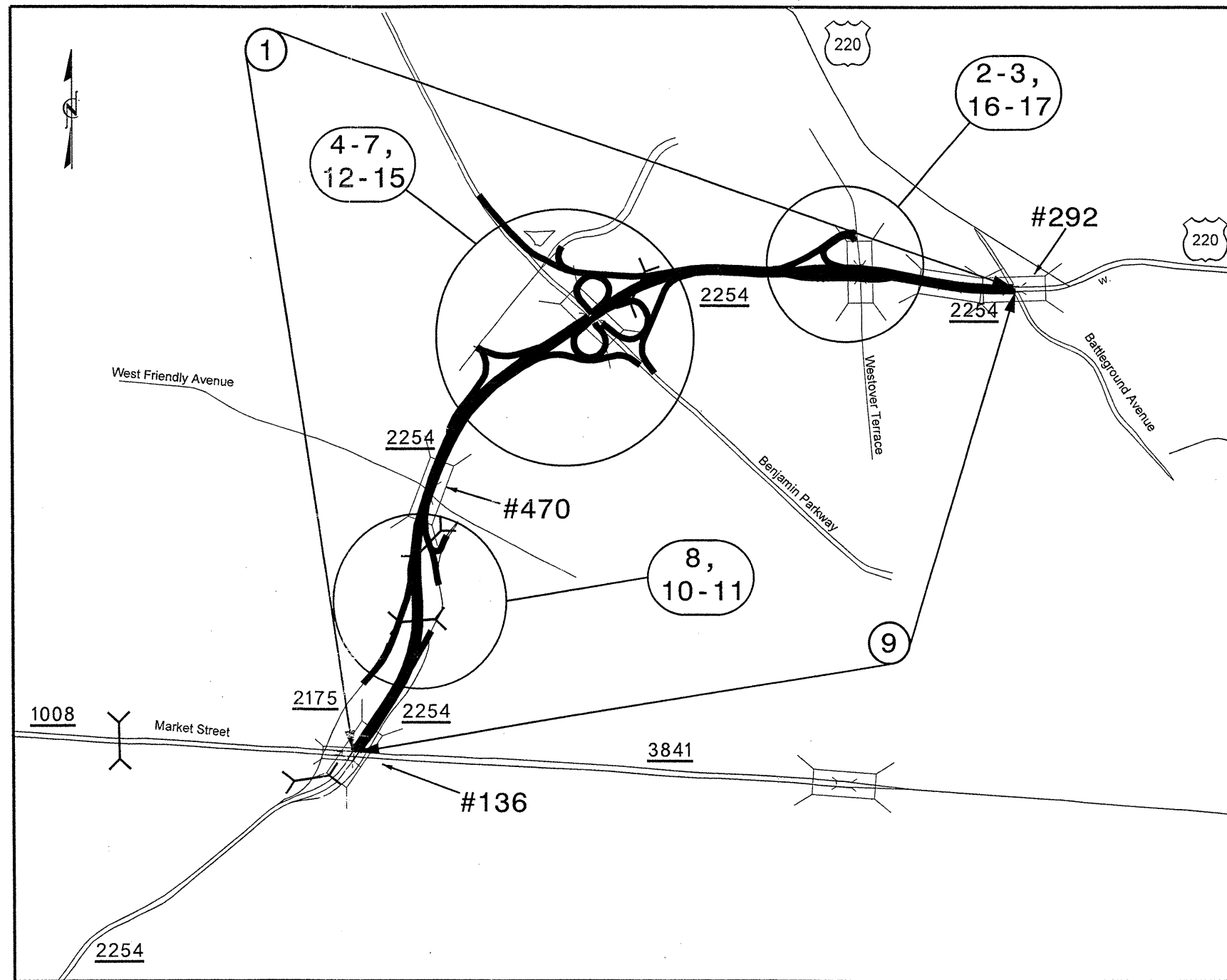
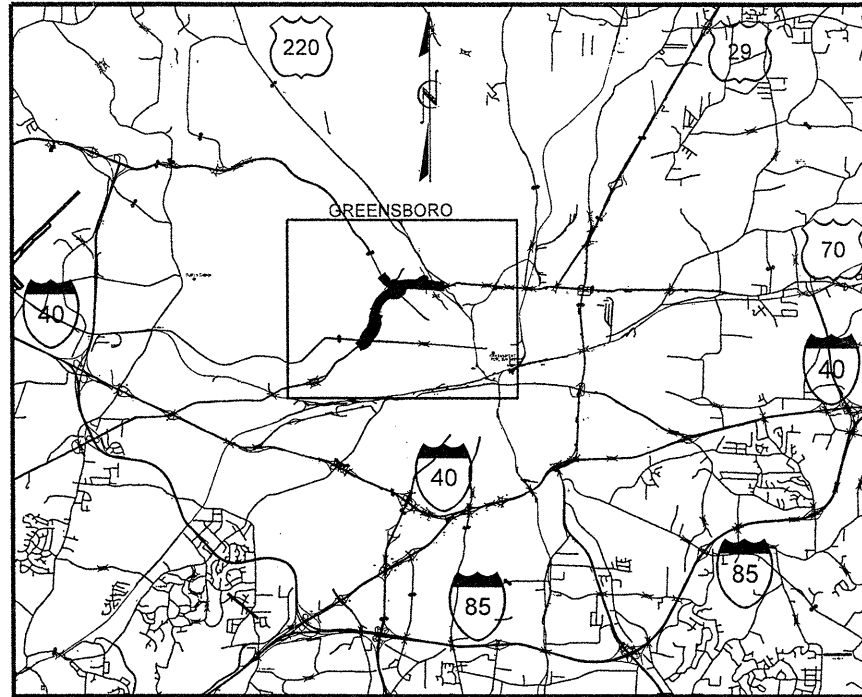


STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.20411.53	1	
F.A. PROJ. NO.			

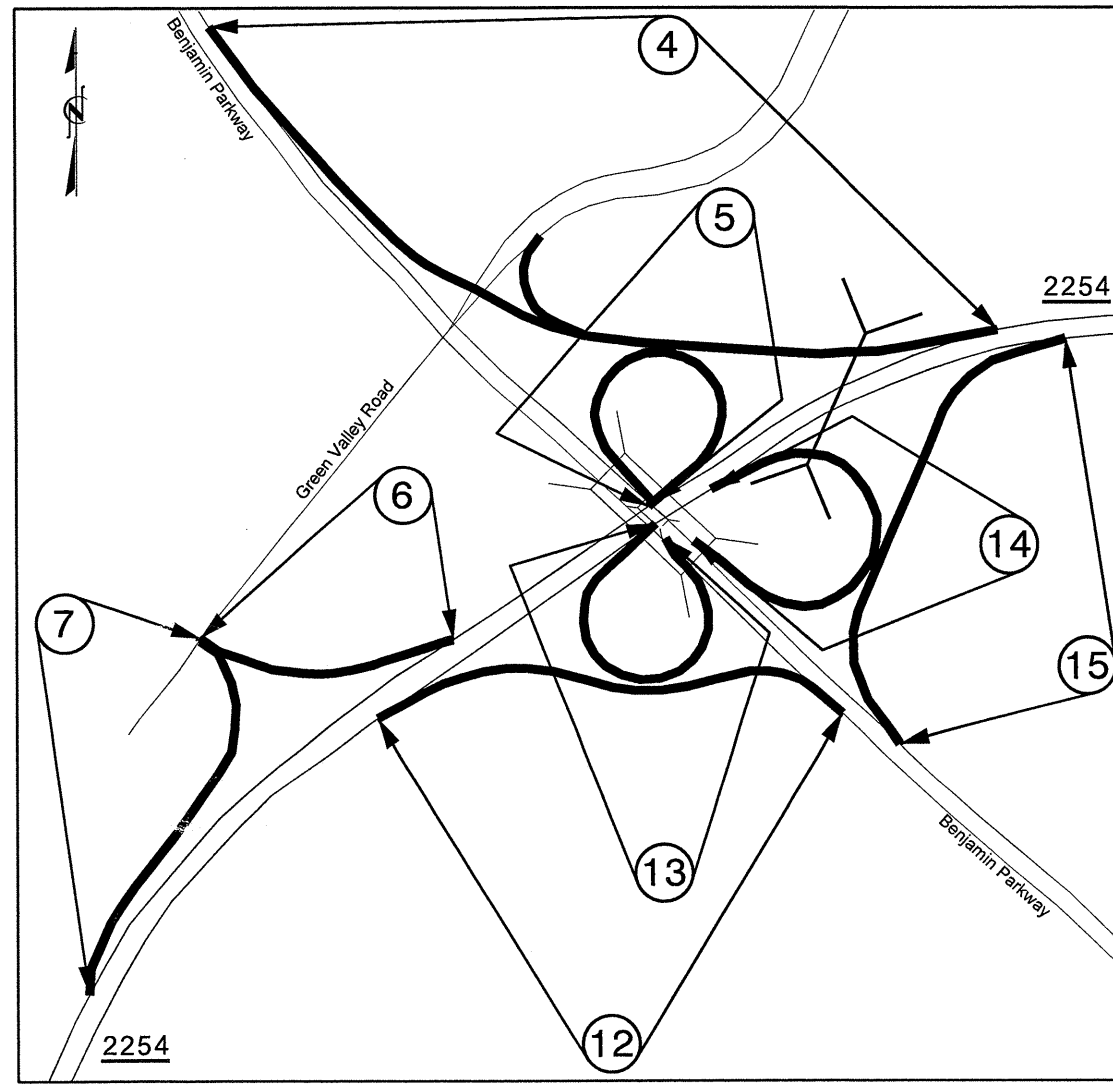
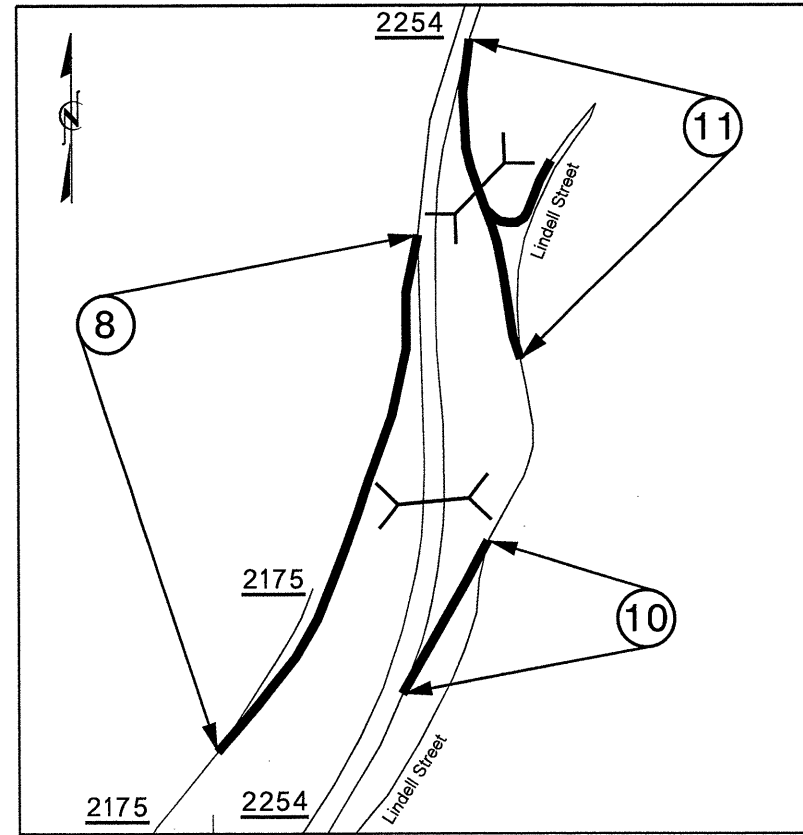
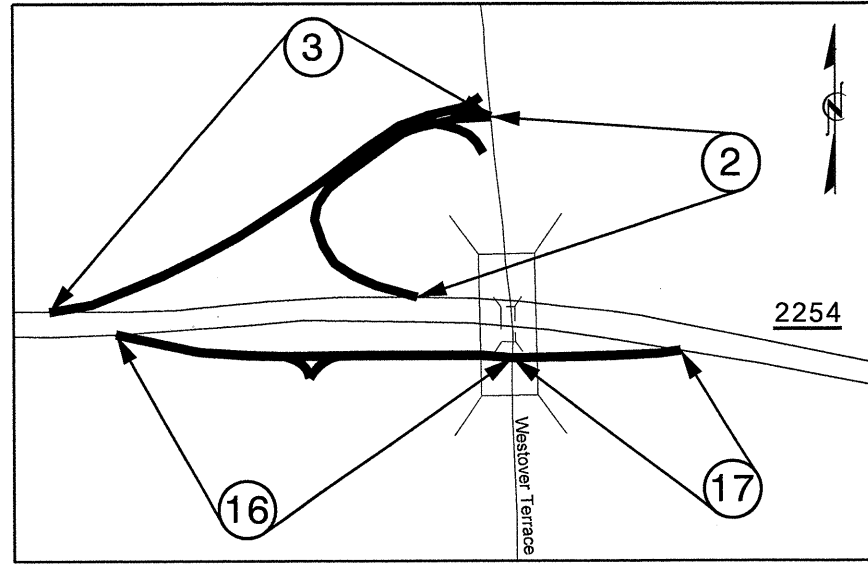
2013 GUILFORD COUNTY



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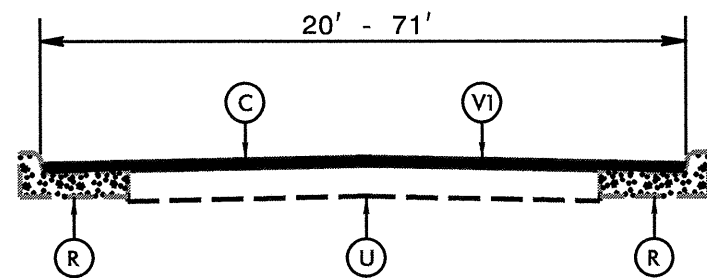
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N.C.	7CR.20411.53	2	
F.A. PROJ. NO.			

2013 GUILFORD COUNTY

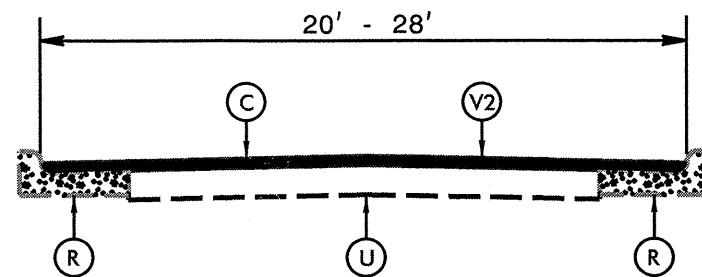


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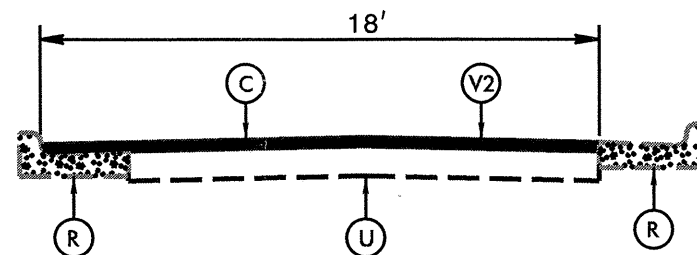
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.20411.53	3	5



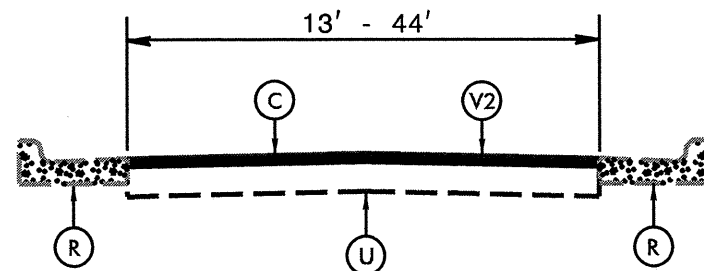
****NOTE: NO PAVEMENT ON BRIDGE #470**
 MAP 1: STA. 72+35 TO STA. 73+20
 MAP 9: STA. 27+95 TO STA. 28+80
TYPICAL SECTION NO. 1
 TO BE USED ON MAPS 1 AND 9



TYPICAL SECTION NO. 3
 TO BE USED ON MAPS 4, 8, 16, AND 17
 MAP 4: STA. 0+00 TO STA. 4+80
 MAP 8: STA. 0+00 TO STA. 2+30

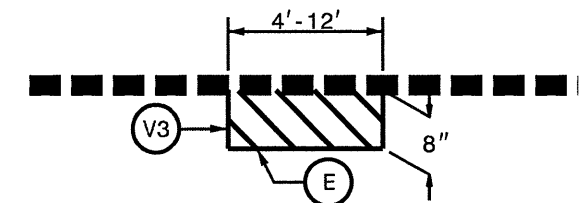


TYPICAL SECTION NO. 4
 TO BE USED ON MAP 6
 MAP 6: STA. 3+35 TO STA. 4+85



****NOTE: NO PAVEMENT ON SECTION ON MAP 4:**
 STA. 10+80 TO STA. 11+65
TYPICAL SECTION NO. 2
 TO BE USED ON MAPS 2, 3, 4, 5, 6, 7, 8,
 10, 11, 12, 13, 14, AND 15
 MAP 4: STA. 4+80 TO STA. 18+95
 MAP 6: STA. 0+00 TO STA. 3+35
 MAP 8: STA. 2+30 TO STA. 8+75

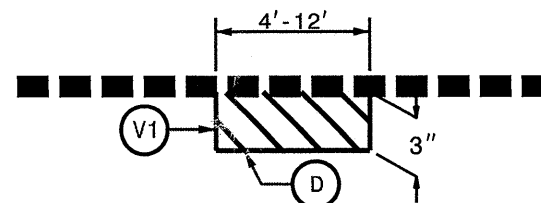
MILLING DETAIL 2



MILL EXISTING ASPHALT PAVEMENT 8" IN DEPTH AND FILL WITH BASE COURSE, TYPE B25.0B AT LOCATIONS AS DIRECTED BY THE ENGINEER.

TO BE USED IN CONJUNCTION WITH MAPS 1 AND 9
 MAP 1: 8" MILLING = 67 SYD
 BASE COURSE, TYPE B25.0B = 30 TONS
 MAP 9: 8" MILLING = 67 SYD
 BASE COURSE, TYPE B25.0B = 30 TONS

MILLING DETAIL 1



MILL EXISTING ASPHALT PAVEMENT 3" IN DEPTH AND FILL WITH INTERMEDIATE COURSE, TYPE I19.0B AT LOCATIONS AS DIRECTED BY THE ENGINEER.

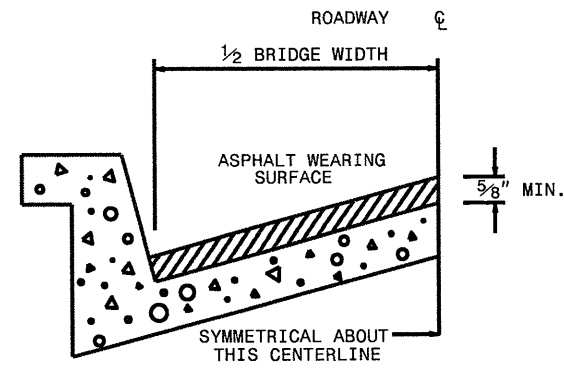
TO BE USED IN CONJUNCTION WITH MAPS 1 AND 9
 MAP 1: 3" MILLING = 400 SYD
 INTERMEDIATE COURSE, TYPE I19.0B = 68 TONS
 MAP 9: 3" MILLING = 400 SYD
 INTERMEDIATE COURSE, TYPE I19.0B = 68 TONS

PAVEMENT SCHEDULE

C	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.		
D	PROP. APPROX. 3" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.		
E	PROP. APPROX. 8" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. IN EACH OF TWO LAYERS		
R	EXISTING CONCRETE CURB & GUTTER		
U	EXISTING PAVEMENT.		
V1	3" MILLING	V2	1½" MILLING
V3	8" MILLING		

\$\$\$\$\$SYTIME\$\$\$\$\$
 \$\$\$\$\$\$DGN\$\$\$\$\$
 \$\$\$\$\$\$USERNAME\$\$\$\$\$

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.20411.53	4	6



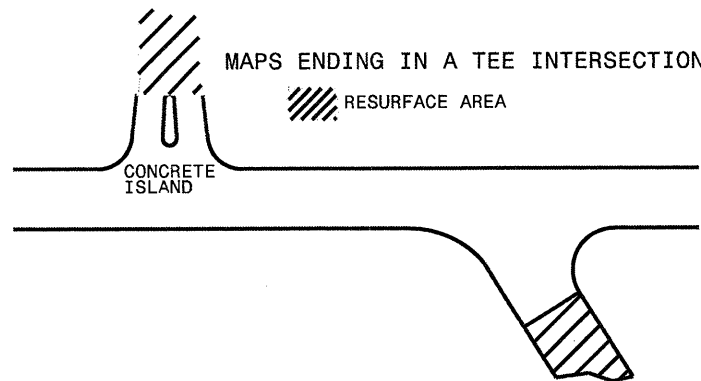
BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN. THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

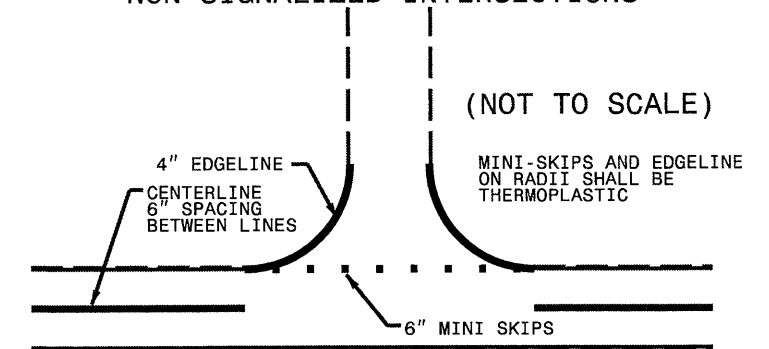
NOTES

ALL UNPAVED S.R. ROUTES TO BE SURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.
 ALL PAVED S.R. ROUTES TO BE RESURFACED TO END OF RADDII, OR AS DIRECTED BY THE ENGINEER. EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES. BRIDGES TO BE RESURFACED AT LOCATIONS AND DEPTH AS DIRECTED BY THE ENGINEER.

PAVING DETAIL 1
 MAIN LINE IS NOT BEING RESURFACED



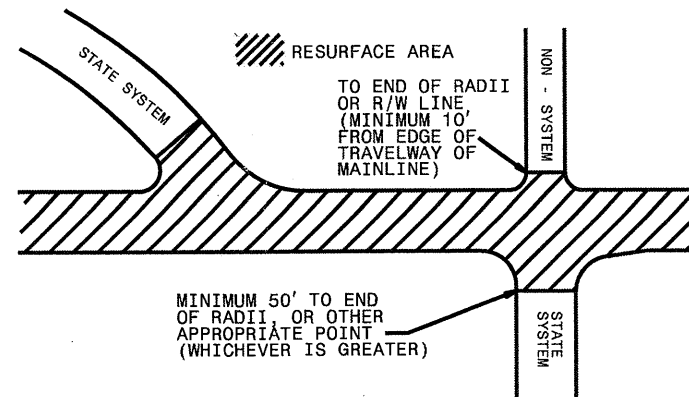
TO BE USED AT ALL
 NON-SIGNALIZED INTERSECTIONS



NOTE: MINI SKIPS SHALL BE PLACED ON A 8' CYCLE, CONTAINING AN 6' AND 2' SKIP, THE WIDTH OF THE SKIP SHALL BE 6".

PAVING DETAIL 2
 MAIN LINE IS BEING RESURFACED

NOTE: NON-SYSTEM (CITY STREET, PRIVATE DRIVE, SCHOOL BUS DRIVE)



PAVEMENT SCHEDULE

C	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.		
D	PROP. APPROX. 3" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.		
E	PROP. APPROX. 8" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. IN EACH OF TWO LAYERS		
R	EXISTING CONCRETE CURB & GUTTER		
U	EXISTING PAVEMENT.		
V1	3" MILLING	V2	1 1/2" MILLING
V3	8" MILLING		

\$\$\$\$\$SYTIME\$\$\$\$\$
 \$\$\$DCN\$\$\$\$\$
 \$\$\$SERNAME\$\$\$\$\$

PROJECT NO.	SHEET NO.	TOTAL NO.
7CR.20411.53	5	

SUMMARY OF QUANTITIES

PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	MILLING ASPHALT PAVEMENT, 8" DEPTH SY	MILLING ASPHALT PAVEMENT, 3" DEPTH SY	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH SY	ASPHALT CONC BASE COURSE, TYPE B25.0B TONS	ASPHALT CONC INTERMEDIATE COURSE, TYPE I19.0B TONS	ASPHALT CONC SURFACE COURSE, TYPE SF9.5A TONS	ASPHALT BINDER FOR PLANT MIX TONS	PORTABLE LIGHTING LS	TRENCHING (UNPAVED) (1) (2") LF	JUNCTION BOX (STANDARD SIZE) EA	2" RISER W/ WEATHERHEAD EA	INDUCTIVE LOOP SAW CUT LF	LEAD-IN CABLE (14-2) LF			
7CR.20411.53	Guilford	1	SR 2254 WESTBOUND (WENDOVER AVENUE)	FROM BRIDGE #292 OVER NON-SYSTEM (BATTLEGROUND AVENUE) - 0.07 TO BRIDGE #136 OVER SR 1008/3841 (MARKET STREET) - 2.00	1	2		NO	NO	0.037	39	67	1,247		30	68	70	9	1								
				BRIDGE #467	1	2		NO	NO	0.089	39				2,036				169	11							
					1	2		NO	NO	0.042	39				961				80	5							
					1	2		NO	NO	0.168	39				3,844				318	21							
					1	2		NO	NO	0.021	39-71				678				56	4							
					1	2		NO	NO	0.041	39				938				78	5							
					1	2		NO	NO	0.041	27-39				794				66	4							
					1	2		NO	NO	0.015	40-47				387				32	2							
					1	2		NO	NO	0.253	40				5,937				491	33							
					1	2		NO	NO	0.039	40-67				1,236				102	7							
					1	2		NO	NO	0.062	40				1,455				120	8							
					1	2		NO	NO	0.041	28-40				818				68	5							
					1	2		NO	NO	0.015	40-49				396				33	2							
					1	2		NO	NO	0.134	40				3,145				260	17							
					1	2		NO	NO	0.039	40-69				1,258				104	7							
					1	2		NO	NO	0.103	40				2,417				200	13							
					1	2		NO	NO	0.041	28-40				818				68	5							
					1	2		NO	NO	0.032	40-49				845				70	5							
					1	2		NO	NO	0.159	40				3,731				309	21							
					1	2		NO	NO	0.016	40				40												
					1	2		NO	NO	0.148	40				3,473				287	19							
					1	2		NO	NO	0.051	40-71				1,676				139	9							
					1	2		NO	NO	0.343	40				8,049				666	45							
								TOTAL FOR MAP NO. 1						1.93		67	46,139		30	68	3,786	257	1				
					2		OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (WESTOVER TERRACE)	2	2		NO	NO	0.081	19			903		75	5						
	2				2	2		NO	NO	0.017	19-44			319		26	2										
	2				2	2		NO	NO	0.025	18			624		52	3										
				TOTAL FOR MAP NO. 2						0.123				1,846		153	10										
	3		ON RAMP	FROM NON-SYSTEM (WESTOVER TERRACE) TO SR 2254 WESTBOUND (WENDOVER AVENUE)	2	2		NO	NO	0.016	18			412		34	2										
	3				2	2		NO	NO	0.02	16-40			329		27	2										
	3				2	2		NO	NO	0.13	16			1,220		101	7										
				TOTAL FOR MAP NO. 3						0.166				1,961		162	11										
	4		OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (BENJAMIN PARKWAY)	3	2		NO	NO	0.04	20-25			540		45	3										
	4				3	2		NO	NO	0.043	20			505		42	3										
	4				3	2		NO	NO	0.009	20-21			111		9	1										
	4				2	2		NO	NO	0.021	16-21			234		19	1										
	4				2	2		NO	NO	0.051	21			628		52	3										
	4				2	2		NO	NO	0.015	21-37			621		51	3										
	4				2	2		NO	NO	0.017	13			130		11	1										
	4				2	2		NO	NO	0.009	13-25			100		8	1										
	4			CONCRETE	2	2		NO	NO	0.016	25																
	4				2	2		NO	NO	0.01	13-25			112		9	1										
	4				2	2		NO	NO	0.128	13			976		81	5										
				TOTAL FOR MAP NO. 4						0.359				3,957		327	22										
	5		ON RAMP	FROM NON-SYSTEM (BENJAMIN PARKWAY) TO SR 2254 WESTBOUND (WENDOVER AVENUE)	2	2		NO	NO	0.126	18			1,331		111	7										
				TOTAL FOR MAP NO. 5						0.126				1,331		111	7										
	6		OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (GREEN VALLEY ROAD)	2	2		NO	NO	0.021	16-24			246		20	1										
	6				2	2		NO	NO	0.043	16			404		34	2										
	6				4	2		NO	NO	0.028	18			296		25	2		50	1	1	400	100				
				TOTAL FOR MAP NO. 6						0.092				946		79	5		50	1	1	400	100				
	7		ON RAMP	FROM NON-SYSTEM (GREEN VALLEY ROAD) TO SR 2254 WESTBOUND (WENDOVER AVENUE)	2	2		NO	NO	0.115	18			1,214		101	7										
				TOTAL FOR MAP NO. 7						0.115				1,214		101	7										
	8		OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO SR 2175 (GREEN VALLEY COURT)	3	2		NO	NO	0.045	28			739		61	4										
				TOTAL FOR MAP NO. 8						0.167				2,457		203	14										

PROJECT NO.	SHEET NO.	TOTAL NO.
7CR.20411.53	6	

SUMMARY OF QUANTITIES

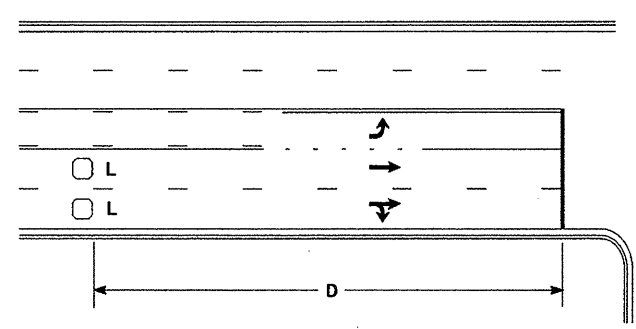
PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	MILLING ASPHALT PAVEMENT, 8" DEPTH	MILLING ASPHALT PAVEMENT, 3" DEPTH	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH	ASPHALT CONC BASE COURSE, TYPE B25.0B	ASPHALT CONC INTERMEDIATE COURSE, TYPE I19.0B	ASPHALT CONC SURFACE COURSE, TYPE SF9.5A	ASPHALT BINDER FOR PLANT MIX	PORTABLE LIGHTING	TRENCHING (UNPAVED) (1) (2")	JUNCTION BOX (STANDARD SIZE)	2" RISER W/ WEATHERHEAD	INDUCTIVE LOOP SAW CUT	LEAD-IN CABLE (14-2)
NO		NO								MI	FT	SY	SY	SY	TONS	TONS	TONS	TONS	LS	LF	EA	EA	LF	LF
				FROM BRIDGE #136 OVER SR 1008/3841 (MARKET STREET) - 2.09 TO BRIDGE #292 OVER NON-SYSTEM (BATTLEGROUND AVENUE) - 4.00	1	2		NO	NO	0.174	40	67	4,483		30	68	338	27						
					1	2		NO	NO	0.043	40-69		1,388				115	8						
					1	2		NO	NO	0.237	40		5,562				460	31						
					1	2		NO	NO	0.043	28-40		858				71	5						
					1	2		NO	NO	0.015	40-48		387				32	2						
					1	2		NO	NO	0.018	40		422				35	2						
				BRIDGE #470	1	2		NO	NO	0.016	40													
					1	2		NO	NO	0.329	40		7,721				639	43						
					1	2		NO	NO	0.033	40-68		1,045				86	6						
					1	2		NO	NO	0.042	40		986				82	5						
					1	2		NO	NO	0.043	28-40		858				71	5						
					1	2		NO	NO	0.012	40-47		310				26	2						
					1	2		NO	NO	0.077	40		1,807				150	10						
					1	2		NO	NO	0.025	40-71		821				68	5						
					1	2		NO	NO	0.06	40		1,408				117	8						
					1	2		NO	NO	0.05	28-40		997				83	6						
					1	2		NO	NO	0.014	40-48		361				30	2						
					1	2		NO	NO	0.152	40		3,567				295	20						
					1	2		NO	NO	0.07	40-68		2,218				183	12						
					1	2		NO	NO	0.171	40		4,013				332	22						
					1	2		NO	NO	0.053	29-40		1,088				90	6						
					1	2		NO	NO	0.017	40-49		449				37	2						
					1	2		NO	NO	0.047	40		1,103				91	6						
				BRIDGE #467	1	2		NO	NO	0.039	40		915				76	5						
					1	2		NO	NO	0.018	40		422				35	2						
					1	2		NO	NO	0.012	40-68		380				31	2						
					1	2		NO	NO	0.044	27-40		878				73	5						
					1	2		NO	NO	0.015	40-47		387				32	2						
					1	2		NO	NO	0.039	40		915				76	5						
				TOTAL FOR MAP NO. 9						1.908		67	45,749		30	68	3,754	256						
10			OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER) TO NON-SYSTEM (LINDELL STREET)	2	2		NO	NO	0.036	16			4,129			89	6						
				TOTAL FOR MAP NO. 10						0.036				4,129			89	6						
11			ON RAMP	FROM NON-SYSTEM (LINDELL STREET) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	2	2		NO	NO	0.108	23			3,154			151	10						
				TOTAL FOR MAP NO. 11						0.108				3,154			151	10						
12			OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (BENJAMIN PARKWAY EASTBOUND)	2	2		NO	NO	0.018	16-23		211				18	1						
					2	2		NO	NO	0.157	16		1,474				123	8						
				TOTAL FOR MAP NO. 12						0.175				1,685			141	9						
13			ON RAMP	FROM NON-SYSTEM (BENJAMIN PARKWAY EASTBOUND) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	2	2		NO	NO	0.021	18-23		259				21	1						
					2	2		NO	NO	0.12	18		1,267				105	7						
				TOTAL FOR MAP NO. 13						0.141				1,526			126	8						
14			OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (BENJAMIN PARKWAY WESTBOUND)	2	2		NO	NO	0.014	19-21		164				14	1						
					2	2		NO	NO	0.134	19		1,494				124	8						
				TOTAL FOR MAP NO. 14						0.148				1,658			138	9						
15			ON RAMP	FROM NON-SYSTEM (BENJAMIN PARKWAY WESTBOUND) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	2	2		NO	NO	0.182	16			1,708			142	10						
					2	2		NO	NO	0.182				1,708			142	10						
				TOTAL FOR MAP NO. 15						0.182				1,708			142	10						
16			OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (WESTOVER TERRACE)	3	2		NO	NO	0.094	20			1,342			203	14						
					3	2		NO	NO	0.06	28			986			82	5		50	1	1	250	100
				TOTAL FOR MAP NO. 16						0.154				2,328			285	19		50	1	1	250	100
17			ON RAMP	FROM NON-SYSTEM (WESTOVER TERRACE) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	3	2		NO	NO	0.041	20			481			40	3						
					3	2		NO	NO	0.041				481			40	3						
				TOTAL FOR MAP NO. 17						0.041				481			40	3						
TOTAL FOR PROJ NO. 7CR.20411.53										1.93		134	91,888	30,381	60	136	9,788	663	1	100	2	2	650	200
GRAND TOTAL										1.93		134	91,888	30,381	60	136	9,788	663	1	100	2	2	650	200

PROJECT NO.	SHEET NO.	TOTAL NO.
7CR.20411.53	7	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4399000000-N	4685000000-E		4686000000-E	4695000000-E	4697000000-E	4700000000-E	4710000000-E	4721000000-E		4725000000-E		4810000000-E		4820000000-E	4825000000-E	4835000000-E	4840000000-N		4845000000-N		4905000000-N					
										TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	4" X 120 M WHITE THERMO	8" X 90 M WHITE THERMO	8" X 120 M WHITE THERMO	12" X 90 M WHITE THERMO	24" X 120 M WHITE THERMO	THERMO MSG ONLY 120 M	THERMO MSG SCHOOL 120 M	THERMO RT ARROW 90 M	THERMO LT ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO LT STR RT ARROW 90 M	4" WHITE PAINT	4" YELLOW PAINT	8" WHITE PAINT	12" WHITE PAINT	24" WHITE PAINT	PAINT MSG ONLY	PAINT MSG SCHOOL	PAINT RT ARROW	PAINT LT ARROW	PAINT STR & RT ARROW	PAINT LT STR RT ARROW	SNOWPLOWABLE PAVEMENT MARKERS EA	
NO	NO	NO	NO	NO	NO	NO	NO	NO	NO	LS	LF	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA				
7CR.20411.53	Guilford	1	SR 2254 WESTBOUND (WENDOVER AVENUE)	FROM BRIDGE #292 OVER NON-SYSTEM (BATTLEGROUND AVENUE) - 0.07 TO BRIDGE #136 OVER SR 1008/3841 (MARKET STREET) - 2.09	1	2		1.93	27-71	1	3,465	10,180	3,925	2,650				8		4				7,390	10,180	2,650			8		4		400			
		2	OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (WESTOVER TERRACE)	2	2		0.123	18-44																											
		3	ON RAMP	FROM NON-SYSTEM (WESTOVER TERRACE) TO SR 2254 WESTBOUND (WENDOVER AVENUE)	2	2		0.166	16-40																											
		4	OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (BENJAMIN PARKWAY)	2-3	2		0.359	13-37			280			160	34									280		194									
		5	ON RAMP	FROM NON-SYSTEM (BENJAMIN PARKWAY) TO SR 2254 WESTBOUND (WENDOVER AVENUE)	2	2		0.126	18																											
		6	OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (GREEN VALLEY ROAD)	2,4	2		0.092	16-24			110			86		112															2	2			
		7	ON RAMP	FROM NON-SYSTEM (GREEN VALLEY ROAD) TO SR 2254 WESTBOUND (WENDOVER AVENUE)	2	2		0.115	18																											
		8	OFF RAMP	FROM SR 2254 WESTBOUND (WENDOVER AVENUE) TO SR 2175 (GREEN VALLEY COURT)	2-3	2		0.167	24-28							945		295																		
		9	SR 2254 EASTBOUND (WENDOVER AVENUE)	FROM BRIDGE #136 OVER SR 1008/3841 (MARKET STREET) - 2.09 TO BRIDGE #292 OVER NON-SYSTEM (BATTLEGROUND AVENUE) - 4.00	1	2		1.908	27-71			3,810	10,065	3,613	3,950										7,423	10,065	3,950								450	
		10	OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (LINDELL STREET)	2	2		0.036	16																											
		11	ON RAMP	FROM NON-SYSTEM (LINDELL STREET) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	2	2		0.108	23																											
		12	OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (BENJAMIN PARKWAY EASTBOUND)	2	2		0.175	16-23																											
		13	ON RAMP	FROM NON-SYSTEM (BENJAMIN PARKWAY EASTBOUND) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	2	2		0.141	18-23																											
		14	OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (BENJAMIN PARKWAY WESTBOUND)	2	2		0.148	19-21																											
		15	ON RAMP	FROM NON-SYSTEM (BENJAMIN PARKWAY WESTBOUND) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	2	2		0.182	16																											
		16	OFF RAMP	FROM SR 2254 EASTBOUND (WENDOVER AVENUE) TO NON-SYSTEM (WESTOVER TERRACE)	3	2		0.154	20-28						120	80		92	4	6			3		2	120		80		92	4	6		3	2	
		17	ON RAMP	FROM NON-SYSTEM (WESTOVER TERRACE) TO SR 2254 EASTBOUND (WENDOVER AVENUE)	3	2		0.041	20																											
TOTAL FOR PROJ NO. 7CR.20411.53											1	7,665	20,245	7,744	7,785	146	295	118	12	6	4	5	2	2	15,409	20,245	7,931	295	118	12	6	4	5	2	2	850
												27,910							18			13			35,654			18			13					
GRAND TOTAL											1	7,665	20,245	7,744	7,785	146	295	118	12	6	4	5	2	2	15,409	20,245	7,931	295	118	12	6	4	5	2	2	850
												27,910																18			13					

High Speed Detection [≥40 mph (64 km/hr)]

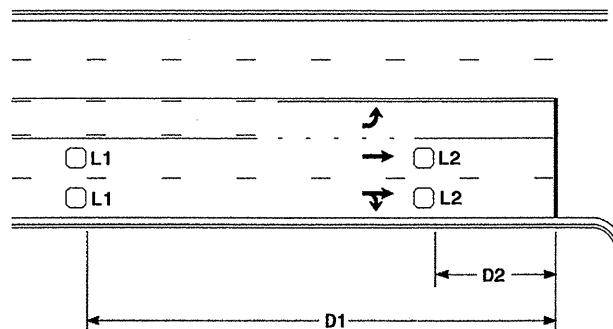


Speed Limit mph (km/hr)	D ft (m)
40 (64)	250 (75)
45 (72)	300 (90)
50 (80)	355 (110)
55 (88)	420 (130)

L = 6ft X 6ft (1.8m X 1.8m)
Wired in series for TS1
Controllers
Wired separately for TS2,
170, and 2070L Controllers

Volume Density Operation

OR

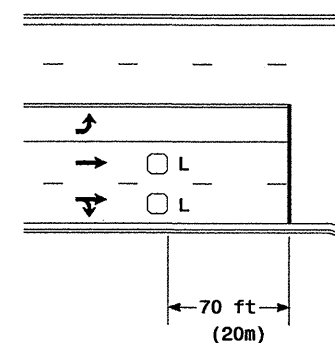


Speed Limit mph (km/hr)	D1 ft (m)	D2 ft (m)
40 (64)	250 (75)	80 (25)
45 (72)	300 (90)	90 (27)
50 (80)	355 (110)	100 (30)
55 (88)	420 (130)	110 (35)

L1 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series
L2 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series

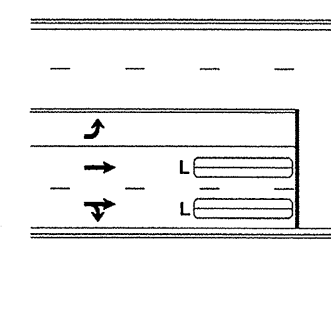
"Stretch" Operation

Low Speed Detection [≤35 mph (56 km/hr)]



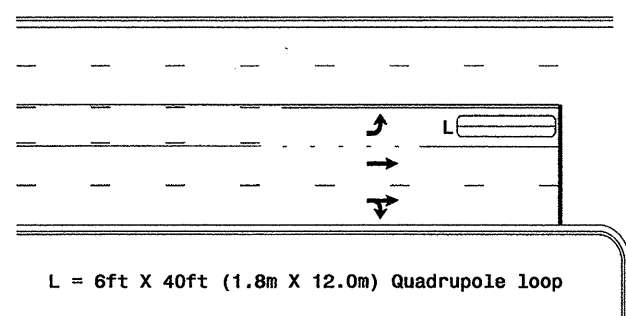
L = 6ft X 6ft (1.8m X 1.8m)
Wired in series

OR



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop, wired separately

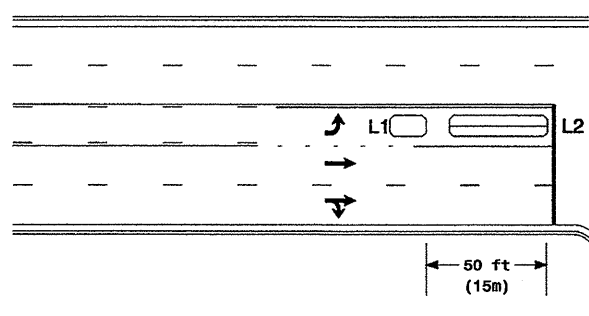
Left Turn Lane Detection



L = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

Presence Loop Detection

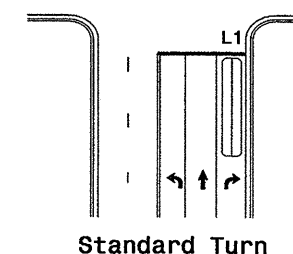
OR



L1 = 6ft X 15ft (1.8m X 4.6m) Queue detector
L2 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

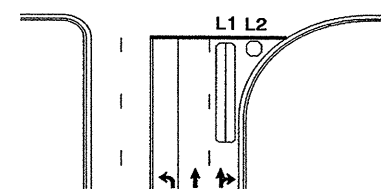
Queue Loop Detection

Right Turn Lane Detection

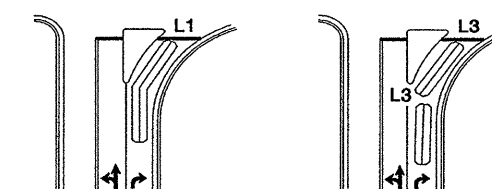


Standard Turn

L1 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop
L2 = 6ft X 6ft (1.8m X 1.8m) [Minimum] Presence loop
Wired separately
L3 = 6ft X 20ft (1.8m X 6.0m) Quadrupole loop
Wired in series

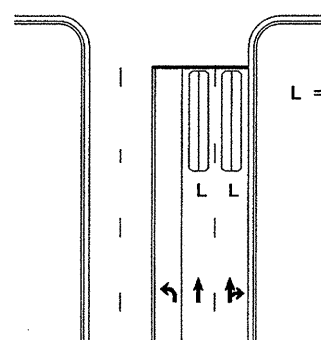


Wide Radius Turn



Channelized Turn

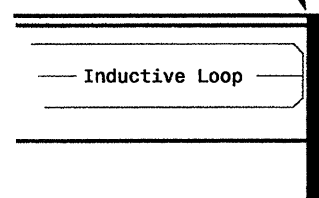
Side Street Detection



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines

Locate loop slightly
behind leading
edge of stop line



Note:
Loop may be located in advance
of stop line when stop line is
greater than 15' (4.5m) from edge
of intersecting roadway; or, when
loop detects a permissive or
protected/permissive left turn.


Recommended Number of Turns

Single 6' X 6' (1.8m X 1.8m)
loop (wired separately):

Length of Lead-in ft (m)	Number of Turns
< 250 (75)	3
250-375 (75-115)	4
375-525 (115-160)	5
> 525 (160)	6

Quadrupole loops: Use 2-4-2 turns

6' X 15' (1.8m X 4.6m) Loops:
Lead-in < 150' (45 m), use 2 turns
Lead-in > 150' (45 m), use 3 turns

Prepared in the Offices of:

Typical Loop Locations
 PLAN DATE: June 2006 REVIEWED BY:
 PREPARED BY: P. L. Alexander REVIEWED BY:
 SCALE: N/A
 REVISIONS: Revise pavement markings
 INIT. DATE: [Signature] [Date]
 SIGNATURE: [Signature] DATE: [Date]
 SEAL: [Seal]
 SIG. INVENTORY NO.: