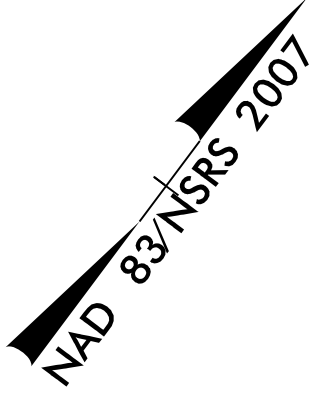


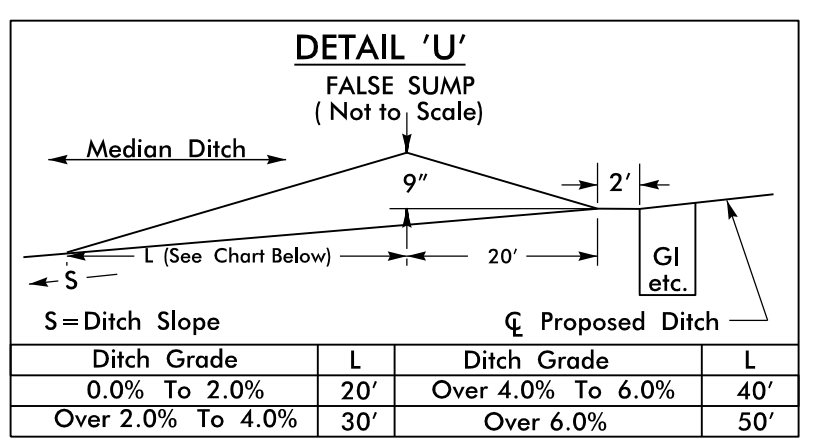
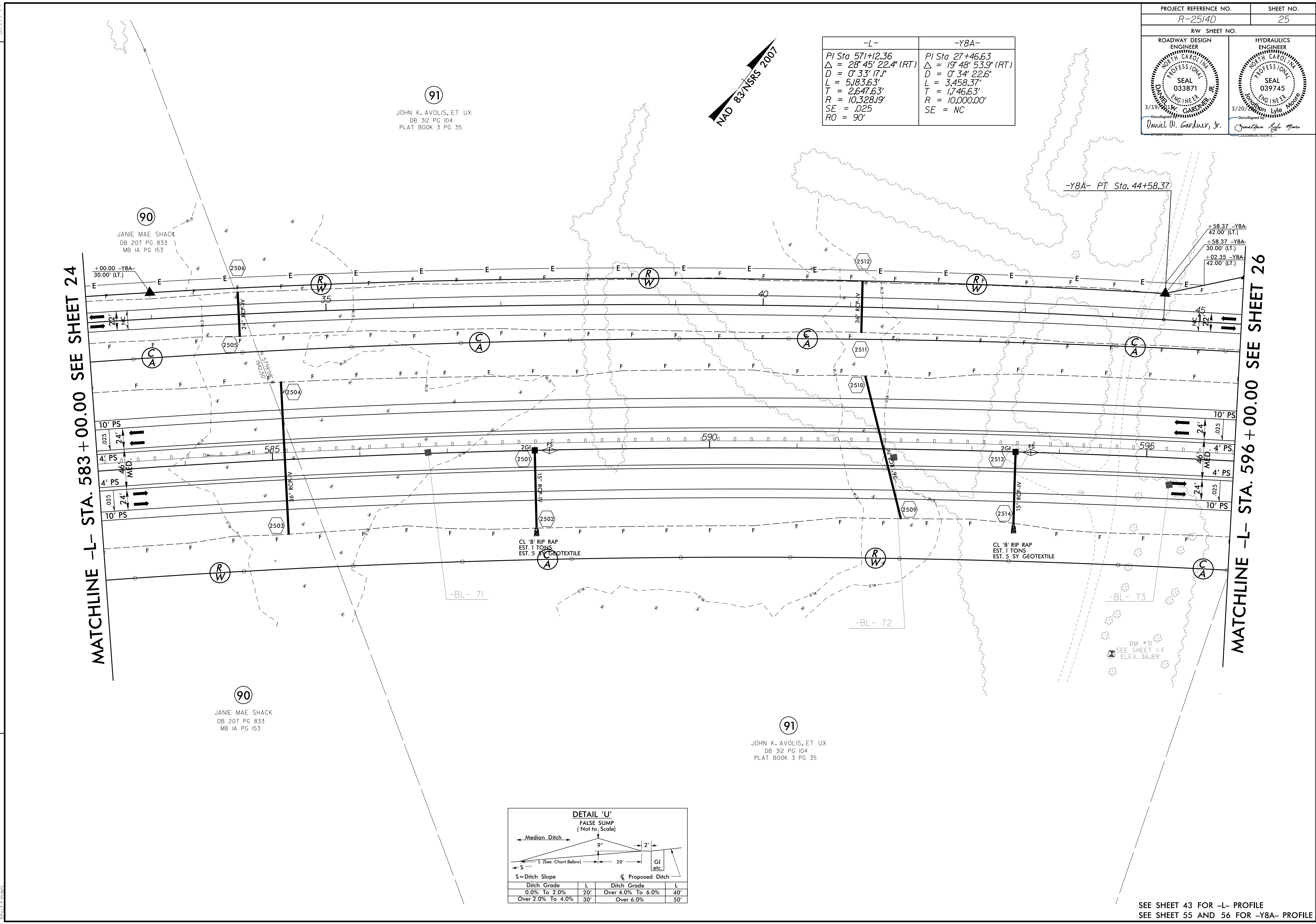
-L-	-Y8A-
PI Sta 571+12.36	PI Sta 27+46.63
$\Delta = 28^\circ 45' 22.4" (RT)$	$\Delta = 19^\circ 48' 53.9" (RT)$
$D = 0^\circ 33' 17.1"$	$D = 0^\circ 34' 22.6"$
$L = 5,183.63'$	$L = 3,458.37'$
$T = 2,647.63'$	$T = 1,746.63'$
$R = 10,328.19'$	$R = 10,000.00'$
$SE = .025$	$SE = NC$
$RO = 90'$	

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JOHN K. AVOLIS, ET UX
DB 312 PG 104
PLAT BOOK 3 PG 35



MATCHLINE -L- STA. 583 + 00.00 SEE SHEET 24

MATCHLINE -L- STA. 596 + 00.00 SEE SHEET 26



SEE SHEET 43 FOR -L- PROFILE
SEE SHEET 55 AND 56 FOR -Y8A- PROFILE

REVISIONS

8/17/99
3/15/2015
P:\2514D\Roadway\Proc\NR2514D_Roadway\Proc\NR2514D_Roadway.psh_25.dgn
DWG: 2514D_Roadway.dwg
DATE: 3/15/2015
TIME: 10:00:00 AM