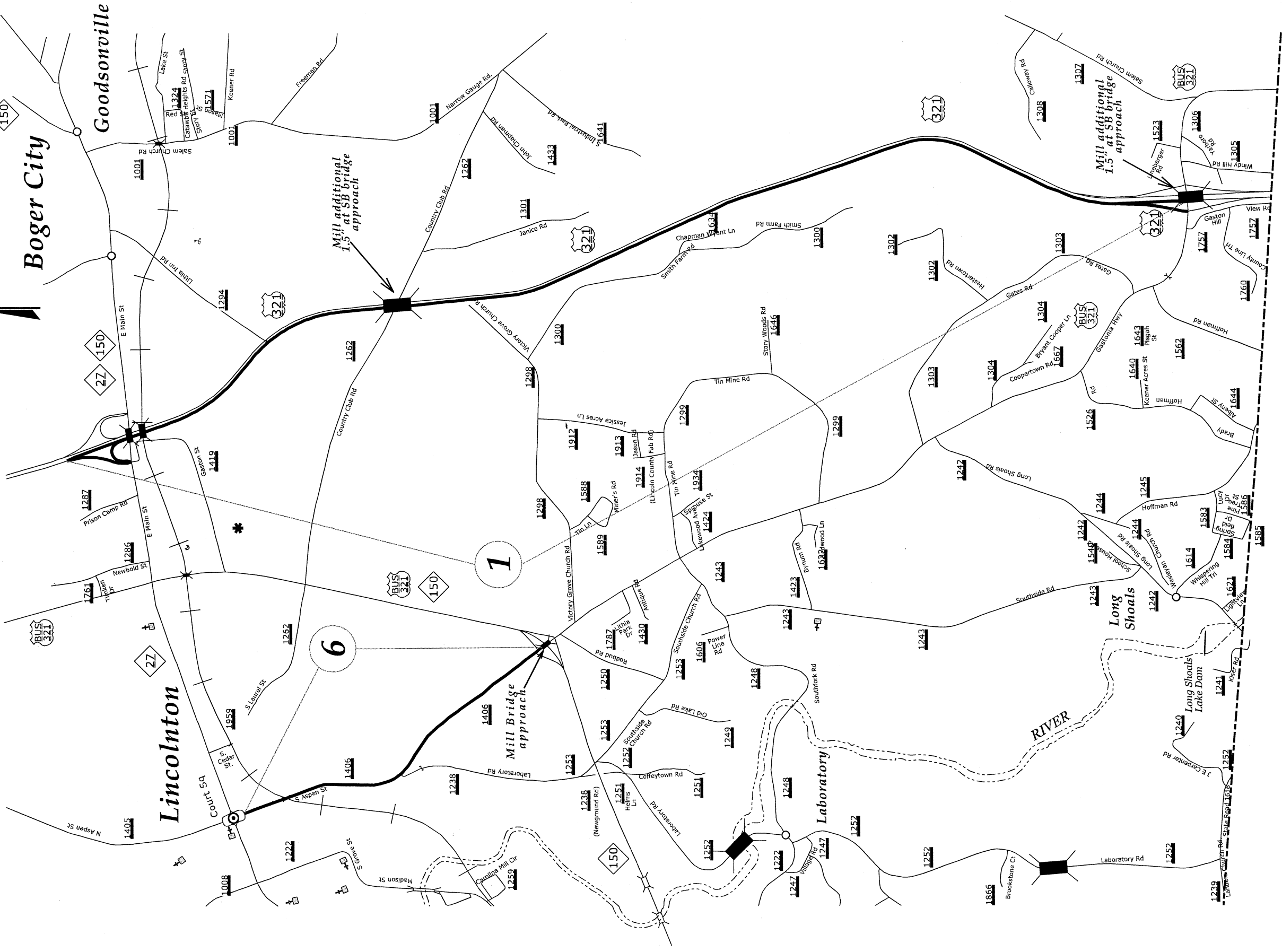


2014-2015

# Resurfacing Map

## LINCOLN COUNTY NC

STATE	PROJECT WBS	SHEET NUMBER
NC	12CR.10551.12	1
	12CR.20551.19	



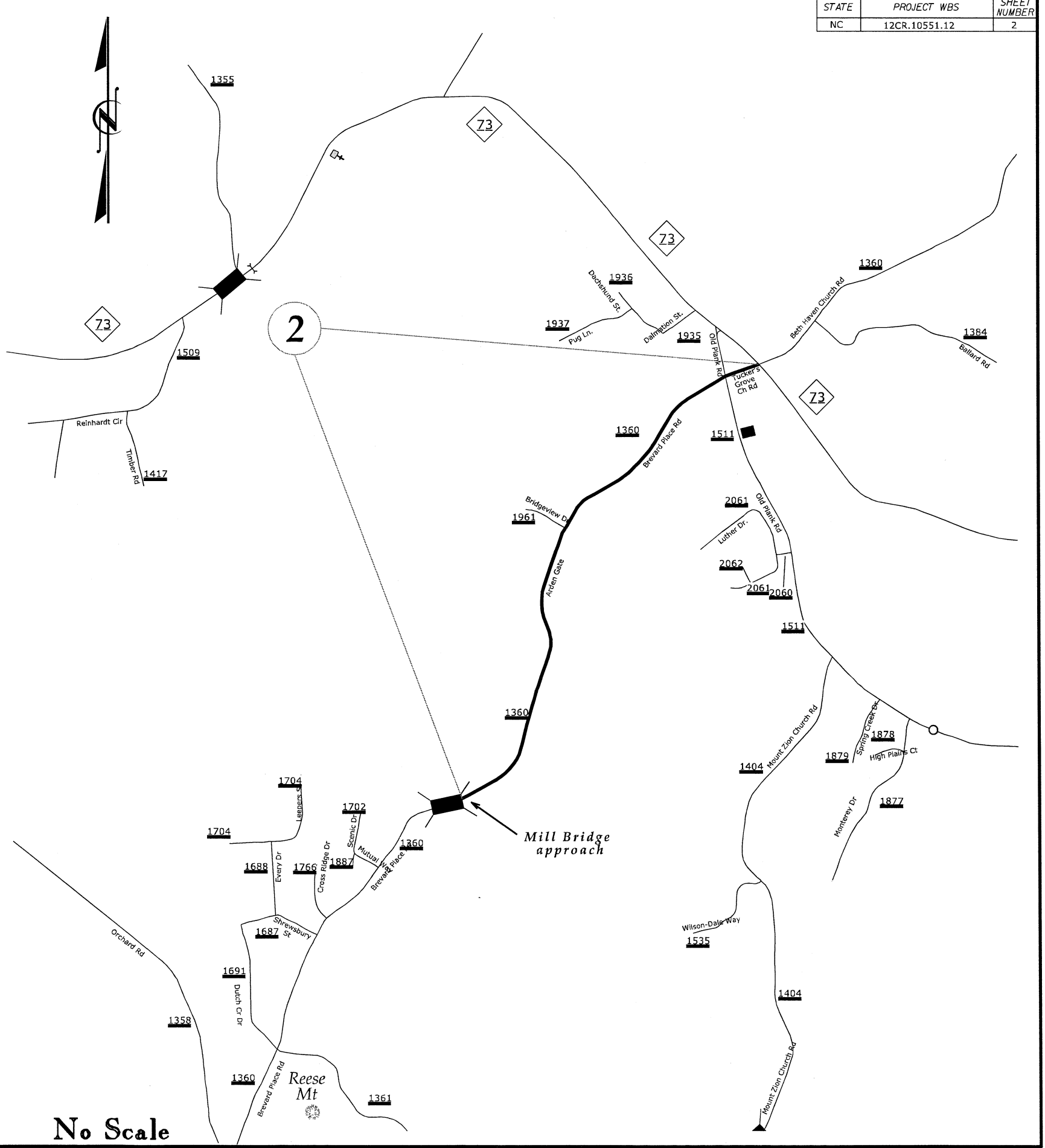
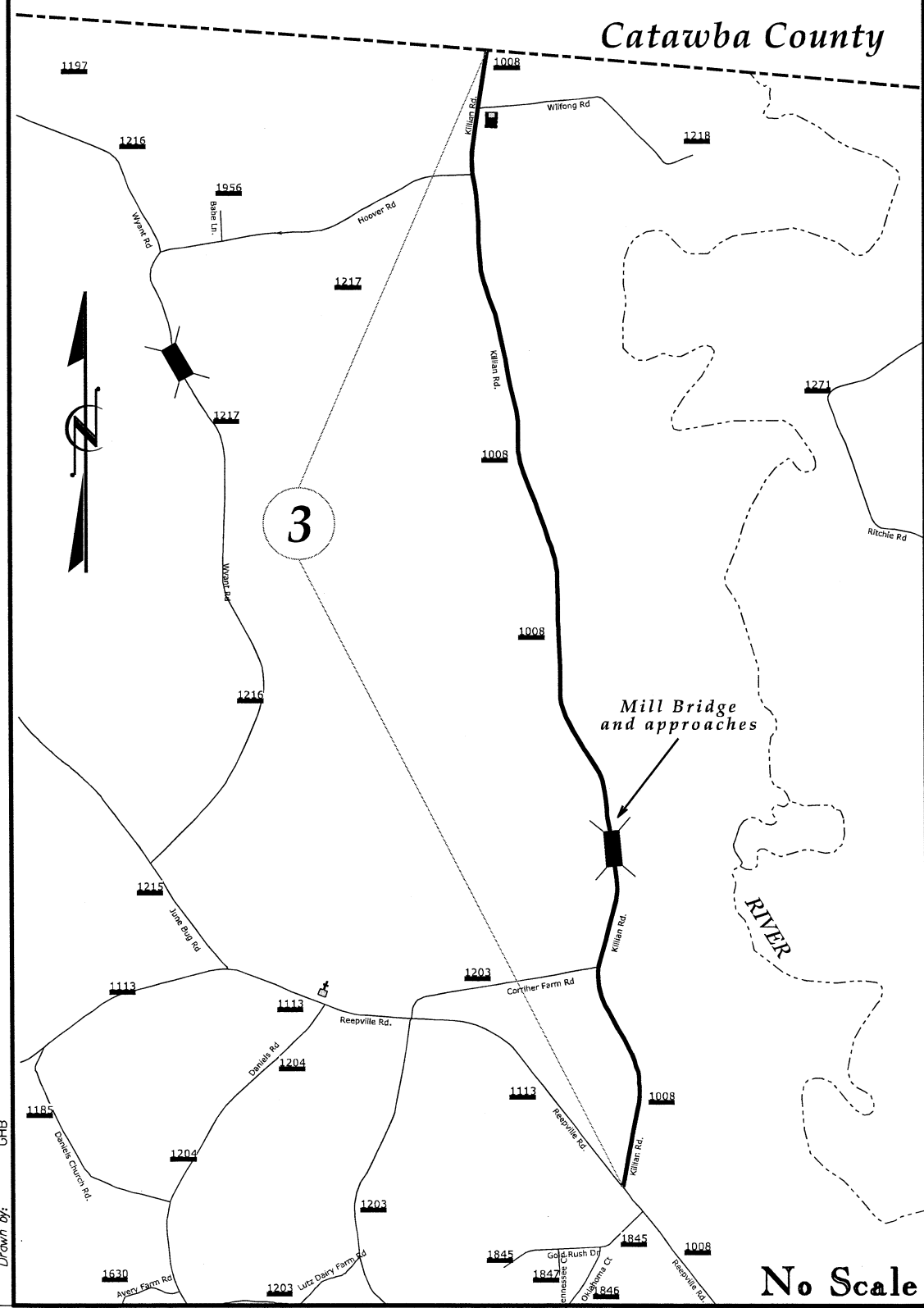
### Gaston County

No Scale

Drawn by: GHB

STATE	PROJECT WBS	SHEET NUMBER
NC	12CR.10551.12	2

2014-2015  
Resurfacing Map  
LINCOLN COUNTY NC



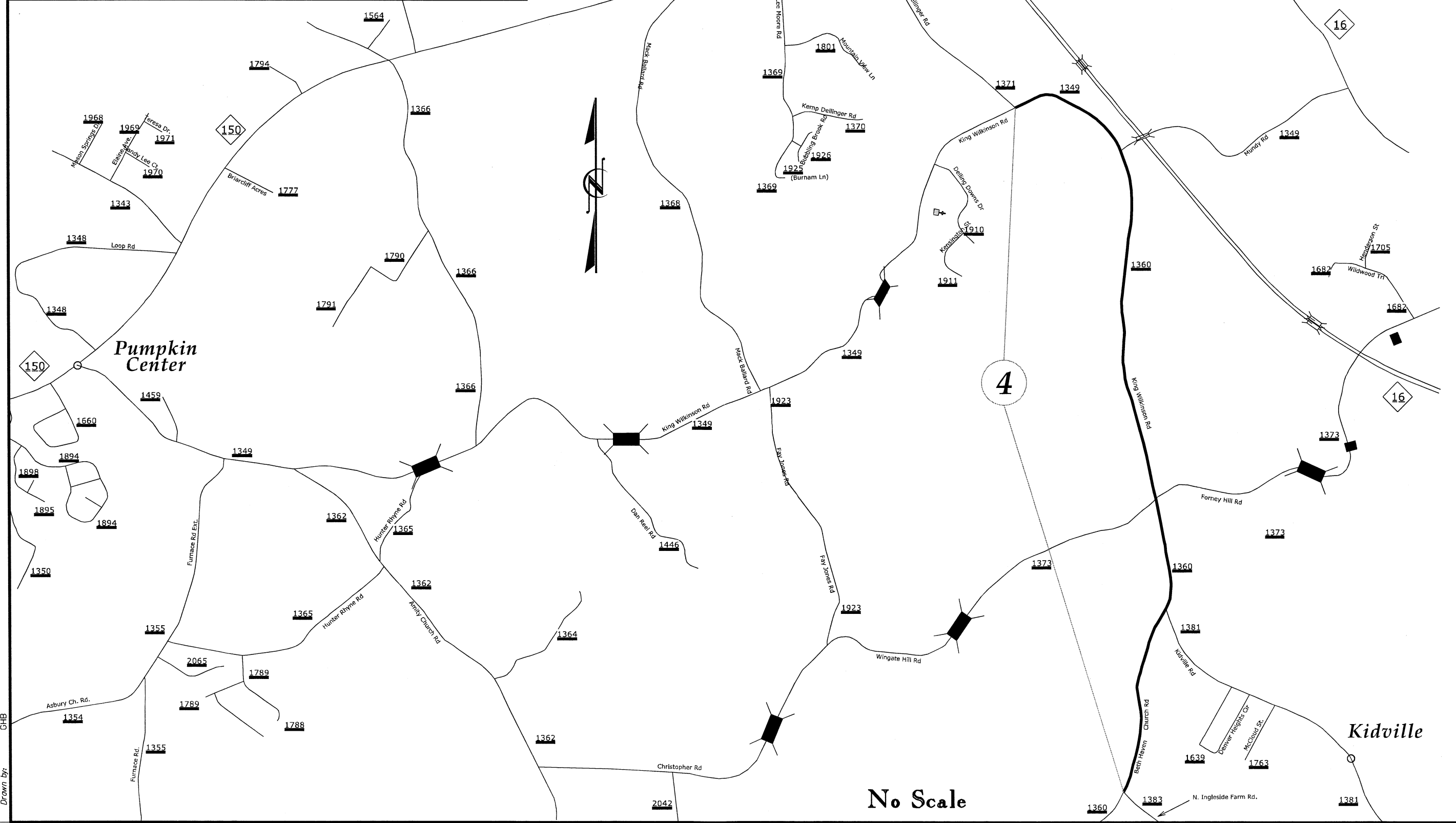
Drawn by: GHB

STATE	PROJECT WBS	SHEET NUMBER
NC	12CR.10551.12	3

Catawba County

2014-2015  
Resurfacing Map

LINCOLN COUNTY NC



4

No Scale

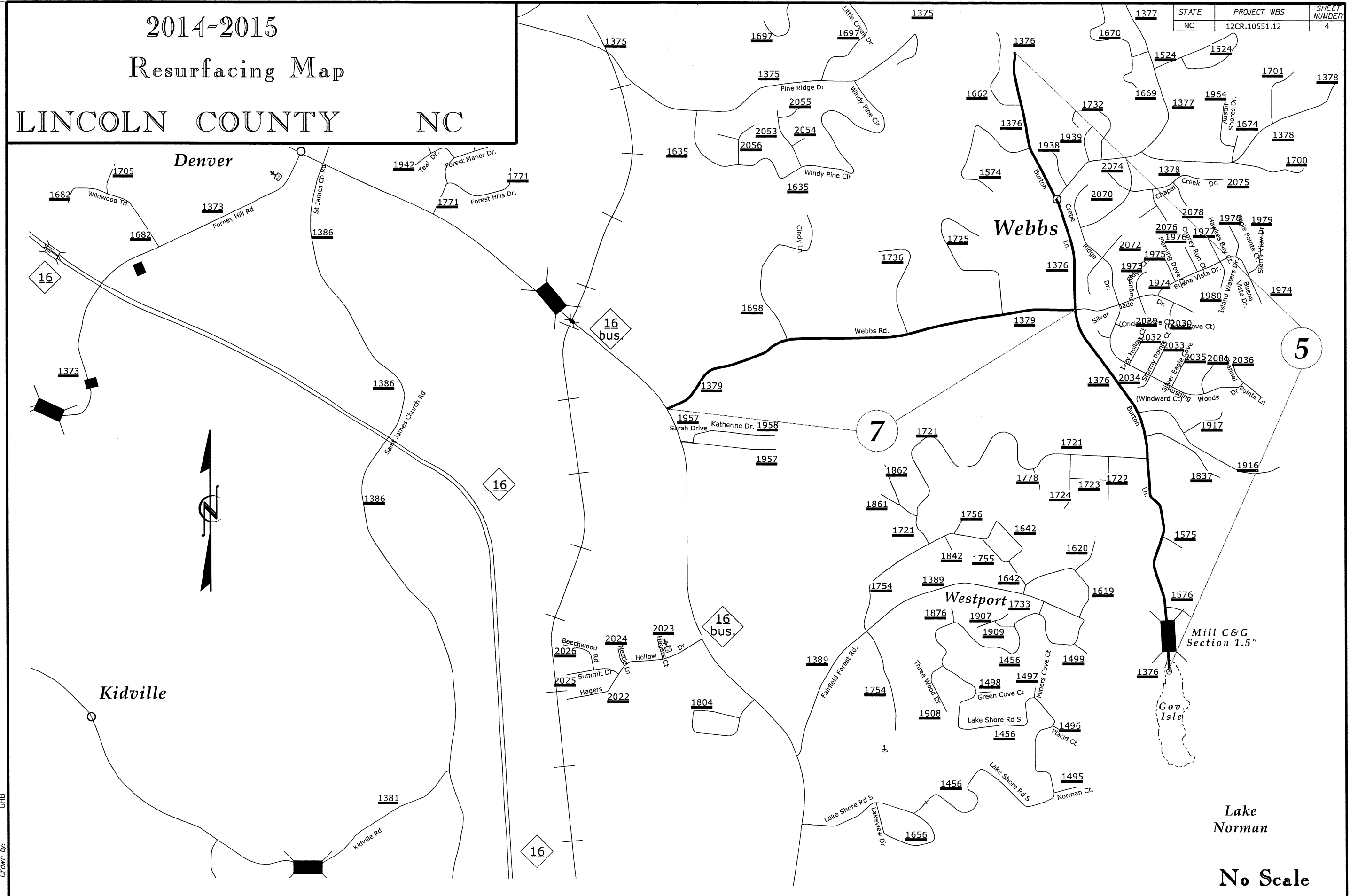
Drawn by: GHB

Pumpkin Center

Kidville

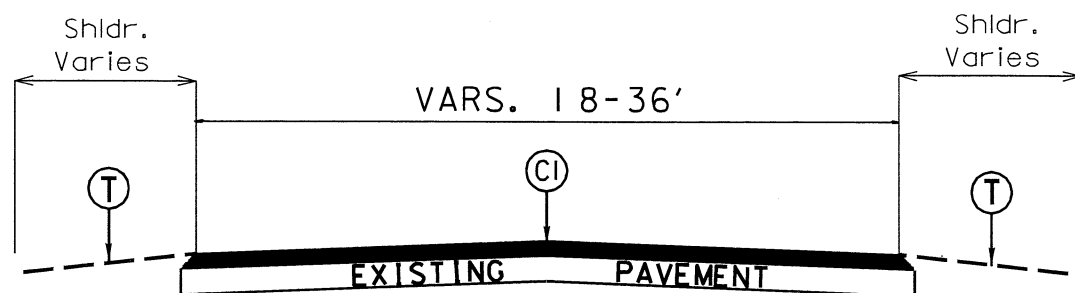
STATE	PROJECT WBS	SHEET NUMBER
NC	12CR.10551.12	4

**2014-2015**  
**Resurfacing Map**  
**LINCOLN COUNTY NC**



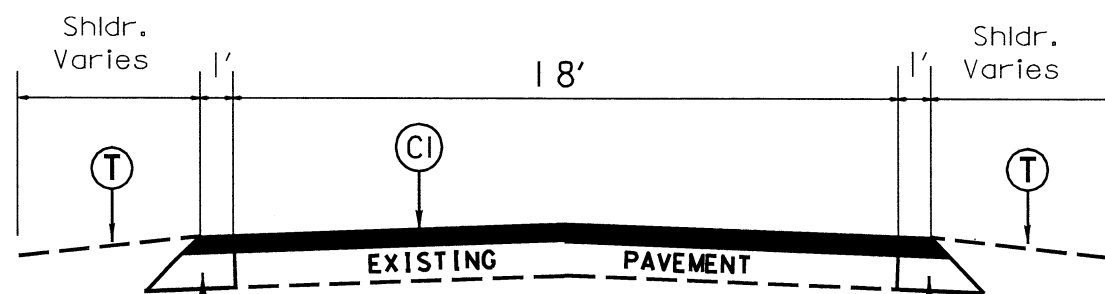
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**No Scale**



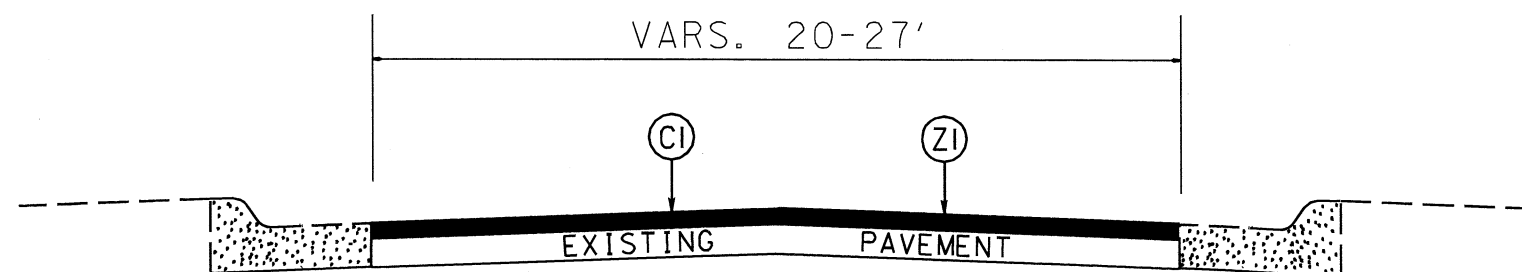
**TYPICAL SECTION NO. 1**

MAP # 2, #3, #4  
 MAP # 5 11+00 to 79+25  
 MAP # 6 0+00 to 47+50  
 MAP # 7



**TYPICAL SECTION NO. 2**

Map # 5 79+25 to 130+50  
 (SR 1379 to boat landing)



**TYPICAL SECTION NO. 3**

MAP # 6 - 63+00 to 69+75  
 MAP # 5 - 0+00 to 11+00

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 8" OF ASPHALT BASE COURSE, TYPE B 25.0B, AT AN AVERAGE RATE OF 456 LBS PER SQ. YD. IN EACH OF TWO LIFTS
T	AGGREGATE SHOULDER BORROW (SHOULDER RECONSTRUCTION)
Y	PROP. APPROX. 0 - 1.5" OF INCIDENTAL MILLING
Y1	MILLED RUMBLE STRIPS
Z1	MILL ASPHALT PAVEMENT APPROX. 1.5" IN DEPTH
Z2	MILL ASPHALT PAVEMENT APPROX. 3.0" IN DEPTH
Z3	MILL ASPHALT PAVEMENT APPROX. ¾" IN DEPTH

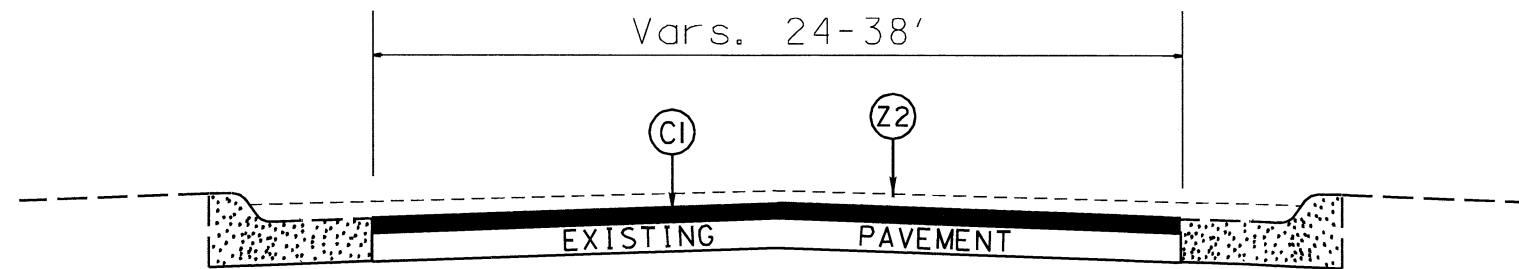
General Notes:

\* Pavement edge slopes are 1:1 unless specified otherwise.

**2014 - 2015  
 Resurfacing Program  
 Typical Sections  
 Lincoln County NC**

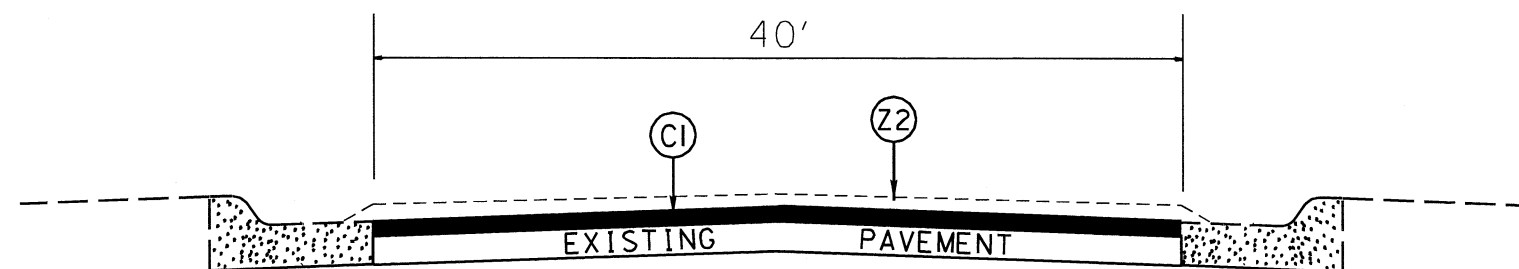
Checked by:

Drawn by: GHB



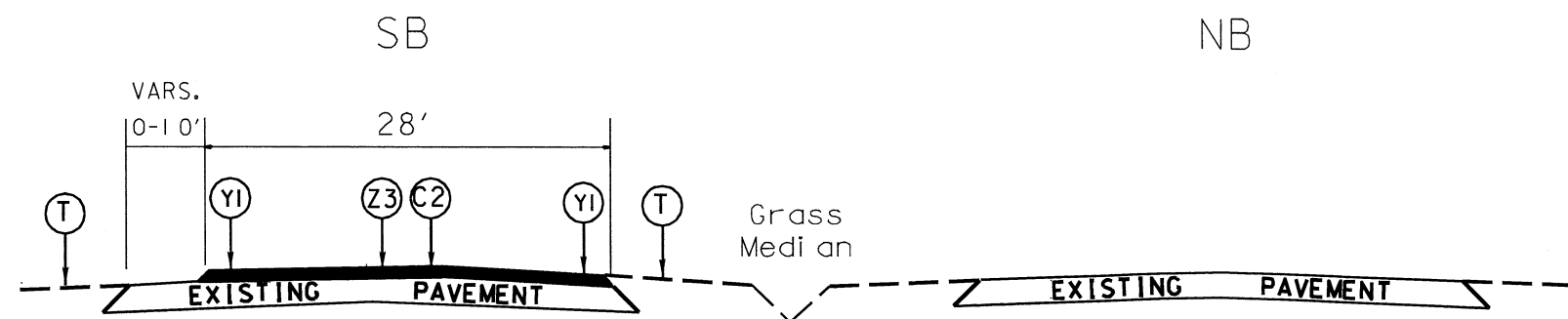
TYPICAL SECTION NO. 4

MAP # 6 - 47+50 to 63+00  
69+75 to 73+75



TYPICAL SECTION NO. 5

MAP # 6 - 73+75 to 76+25



TYPICAL SECTION NO. 6

MAP # 1

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 8" OF ASPHALT BASE COURSE, TYPE B 25.0B, AT AN AVERAGE RATE OF 456 LBS PER SQ. YD. IN EACH OF TWO LIFTS
T	AGGREGATE SHOULDER BORROW (SHOULDER RECONSTRUCTION)
Y	PROP. APPROX. 0 - 1.5" OF INCIDENTAL MILLING
Y1	MILLED RUMBLE STRIPS
Z1	MILL ASPHALT PAVEMENT APPROX. 1.5" IN DEPTH
Z2	MILL ASPHALT PAVEMENT APPROX. 3.0" IN DEPTH
Z3	MILL ASPHALT PAVEMENT APPROX. ¾" IN DEPTH

\* Shoulder Reconstruction will not be required in areas with extended outside paved shoulder.

\* In areas with extended paved shoulder taper lift down approx. 1 foot beyond milled rumble strip.

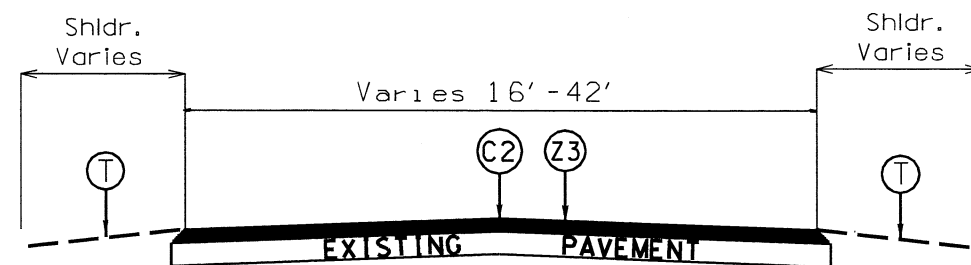
General Notes:

\* Pavement edge slopes are 1:1 unless specified otherwise.

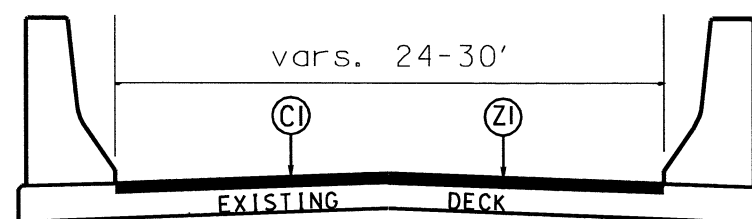
2014 - 2015  
Resurfacing Program  
Typical Sections  
Lincoln County NC

Checked by:

Drawn by: GHB



**TYPICAL SECTION NO. 7**  
MAP # 1 (SB RAMPS ONLY)

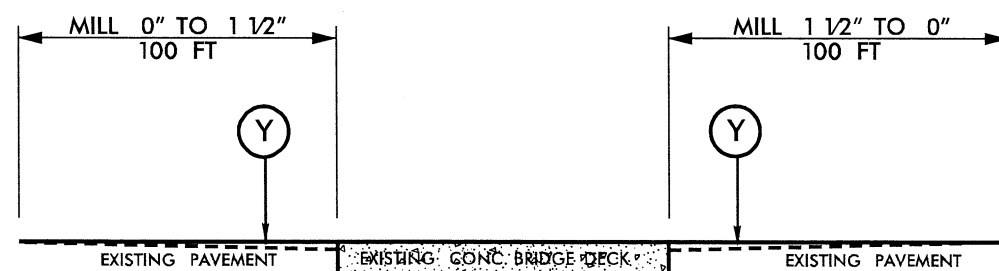


**ASPHALTED BRIDGE SECTION**

- MAP # 3 - Brdg. 120
- MAP # 6 - Brdg. 241
- MAP # 6 - Brdg. 243

\* MILL APPROACHES PER DETAIL A

**DETAIL A**  
**MILLING BRIDGE APPROACHES**



- Map # 1 - Bridge # 269 - MILL N. APPROACH
- Map # 1 - Bridge # 271 - MILL APPROACHES
- Map # 2 - Bridge # 015 - MILL N. APPROACH
- Map # 5 - Bridge # 306 - MILL N. APPROACH
- Map # 6 - Bridge # 030 - MILL APPROACHES
- Map # 6 - Bridge # 242 - MILL APPROACHES

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 8" OF ASPHALT BASE COURSE, TYPE B 25.0B, AT AN AVERAGE RATE OF 456 LBS PER SQ. YD. IN EACH OF TWO LIFTS
T	AGGREGATE SHOULDER BORROW (SHOULDER RECONSTRUCTION)
Y	PROP. APPROX. 0 - 1.5" OF INCIDENTAL MILLING
Y1	MILLED RUMBLE STRIPS
Z1	MILL ASPHALT PAVEMENT APPROX. 1.5" IN DEPTH
Z2	MILL ASPHALT PAVEMENT APPROX. 3.0" IN DEPTH
Z3	MILL ASPHALT PAVEMENT APPROX. 3/4" IN DEPTH

General Notes:

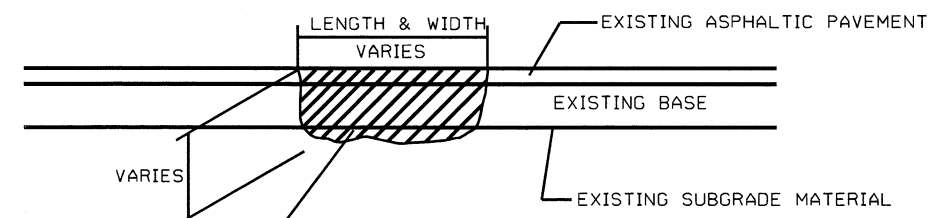
\* Pavement edge slopes are 1:1 unless specified otherwise.

**2014 - 2015**  
**Resurfacing Program**  
**Typical Sections**  
**Lincoln County NC**

Checked by:

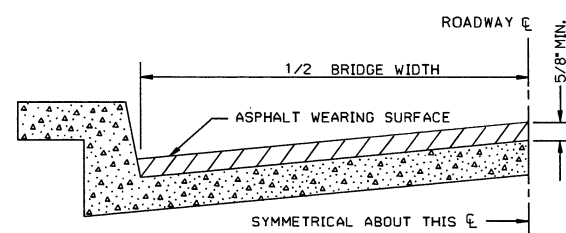
Drawn by: GHB

**DETAIL B**  
**PATCHING EXISTING PAVEMENT**



MILL EXISTING ASPHALT PAVEMENT AND REMOVE EXISTING LOOSE BASE AND/OR SUBGRADE MATERIAL AND REPLACE WITH ACIC TYPE, I19.0X AND ACSC TYPE, S9.5X AS DIRECTED BY THE ENGINEER.

**DETAIL C**  
**BRIDGE HALF TYPICAL SECTION**



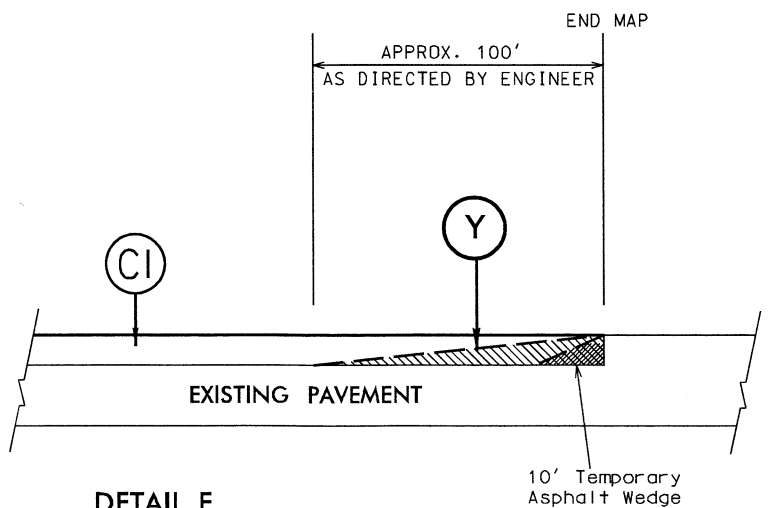
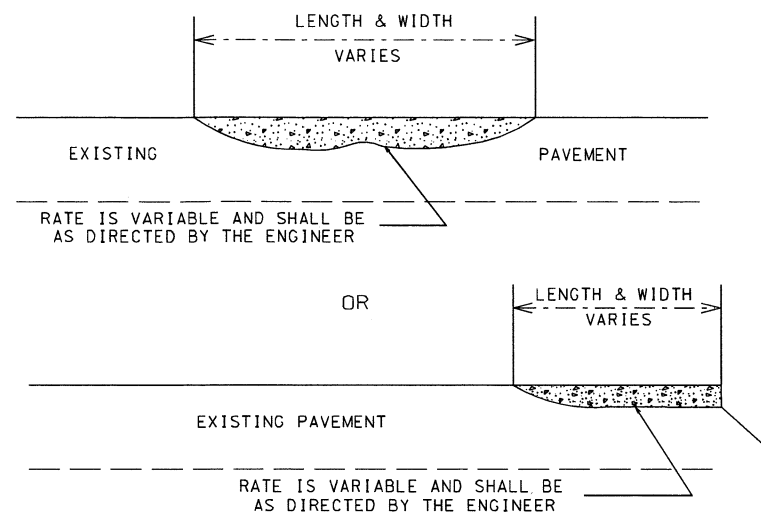
FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN.

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

**NOTES**

ALL UNPAVED S.R. ROADS TO BE SURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.  
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.  
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.  
SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE NOTED.  
BRIDGES TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

**DETAIL D**  
**ASPHALT CONCRETE SURFACE COURSE**  
**TYPE S9.5B or S9.5C. (LEVELING COURSE)**



**DETAIL E**  
**TIE-IN MILLING DETAIL**

**PAVEMENT SCHEDULE**

C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 8" OF ASPHALT BASE COURSE, TYPE B 25.0B, AT AN AVERAGE RATE OF 456 LBS PER SQ. YD. IN EACH OF TWO LIFTS
T	AGGREGATE SHOULDER BORROW (SHOULDER RECONSTRUCTION)
Y	PROP. APPROX. 0 - 1.5" OF INCIDENTAL MILLING
Y1	MILLED RUMBLE STRIPS
Z1	MILL ASPHALT PAVEMENT APPROX. 1.5" IN DEPTH
Z2	MILL ASPHALT PAVEMENT APPROX. 3.0" IN DEPTH
Z3	MILL ASPHALT PAVEMENT APPROX. 3/4" IN DEPTH

2014 - 2015  
Resurfacing Program  
Typical Sections  
Lincoln County NC

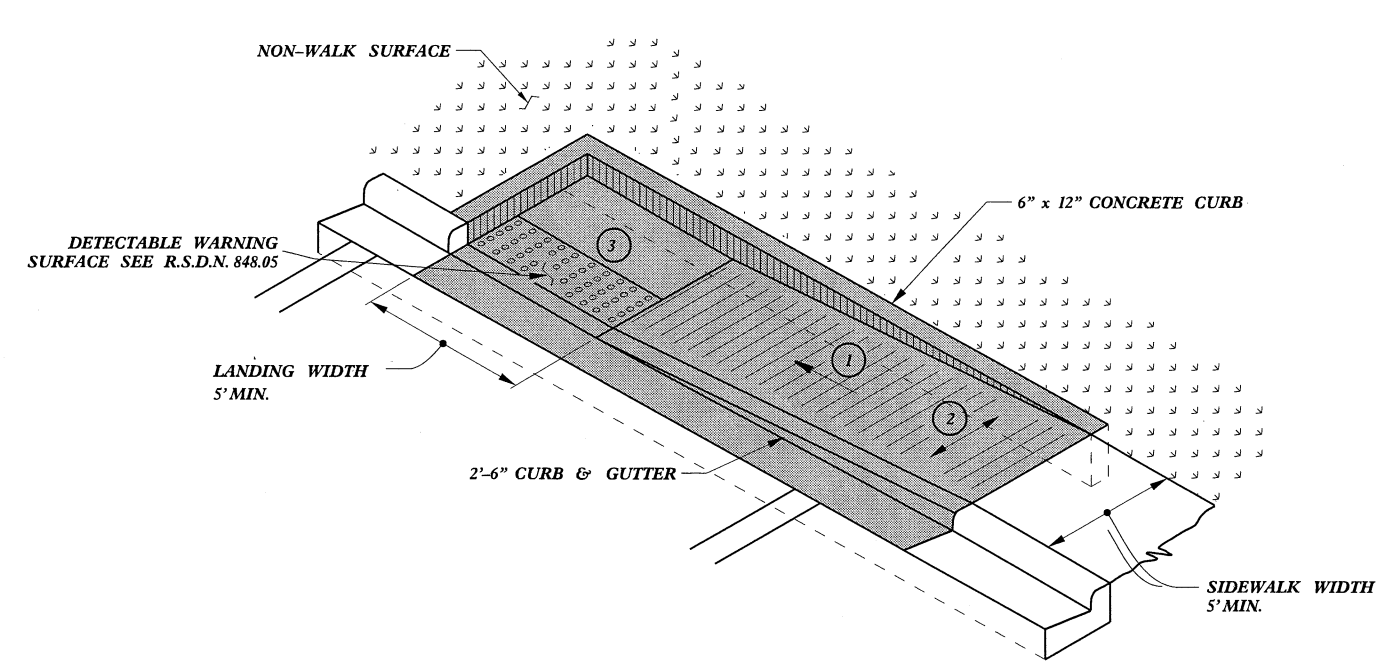
General Notes:

\* Pavement edge slopes are 1:1 unless specified otherwise.

Checked by: GHB

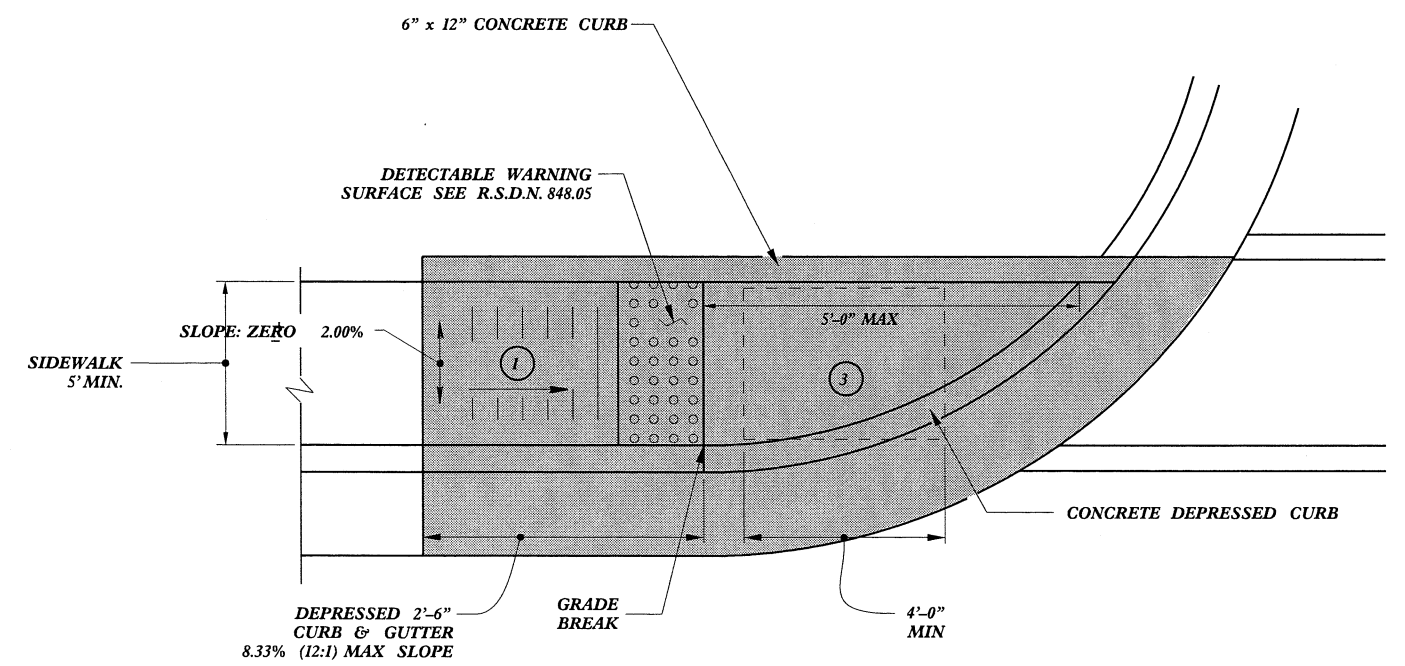


5/14/99  
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J.Howerton AT: CSD27581



PAY LIMITS FOR CURB RAMP

**TYPE 1A**

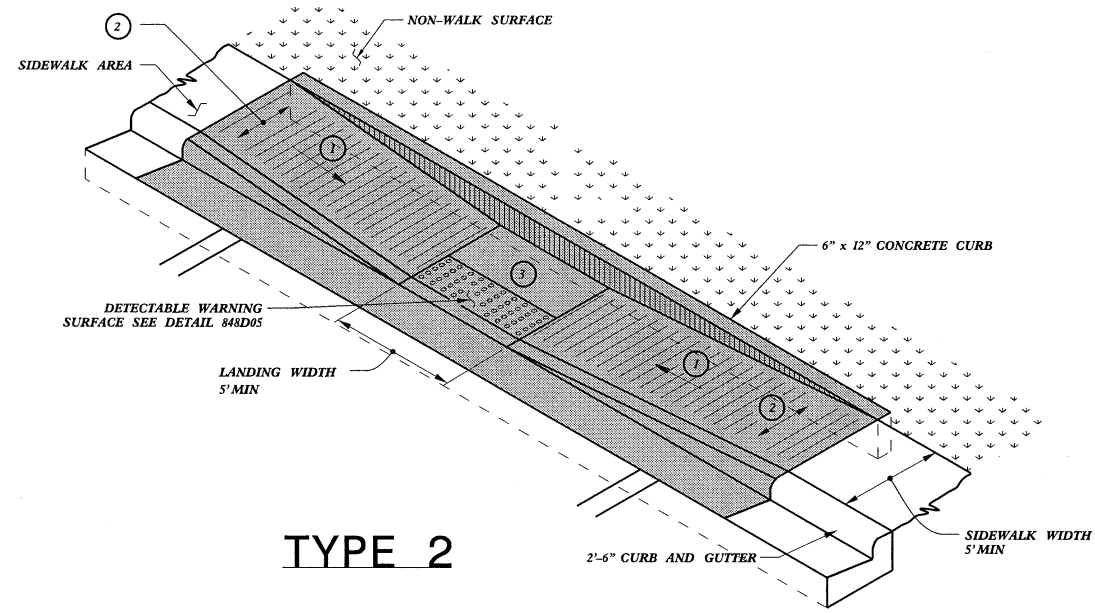


**TYPE 1**

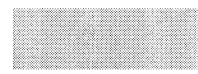
- ① 8.33% (12:1) MAX RAMP SLOPE
- ② CROSS SLOPE: 2.00%
- ③ CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.

REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

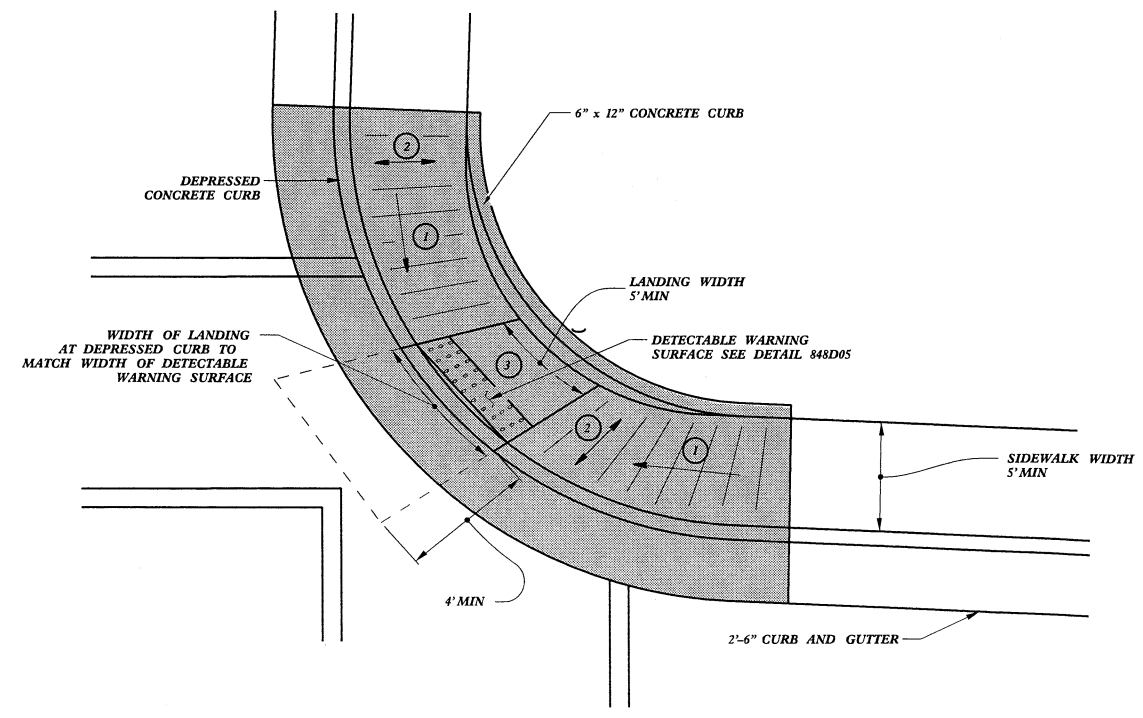
<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950 FAX 919-250-4119	
<b>CURB RAMPS</b>	
Directional Ramps	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dgn	



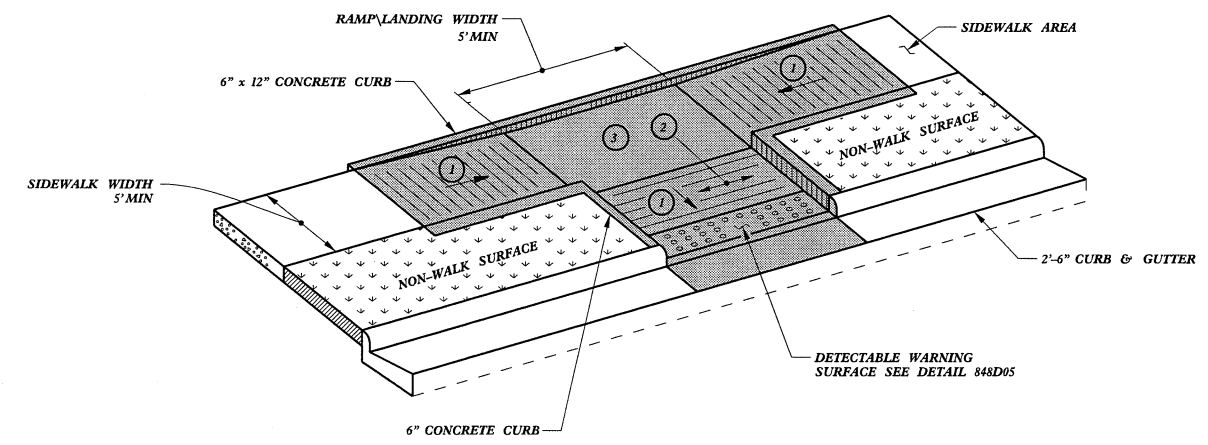
**TYPE 2**

 PAY LIMITS FOR CURB RAMP

- ① 8.33% (12:1) MAX RAMP SLOPE
- ② CROSS SLOPE: 2.00%
- ③ CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



**TYPE 2A**

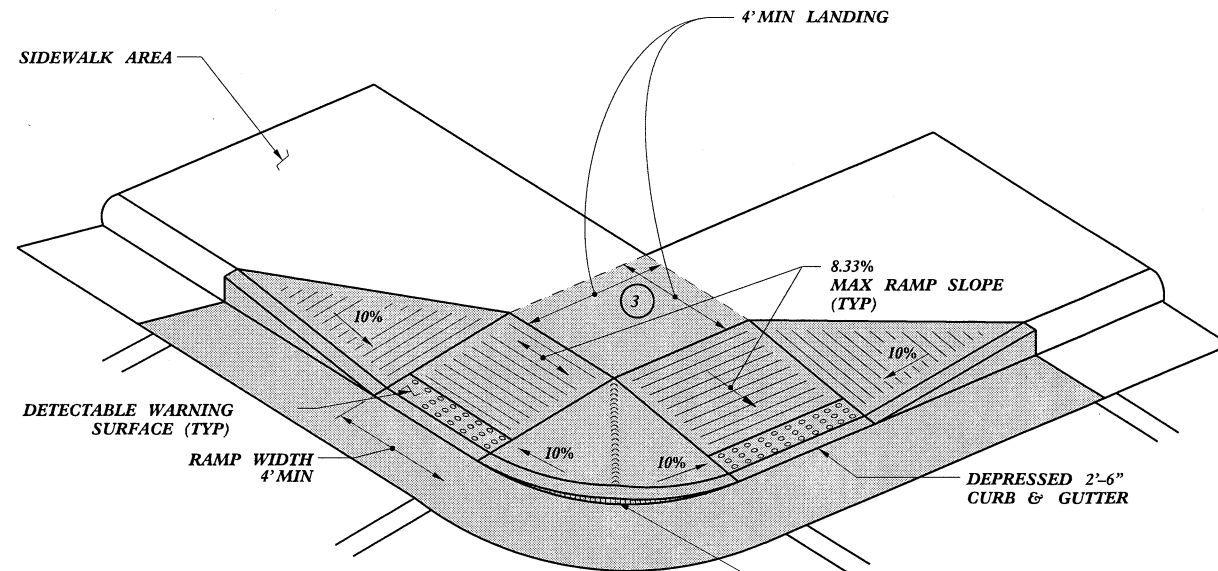


**TYPE 3**

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 J.Howerton AT C5023788

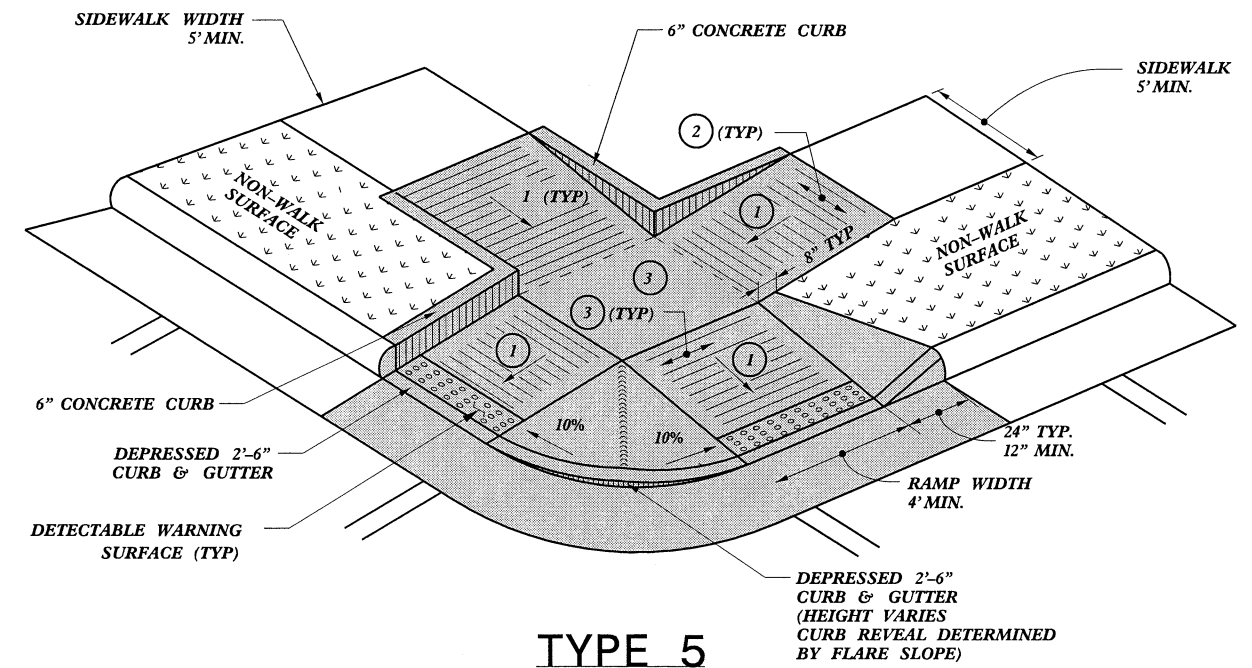
REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b> Office 919-707-6950 FAX 919-250-4119	
<b>CURB RAMPS</b> Parallel Ramps	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dgn	



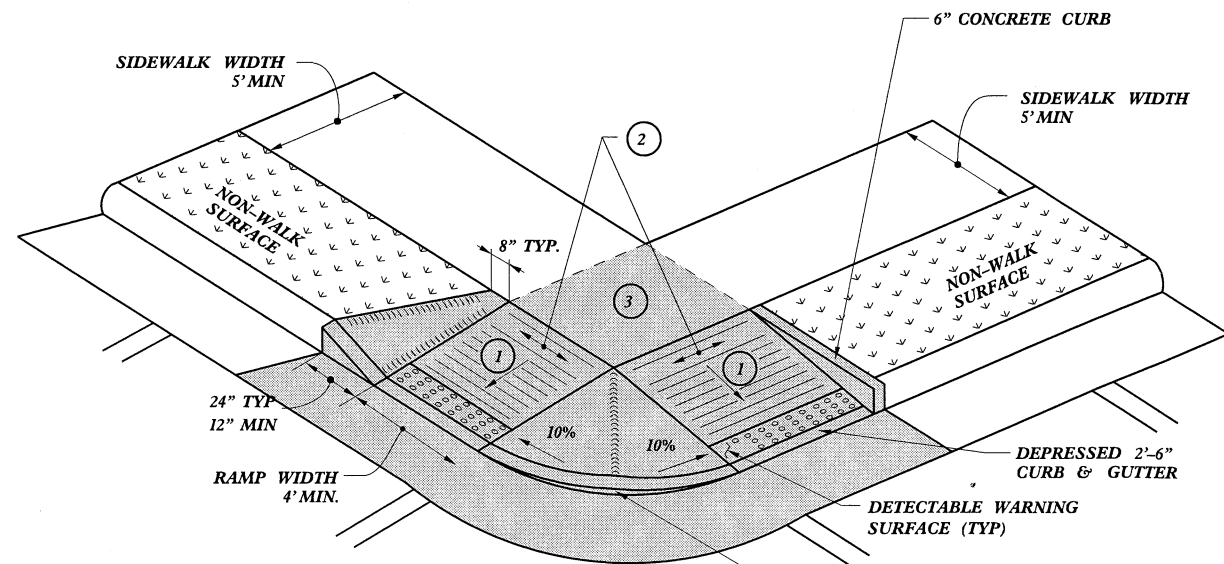
**TYPE 4**

DEPRESSED 2'-6" CURB & GUTTER (HEIGHT VARIES CURB REVEAL DETERMINED BY FLARE SLOPE)



**TYPE 5**

PAY LIMITS FOR CURB RAMP



**TYPE 4A**

DEPRESSED 2'-6" CURB & GUTTER (HEIGHT VARIES CURB REVEAL DETERMINED BY FLARE SLOPE)

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.

CONTRACT STANDARDS AND DEVELOPMENT UNIT  
Office 919-707-6950 FAX 919-250-4119

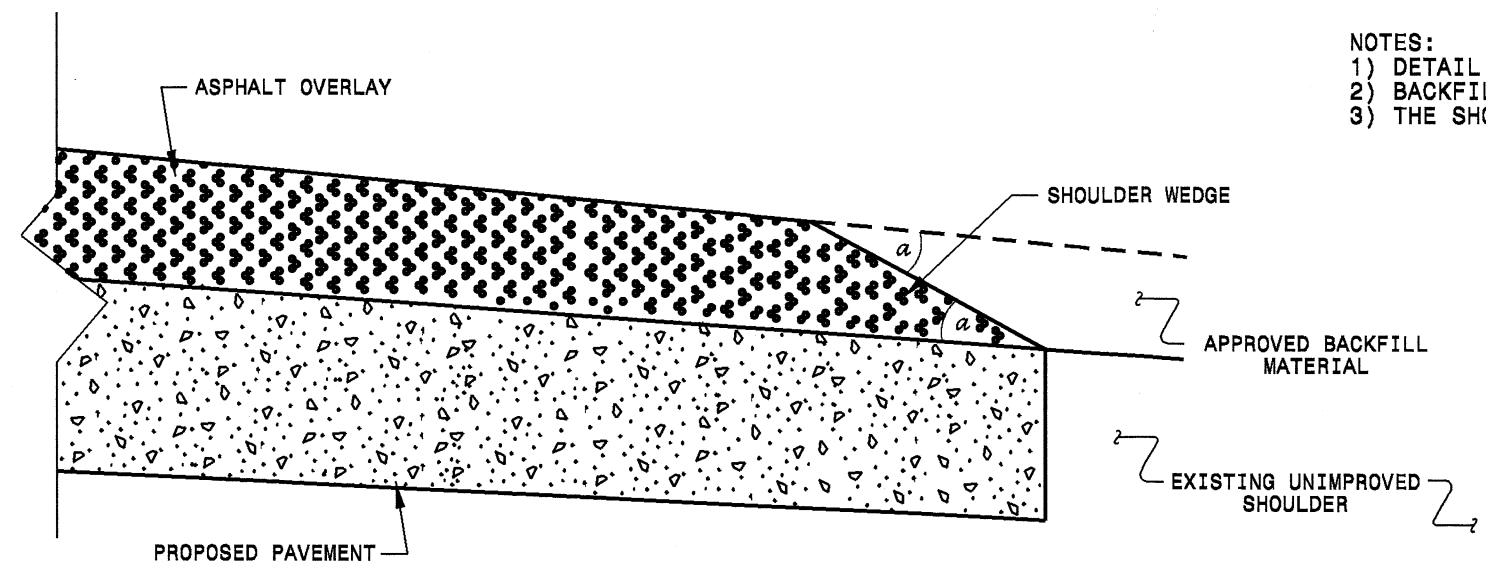
**CURB RAMPS**  
Shared Landing

ORIGINAL BY: J.S. HOWERTON DATE: 7/7/11  
MODIFIED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dwg

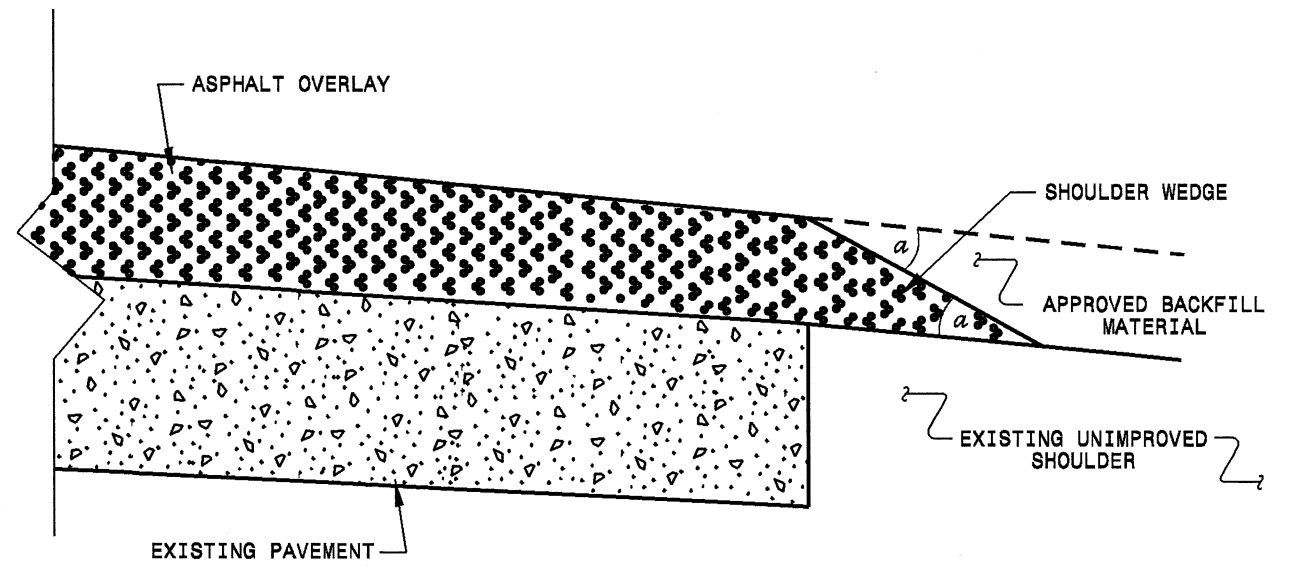
REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

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 JHowerton AT 150237581

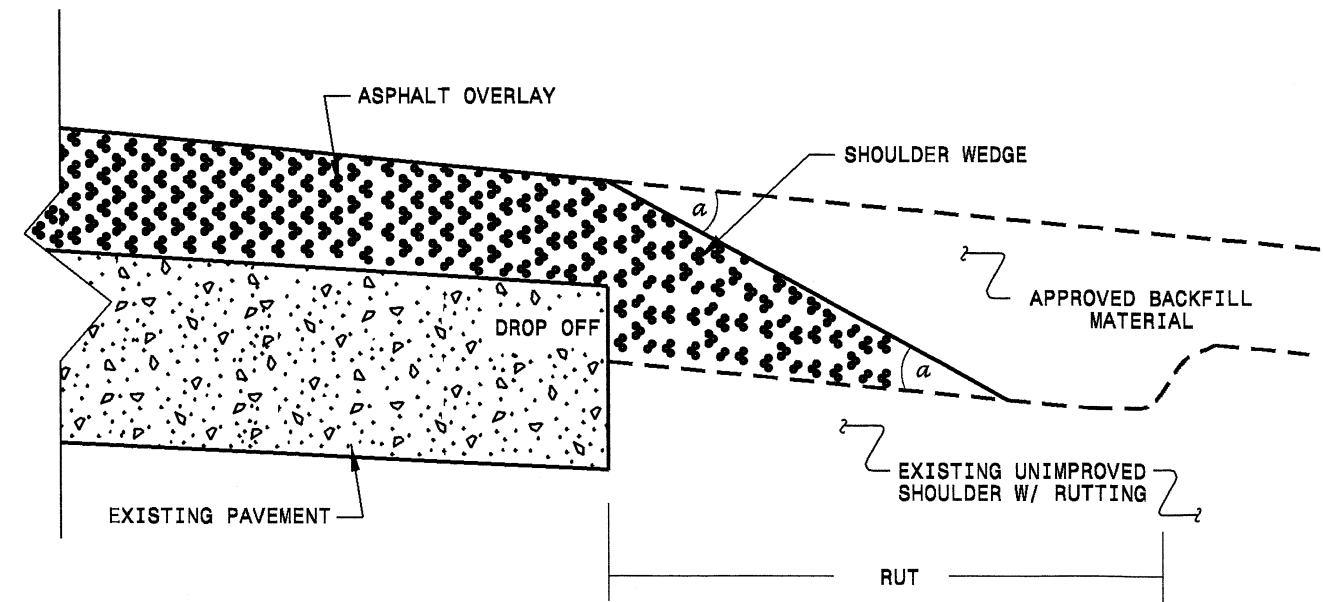
- NOTES:  
 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.  
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.  
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>			
Office 818-707-6950		FAX 818-250-4119	
<b>SHOULDER WEDGE DETAILS</b>			
ORIGINAL BY:	T. BPELL	DATE:	7-19-11
MODIFIED BY:		DATE:	10/18/12
CHECKED BY:		DATE:	
FILE SPEC:	s:\user\details\stand\shoulderwedgedetail.dgn		

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 \$\$\$USERNAME\$\$\$

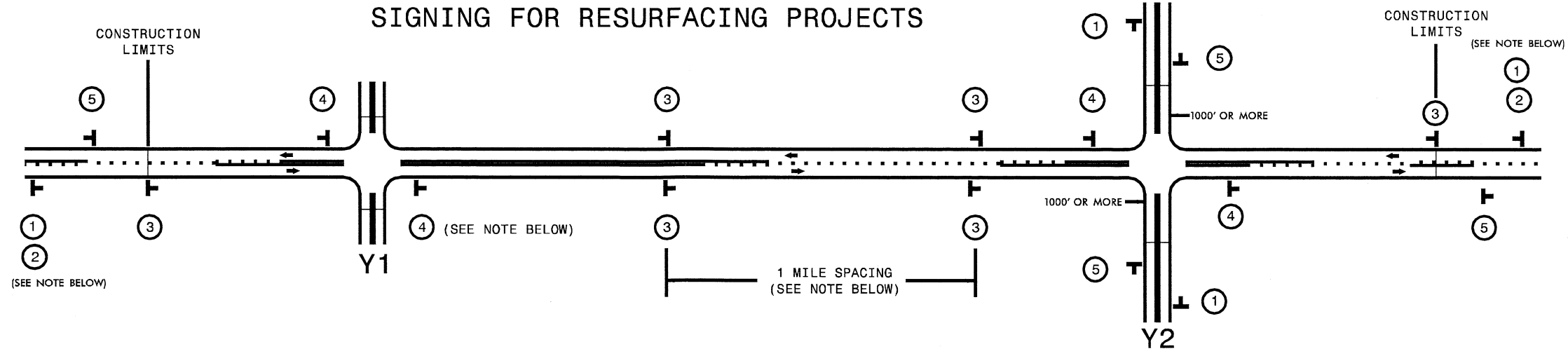
PROJECT NO.	SHEET NO.	TOTAL NO.
12cr.10551.12	13	
12cr.20551.19		

### SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	AGGREGATE SHOULDER BORROW TONS	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	MILLED RUMBLE STRIP (STD. 665.01) LF	3/4" MILLING SY	1 1/2" MILLING SY	3" MILLING SY	INCIDENTAL MILLING SY	BASE COURSE, B25.0B TONS	SURFACE COURSE, S9.5B TONS	LEVELING COURSE, S9.5B TONS	SURFACE COURSE, S9.5C TONS	LEVELING COURSE, S9.5C TONS	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	CURB RAMP (STD. 848.05, 848.06, and/or special detail in plans) EA	RETROFIT EXST. CURB RAMP (STD. 848.05, 848.06, and/or special detail in plans) EA	ADJUST DROP INLET EA	ADJUST MANHOLES EA	ADJUST METER OR VALVE BOX EA
12cr.10551.12	Lincoln	1	US 321 (SB LANES & RAMPS)	FROM BRIDGE #269 AT HIGH SHOALS N. TO A POINT 2000' N. OF THE NC 27 OVERPASS INCLUDING SB RAMPS	6,7	2	NO	NO	5.06	28	625	40	10.50	53,500	96,500			1,250				9,766	1,000	641	1,500					
<b>TOTAL FOR PROJ NO. 12cr.10551.12</b>									<b>5.06</b>		<b>625</b>	<b>40</b>	<b>10.50</b>	<b>53,500</b>	<b>96,500</b>			<b>1,250</b>			<b>9,766</b>	<b>1,000</b>	<b>641</b>	<b>1,500</b>						
12cr.20551.19	Lincoln	2	SR 1360 (BREVA RD PL RD)	FROM NC 73 TO BRIDGE #015	1	2	NO	NO	2.06	20	200	100	4.12					425		2,247	200			148	250					
12cr.20551.19	Lincoln	3	SR 1008 (KILLIAN ROAD)	FROM SR 1113 TO CA TAWBA CO	1	2	NO	NO	3.66	18	375	60	7.32					1,150		3,585	800			267	750					
12cr.20551.19	Lincoln	4	SR 1349/1360 (KING WILKINSON RD)	FROM SR 1371 TO SR 1383 FROM EDGE OF GOV. ISLE ROUNDABOUT TO THE END OF MAINT. AT THE BOAT LANDING	1,2,3	2	NO	NO	3.33	20	325	60	6.66							3,524	800			263	750					
12cr.20551.19	Lincoln	5	SR 1376 (BURTON LA NE)	FROM US321B/150 BRIDGE TO NC 27 AT THE COURT SQUARE	1,2,3	2	NO	NO	2.47	Vars. 18-20	225	40	4.52		2,450		250	850	2,532	300			209	600						
12cr.20551.19	Lincoln	6	SR1406 (S. ASPEN ST)	FROM NC 16 BUS. TO SR 1376	1,3,4,5	2	NO	NO	1.44	Vars. 22-40	90	20	1.80			2,100	7,700	2,750		1,890	250			130	500	5	9	3	6	5
12cr.20551.19	Lincoln	7	SR 1379 (WEBBS RD)	FROM NC 16 BUS. TO SR 1376	1	2	NO	NO	1.69	Vars. 24-36	150	60	3.38							2,457	200			160	400					
<b>TOTAL FOR PROJ NO. 12cr.20551.19</b>									<b>14.65</b>		<b>1,365</b>	<b>340</b>	<b>27.80</b>		<b>4,550</b>	<b>7,700</b>	<b>4,575</b>	<b>850</b>	<b>16,235</b>	<b>2,550</b>			<b>1,177</b>	<b>3,250</b>	<b>5</b>	<b>9</b>	<b>3</b>	<b>6</b>	<b>5</b>	
<b>GRAND TOTAL</b>									<b>19.71</b>		<b>1,990</b>	<b>380</b>	<b>38.30</b>	<b>53,500</b>	<b>96,500</b>	<b>4,550</b>	<b>7,700</b>	<b>5,825</b>	<b>850</b>	<b>16,235</b>	<b>2,550</b>	<b>9,766</b>	<b>1,000</b>	<b>1,818</b>	<b>4,750</b>	<b>5</b>	<b>9</b>	<b>3</b>	<b>6</b>	<b>5</b>



### SIGNING FOR RESURFACING PROJECTS



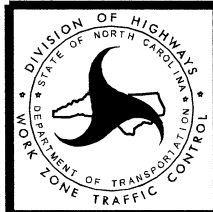
LEGEND	
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW

#### MAINLINE (-L-) SIGNING

#### -Y- LINE SIGNING

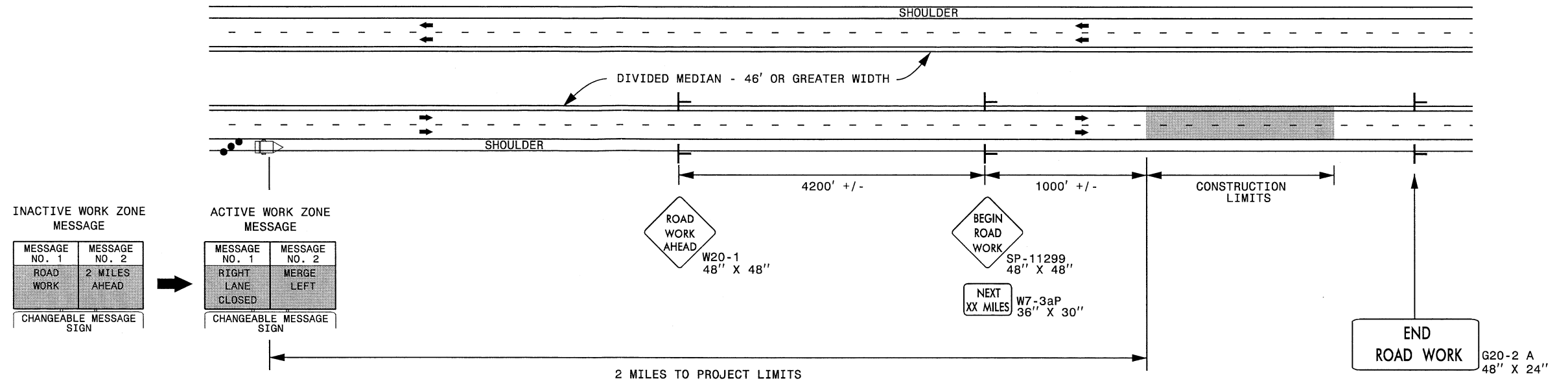
SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING		
	1		NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:		
	2		1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE		
	3		2) SUBDIVISION ROADS		
	4		3) DEAD END ROADS		
5		WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.			
			PLACED 500' IN ADVANCE OF FLAGGER.	PLACED 250' IN ADVANCE OF FLAGGER.	

\*\*\*\*\*SYTIME\*\*\*\*\*  
 \*\*\*\*\*DON\*\*\*\*\*  
 \*\*\*\*\*\*\*\*\*\*  
 \*\*\*\*\*\*\*\*\*\*

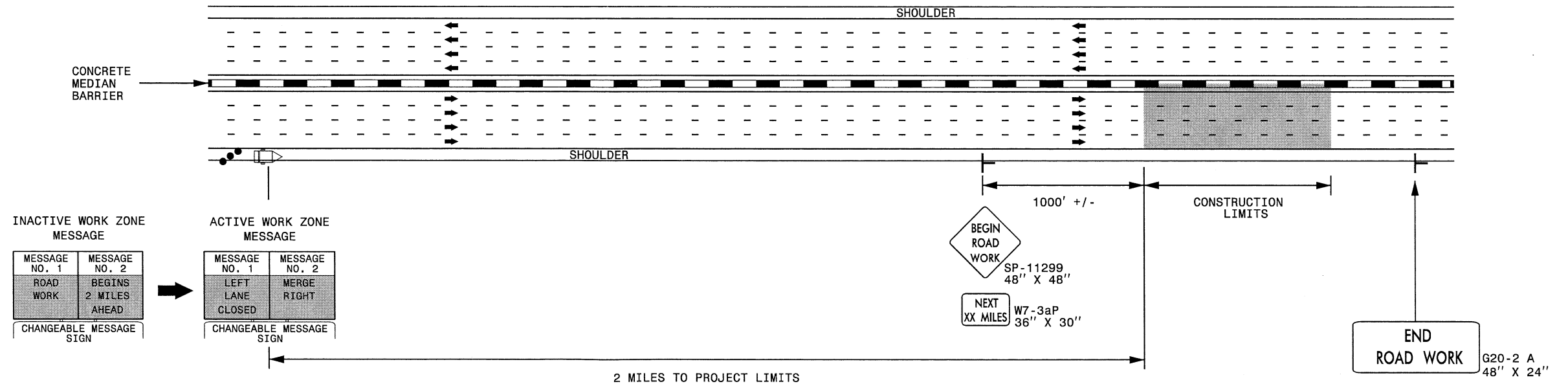


RESURFACING  
 ADVANCE WARNING SIGNS  
 FOR  
 RURAL AND SUBURBAN  
 2 LANE ROADWAYS

## DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



## DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER

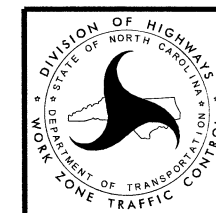


### NOTES:

- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
- 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
- 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

### LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM



**RESURFACING ADVANCE  
WARNING SIGNS FOR  
HIGH SPEED FACILITIES  
≥ 60 MPH**