

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
MECKLENBURG COUNTY

LOCATION: CHARLOTTE - GRIER RD. GRADE SEPARATION FROM
SOUTH OF ORR RD. TO OLD CONCORD RD.

SUBMITTAL: 100% PAVEMENT MARKING PLAN
DATE: 2-8-2013

TIP: P-5208H

CONTRACT: C203148

ROADWAY STANDARD DRAWING

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO-LANE AND MULTI-LANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALK
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPE
1264.02	OBJECT MARKERS - INSTALLATION

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PMP-2 THROUGH PMP-5	PAVEMENT MARKING DETAILS
PMP-6 THROUGH PMP-12	CDOT STANDARD DRAWINGS
PMP-13	NCDOT DETAIL

GENERAL NOTES

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

- A) INSTALL PAVEMENT MARKINGS AN PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ASPHALT SURFACES	THERMOPLASTIC	PERMANENT RAISED
CONCRETE SURFACES	COLD APPLIED PLASTIC (TYPE 3)	PERMANENT RAISED
- B) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- C) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS.
- D) STOPBAR LOCATIONS AT NON-SIGNALIZED INTERSECTIONS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER.
- E) USE NCDOT STANDARDS FOR OLD CONCORD RD. USE CDOT STANDARD DRAWINGS FOR ALL OTHER ROADS.

PAVEMENT MARKING SCHEDULE

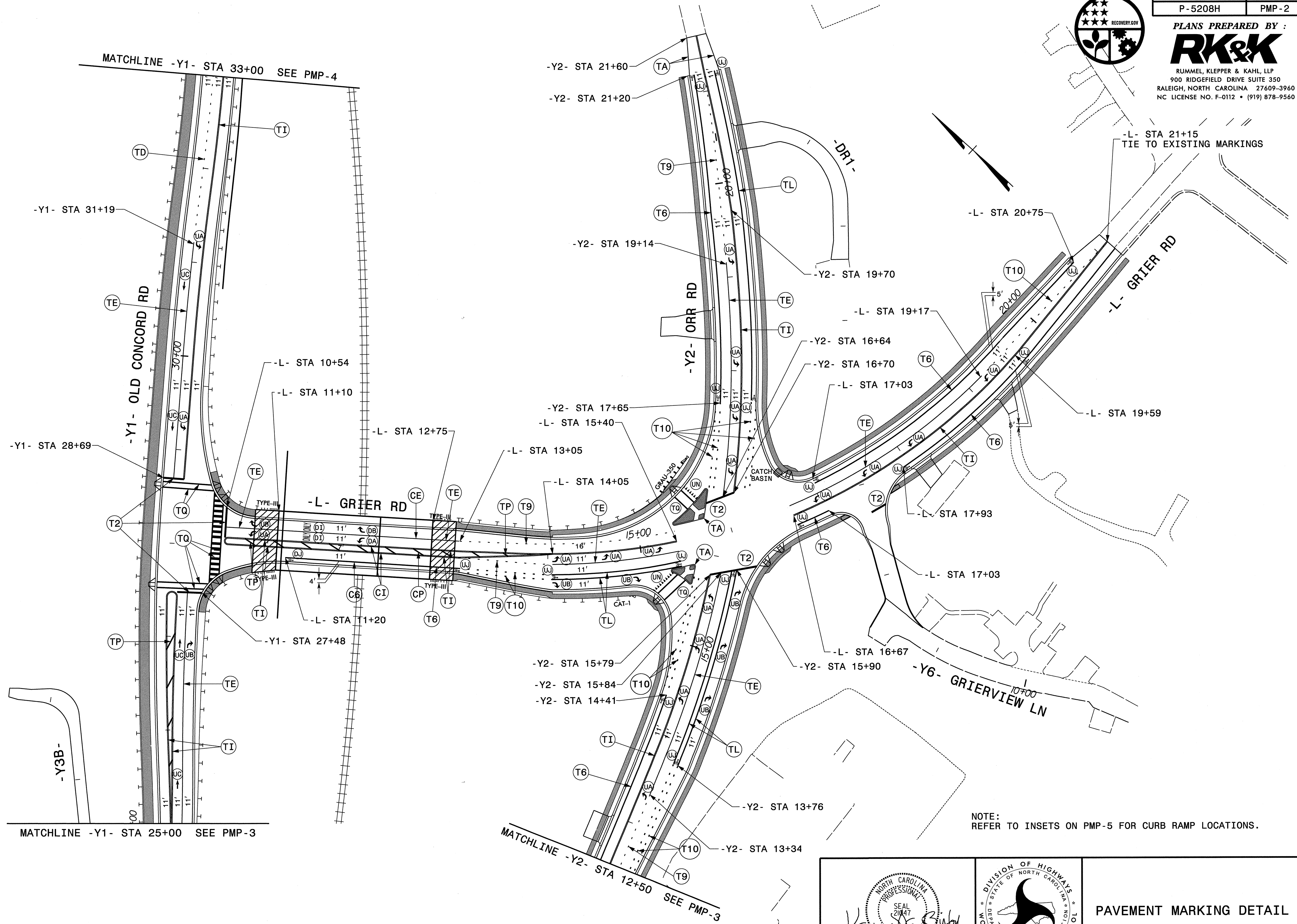
THERMOPLASTIC (4", 90 MILS)	COLD APPLIED PLASTIC (4") Type3 - Permanent Wet Reflective
TA WHITE EDGELINE	CE WHITE SOLID LANE LINE
THERMOPLASTIC (4", 120 MILS)	CI YELLOW DOUBLE CENTER
T9 2 FT. - 10 FT./SP WHITE MINISKIP	COLD APPLIED PLASTIC (6") Type3 - Permanent Wet Reflective
TD 3 FT. - 9 FT./SP WHITE MINISKIP	C6 WHITE EDGELINE
TE WHITE SOLID LANE LINE	COLD APPLIED PLASTIC (8") Type3 - Permanent Wet Reflective
TI YELLOW DOUBLE CENTER	CP YELLOW DIAGONAL
THERMOPLASTIC (6", 90 MILS)	COLD APPLIED PLASTIC PAVEMENT MARKING CHARACTER Type3 - Permanent Wet
T6 WHITE EDGELINE	DI ALPHANUMERIC CHAR.
THERMOPLASTIC (6", 120 MILS)	COLD APPLIED PLASTIC PAVEMENT MARKING SYMBOL Type3 - Permanent Wet
T10 2 FT. - 10 FT./SP WHITE MINISKIP	DA LEFT TURN ARROW
TL WHITE LANE LINE	DB RIGHT TURN ARROW
THERMOPLASTIC (8", 90 MILS)	DJ BICYCLE SYMBOL
TP YELLOW DIAGONAL	PERMANENT RAISED PAVEMENT MARKERS
THERMOPLASTIC (8", 120 MILS)	MA YELLOW & YELLOW
TQ WHITE CROSSWALK LINE	MB CRYSTAL & RED
THERMOPLASTIC (24", 120 MILS)	
T2 WHITE STOPBAR	
THERMOPLASTIC PAVEMENT MARKING SYMBOLS (90 MILS)	
UA LEFT TURN ARROW	
UB RIGHT TURN ARROW	
UC STRAIGHT ARROW	
UJ BICYCLE SYMBOL	
UN 24" YIELD LINE TRIANGLE	

PAVEMENT MARKING PLAN
PLANS PREPARED BY :
K.W. BISBY, P.E. PROJECT ENGINEER
PROJECT DESIGNER

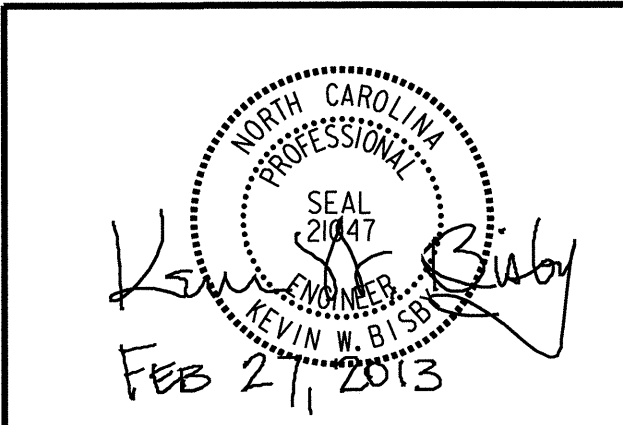
PLANS PREPARED BY :
RK&K
RUMMEL, KLEPPER & KAHL, LLP
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NOTE:
REFER TO INSETS ON PMP-5 FOR CURB RAMP LOCATIONS.



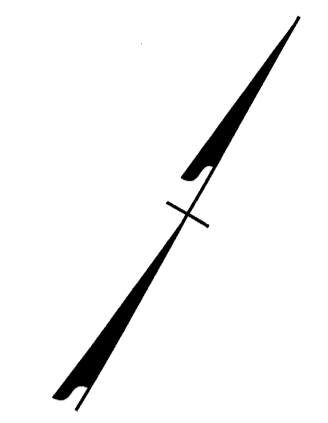
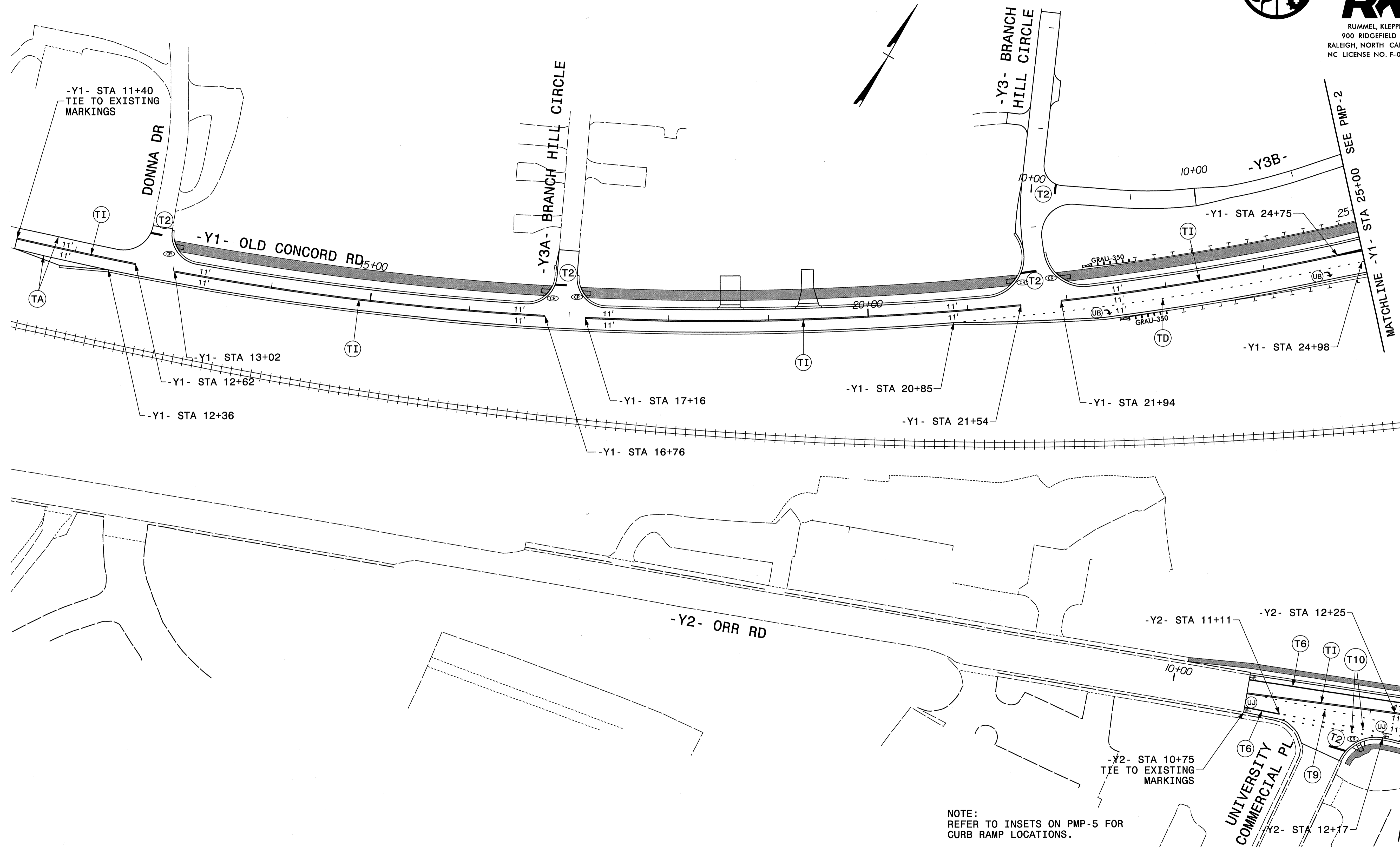
PAVEMENT MARKING DETAIL

DCN 0102DEL_P10a6



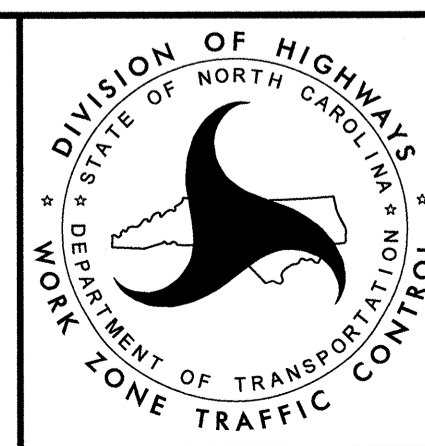
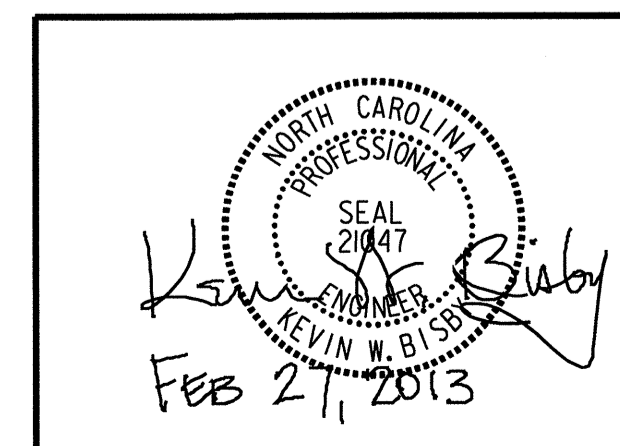
PROJ. REFERENCE NO. P-5208H SHEET NO. PMP-3

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NOTE: REFER TO INSETS ON PMP-5 FOR CURB RAMP LOCATIONS.

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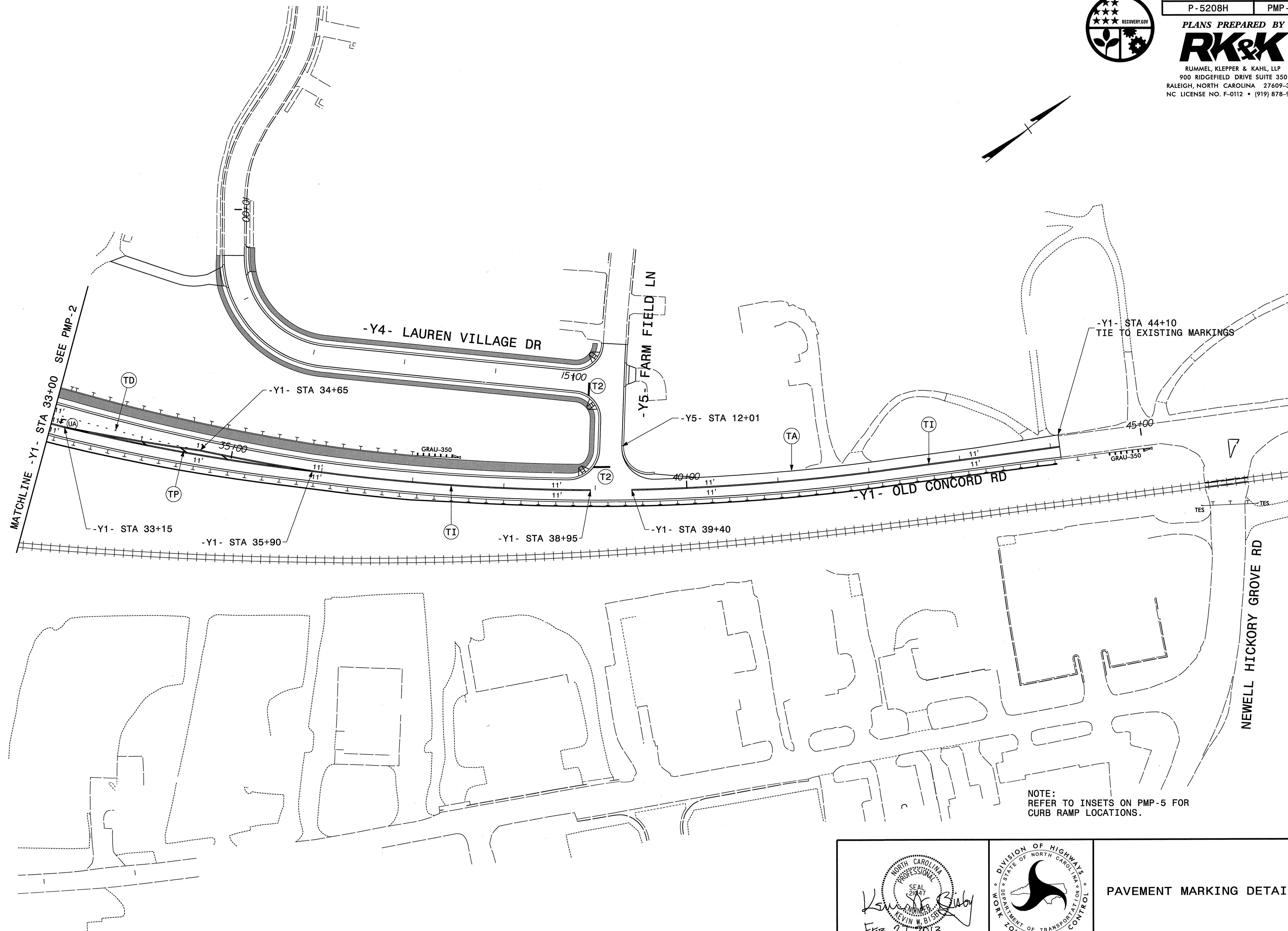
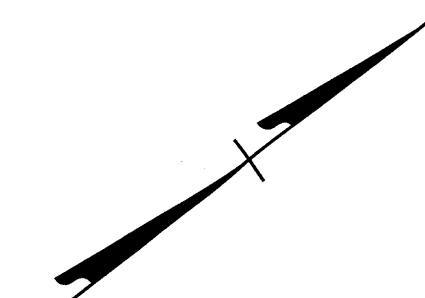
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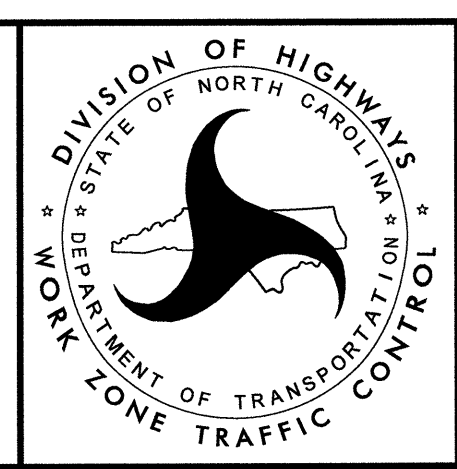
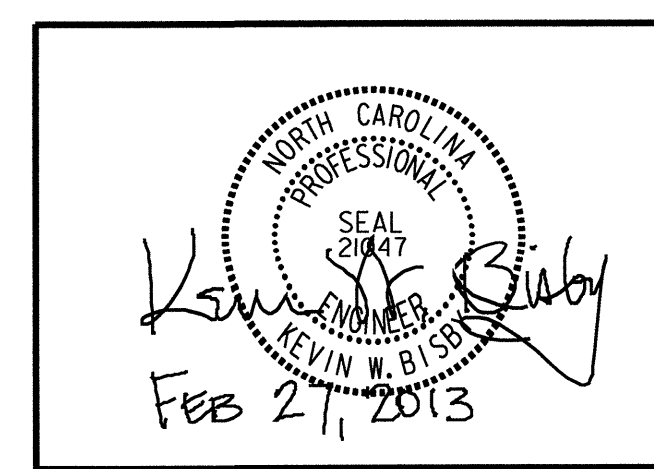
PROJ. REFERENCE NO.	SHEET NO.
P-5208H	PMP-4

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NOTE:
 REFER TO INSETS ON PMP-5 FOR
 CURB RAMP LOCATIONS.

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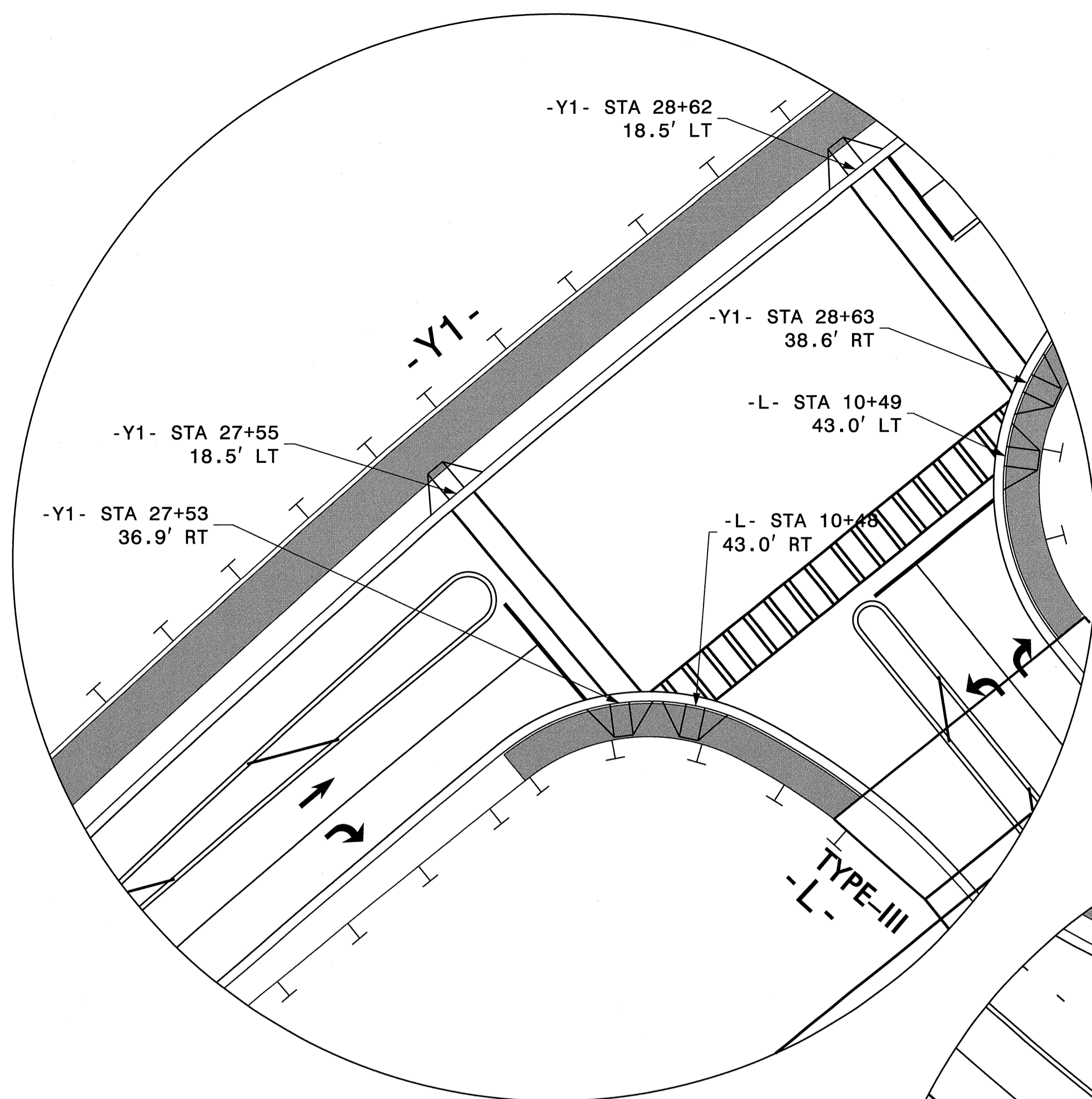
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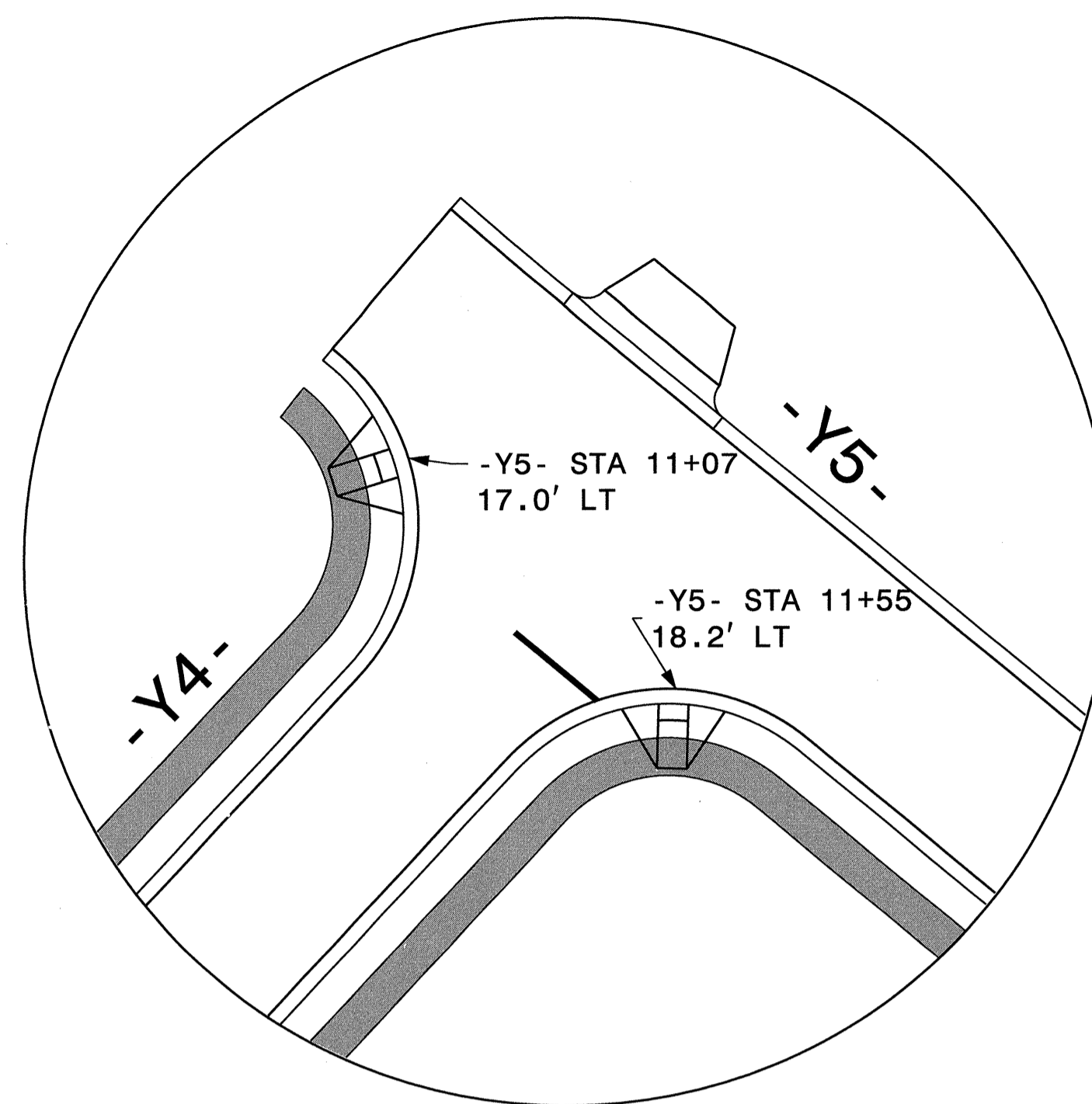


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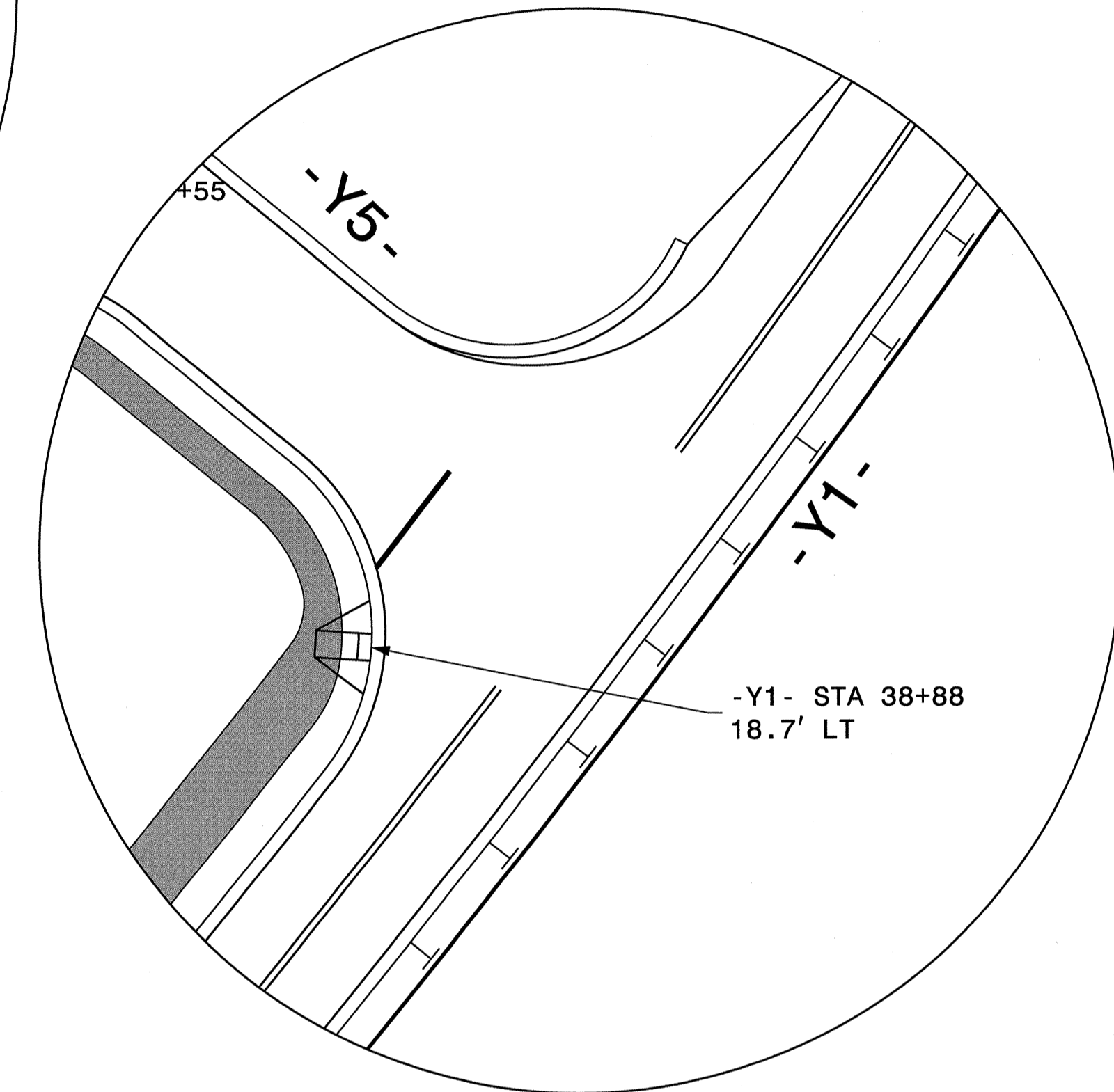
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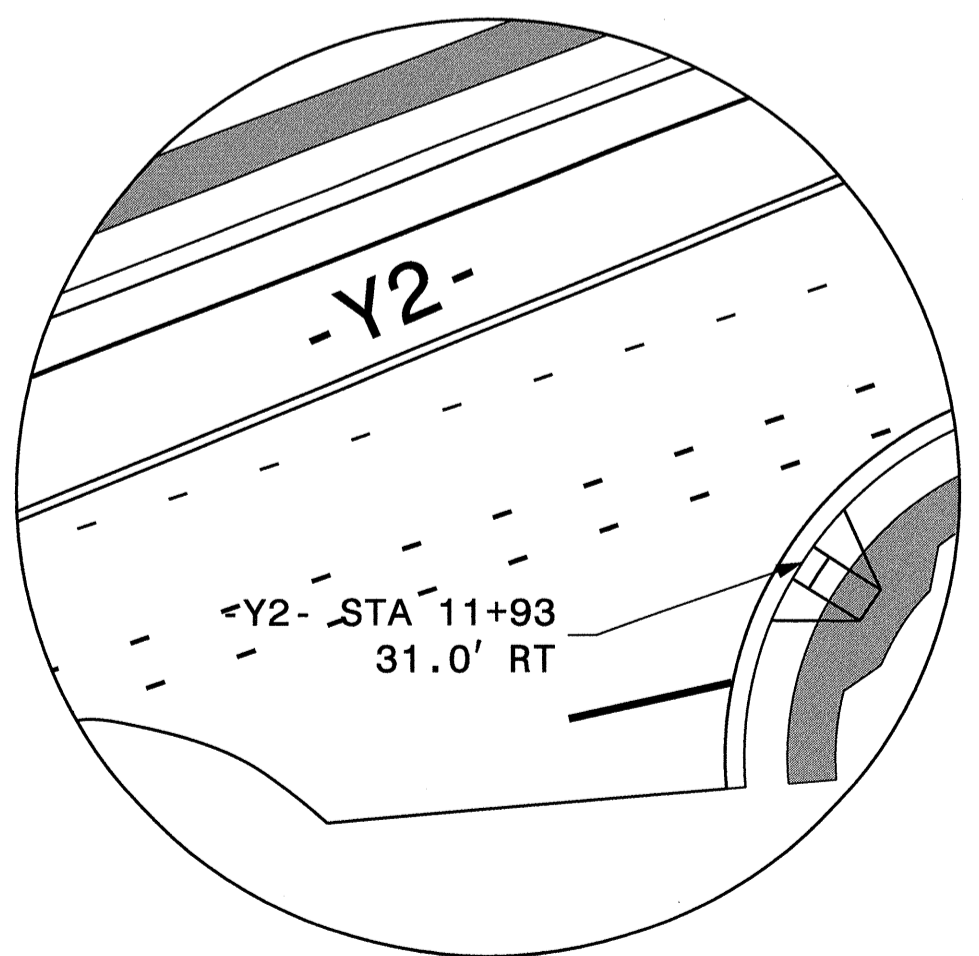
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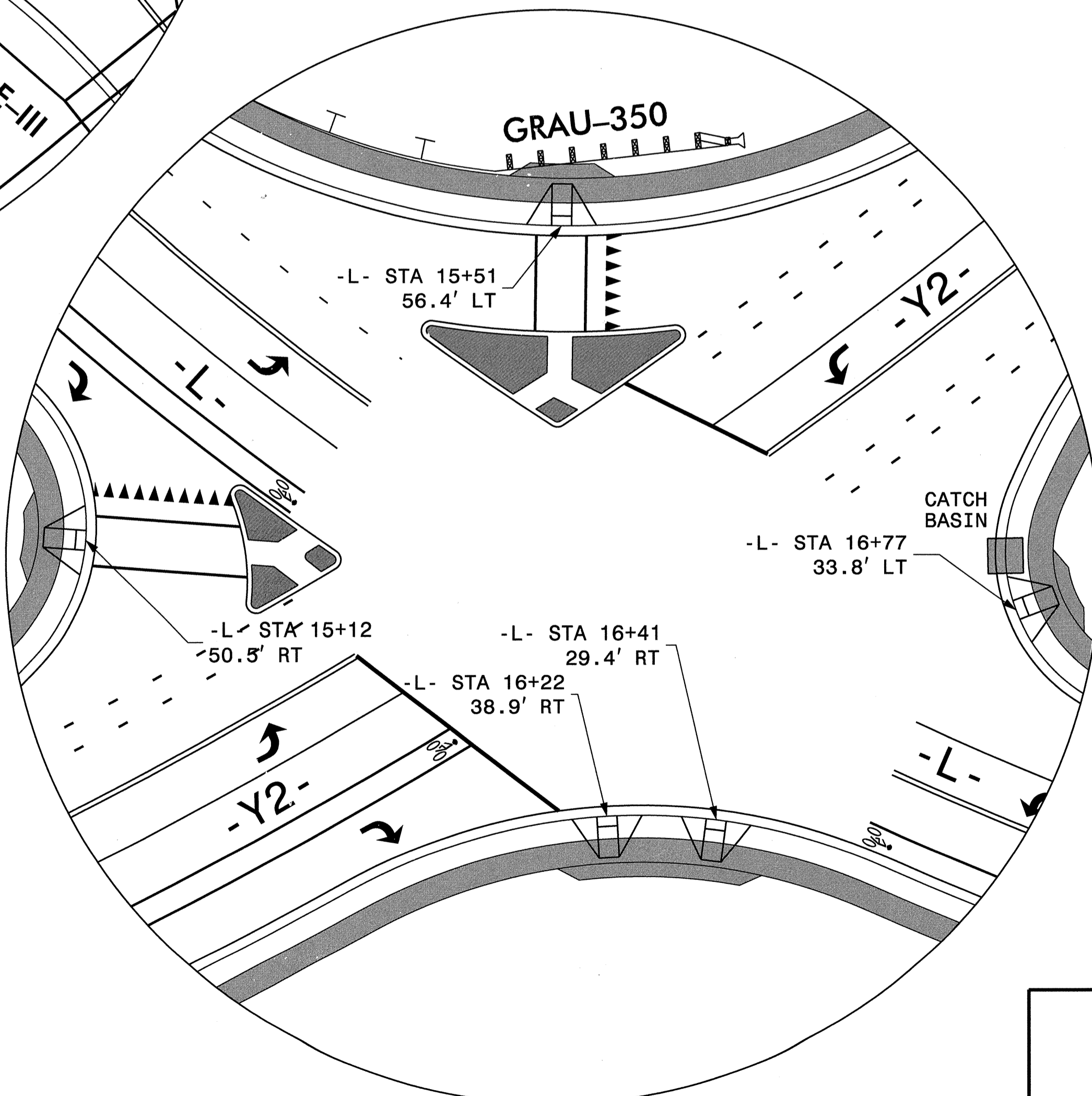
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INSET 'D'

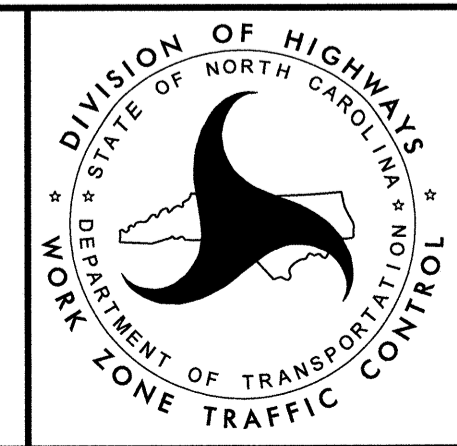
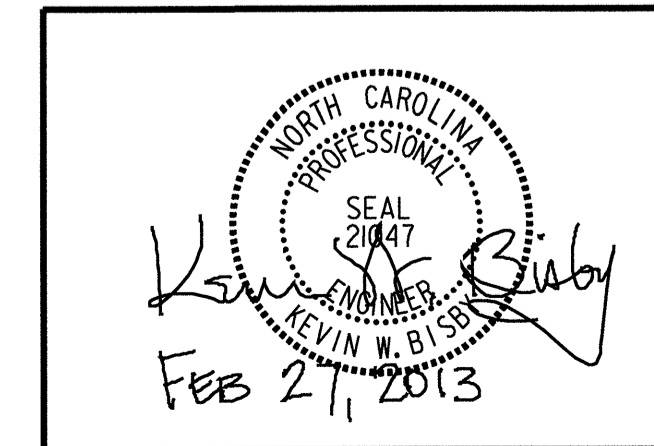


INSET 'C'



INSET 'A'

NOTE:
LOCATIONS SHOWN FOR CURB RAMP ARE AT THE CENTERLINE OF THE CURB RAMP AT THE FACE OF THE CURB.
REFER TO RDWY STD 848.05 FOR CURB RAMP CONSTRUCTION.



CURB RAMP LOCATIONS

2/27/2013
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INTRODUCTION:

THE CHARLOTTE DEPARTMENT OF TRANSPORTATION DESIGNS AND INSTALLS PAVEMENT MARKINGS IN ACCORDANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE TRAFFIC CONTROL DEVICES HANDBOOK (TCDH).

PURPOSE:

SINCE THE MUTCD AND THE TCDH ALLOW FOR SOME FLEXIBILITY IN THE WAY CERTAIN MARKINGS CAN BE DONE, THE PURPOSE OF THIS DOCUMENT IS TO STANDARDIZE PAVEMENT MARKING APPLICATIONS WITHIN THE CITY OF CHARLOTTE. THIS DOCUMENT SHOULD SERVE AS A REFERENCE FOR THE INSTALLATION AND MAINTENANCE OF PAVEMENT MARKINGS IN CHARLOTTE.

DEFINITIONS:

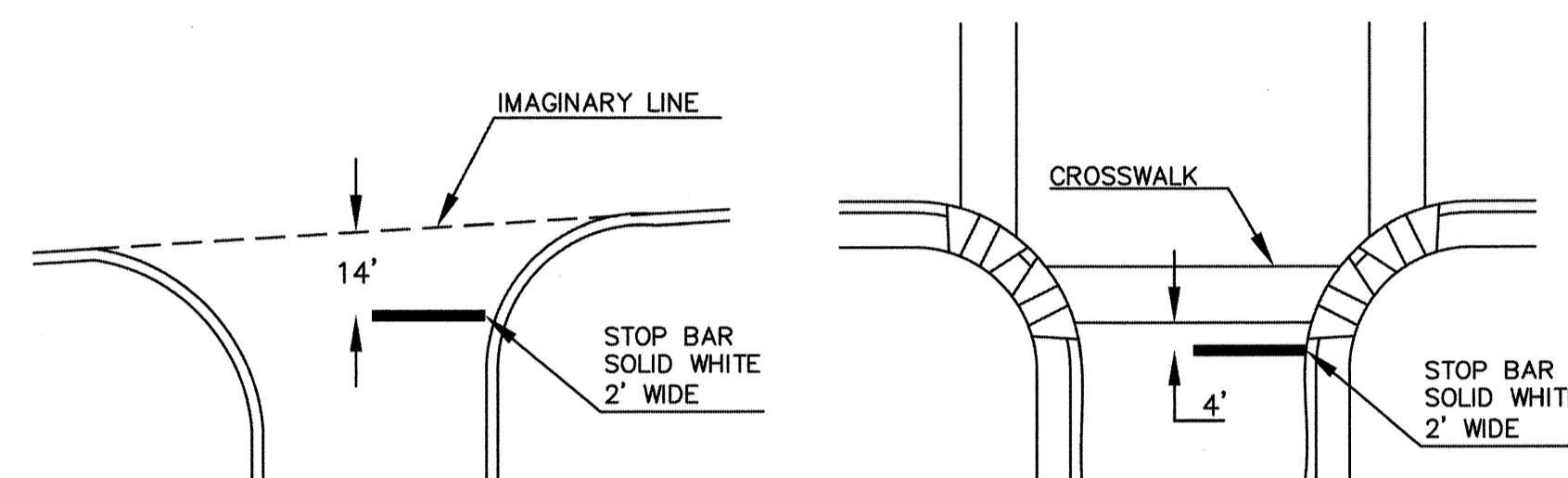
INTERSECTION: THE CROSSING OF TWO OR MORE STREETS AT GRADE.

BAY TURN LANE: TURN LANE DESIGNED NOT TO ENTRAP THROUGH TRAFFIC. SUCH A LANE IS PROVIDED PHYSICAL CONSTRUCTION OR PAVEMENT MARKINGS TO KEEP THROUGH TRAFFIC FROM ACCIDENTALLY ENTERING IT. A BAY TURN LANE THAT EXTENDS BETWEEN TWO INTERSECTIONS SHALL BE CONSIDERED A DROP TURN LANE.

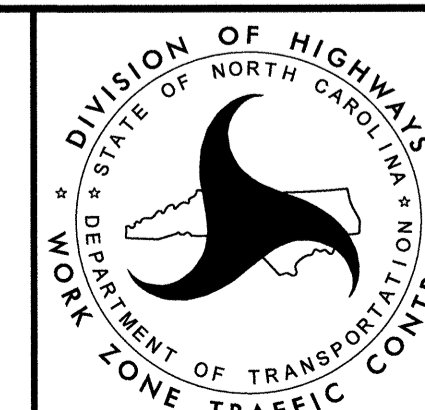
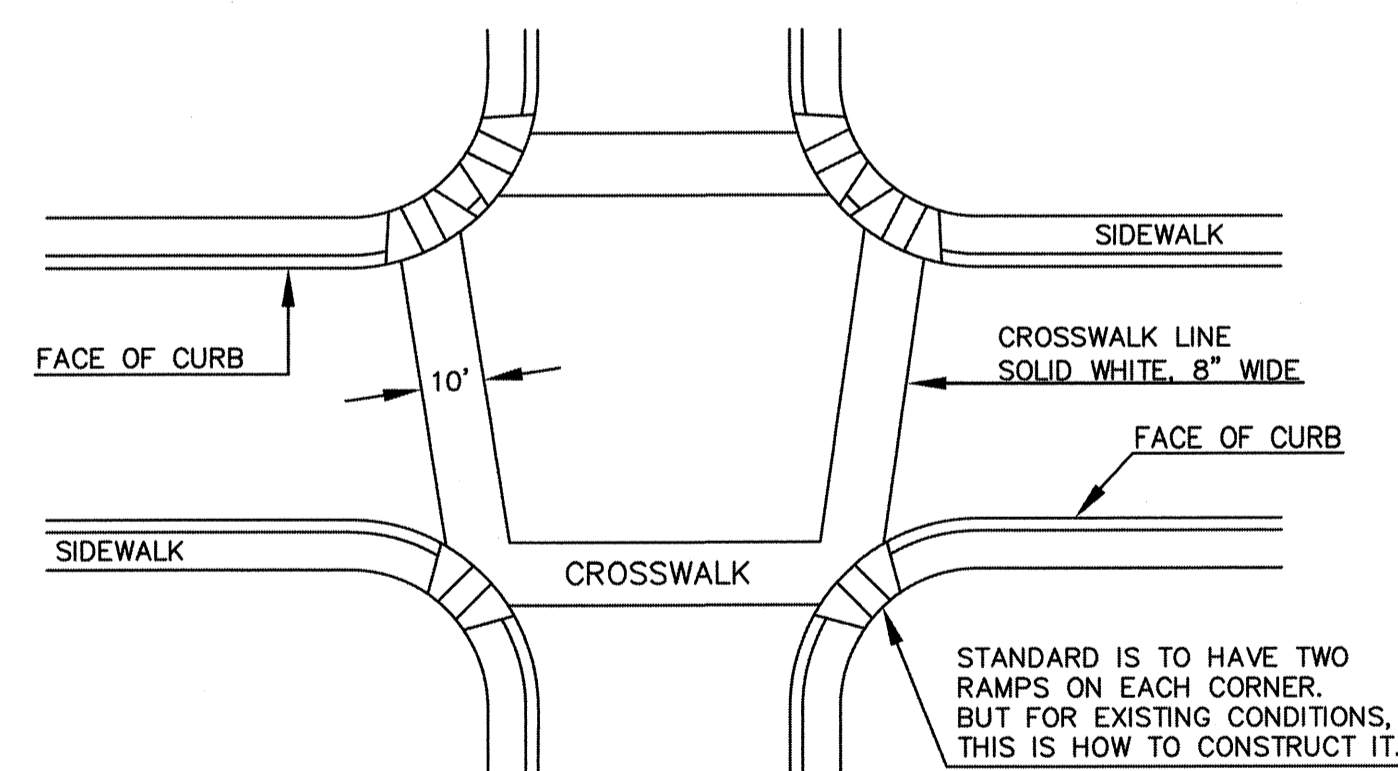
DROP TURN LANE: THROUGH LANE THAT BECOMES A MANDATORY TURN LANE AT AN INTERSECTION. THROUGH TRAFFIC IN SUCH A LANE CAN BE TRAPPED IF NOT WARNED BY SIGNS AND MARKINGS. THE THROUGH TRAFFIC MUST INTENTIONALLY MANEUVER OUT OF SUCH LANE OR BE REQUIRED TO TURN. A BAY TURN LANE THAT EXTENDS BETWEEN TWO INTERSECTIONS SHALL BE CONSIDERED A DROP TURN LANE.

PAVEMENT MARKINGS GUIDELINE:

STOP BARS: A STOP BAR SHALL BE SOLID WHITE AND 24" IN WIDTH. WHEN INSTALLED, A STOP BAR SHOULD BE PLACED A MINIMUM OF 14' FROM THE EDGE OF THE CROSSING ROAD. IF CROSSWALKS ARE PRESENT, THE STOP BAR SHALL BE A MINIMUM OF 4' IN ADVANCE OF THE NEAREST CROSSWALK LINE. A STOP BAR SHOULD BE INSTALLED PERPENDICULAR TO THE LEG OF THE INTERSECTION IT IS BEING INSTALLED ON.



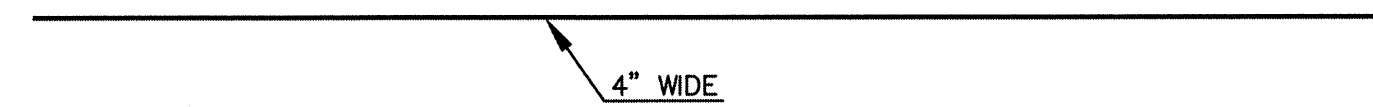
CROSS WALK: CROSSWALKS SHOULD PROVIDE DIRECT AND UNOBSTRUCTED CONNECTION BETWEEN ACCESSIBLE RAMPS. CROSSWALK LINES SHALL BE SOLID WHITE AND 8" IN WIDTH. THE CROSSWALK WIDTH SHOULD BE 10' WIDE UNLESS OTHERWISE SPECIFIED ON THE PLAN. THE CROSSWALK WIDTH SHALL NOT BE LESS THAN 6'. CROSSWALK LINES NEAREST TO THE CENTER OF THE INTERSECTION ALONG A SPECIFIC STREET SHOULD, WHENEVER POSSIBLE, EXTEND FROM FACE OF CURB TO FACE OF CURB ACROSS THE INTERSECTING ROAD.





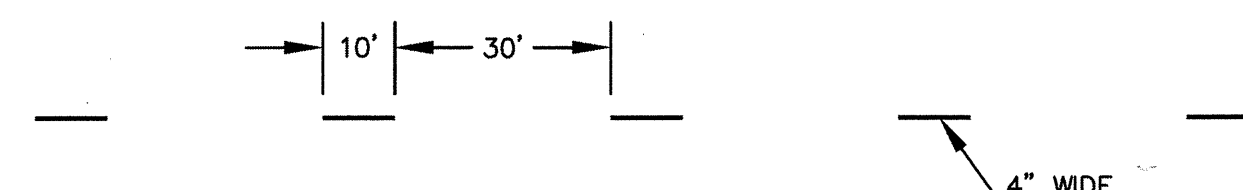
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EDGE LINES: EDGE LINES SHALL BE 4" WIDE. SOLID WHITE EDGE LINES SHALL BE INSTALLED TO THE RIGHT OF MOTORIST TRAVELING IN THE EXTREME RIGHT LANE WHEN CONCRETE CURB AND GUTTER ARE NOT PRESENT OR WHEN CONCRETE CURB IS PRESENT BUT GUTTER IS EITHER FILLED IN WITH ASPHALT OR NOT PRESENT. SOLID YELLOW EDGE LINES SHALL BE INSTALLED ON ALL DIVIDED ROADS AND ONE WAY STREETS TO THE LEFT OF MOTORISTS TRAVELING IN THE EXTREME LEFT LANE (EVEN IF CONCRETE CURB AND GUTTER IS PRESENT). EDGE LINE SHALL BE BROKEN FOR INTERSECTIONS AND STREET-TYPE ENTRANCE DRIVEWAYS (TYPES III AND IV, CHARLOTTE LAND DEVELOPMENT STANDARDS 10.28 AND 10.25F, RESPECTIVELY). EDGE LINES SHALL NOT BE BROKEN FOR DROP CURB CONCRETE RAMP DRIVEWAYS (TYPES I AND II, CHARLOTTE LAND DEVELOPMENT STANDARDS 10.24 - 10.27).

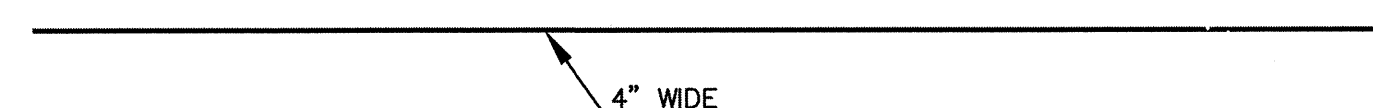


LANE LINES: LANE LINES SHALL BE WHITE AND 4" WIDE. LANE LINES CAN BE SKIP OR SOLID AND SHALL BE INSTALLED TO SEPARATE LANES OR TRAFFIC TRAVELING IN THE SAME DIRECTION.

SKIP LANE LINES: SKIP LANE LINES SHALL BE INSTALLED TO SEPARATE THROUGH LANES TRAVELING IN THE SAME DIRECTION. EACH SKIP LINE SHALL BE 10' IN LENGTH PLACED AT 30' INTERVALS.

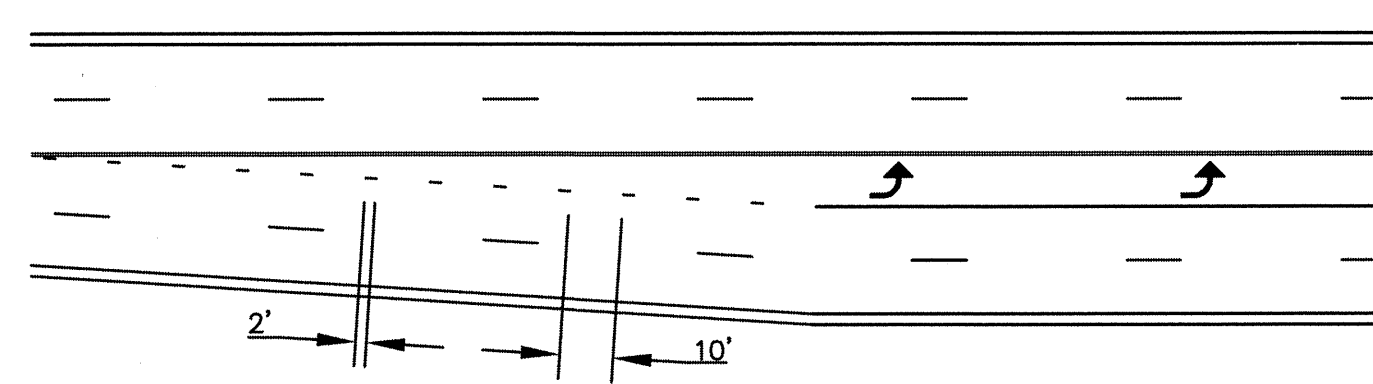


SOLID LANE LINES: SOLID LANE LINES SHALL BE INSTALLED TO SEPARATE DUAL TURN LANES AS WELL AS THROUGH LANES FROM TURN LANES THAT ARE TRAVELING IN THE SAME DIRECTION.



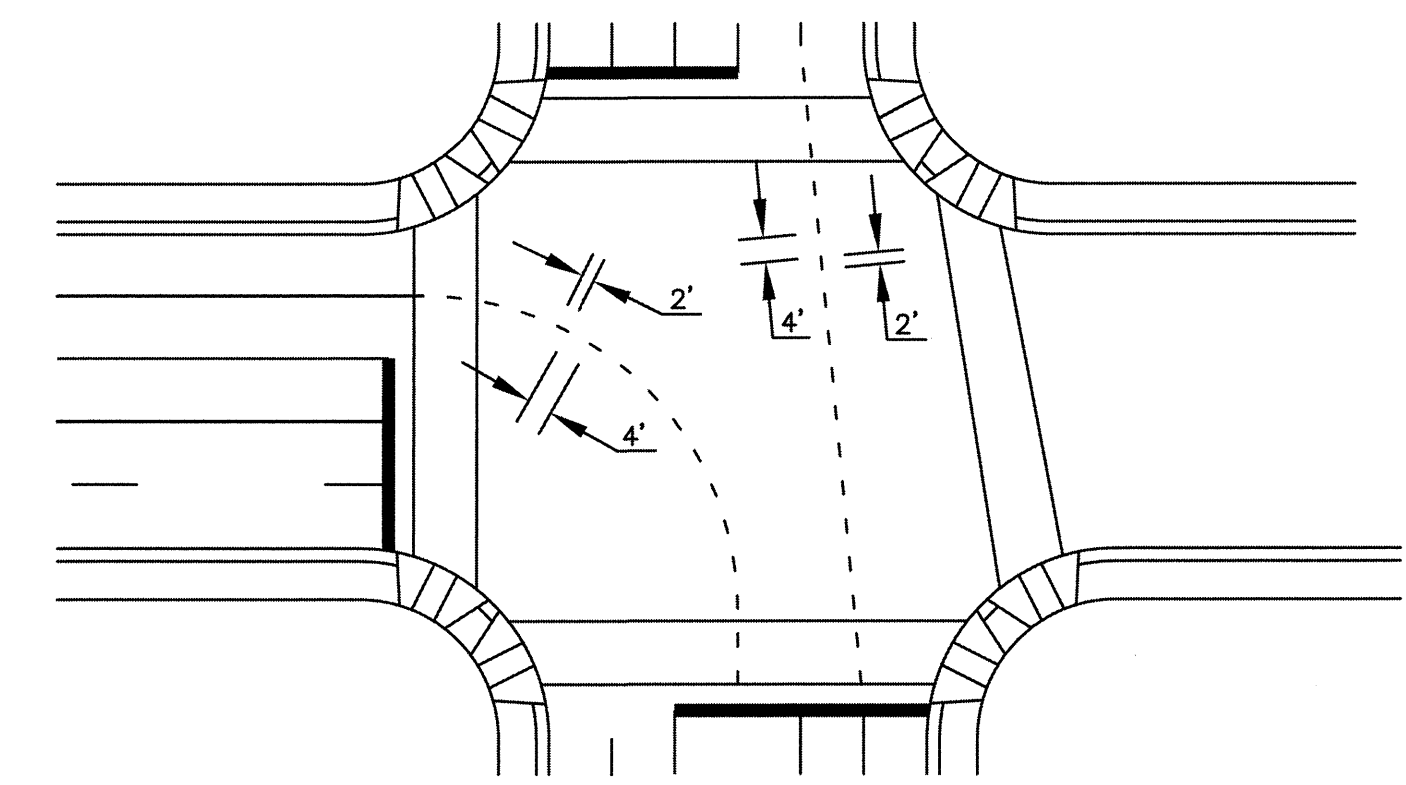
MINI-SKIP LINES: MINI-SKIP LINES SHALL BE WHITE, 2' LONG, AND 4" WIDE.

MINI-SKIP LINE AS TAPERS: MINI-SKIP LINES SHALL BE SEPARATED BY 10' GAPS WHEN USED TO TAPER THROUGH TRAFFIC AWAY FROM TURN LANES.

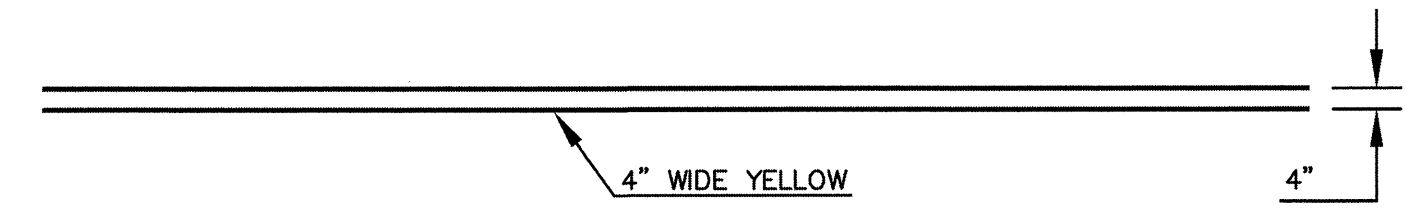


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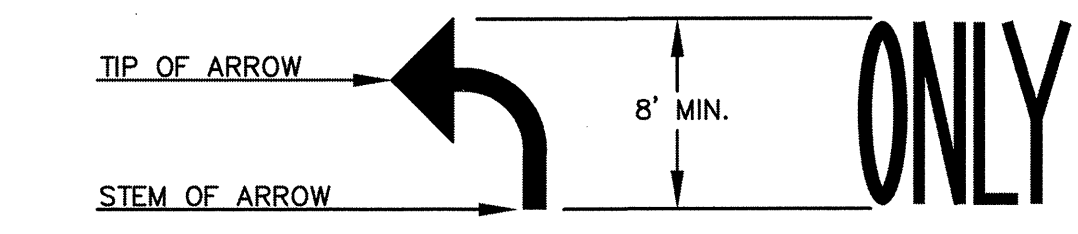
MINI-SKIP LINE THROUGH INTERSECTION: MINI-SKIP LINES SHALL BE SEPARATED BY 4' GAPS WHEN USED THROUGH AN INTERSECTION, i.e. FOR DUAL TURNS OR TO DELINEATE AN OFFSET.



CENTER LINES: CENTER LINES SHALL BE A DOUBLE YELLOW LINE. A DOUBLE YELLOW LINE SHALL CONSIST OF TWO SOLID 4" SIDE YELLOW LINES PLACED 4" APART. WHEN WARRANTED, CENTER LINES SHALL BE USED TO SEPARATE TRAFFIC TRAVELING IN OPPOSITE DIRECTIONS WHERE A MEDIAN ISLAND IS NOT PRESENT.



TURN ARROWS AND "ONLY" LEGENDS: TURN ARROWS AND "ONLY" LEGENDS SHALL BE WHITE. ALTHOUGH ALL TURN LANES MUST HAVE ARROWS IN THEM, SOME MUST ALSO HAVE "ONLY" LEGENDS. THE LOCATIONS OF ARROWS AND "ONLY" LEGENDS AND THE USE OF THE "ONLY" LEGEND SHALL DEPEND ON WHETHER THE TURN LANE IS CONSIDERED A "BAY TURN LANE" OF A "DROP TURN LANE" (SEE DEFINITIONS SECTION).



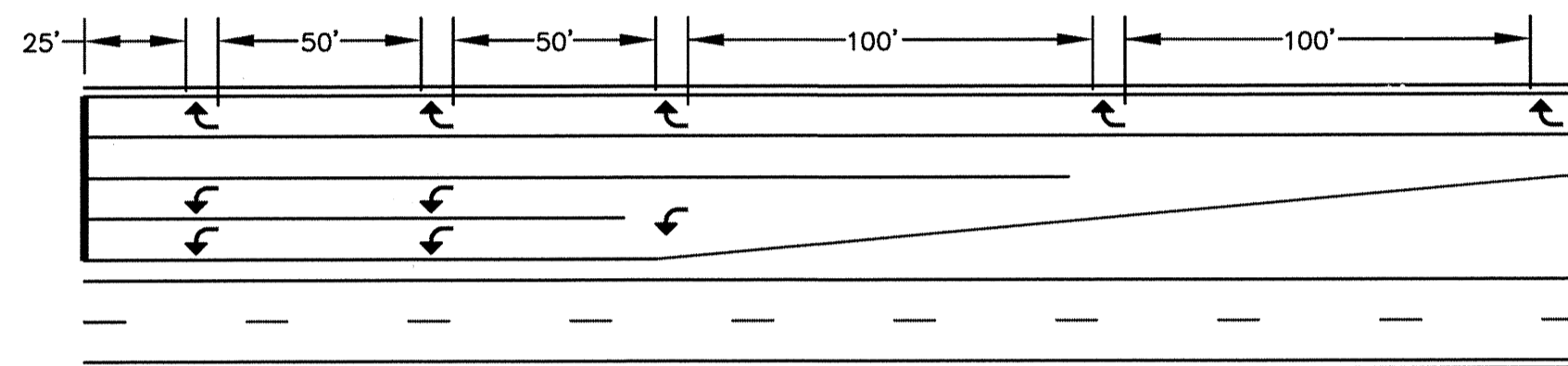
4

		<p>CDOT STANDARD DRAWING</p>
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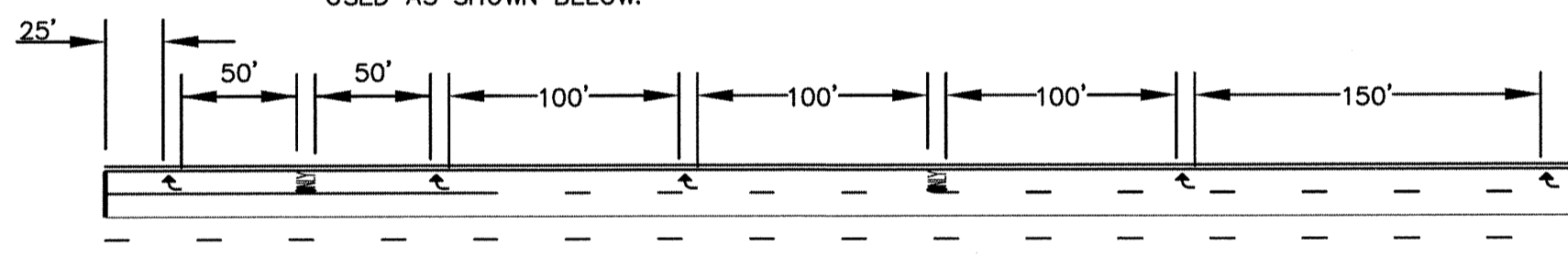
BAY TURN LANES: BAY TURN LANES SHALL HAVE ARROWS BUT NOT "ONLY" LEGENDS. THE NUMBER OF ARROWS TO BE INSTALLED IN A BAY TURN LANE SHALL DEPEND ON THE LENGTH OF THE TURN LANE. THE SPACING BETWEEN THE ARROWS IN A BAY TURN LANE SHALL BE AS FOLLOWS:

- * TIP OF 1ST ARROW AT 25' FROM STOP BAR OR EXIT POINT OF THE TURN LANE
- * TIP OF 2ND ARROW AT 50' FROM THE STEM OF THE 1ST ARROW
- * TIP OF 3RD ARROW AT 50' FROM THE STEM OF THE 2ND ARROW
- * TIP OF 4TH ARROW AT 100' FROM THE STEM OF THE 3RD ARROW
- * TIP OF 5TH ARROW AT 100' FROM THE STEM OF THE 4TH ARROW
- * TIP OF 6TH ARROW AT 100' FROM THE STEM OF THE 5TH ARROW
- * TIP OF 7TH ARROW AT 150' FROM THE STEM OF THE 6TH ARROW
- * TIP OF 8TH ARROW AT 150' FROM THE STEM OF THE 7TH ARROW

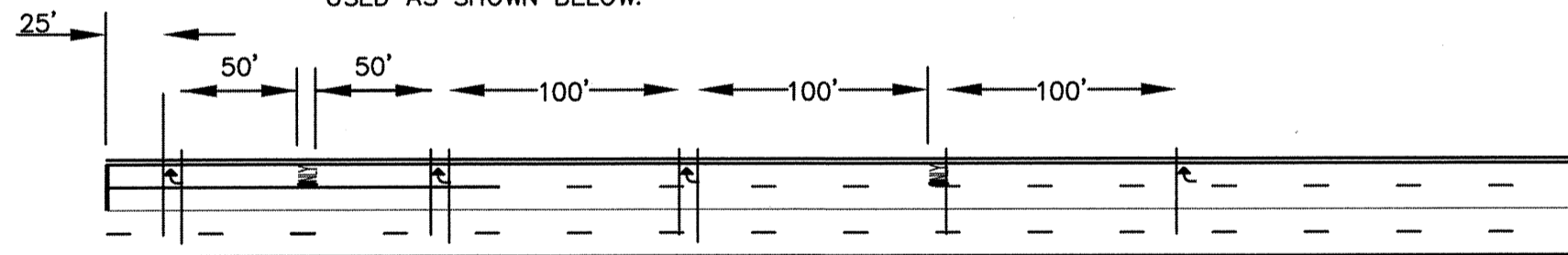


DROP TURN LANES: DROP TURN LANES SHALL HAVE ARROWS AND "ONLY" LEGENDS. THE NUMBER AND LOCATION OF THE ARROWS AND THE "ONLY" LEGENDS SHALL DEPEND ON THE POSTED SPEED LIMIT EXCEPT WHERE THE DROP TURN LANE IS ON THE STEM OF A T - INTERSECTION.

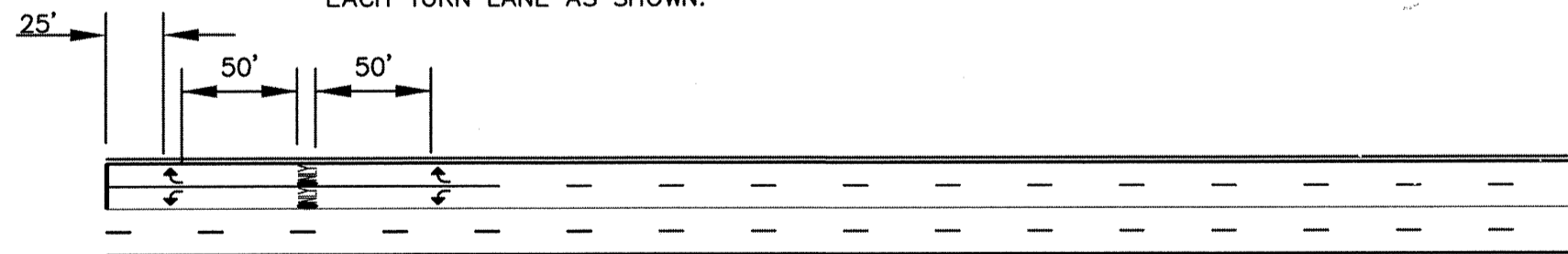
- FOR POSTED SPEED OF 40 MPH OR MORE, 5 ARROWS AND 2 "ONLY" LEGENDS SHALL BE USED AS SHOWN BELOW:



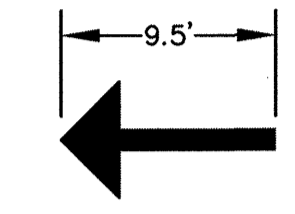
- FOR POSTED SPEED OF 35 MPH OR MORE, 4 ARROWS AND 2 "ONLY" LEGENDS SHALL BE USED AS SHOWN BELOW:



- FOR STEM OF A T - INTERSECTION, 2 ARROWS AND 1 "ONLY" LEGEND SHALL BE USED IN EACH TURN LANE AS SHOWN:



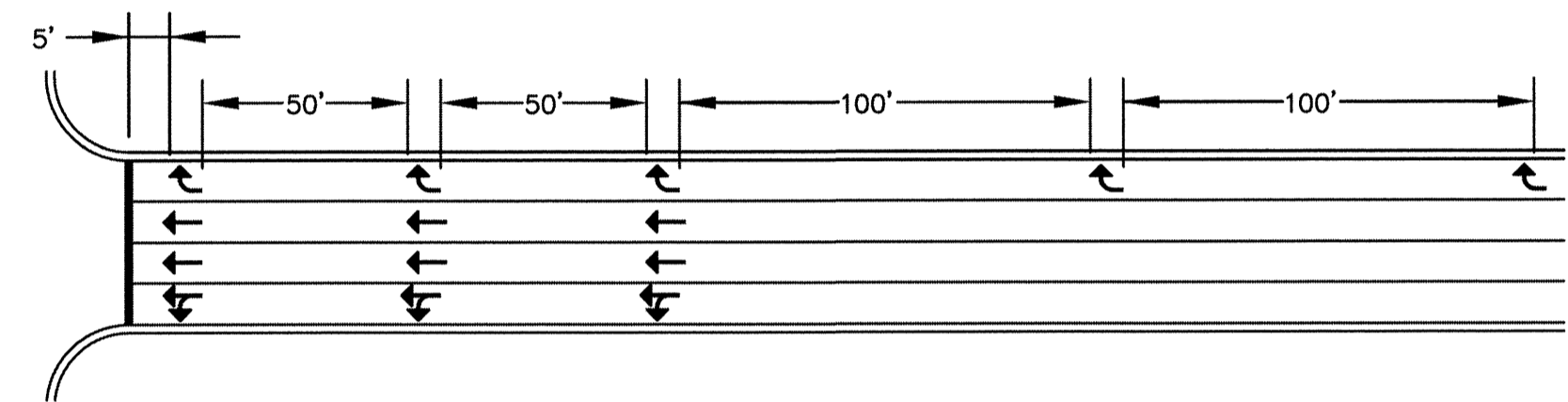
STRAIGHT ARROWS: STRAIGHT ARROWS SHALL BE WHITE. THE ONLY STANDARD APPLICATION REGARDING STRAIGHT ARROWS IS IN THE THROUGH LANES OF ONE-WAY STREETS THAT ARE SIGNALIZED.



A SET OF 3 STRAIGHT ARROWS SHALL BE USED IN EACH THROUGH LANE SPACED AS FOLLOWS:

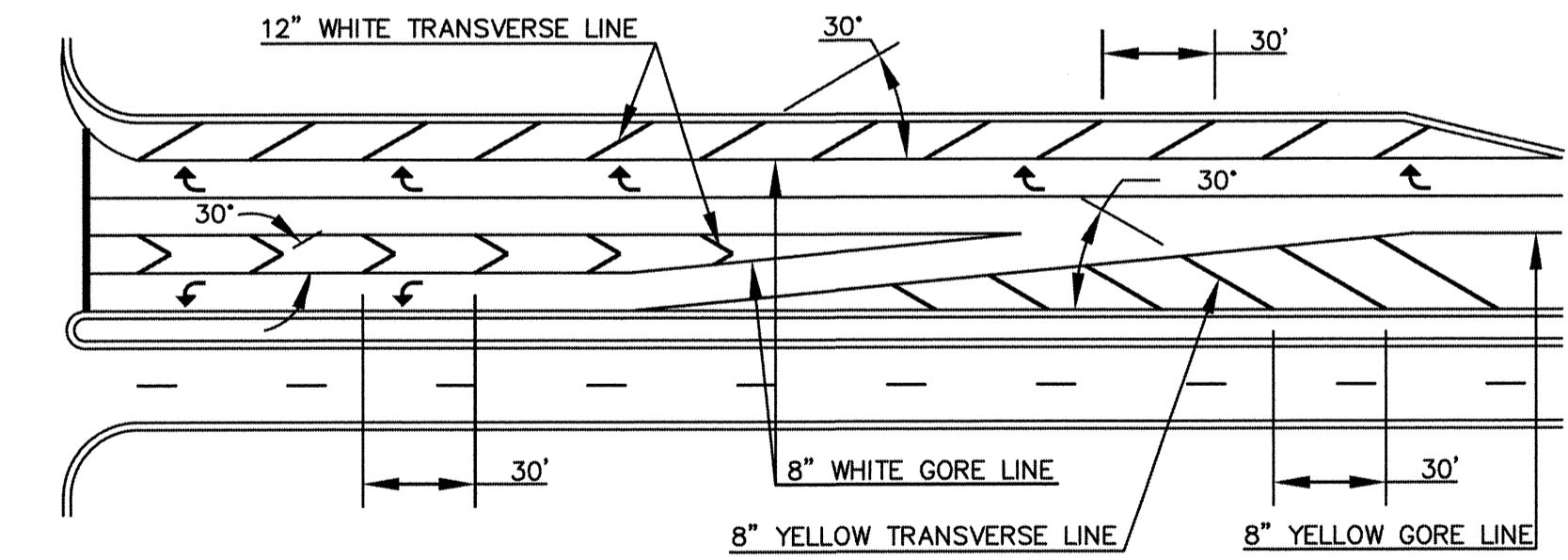
- TIP OF ST. ARROW AT 5' FROM STOP BAR
- TIP OF 2ND ARROW AT 50' FROM THE STEM OF THE 1ST ARROW
- TIP OF 3RD ARROW AT 50' FROM THE STEM OF THE 2ND ARROW

WHERE STRAIGHT ARROWS ARE USED, ARROWS SHOULD BE PLACED IN EACH LANE. COMBINATION STRAIGHT/TURN ARROWS SHOULD BE USED APPROPRIATELY.



GORE LINES: GORE LINES SHALL BE SOLID 8" WIDE LINES. A GORE LINE SHALL BE WHITE WHEN PLACED TO FORM A CHANNELIZATION ISLAND SEPARATING TRAFFIC TRAVELING IN THE SAME DIRECTION. A GORE LINE SHALL BE WHITE WHEN PLACED TO RIGHT OF MOTORISTS TRAVELING IN THE EXTREME RIGHT LANE, AND SHALL BE YELLOW WHEN PLACED TO THE LEFT OF MOTORISTS TRAVELING IN THE EXTREME LEFT LANE.

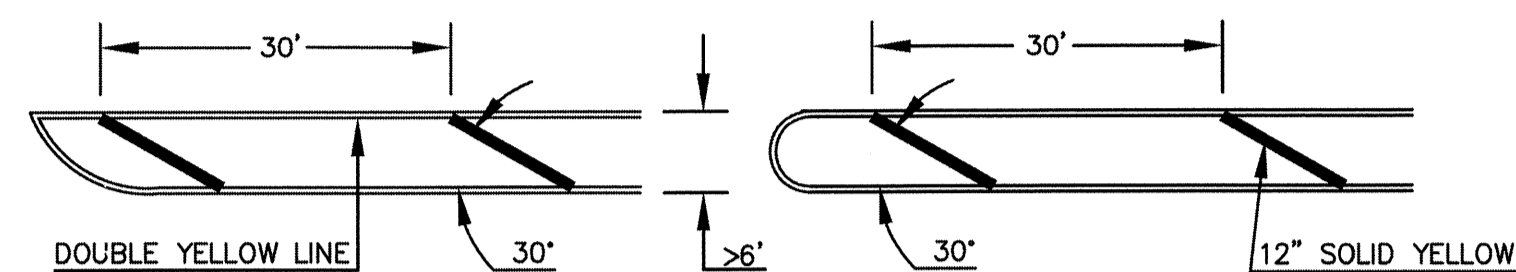
TRANSVERSE LINES: TRANSVERSE (HATCH) LINES SHALL BE SOLID 12" WIDE LINES. TRANSVERSE LINES SHALL BE YELLOW WHEN PLACED TO THE LEFT OF MOTORISTS TRAVELING IN THE EXTREME LEFT LANE. TRANSVERSE LINES SHALL BE WHITE WHEN PLACED TO THE RIGHT OF MOTORISTS TRAVELING IN THE EXTREME RIGHT, AND WHEN PLACED IN CHANNELIZATION ISLANDS SEPARATING TRAFFIC TRAVELING IN THE SAME DIRECTION. TRANSVERSE LINES SHOULD BE ANGLED 30° TO THE APPROACHING TRAFFIC. THE DIRECTION OF THE ANGLE SHOULD BE SUCH THAT THE TRAFFIC FOLLOWING THE TRANSVERSE LINES IS DIRECTED BACK INTO THE TRAVEL LANE.



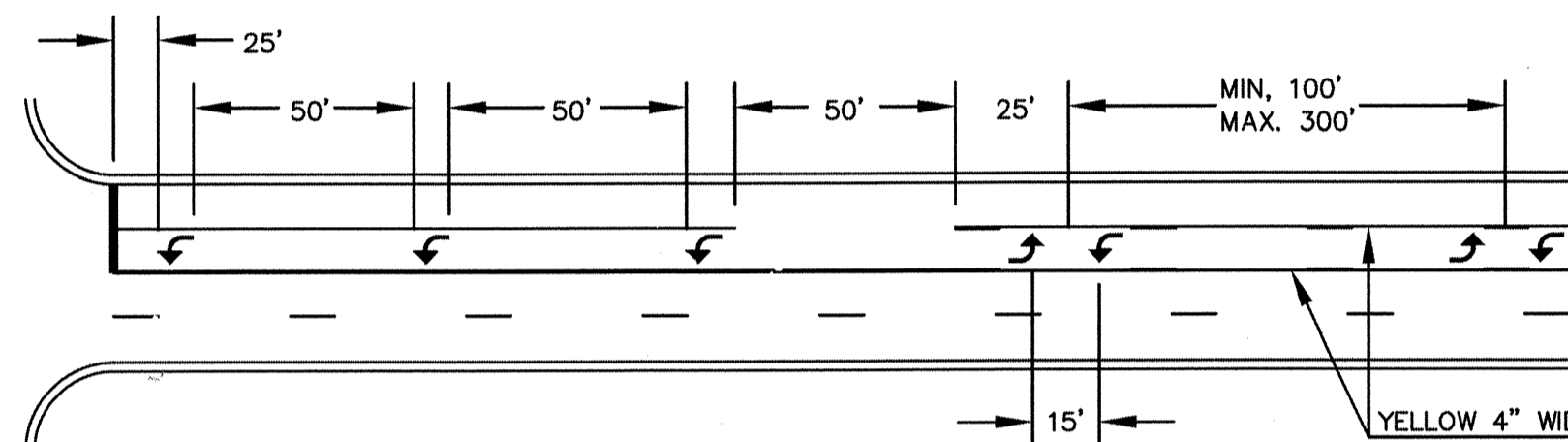
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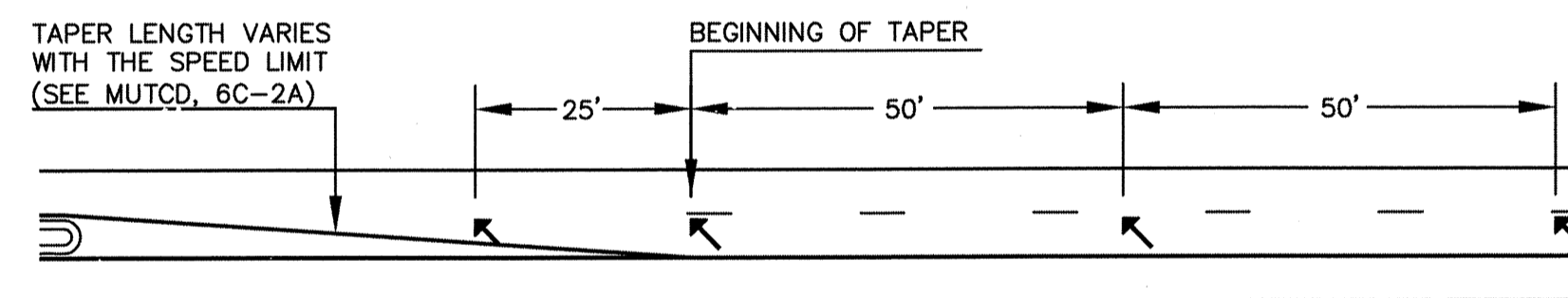
MEDIAN ISLANDS: TWO DOUBLE YELLOW LINES SHALL BE USED TO FORM A MARKED MEDIAN ISLAND THAT SEPARATES TRAFFIC TRAVELING IN OPPOSITE DIRECTIONS. FOR ISLANDS WITH A WIDTH THAT IS GREATER THAN OR EQUAL TO 6', TWO SOLID YELLOW TRANSVERSE LINES, 12" WIDE, SHOULD WHENEVER POSSIBLE BE INSTALLED AT THE NOSE OF THE ISLAND AS SHOWN BELOW.



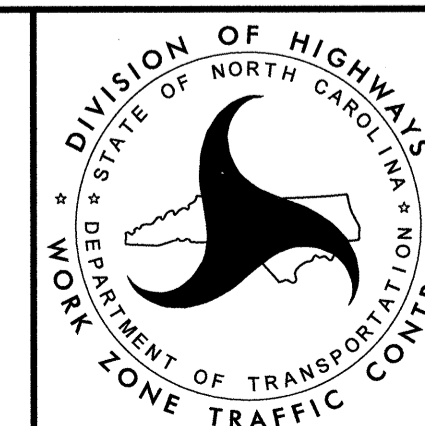
TWO WAY LEFT TURN LANES: TWO WAY LEFT TURN LANES (TWTL), COMMONLY REFERRED TO AS CENTER TURN LANES, SHALL BE MARKED USING A COMBINATION OF A YELLOW SKIP LINE AND A SOLID YELLOW LINE ON EACH EDGE OF THE LANE. THE SOLID LINE AND THE SKIP LINE ARE PLACED 4" APART WITH THE SKIP LINE ON THE INSIDE OF THE LANE. WHERE THE TWTL IS TERMINATED BY AN EXCLUSIVE TURN LANE, THERE SHALL BE A 50' LONG GAP BETWEEN THE SOLID SKIP YELLOW LINE OF THE TWTL AND THE WHITE LANE LINE OF THE EXCLUSIVE TURN LANE. WHITE ARROWS SHALL BE INSTALLED IN THE LANE AS SHOWN BELOW.



MERGE LANE ARROWS: STRAIGHT WHITE ARROWS AT 45° ANGLE TO THE LANE LINE SHALL BE INSTALLED IN THE THROUGH LANE THAT IS MERGED INTO AN ADJACENT THROUGH LANE AS SHOWN BELOW.



RAISED MARKERS: WHEN USED, RAISED PAVEMENT MARKERS SHALL BE INSTALLED IN ACCORDANCE WITH STANDARDS 1250.01, 1251.01 AND 1253.01 OF THE NORTH CAROLINA ROADWAY STANDARDS DRAWINGS.



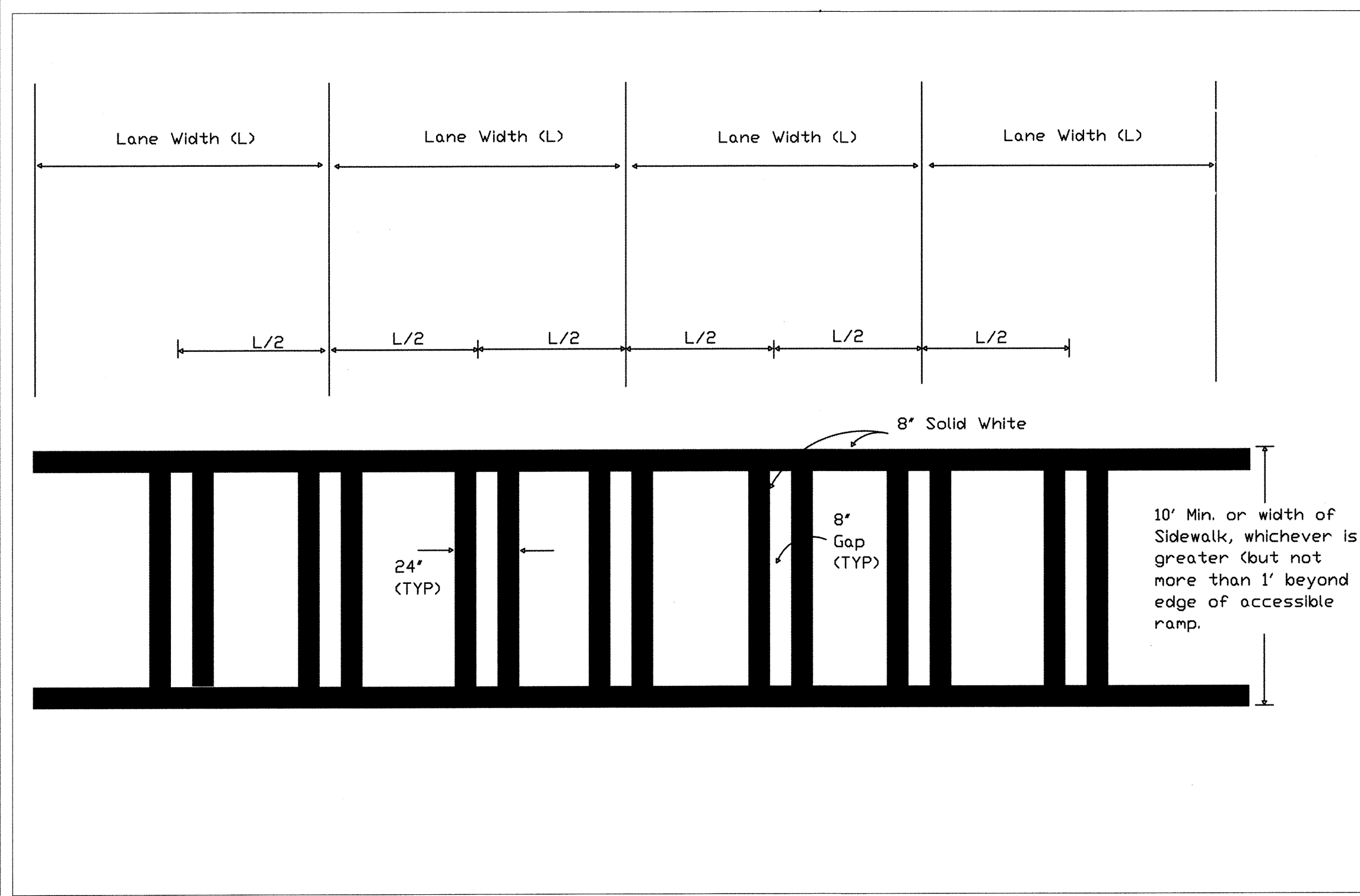


PLANS PREPARED BY :

RK&K

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 RALEIGH, NORTH CAROLINA 27609-3960
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CDOT "Piano" Style Crosswalk Details



2003 Edition MUTCD

Standard:
 When crosswalk lines are used, they shall consist of solid white lines that mark the crosswalk. They shall be not less than 150 mm (6 in) nor greater than 600 mm (24 in) in width.

Guidance:
 If transverse lines are used to mark a crosswalk, the gap between the lines should not be less than 1.8 m (6 ft). If diagonal or longitudinal lines are used without transverse lines to mark a crosswalk, the crosswalk should not be less than 1.8 m (6 ft) wide.

Guidance:
 If used, the diagonal or longitudinal lines should be 300 to 600 mm (12 to 24 in) wide and spaced 300 to 1500 mm (12 to 60 in) apart. The marking design should avoid the wheel paths, and the spacing should not exceed 2.5 times the line width.

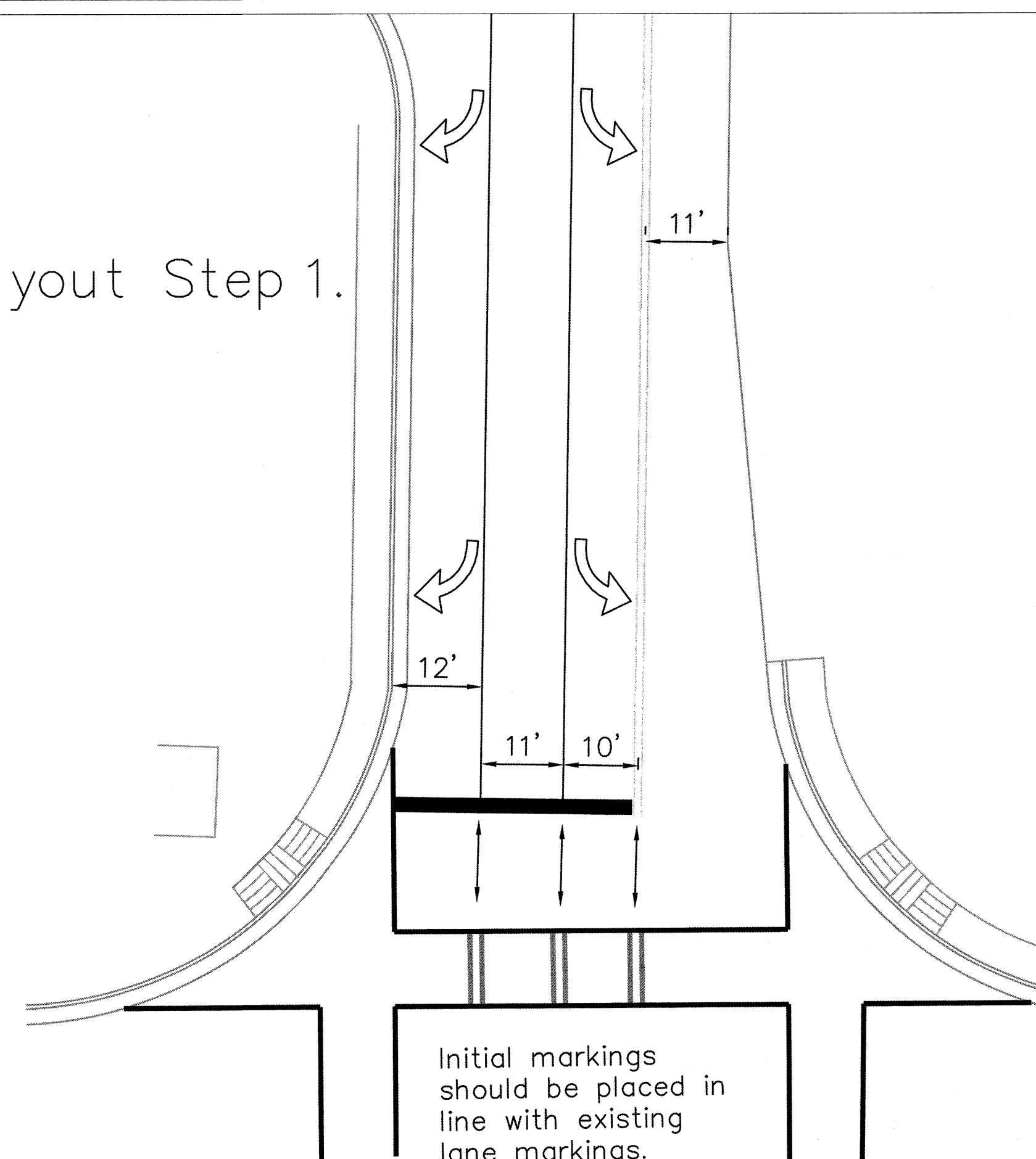
CDOT Traffic Safety
 March 28, 2008

CITY OF CHARLOTTE

DEPARTMENT OF TRANSPORTATION
 Engineering & Operations Division

Technical Memorandum No. 08-06-WLK-01
 Piano Style Crosswalks
 Example Layout Steps

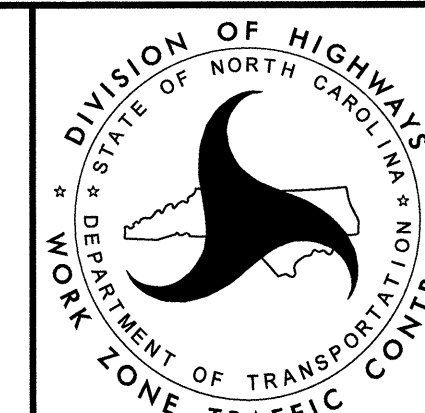
Layout Step 1.



Initial markings should be placed in line with existing lane markings.

**RED added for drawing emphasis. All markings are white.

NORTH CAROLINA PROFESSIONAL SEAL 21047
 ENGINEER
 KEVIN W. BISHOP
 FEB 21, 2013



CDOT STANDARD DRAWING



PLANS PREPARED BY :

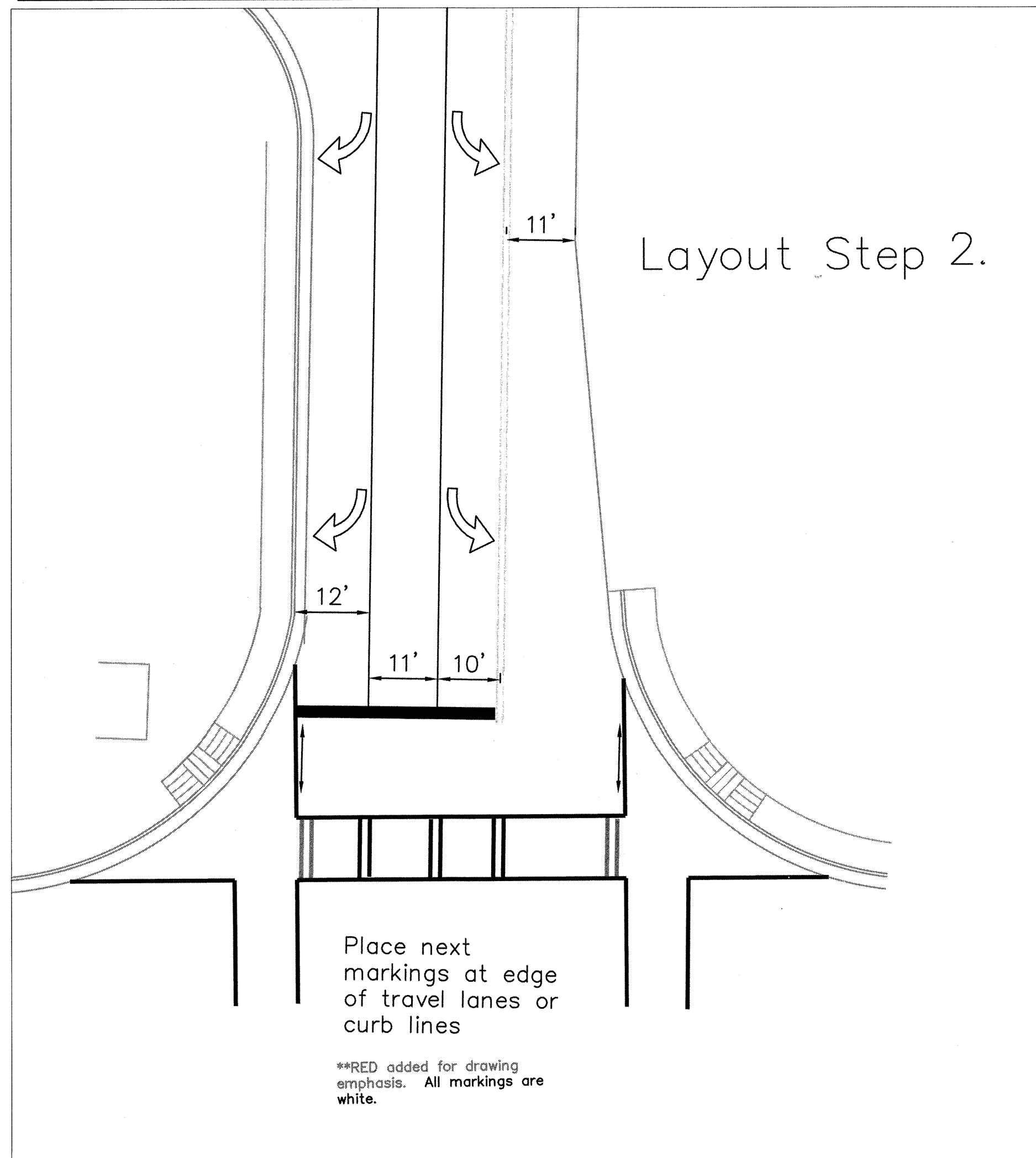


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Engineering & Operations Division

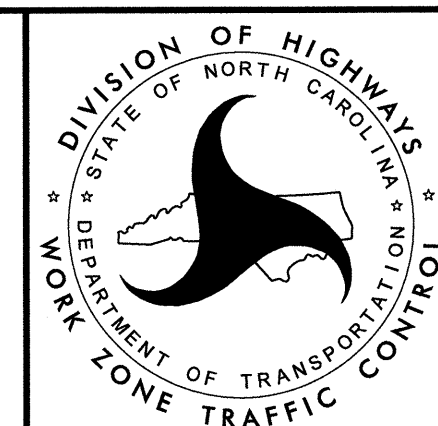
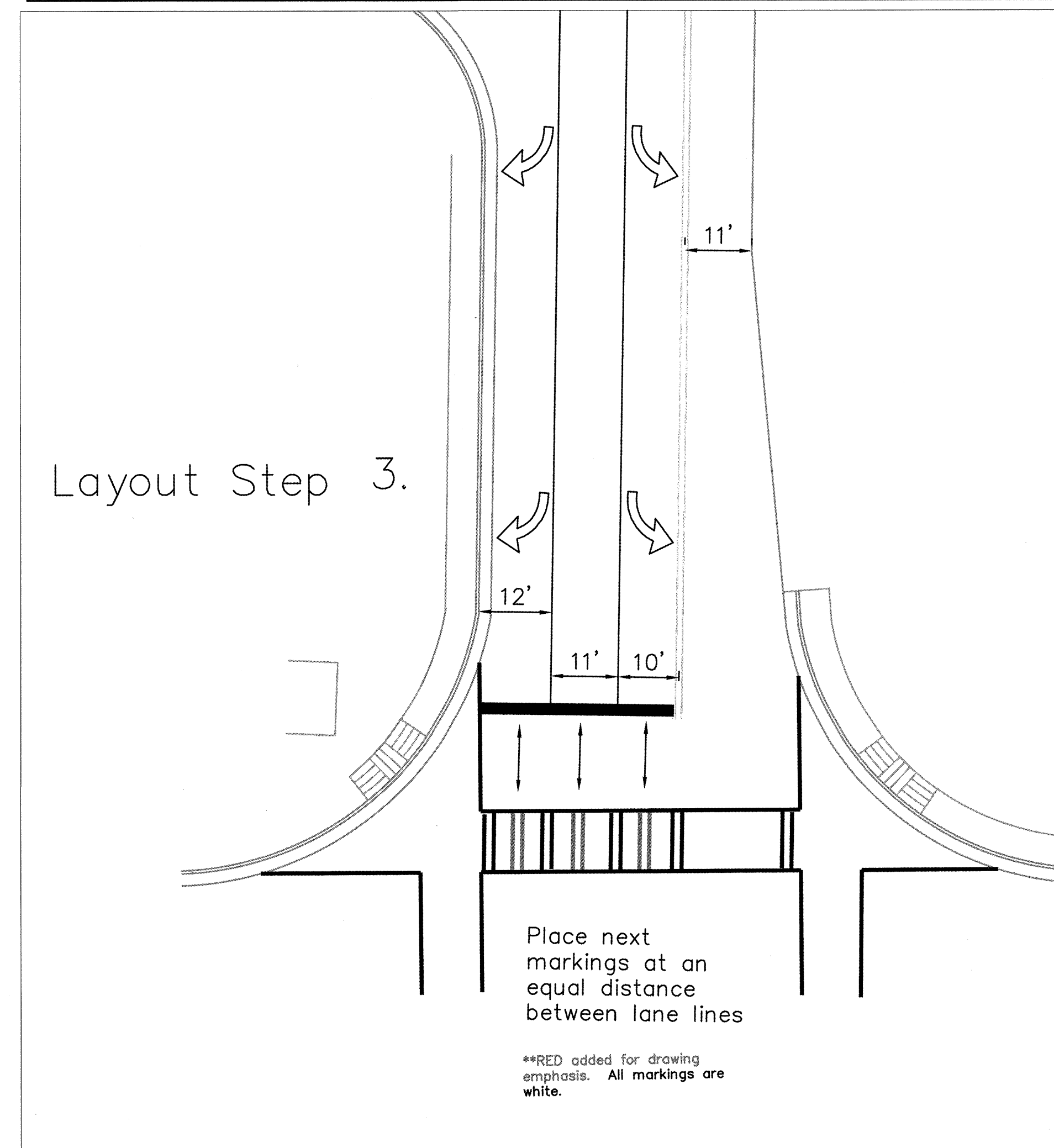
Technical Memorandum No. 08-06-WLK-01
Piano Style Crosswalks
Example Layout Steps



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Technical Memorandum No. 08-06-WLK-01
Piano Style Crosswalks
Example Layout Steps



CDOT STANDARD
DRAWING

DCN
0102DEL_P10a6



PROJ. REFERENCE NO. P - 5208H	SHEET NO. PMP - 12
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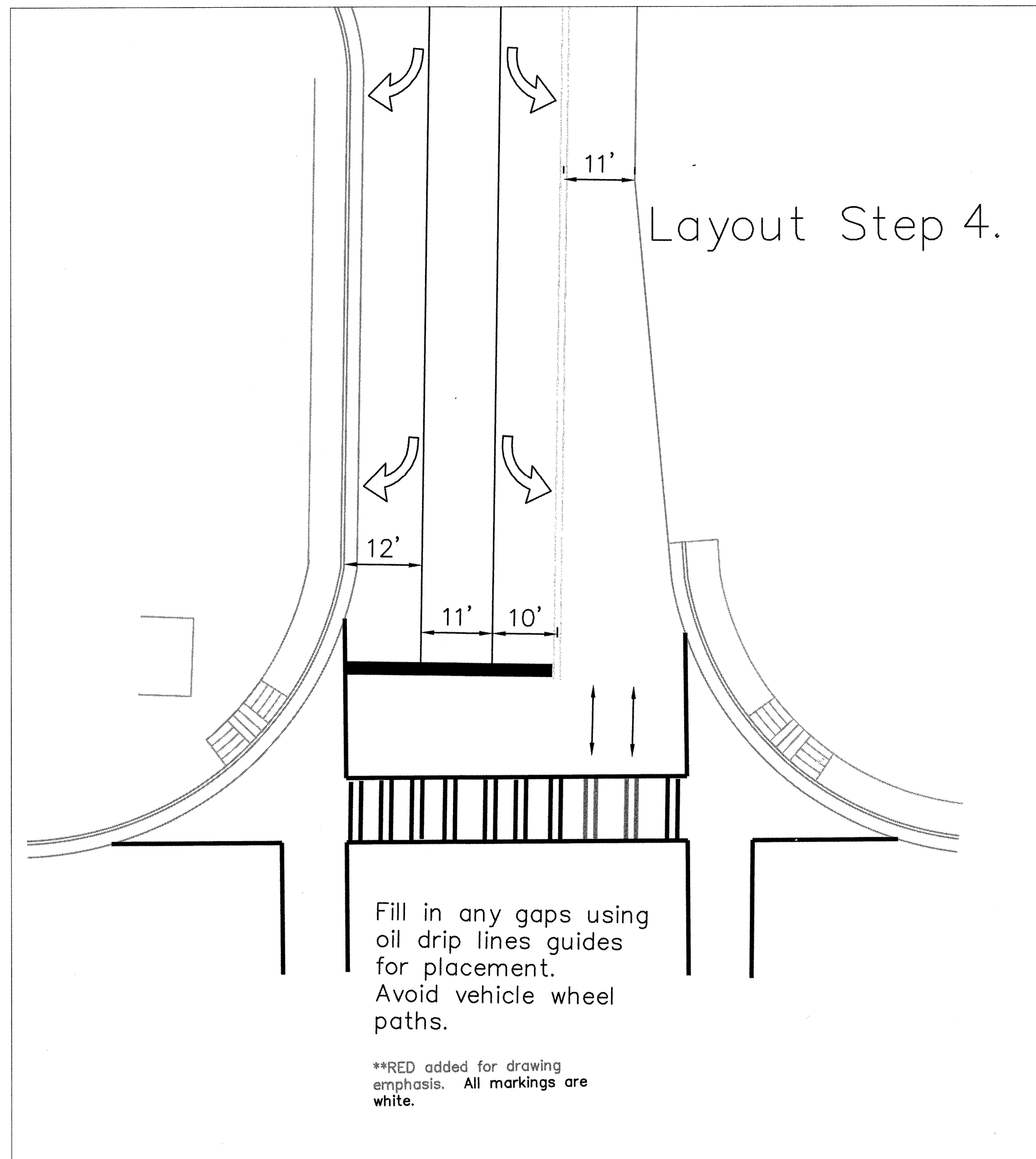
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Technical Memorandum No. 08-06-WLK-01
Piano Style Crosswalks
Example Layout Steps

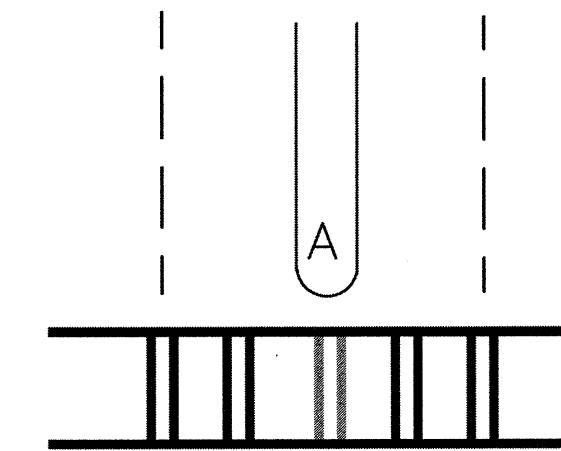


CITY OF CHARLOTTE

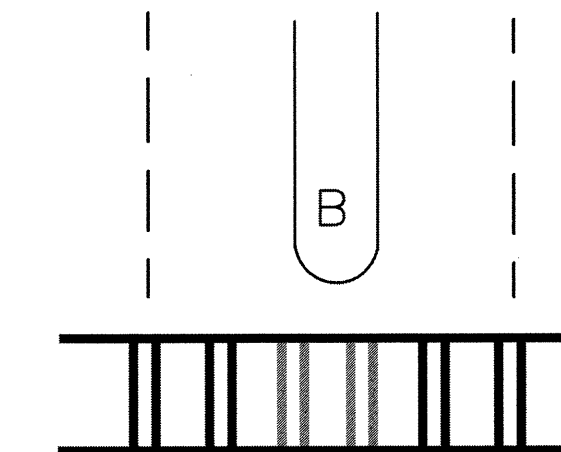
DEPARTMENT OF TRANSPORTATION
Engineering & Operations Division

Technical Memorandum No. 08-06-WLK-01
Piano Style Crosswalks
Median Marking Details

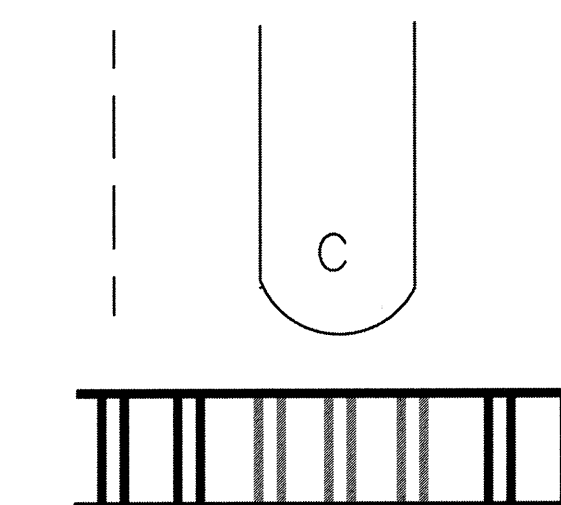
A. For medians 5 feet or less in width use a single marking centered on the median.



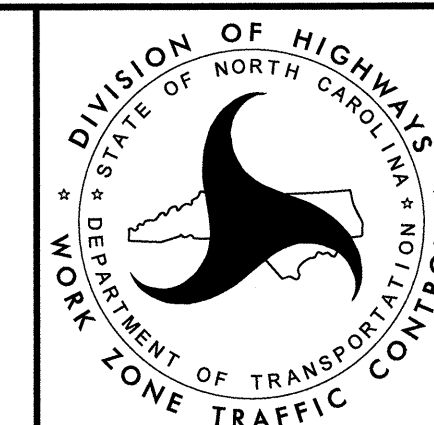
B. For medians 5 feet to 10 feet in width use two markings on either edge of the median.



C. For medians greater than 10 feet in width use two markings on either side of the median edge and add additional markings in the median area with spacing not to exceed 60 inches or less than 24 inches.



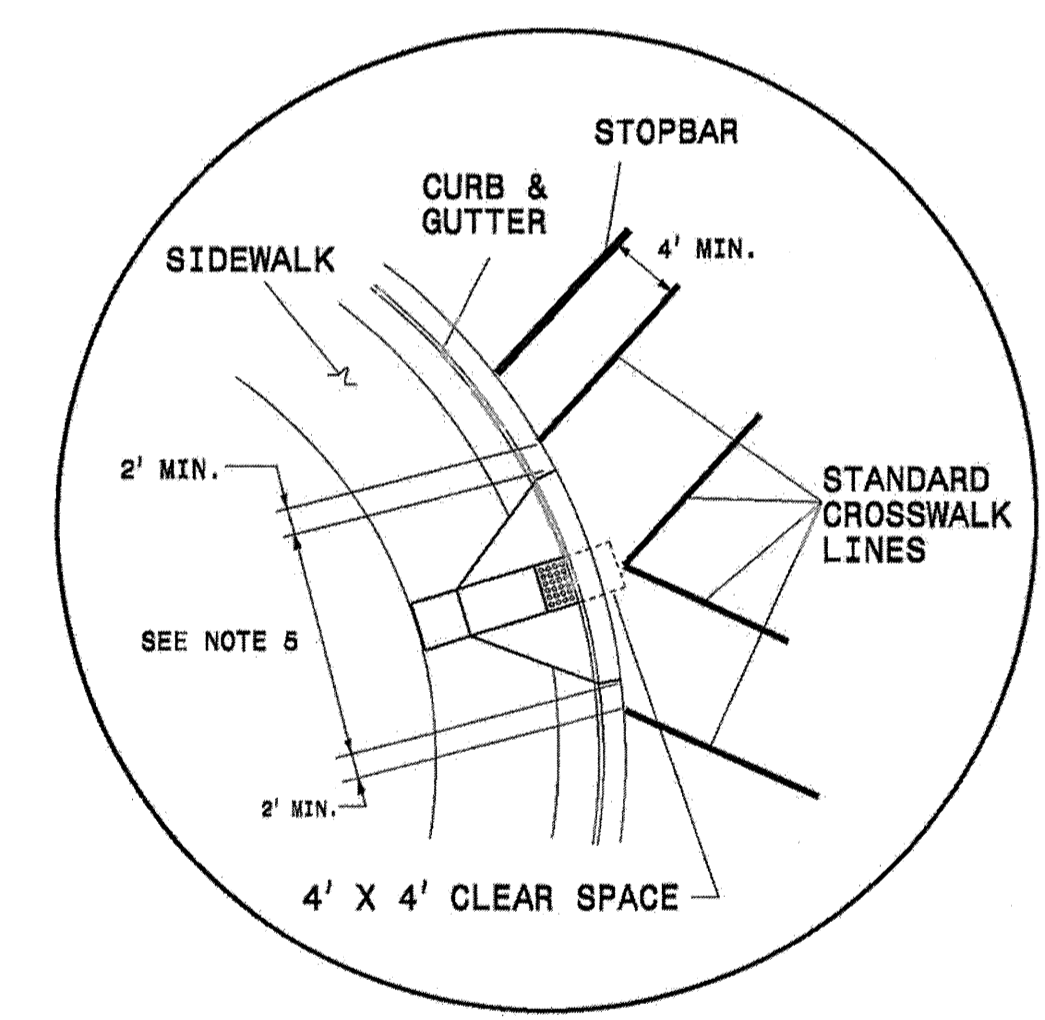
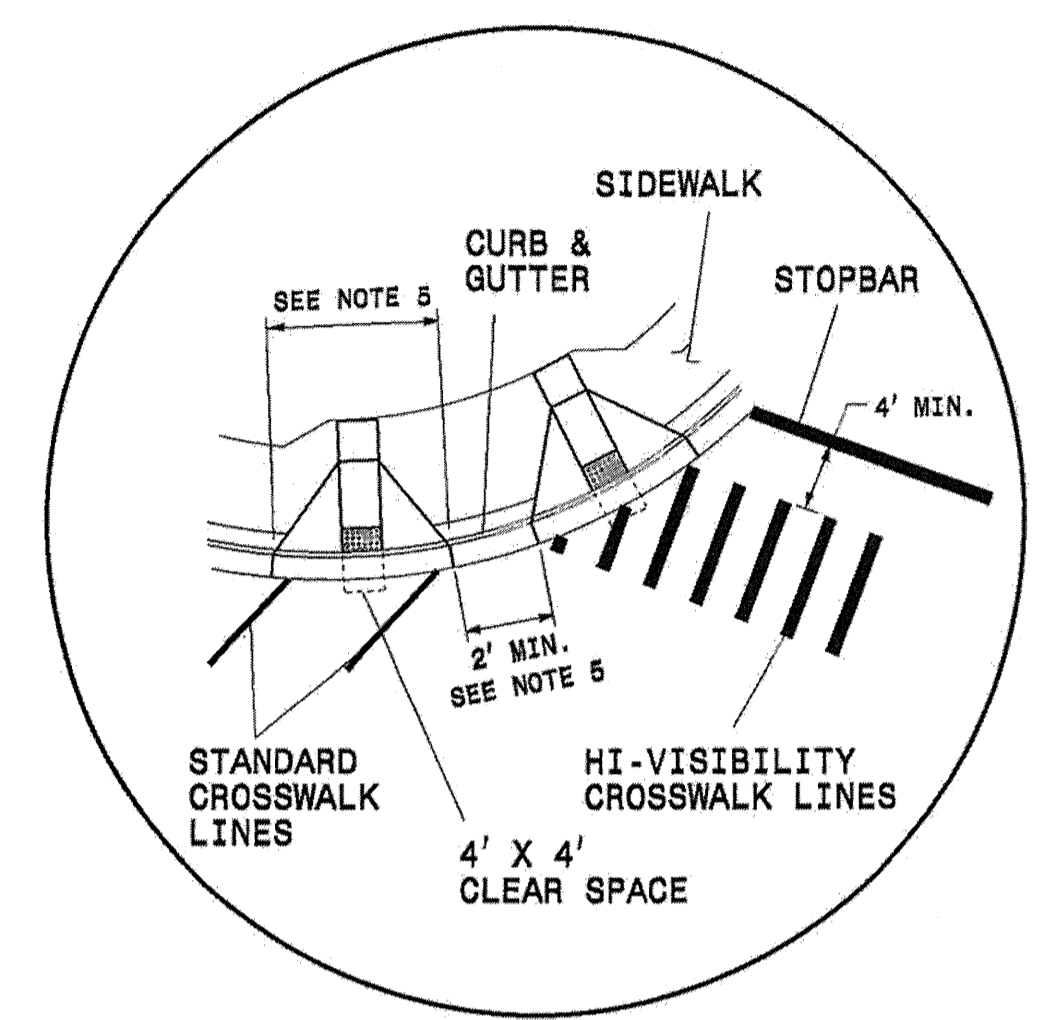
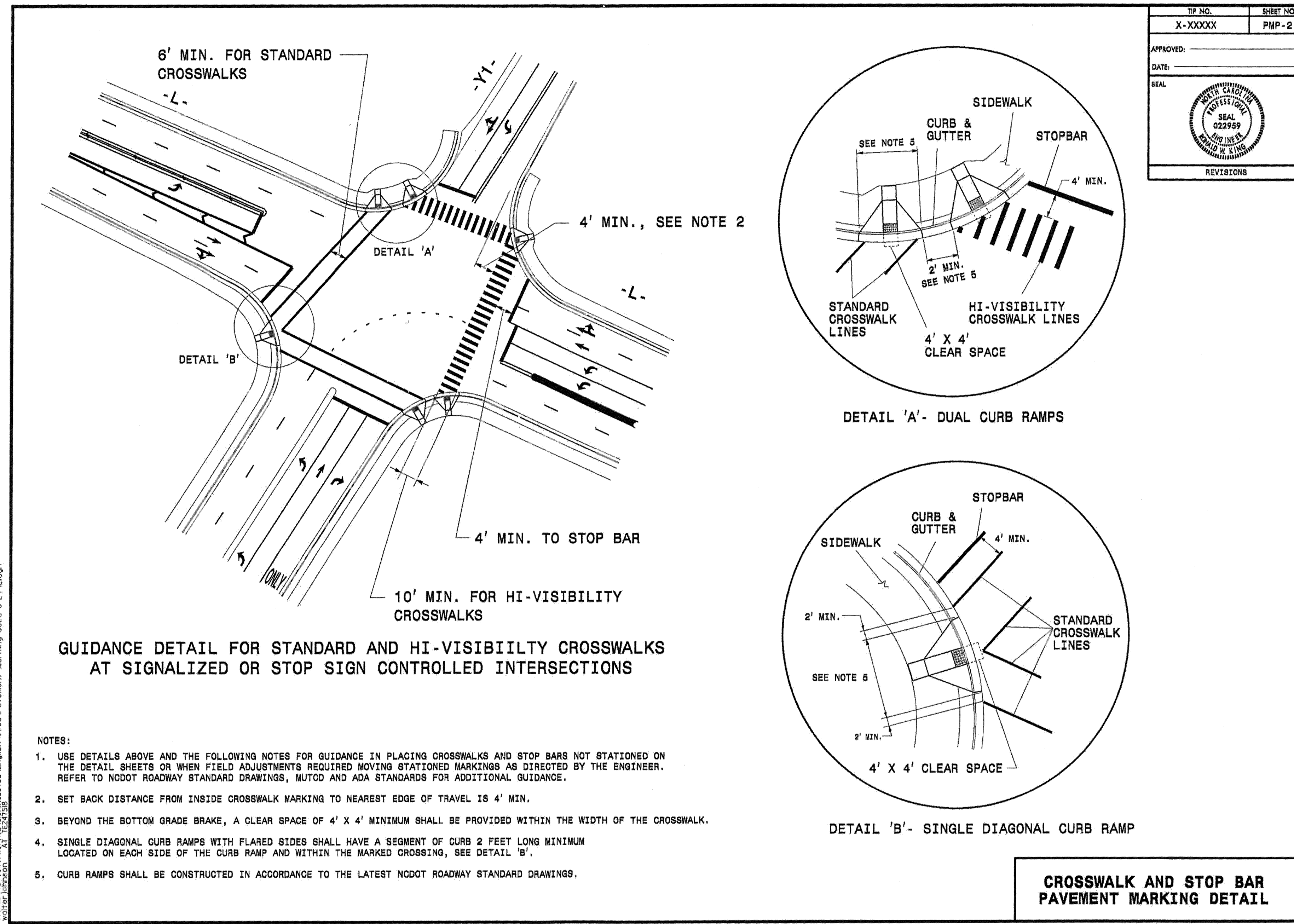
2/27/2013
P-5208H_PMP - STDS.dgn
KBISBY



CDOT STANDARD
DRAWING



TIP NO. X-XXXXX	SHEET NO. PMP - 2
APPROVED: _____	
DATE: _____	
SEAL	
REVISIONS	



CROSSWALK AND STOP BAR PAVEMENT MARKING DETAIL

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 2/27/2013 10:54:13 AM
 KBI/STDS

		<p>NCDOT DETAIL</p>
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