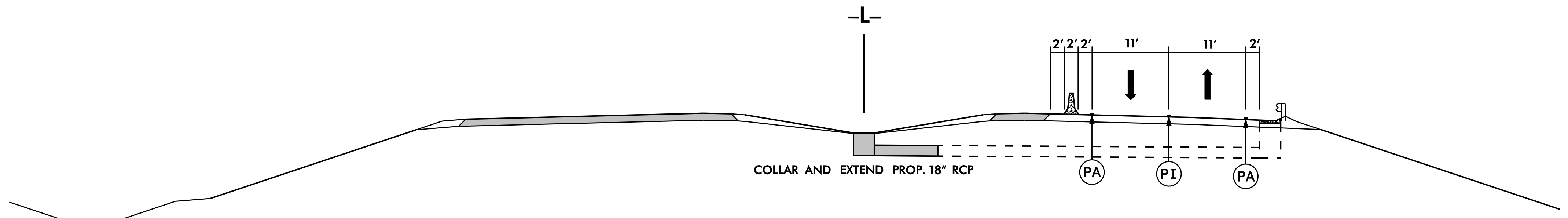
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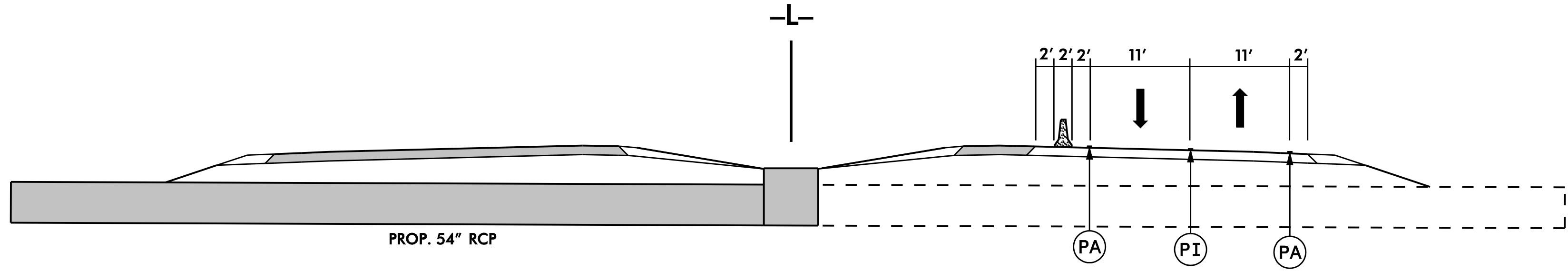
-L- STA. 94+50 +/-
PHASE 2 R-R




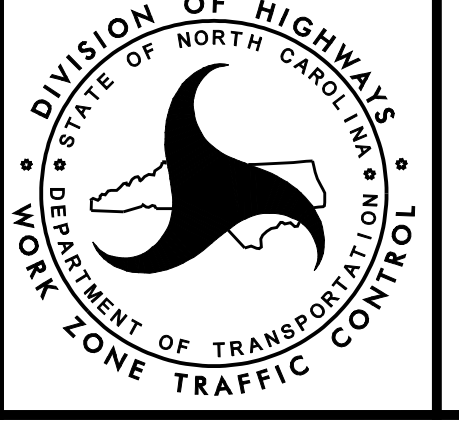
-L- STA. 101+00 +/-
PHASE 2 S-S

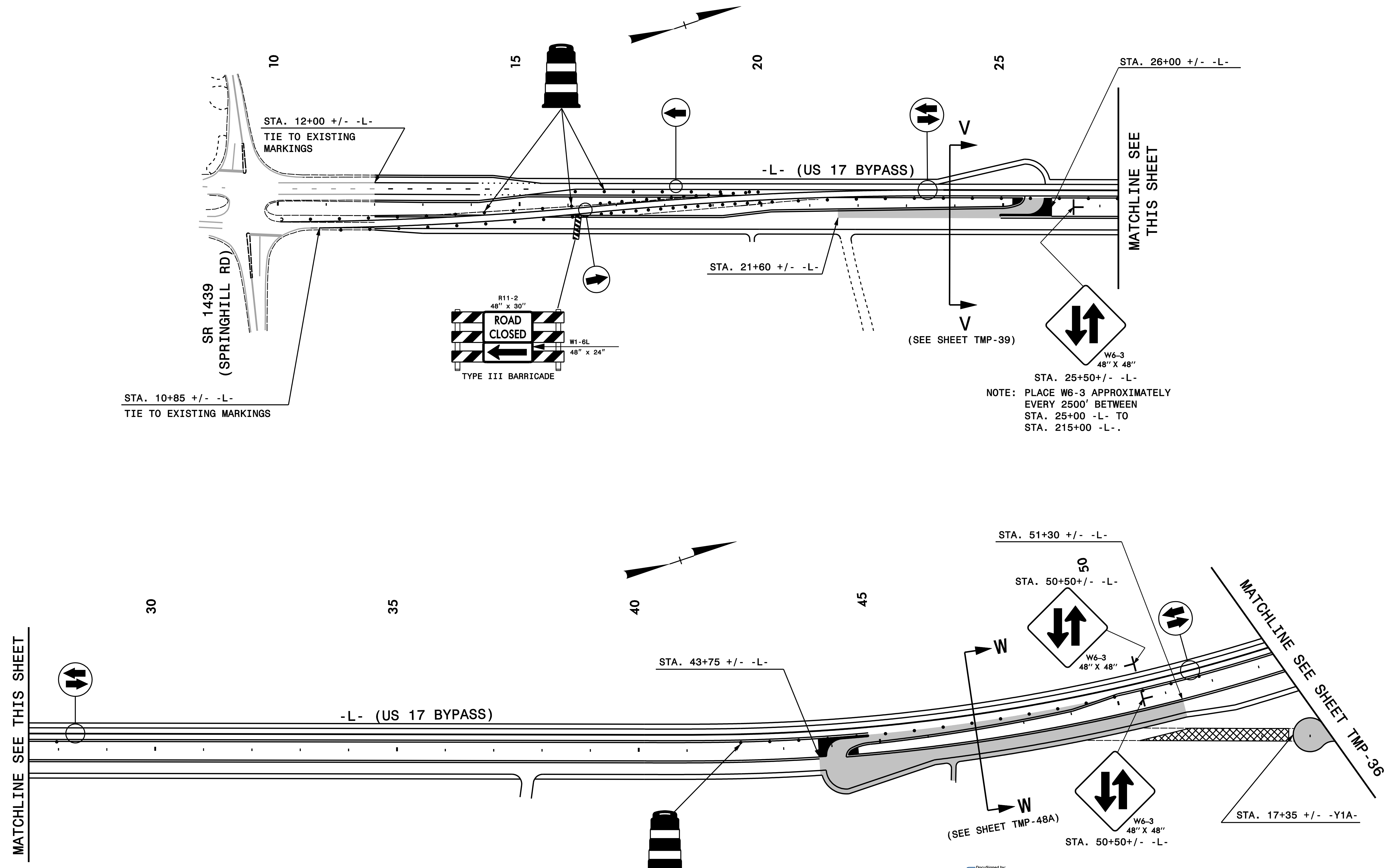


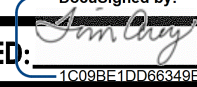
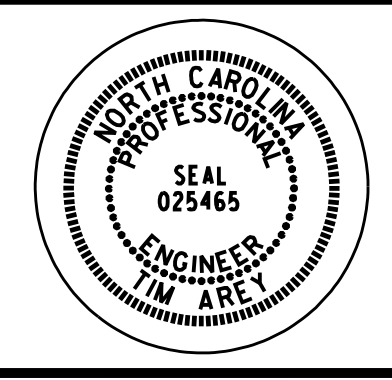
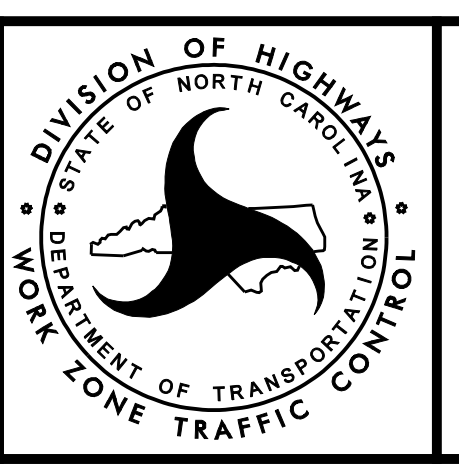
-L- STA. 117+00 +/-
PHASE 2 T-T

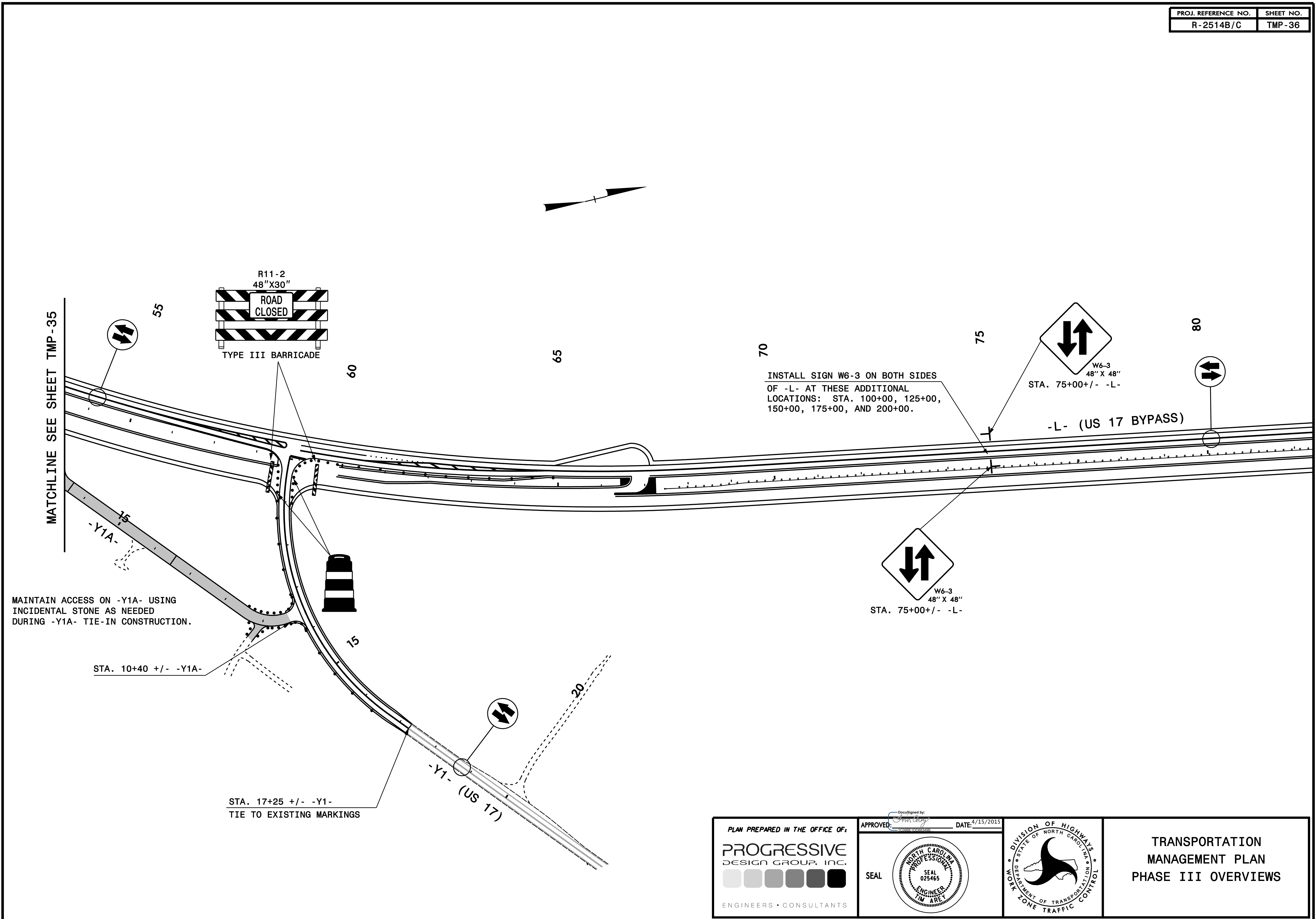


-L- STA. 173+00 +/-
PHASE 2 U-U

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> 		<p>TRANSPORTATION MANAGEMENT PLAN CROSS SECTIONS</p>
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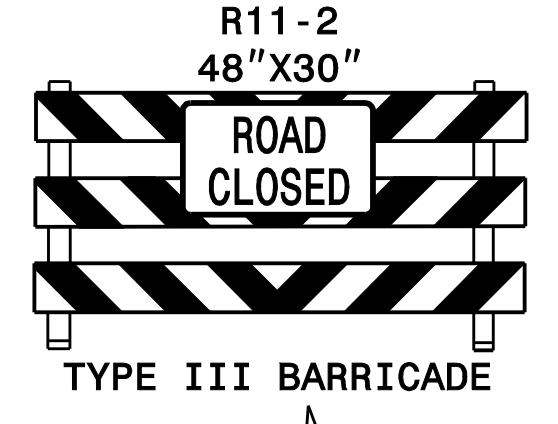


<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> <p></p>	<p></p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE III OVERVIEWS</p>
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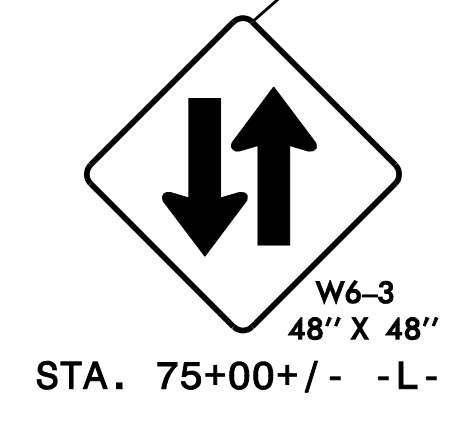
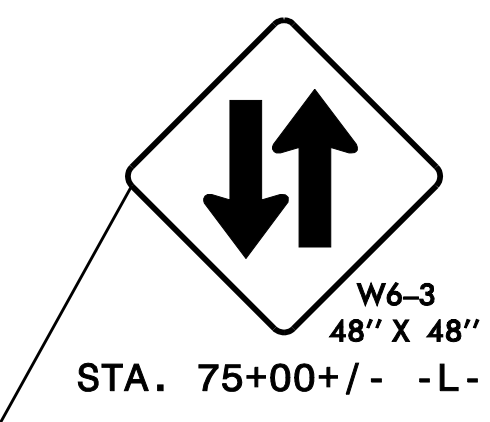


MATCHLINE SEE SHEET TMP-35

MAINTAIN ACCESS ON -Y1A- USING INCIDENTAL STONE AS NEEDED DURING -Y1A- TIE-IN CONSTRUCTION.



INSTALL SIGN W6-3 ON BOTH SIDES OF -L- AT THESE ADDITIONAL LOCATIONS: STA. 100+00, 125+00, 150+00, 175+00, AND 200+00.



STA. 10+40 +/- -Y1A-

STA. 17+25 +/- -Y1-
TIE TO EXISTING MARKINGS

-Y1- (US 17)

PLAN PREPARED IN THE OFFICE OF:

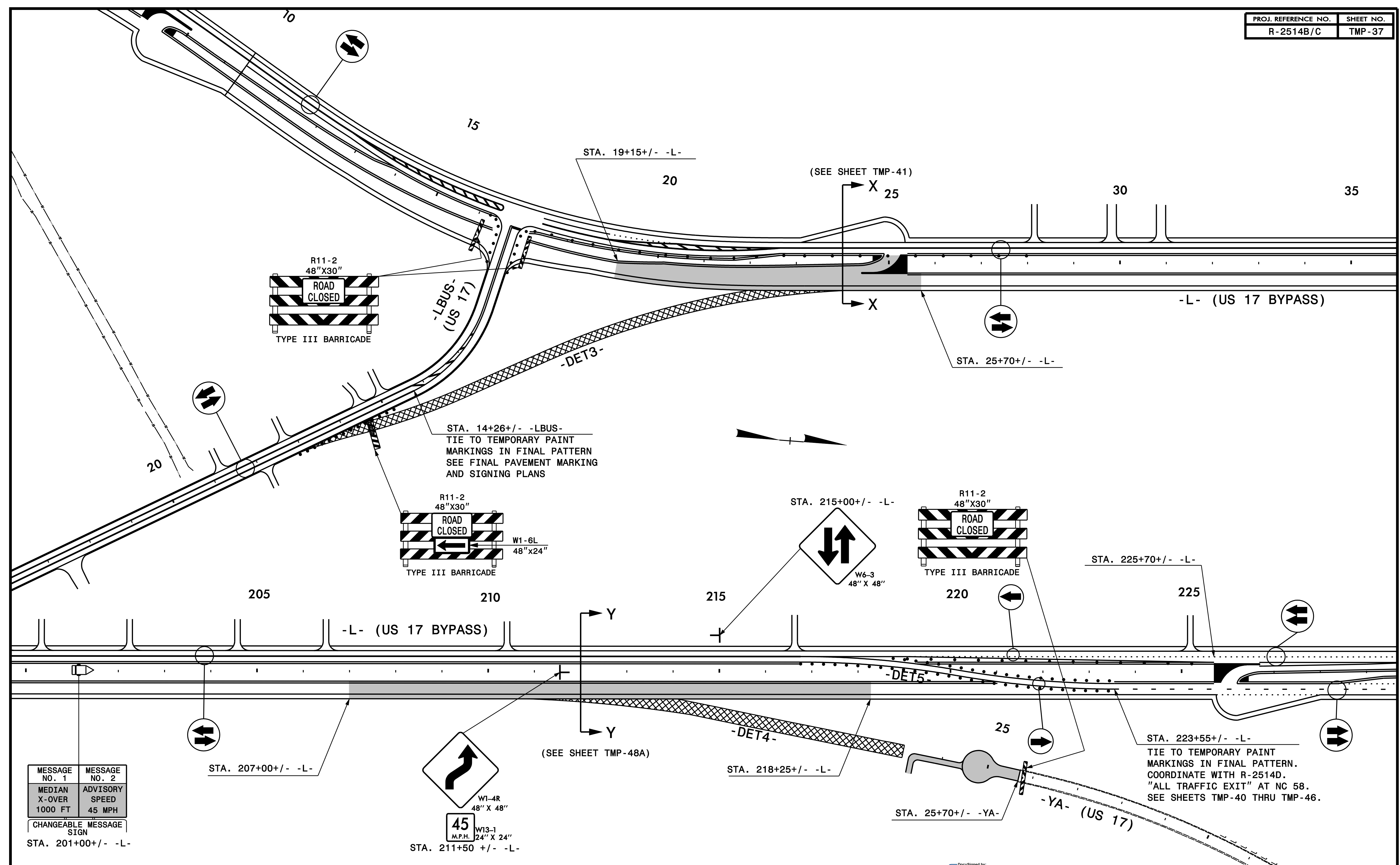
PROGRESSIVE
DESIGN GROUP, INC.

ENGINEERS • CONSULTANTS

APPROVED: *[Signature]* DATE: 4/15/2015

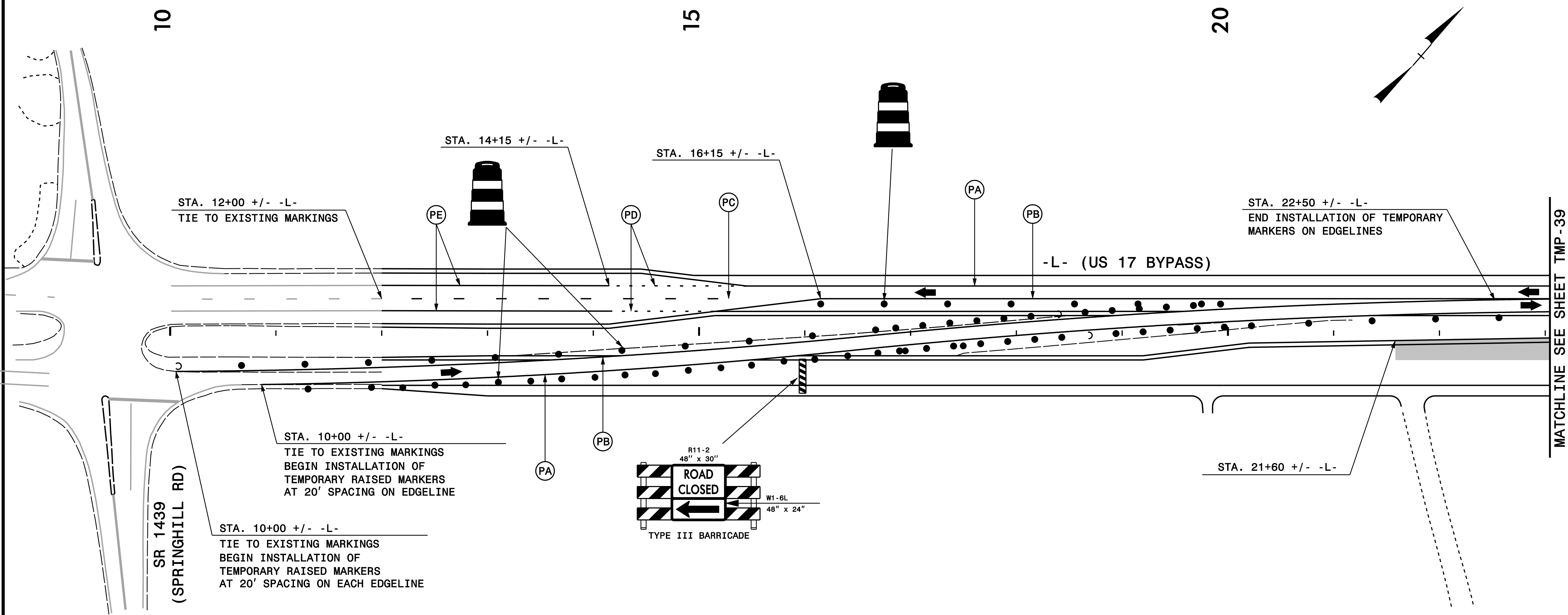
SEAL

TRANSPORTATION
MANAGEMENT PLAN
PHASE III OVERVIEWS



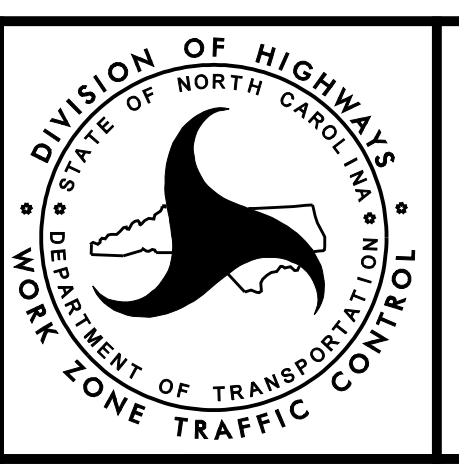
MESSAGE NO. 1	MESSAGE NO. 2
MEDIAN X-OVER 1000 FT	ADVISORY SPEED 45 MPH
CHANGEABLE MESSAGE SIGN	
STA. 201+00+/- -L-	

PLAN PREPARED IN THE OFFICE OF: PROGRESSIVE DESIGN GROUP, INC. ENGINEERS • CONSULTANTS	APPROVED: DATE: 4/15/2015			TRANSPORTATION MANAGEMENT PLAN PHASE III OVERVIEWS
	SEAL			



PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE
DESIGN GROUP, INC.
ENGINEERS • CONSULTANTS

APPROVED: *[Signature]* DATE: 4/15/2015
SEAL
NORTH CAROLINA
PROFESSIONAL
ENGINEER
TIM ARLEY

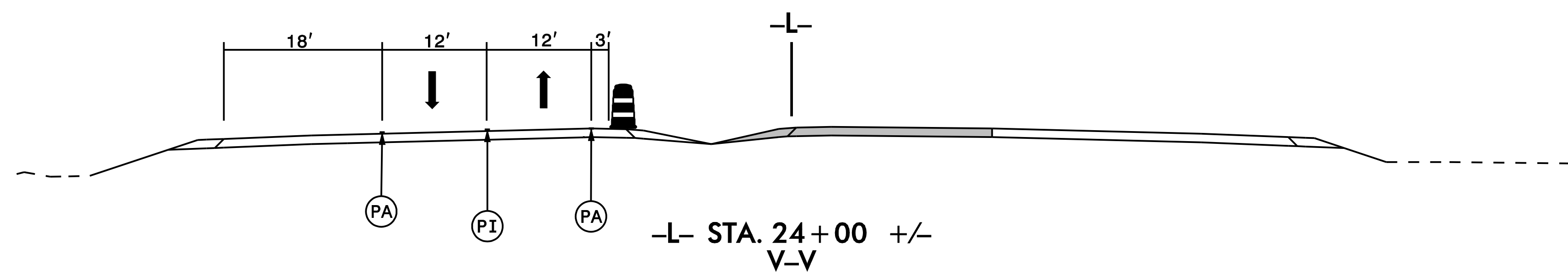
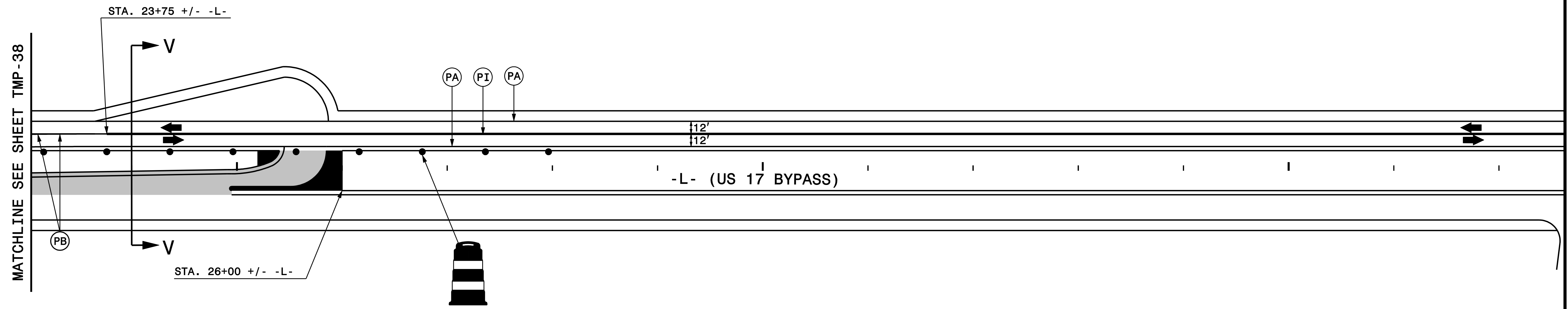
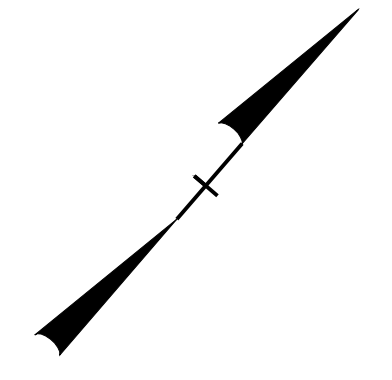


TRANSPORTATION
MANAGEMENT PLAN
PHASE III DETAILS

25

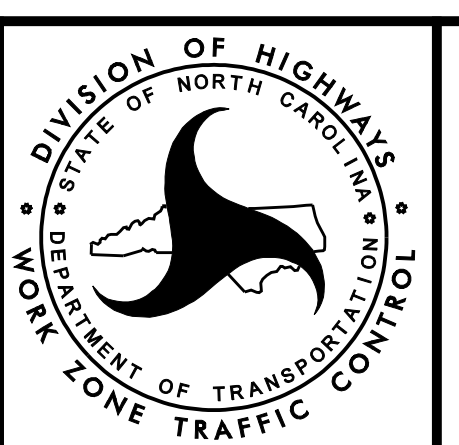
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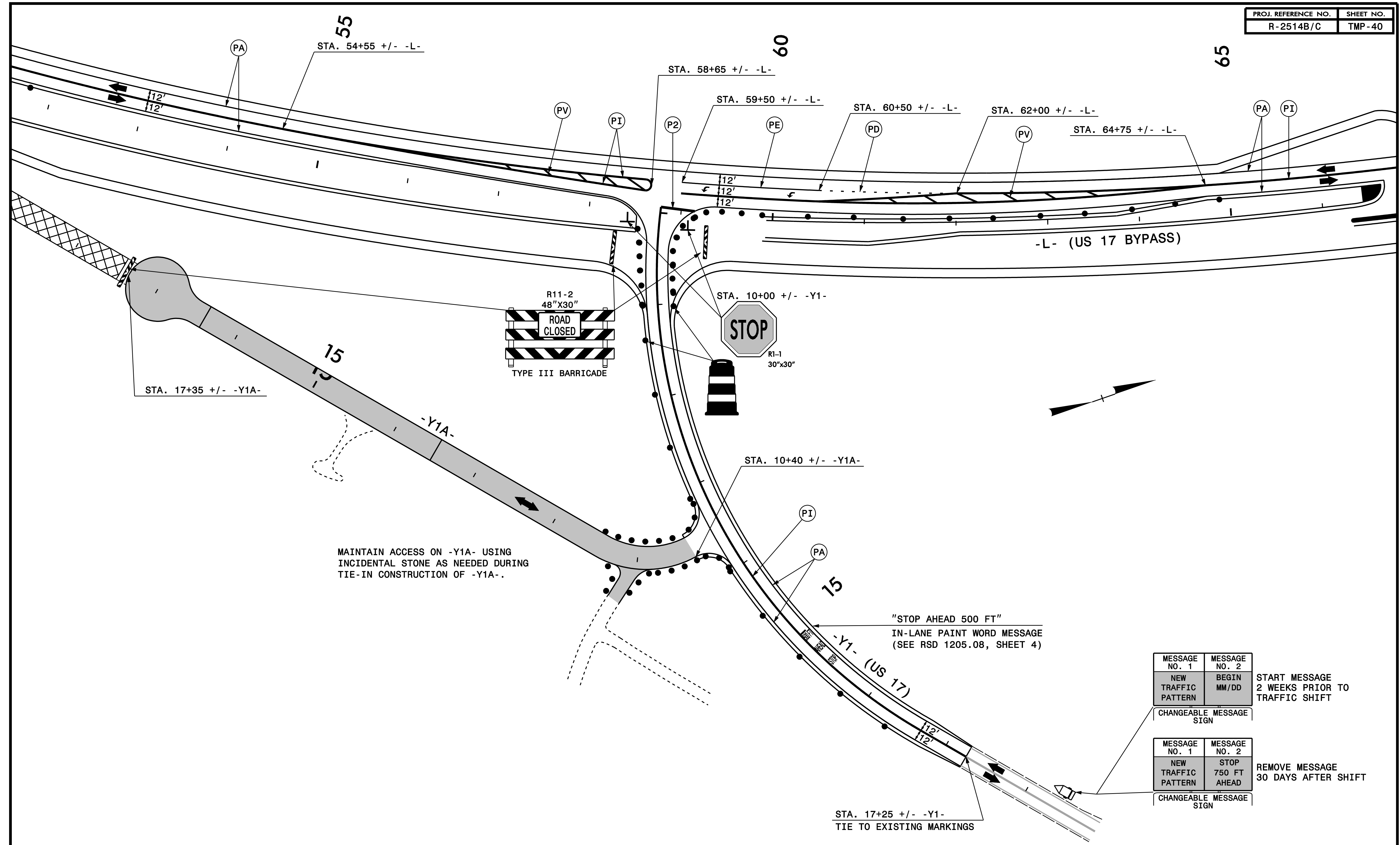


PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE
DESIGN GROUP, INC.
ENGINEERS • CONSULTANTS

APPROVED: *[Signature]* DATE: 4/15/2015
SEAL
NORTH CAROLINA PROFESSIONAL ENGINEER
TIM ARY



TRANSPORTATION
MANAGEMENT PLAN
PHASE III DETAILS



MAINTAIN ACCESS ON -Y1A- USING INCIDENTAL STONE AS NEEDED DURING TIE-IN CONSTRUCTION OF -Y1A-.

"STOP AHEAD 500 FT"
IN-LANE PAINT WORD MESSAGE
(SEE RSD 1205.08, SHEET 4)

MESSAGE NO. 1	MESSAGE NO. 2	START MESSAGE 2 WEEKS PRIOR TO TRAFFIC SHIFT
NEW TRAFFIC PATTERN	BEGIN MM/DD	
CHANGEABLE MESSAGE SIGN		

MESSAGE NO. 1	MESSAGE NO. 2	REMOVE MESSAGE 30 DAYS AFTER SHIFT
NEW TRAFFIC PATTERN	STOP 750 FT AHEAD	
CHANGEABLE MESSAGE SIGN		

STA. 17+25 +/- -Y1-
TIE TO EXISTING MARKINGS

PLAN PREPARED IN THE OFFICE OF:

PROGRESSIVE DESIGN GROUP, INC.

ENGINEERS • CONSULTANTS

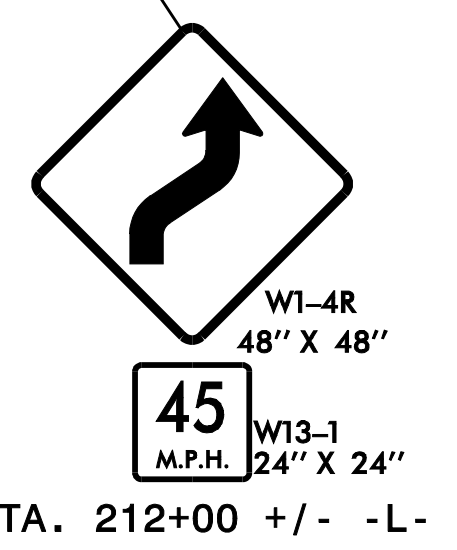
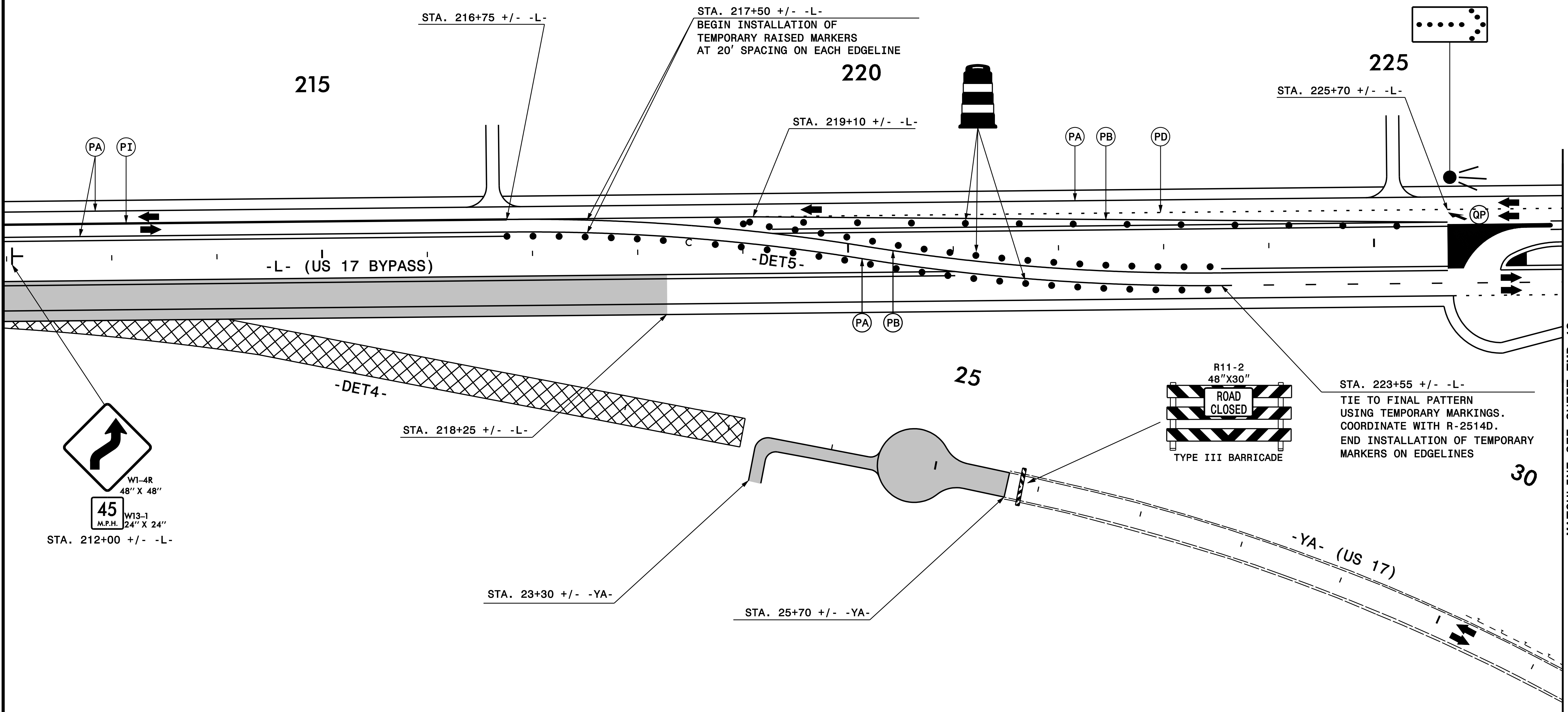
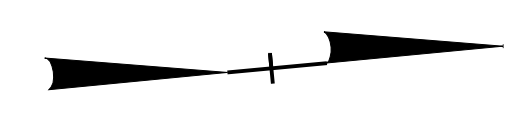
APPROVED: *[Signature]* DATE: 4/15/2015

SEAL

PROFESSIONAL ENGINEER
IM. AR. 1
SEAL 025465

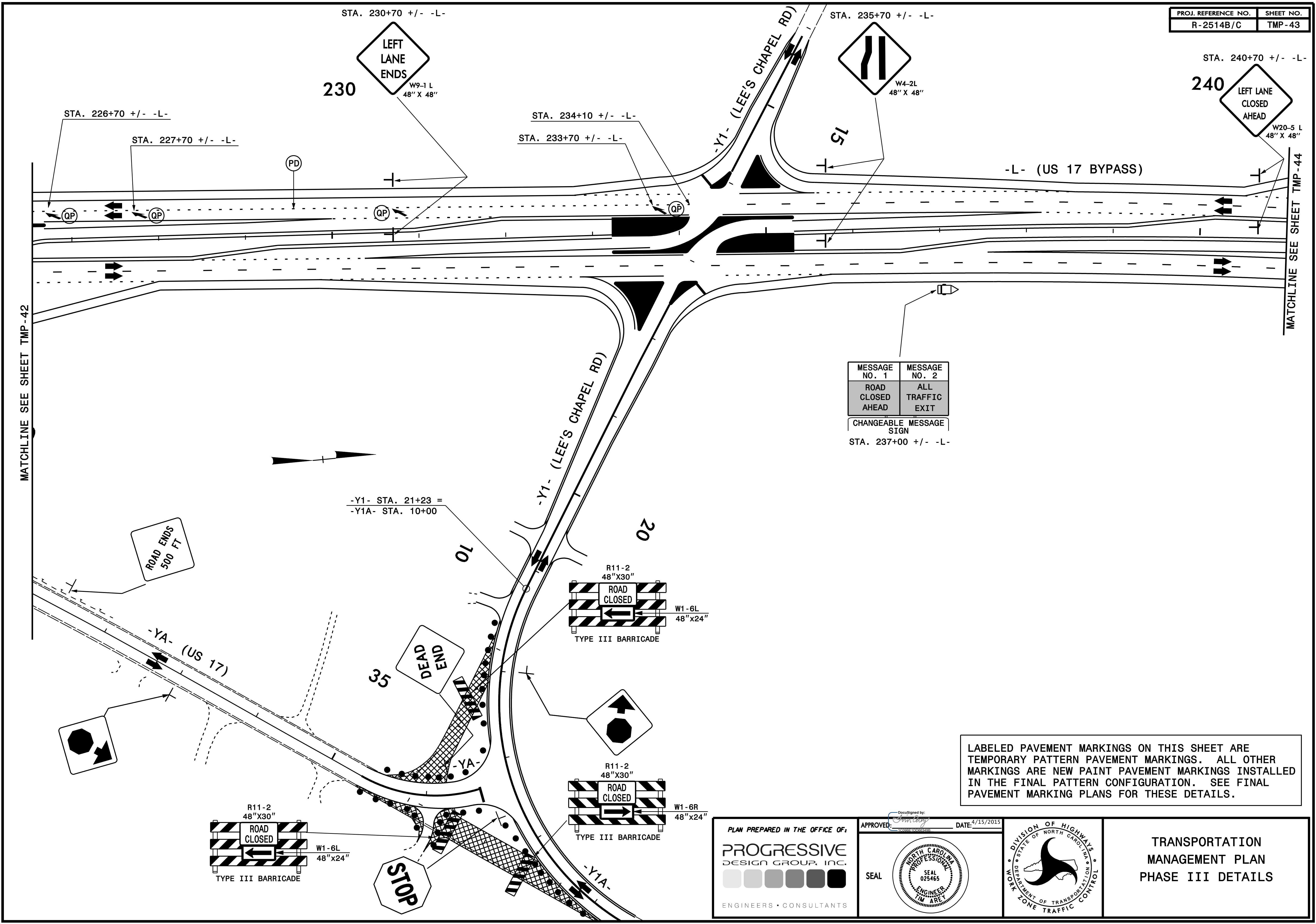
DIVISION OF HIGHWAYS
DEPARTMENT OF TRANSPORTATION
STATE OF NORTH CAROLINA
WORK ZONE TRAFFIC CONTROL

**TRANSPORTATION MANAGEMENT PLAN
PHASE III DETAILS**



MATCHLINE SEE SHEET TMP-43

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p>		<p>TRANSPORTATION MANAGEMENT PLAN PHASE III DETAILS</p>
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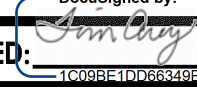
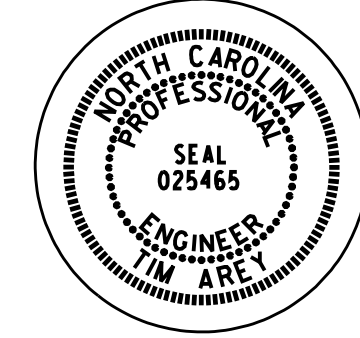
MATCHLINE SEE SHEET TMP-42

MATCHLINE SEE SHEET TMP-44

MESSAGE NO. 1	MESSAGE NO. 2
ROAD CLOSED AHEAD	ALL TRAFFIC EXIT
CHANGEABLE MESSAGE SIGN	
STA. 237+00 +/- -L-	

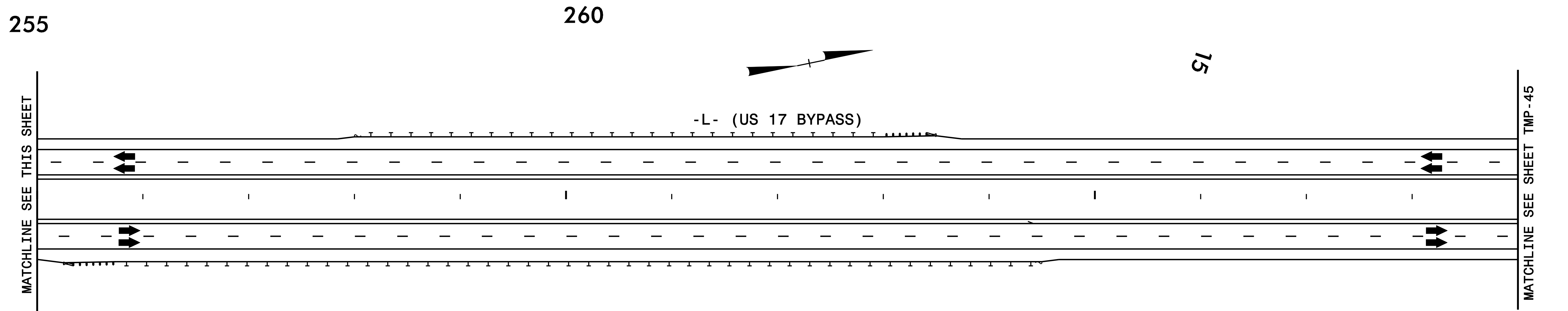
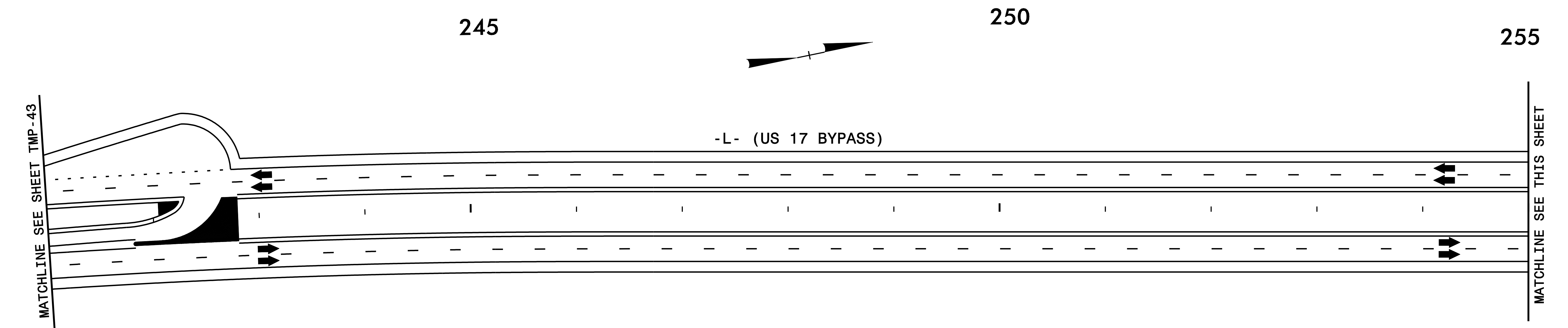
LABELLED PAVEMENT MARKINGS ON THIS SHEET ARE TEMPORARY PATTERN PAVEMENT MARKINGS. ALL OTHER MARKINGS ARE NEW PAINT PAVEMENT MARKINGS INSTALLED IN THE FINAL PATTERN CONFIGURATION. SEE FINAL PAVEMENT MARKING PLANS FOR THESE DETAILS.

PLAN PREPARED IN THE OFFICE OF:
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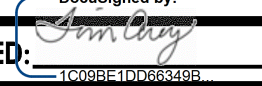
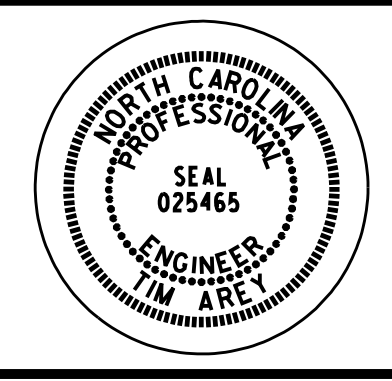
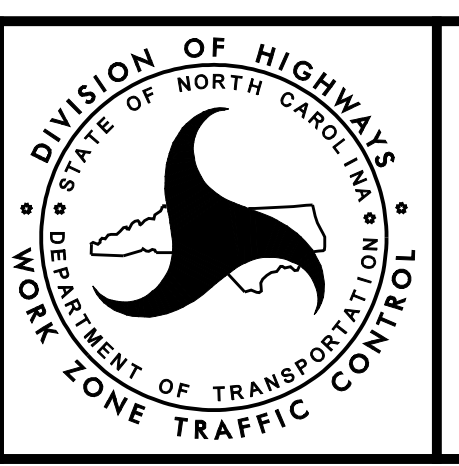
APPROVED:  DATE: 4/15/2015
SEAL: 



TRANSPORTATION MANAGEMENT PLAN
PHASE III DETAILS



ALL PAVEMENT MARKINGS SHOWN ON THIS SHEET ARE NEW PAINT PAVEMENT MARKINGS INSTALLED IN THE FINAL PATTERN CONFIGURATION. SEE FINAL PAVEMENT MARKING PLANS FOR THESE DETAILS.

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> 		<p>TRANSPORTATION MANAGEMENT PLAN PHASE III DETAILS</p>
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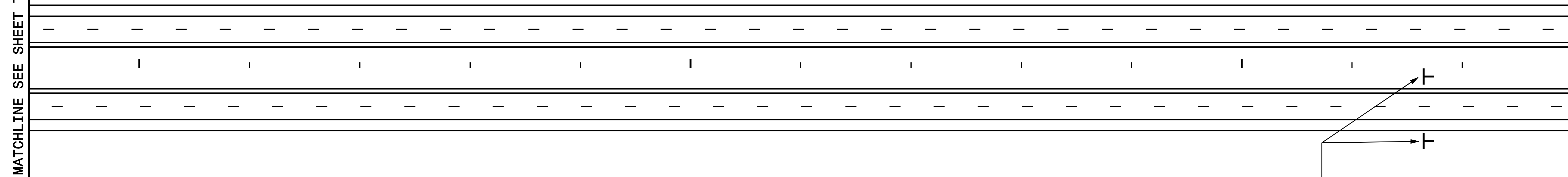
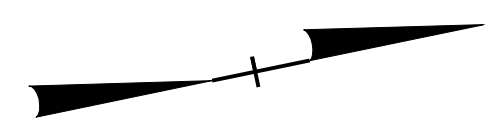
270

275

280

MATCHLINE SEE SHEET TMP-44

MATCHLINE SEE THIS SHEET



**ALL TRAFFIC
USE NEXT EXIT**
G21-65
120" X 48"
STA. 281+65 +/- -L-

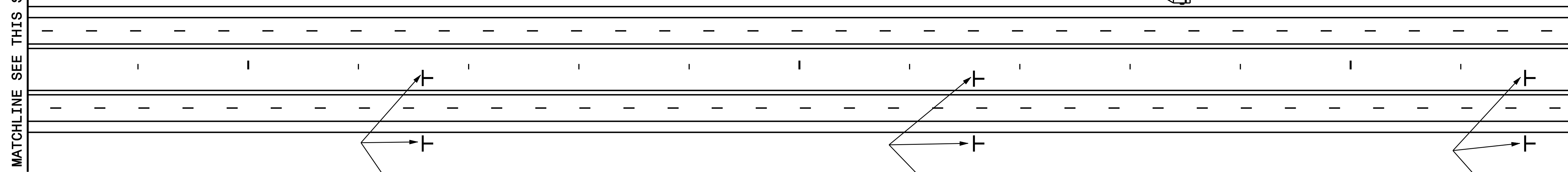
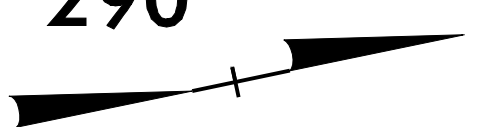
285

290

295

MATCHLINE SEE THIS SHEET

MATCHLINE SEE SHEET TMP-46



STA. 293+50 +/- -L-
MESSAGE NO. 1
MESSAGE NO. 2
LEFT MERGE
LANE RIGHT
CLOSED
CHANGEABLE MESSAGE SIGN

**LEFT LANE
CLOSED
AHEAD**
W20-5 L
48" X 48"

STA. 286+65 +/- -L-

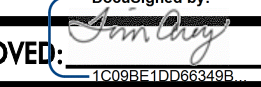

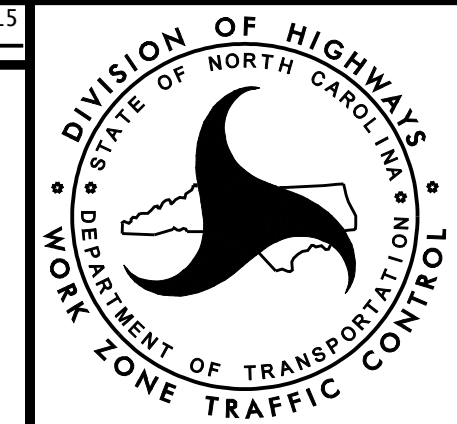
W4-2L
48" X 48"

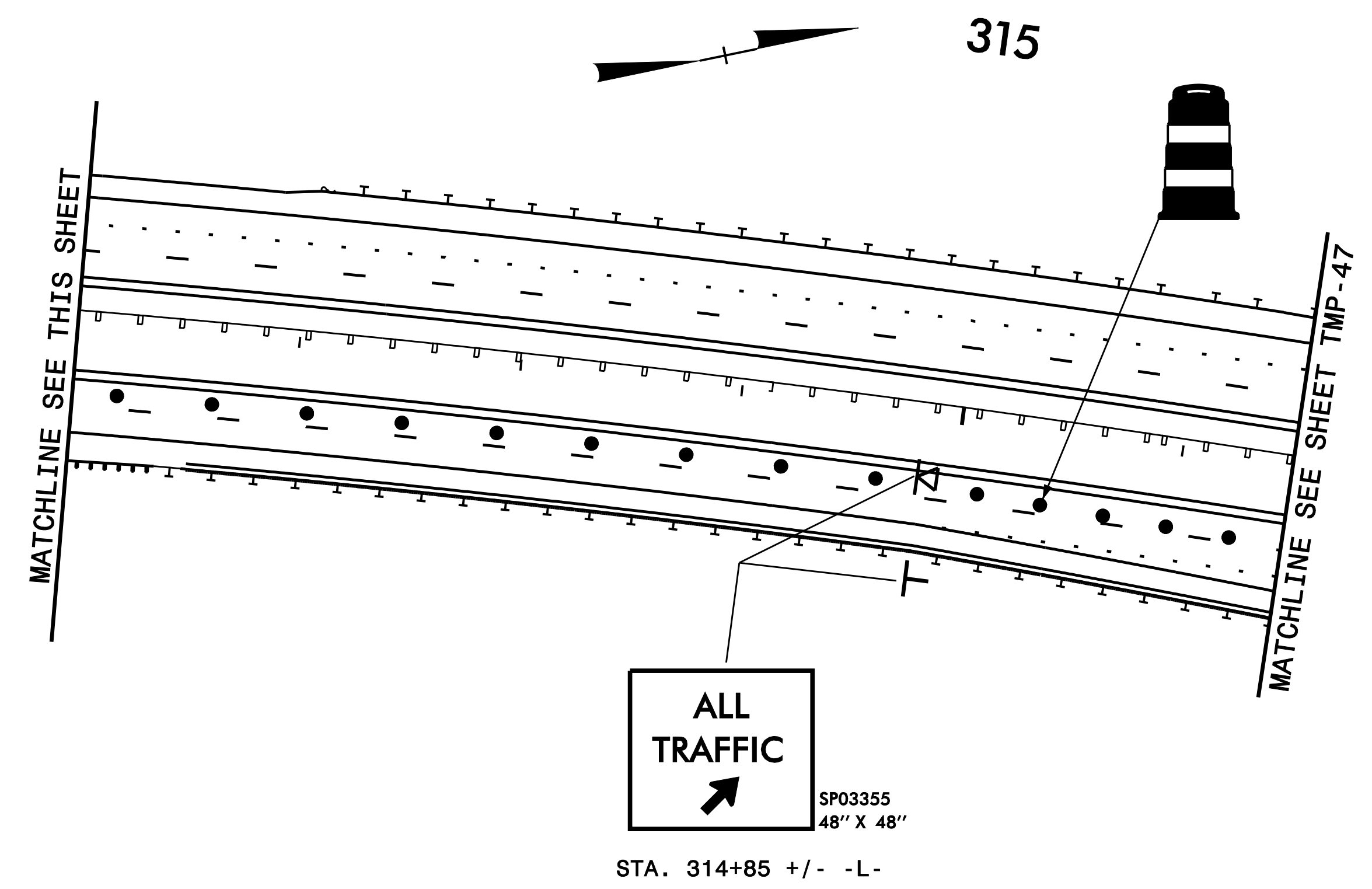
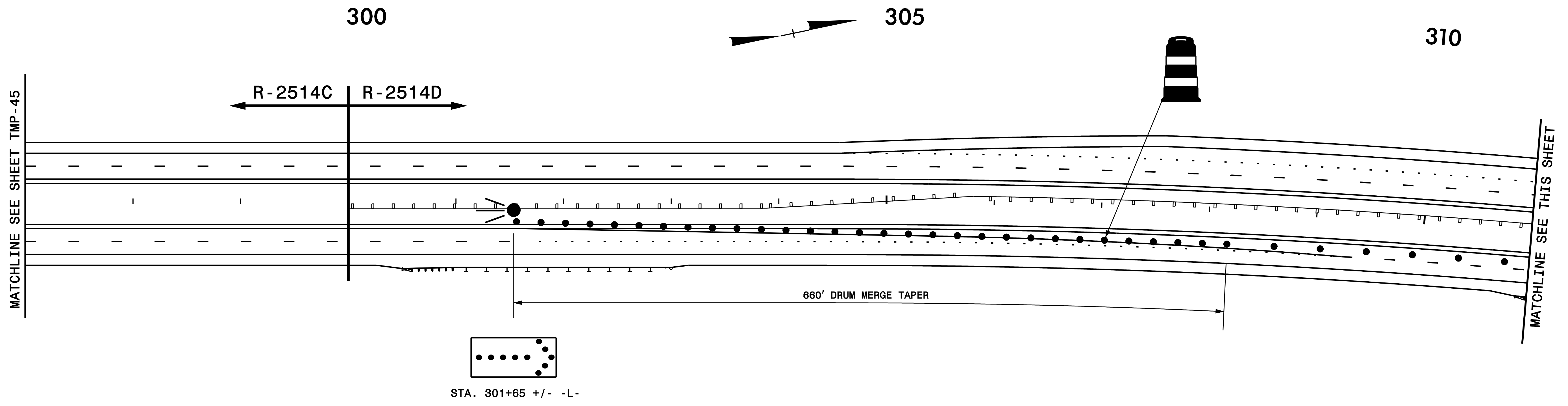
STA. 291+65 +/- -L-

**LEFT LANE
CLOSED**
W20-5 L
48" X 48"

STA. 296+65 +/- -L-

ALL PAVEMENT MARKINGS SHOWN ON THIS SHEET ARE NEW PAINT PAVEMENT MARKINGS INSTALLED IN THE FINAL PATTERN CONFIGURATION. SEE FINAL PAVEMENT MARKING PLANS FOR THESE DETAILS.

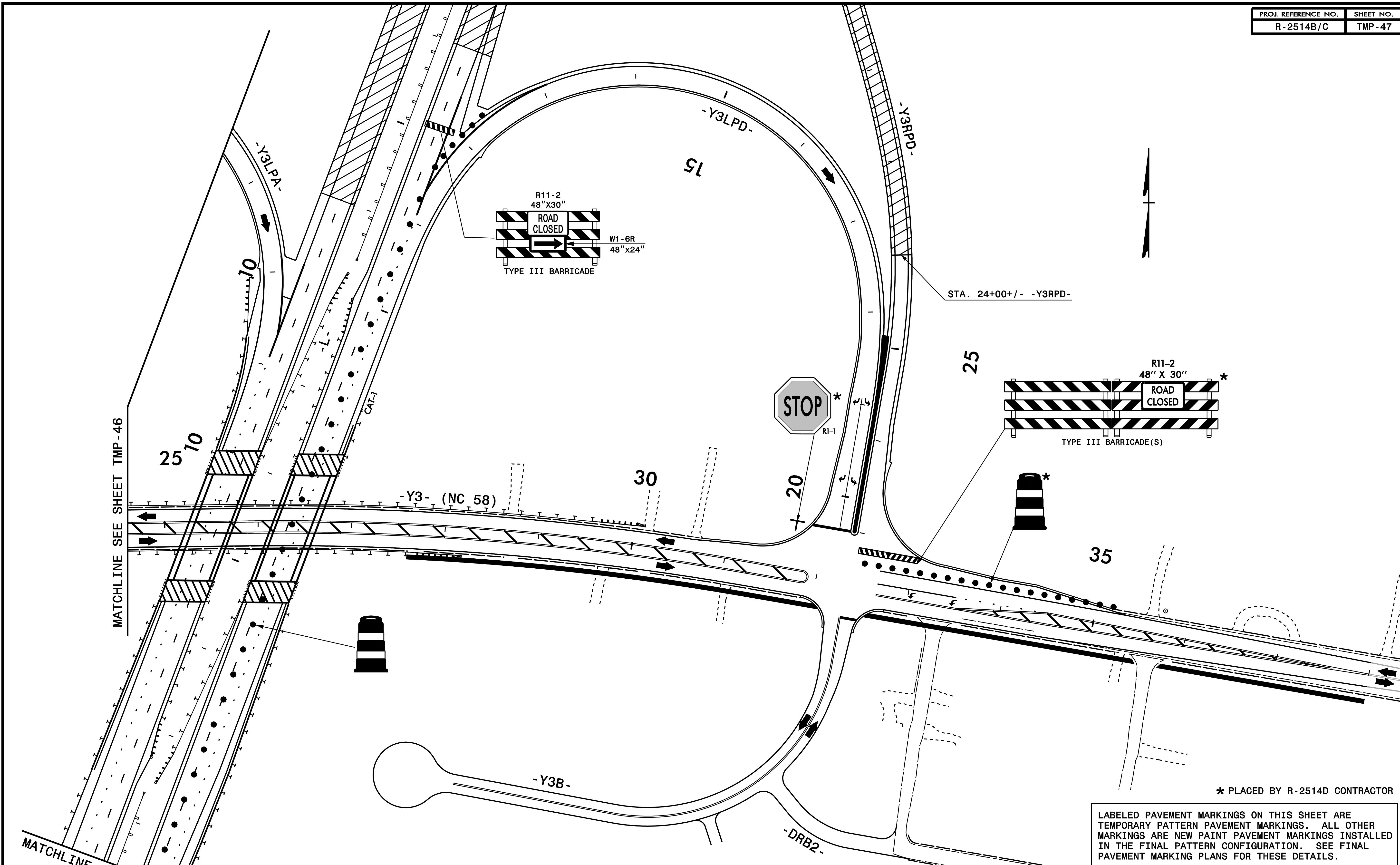
<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> <p></p>	<p></p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE III DETAILS</p>
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TEMPORARY PAVEMENT MARKINGS PLACED BY THE RESPECTIVE CONTRACTOR. TRAFFIC CONTROL DEVICES (SIGNS, FLASHING ARROW PANELS, DRUMS, CHANGEABLE MESSAGE SIGNS) PLACED BY THE R-2514C CONTRACTOR EXCEPT AS NOTED.

ALL PAVEMENT MARKINGS SHOWN ON THIS SHEET ARE NEW PAINT PAVEMENT MARKINGS INSTALLED IN THE FINAL PATTERN CONFIGURATION. SEE FINAL PAVEMENT MARKING PLANS FOR THESE DETAILS.

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: _____ DATE: 4/15/2015</p> <p>SEAL</p>		<p>TRANSPORTATION MANAGEMENT PLAN PHASE III DETAILS</p>
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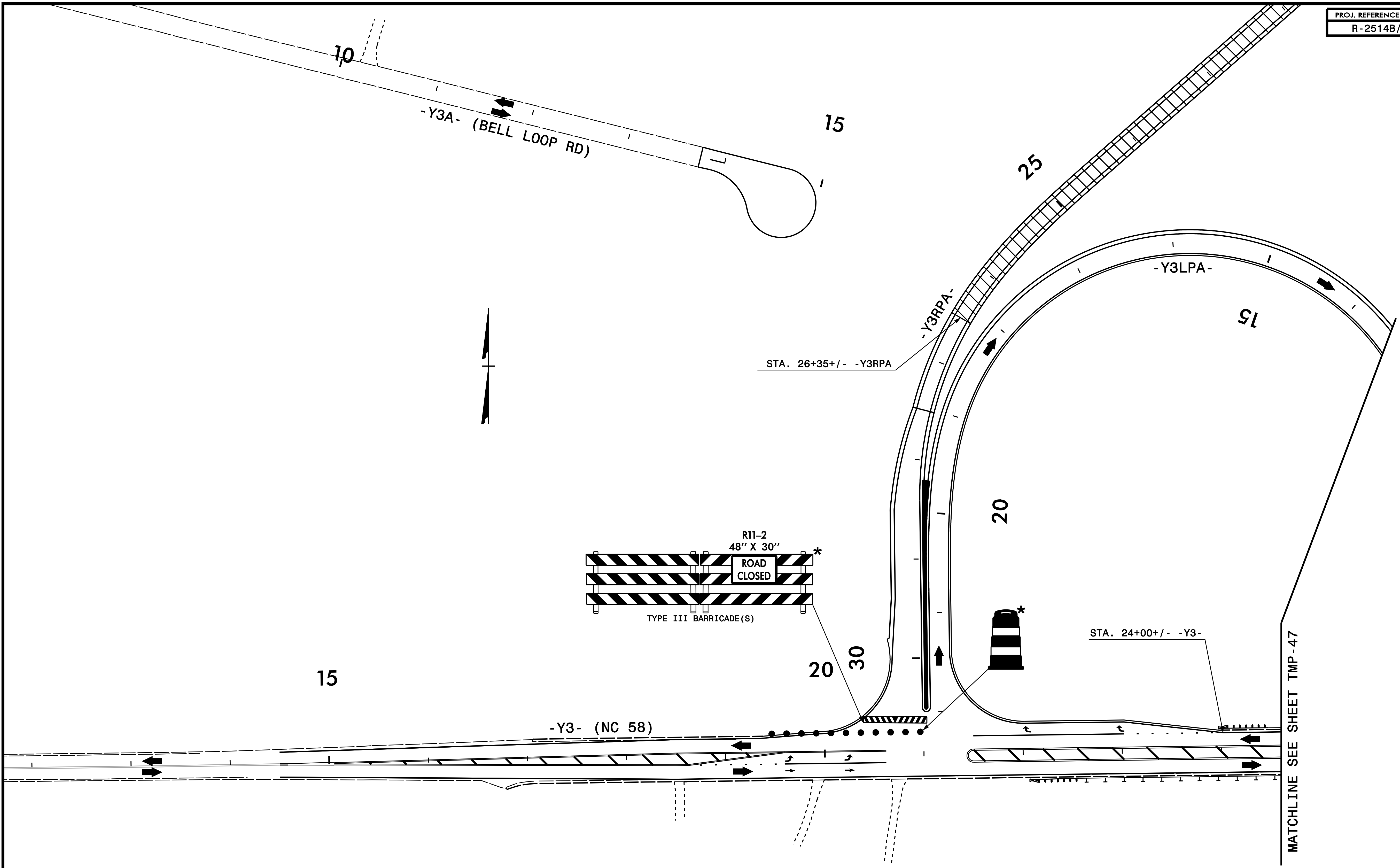
MATCHLINE SEE SHEET TMP-46

MATCHLINE SEE SHEET TMP-44

* PLACED BY R-2514D CONTRACTOR

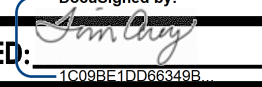
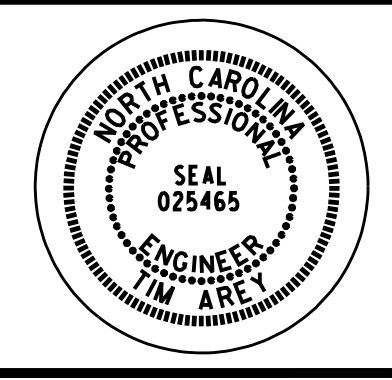
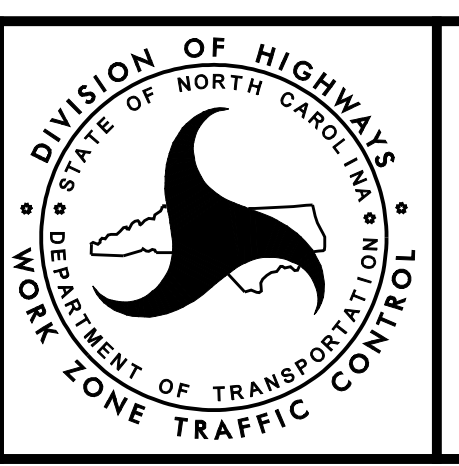
LABELLED PAVEMENT MARKINGS ON THIS SHEET ARE TEMPORARY PATTERN PAVEMENT MARKINGS. ALL OTHER MARKINGS ARE NEW PAINT PAVEMENT MARKINGS INSTALLED IN THE FINAL PATTERN CONFIGURATION. SEE FINAL PAVEMENT MARKING PLANS FOR THESE DETAILS.

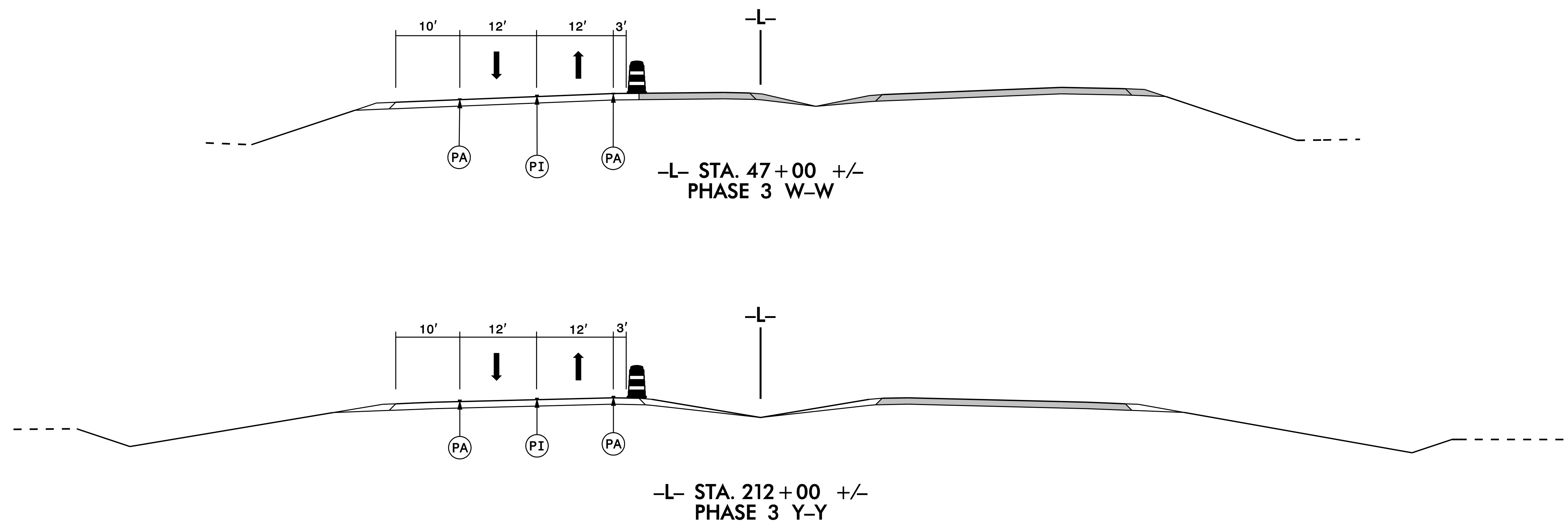
<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED: <i>[Signature]</i> DATE: 4/15/2015</p> <p>SEAL</p> <p>PROFESSIONAL ENGINEER TIM ARY SEAL 025465</p>	<p>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</p>	<p>TRANSPORTATION MANAGEMENT PLAN PHASE III DETAILS</p>
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MATCHLINE SEE SHEET TMP-47

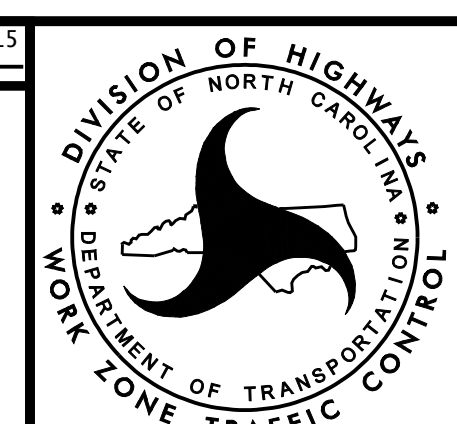
* PLACED BY R-2514D CONTRACTOR

<p>PLAN PREPARED IN THE OFFICE OF:</p> <p>PROGRESSIVE DESIGN GROUP, INC.</p> <p>ENGINEERS • CONSULTANTS</p>	<p>APPROVED:  DATE: 4/15/2015</p> <p>SEAL</p> 		<p>TRANSPORTATION MANAGEMENT PLAN PHASE III DETAILS</p>
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PLAN PREPARED IN THE OFFICE OF:
PROGRESSIVE
 DESIGN GROUP, INC.
 ENGINEERS • CONSULTANTS

APPROVED: *[Signature]* DATE: 4/15/2015
 SEAL
 NORTH CAROLINA PROFESSIONAL ENGINEER
 SEAL 025465
 TIM ARY



TRANSPORTATION
 MANAGEMENT PLAN
 CROSS SECTIONS