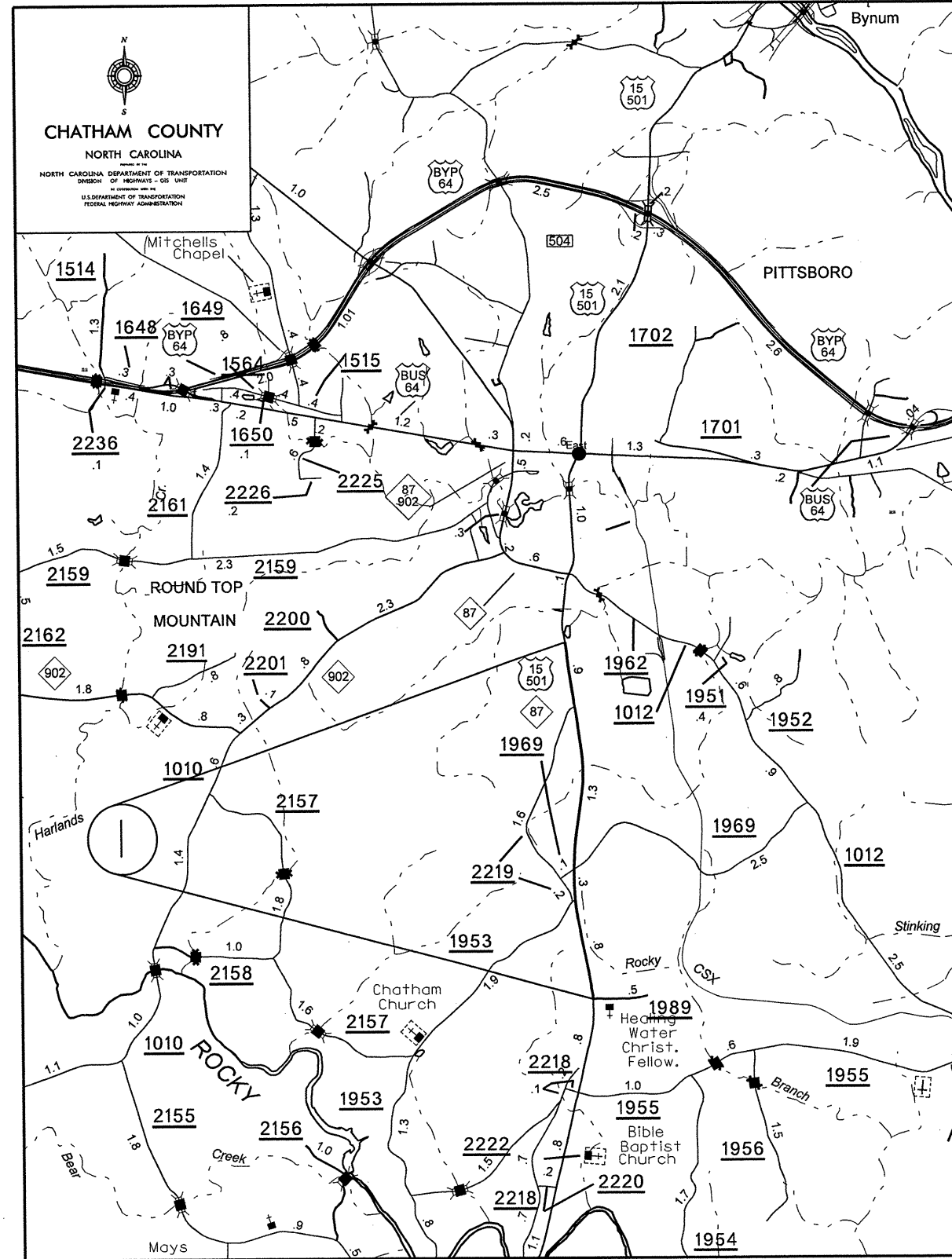


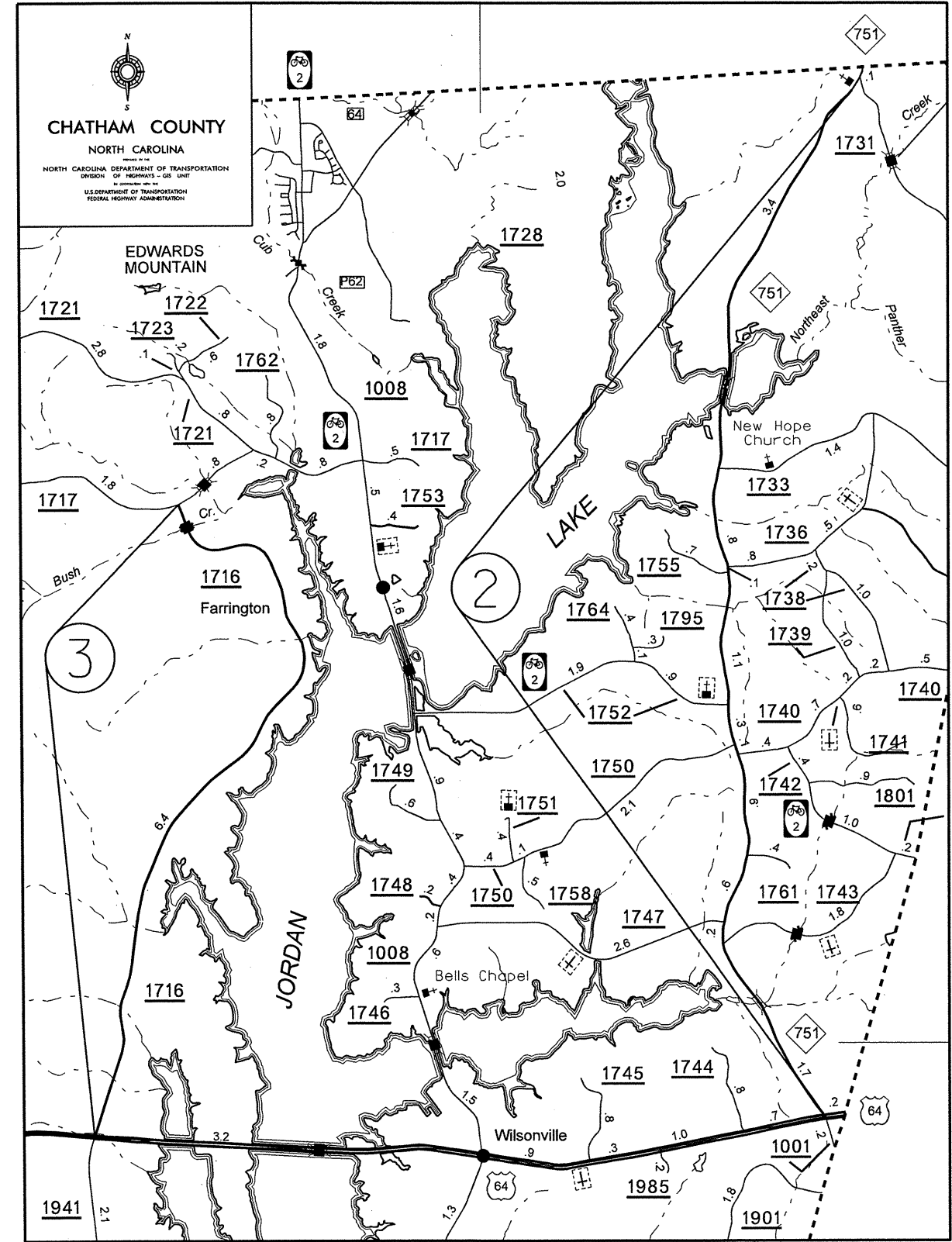
CHATHAM COUNTY

040397

MAP #1

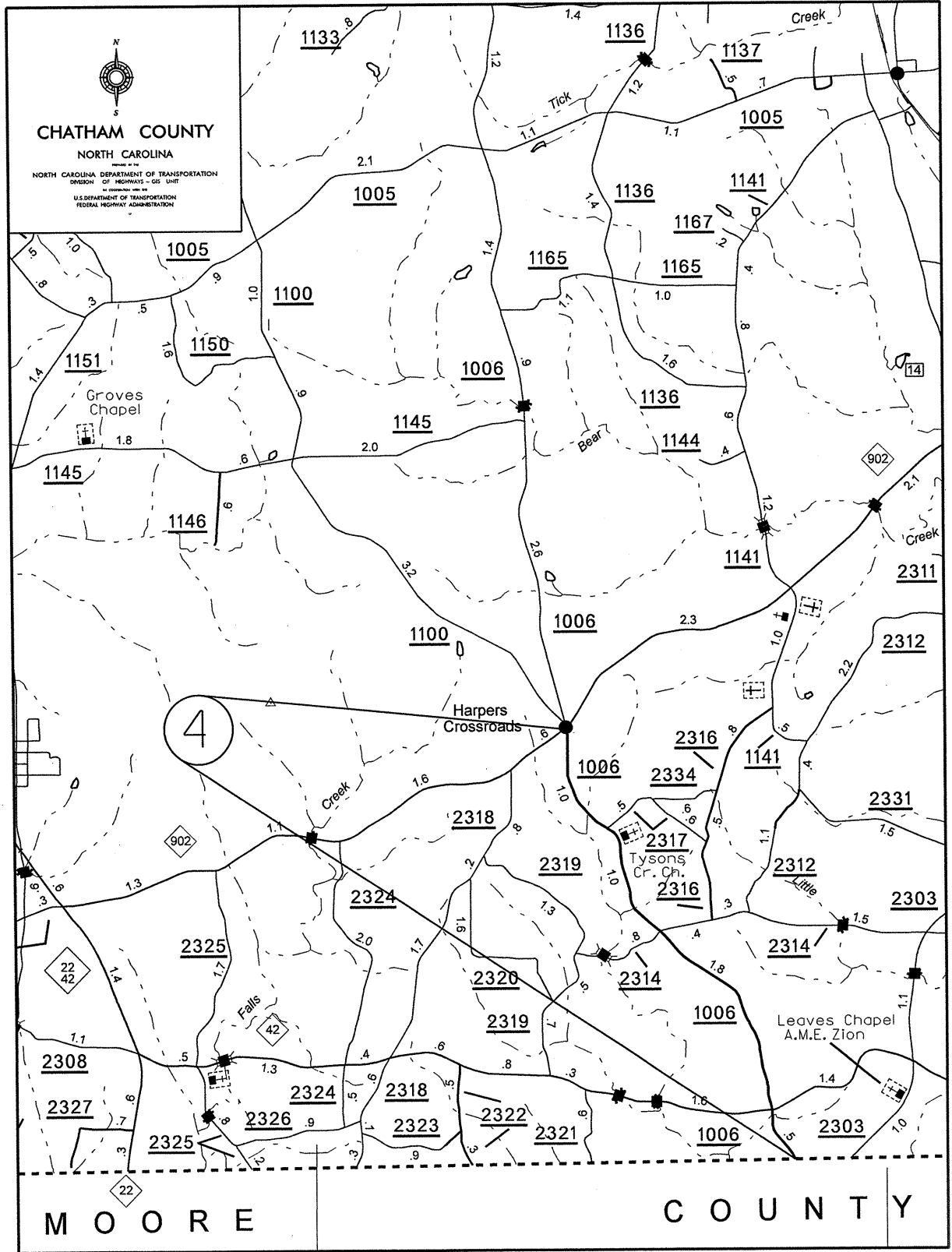


MAP #2 & #3



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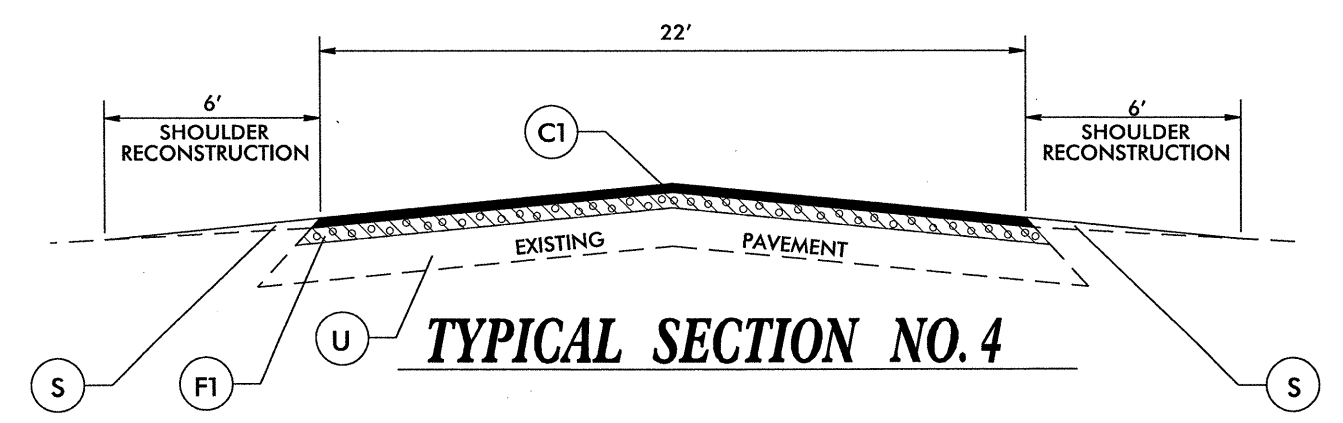
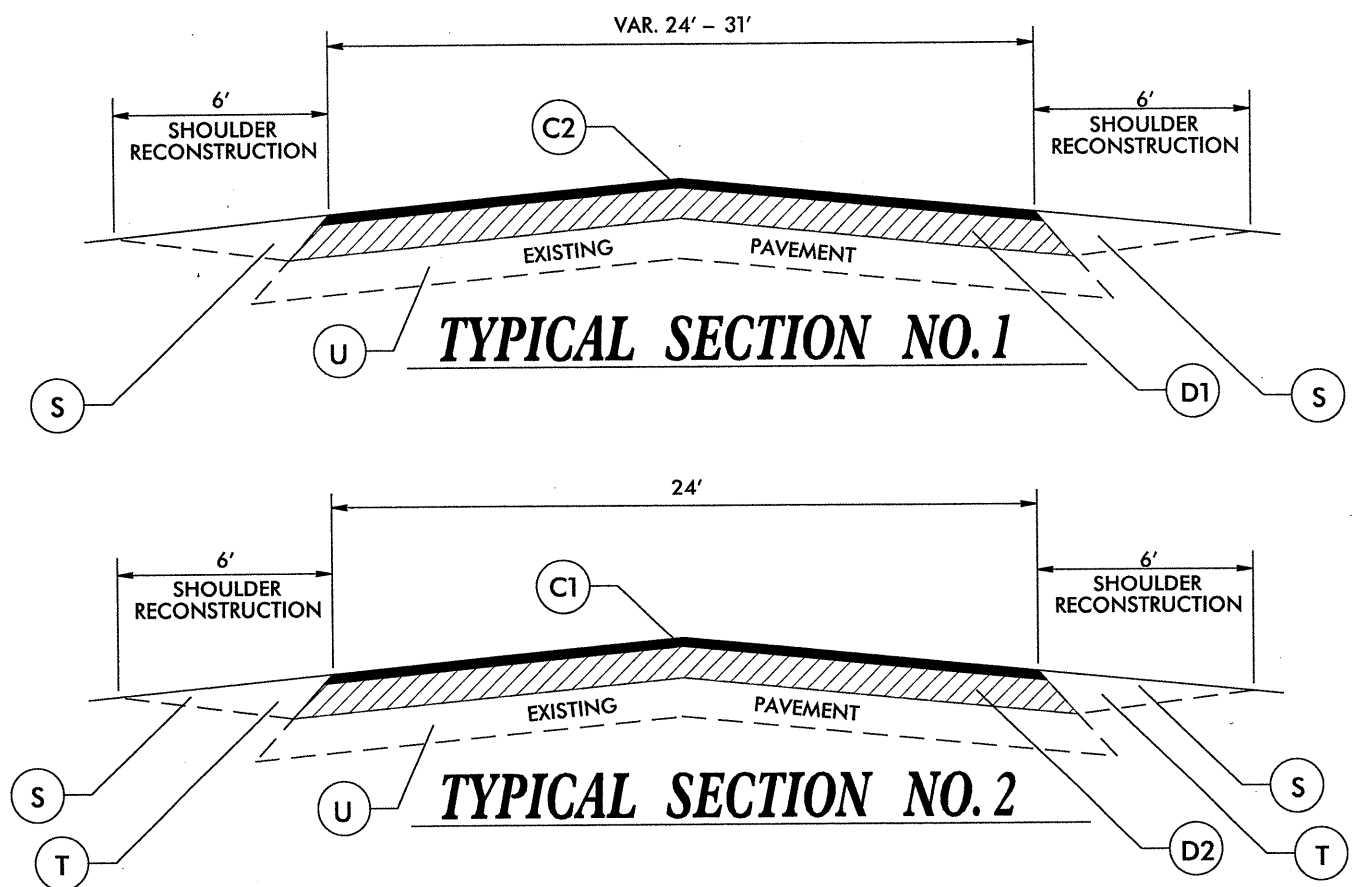
MAP #4



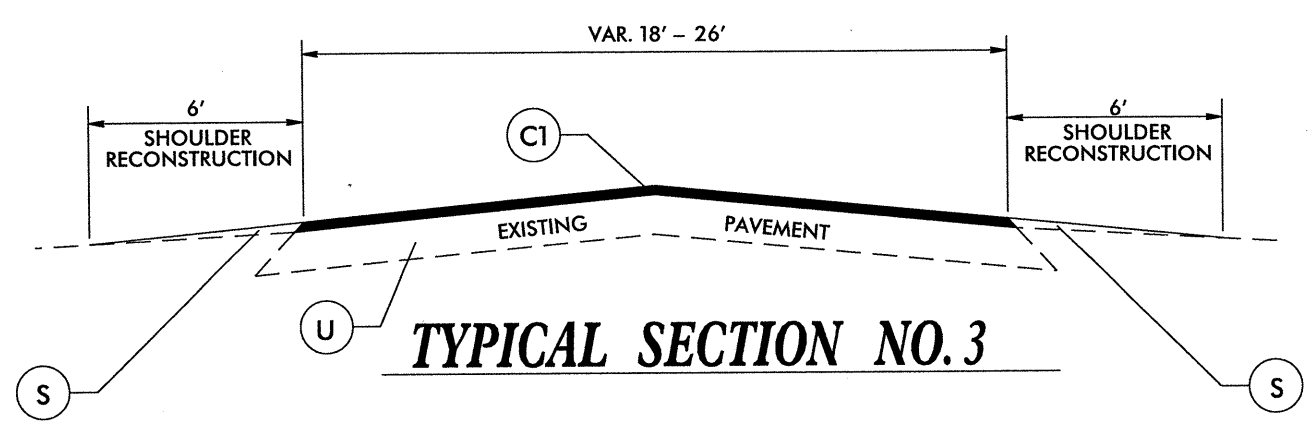
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 2013 Resurfacing Chatham Maps - Typicals.dgn
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040397



NOTE: AGGREGATE SHOULDER BORROW WILL BE USED AT LOCATIONS AS DIRECTED BY THE ENGINEER.

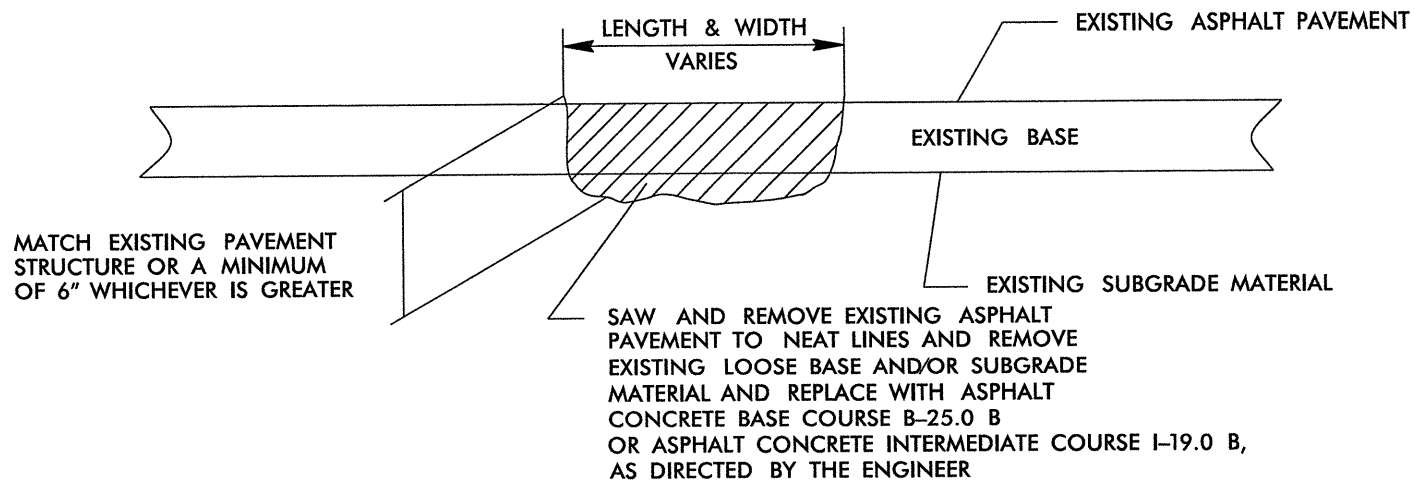


PAVEMENT SCHEDULE

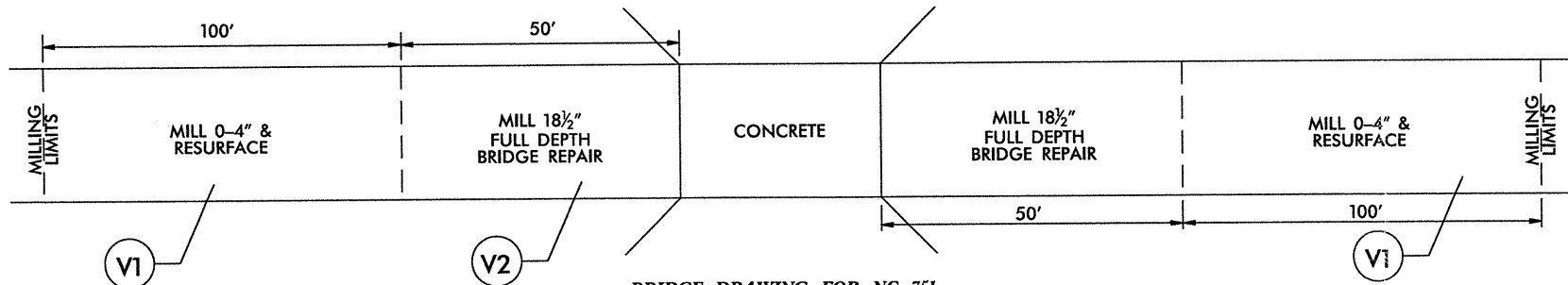
| | |
|----|--|
| C1 | PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| C2 | PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| D1 | PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| D2 | PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| D3 | PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. |
| E1 | PROP. APPROX. 11.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C AT AN AVERAGE RATE OF 437 LBS. PER SQ. YD. IN EACH OF THREE LAYERS |
| F1 | PROPOSED ASPHALT SURFACE TREATMENT, MAT COAT WITH #6 STONE |
| S | AGGREGATE SHOULDER BORROW |
| T | EARTH MATERIAL. |
| U | EXISTING PAVEMENT. |
| V1 | MILLING 0" TO 4" IN DEPTH |
| V2 | MILLING 18.5" |

21-MAY-2012 14:07 C:\p9\p9\48726\2013 Resurfacing\Chatham\Maps_Typical.dgn

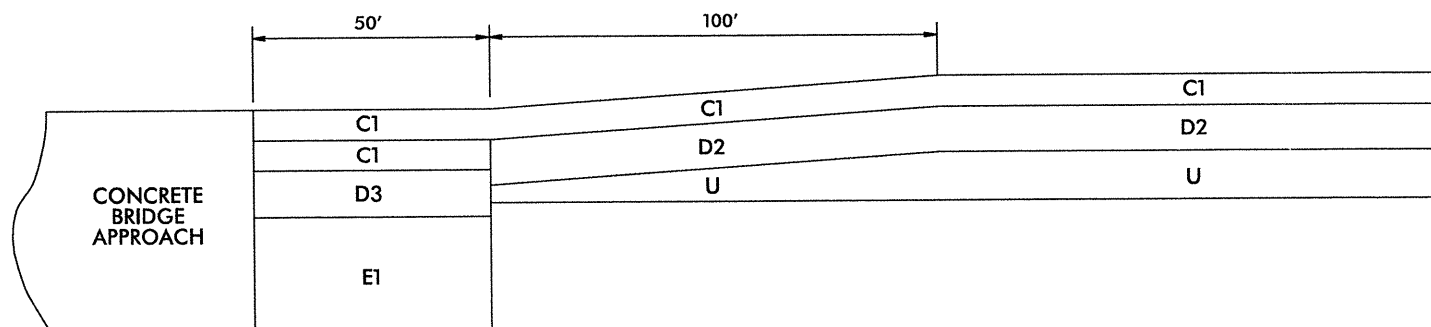
DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING DETAIL



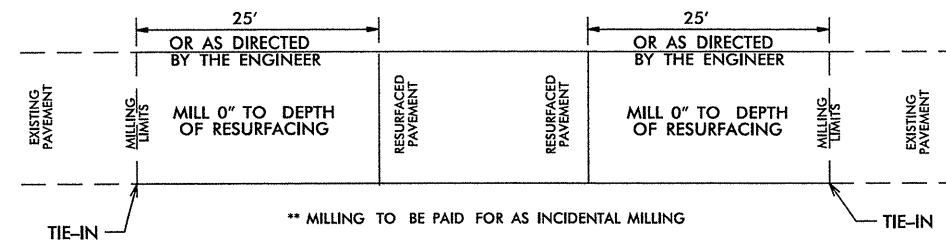
SAW AND REMOVE EXISTING ASPHALT PAVEMENT TO NEAT LINES AND REMOVE EXISTING LOOSE BASE AND/OR SUBGRADE MATERIAL AND REPLACE WITH ASPHALT CONCRETE BASE COURSE B-25.0 B OR ASPHALT CONCRETE INTERMEDIATE COURSE I-19.0 B, AS DIRECTED BY THE ENGINEER



BRIDGE DRAWING FOR NC 751
USE FOR MAP #2
USE FOR BRIDGE #13 & #21
* SEE INSET NO. 1



INSET NO. 1
FULL DEPTH BRIDGE REPAIR
USE FOR MAP #2
BRIDGE #13 & #21



PAVEMENT TIE-IN DETAIL

| | |
|----------------------------|-----------|
| WBS ELEMENT | SHEET NO. |
| 8CR.10191.18, 8CR.20191.18 | 5 |

PAVEMENT SCHEDULE

| | |
|----|--|
| C1 | PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| C2 | PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| D1 | PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| D2 | PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| D3 | PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. |
| E1 | PROP. APPROX. 11.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C AT AN AVERAGE RATE OF 437 LBS. PER SQ. YD. IN EACH OF THREE LAYERS |
| F1 | PROPOSED ASPHALT SURFACE TREATMENT, MAT COAT WITH #6 STONE |
| S | AGGREGATE SHOULDER BORROW |
| T | EARTH MATERIAL. |
| U | EXISTING PAVEMENT. |
| V1 | MILLING 0" TO 4" IN DEPTH |
| V2 | MILLING 18.5" |

| | | |
|----------------------------|-----------|-----------|
| PROJECT NO. | SHEET NO. | TOTAL NO. |
| 8CR.10191.18, 8CR.20191.18 | 6 | |

SUMMARY OF QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | TYP NO | FINAL SURFACE TESTING REQUIRED | LENGTH MI | WIDTH FT | BORROW EXCAVATION CY | GENERIC GRADING ITEM AGGREGATE SHOULDER BORROW TON | INCIDENTAL STONE BASE TONS | SHOULDER RECONSTRUCTION SMI | 18.5" MILLING SY | 0" TO 4" MILLING SY | INCIDENTAL MILLING SY | BASE COURSE, B25.0B TONS | INTERMEDIATE COURSE, I19.0B TONS | INTERMEDIATE COURSE, I19.0C TONS | SURFACE COURSE, S9.5B TONS | SURFACE COURSE, S9.5C TONS | ASPHALT BINDER FOR PLANT MIX TONS | PATCHING EXISTING PAVEMENT TONS | ASPHALT SURFACE TREATMENT, MATCOAT, #6 STONE SY | WATTLE LF | SEED & MULCHING AC |
|--|---------|--------|-------------------|--|--------|--------------------------------|---------------|----------|----------------------|--|----------------------------|-----------------------------|------------------|---------------------|-----------------------|--------------------------|----------------------------------|----------------------------------|----------------------------|----------------------------|-----------------------------------|---------------------------------|---|-----------|--------------------|
| 8CR.10191.18 | Chatham | 1 | US 15-501 / NC 87 | FROM SR 1989 TO WIDTH CHANGE 0.7 MI SOUTH OF SR 1012 | 1 | NO | 2.8 | 31 | | 672.00 | 280 | 5.60 | | | 500 | | | 8,540 | | 5,020 | 706 | 50 | | | |
| | | 2 | NC 751 | FROM US 64 TO JOINT 550' SOUTH OF SR 1931 | 2 | NO | 9.034 | 24 | 2,710 | 2,000.00 | 903 | 18.07 | 535 | 1,070 | 250 | 365 | 19,830 | | 12,145 | | 1,697 | 150 | | 80 | 9.03 |
| TOTAL FOR PROJ NO. 8CR.10191.18 | | | | | | | 11.834 | | 2,710 | 2,672.00 | 1,183 | 23.67 | 535 | 1,070 | 750 | 365 | 19,830 | 8,540 | 12,145 | 5,020 | 2,403 | 200 | | 80 | 9.03 |
| 8CR.20191.18 | Chatham | 3 | SR 1716 | FROM US 64 TO SR 1717 | 3 | NO | 6.31 | 20 | | 1,514.40 | 631 | 12.62 | | | 225 | | | | 6,830 | | 410 | 715 | | | |
| | | 4 | SR 1006 | FROM MOORE CO LINE TO NC 902 | 4 | NO | 4.235 | 22 | | 1,016.40 | 424 | 8.47 | | | 470 | | | | 4,955 | | 297 | 260 | 54,700.00 | | |
| TOTAL FOR PROJ NO. 8CR.20191.18 | | | | | | | 10.545 | | | 2,530.80 | 1,055 | 21.09 | | | 695 | | | | 11,785 | | 707 | 975 | 54,700.00 | | |
| GRAND TOTAL | | | | | | | 22.379 | | 2,710 | 5,202.80 | 2,238 | 44.76 | 535 | 1,070 | 1,445 | 365 | 19,830 | 8,540 | 23,930 | 5,020 | 3,110 | 1,175 | 54,700.00 | 80 | 9.03 |

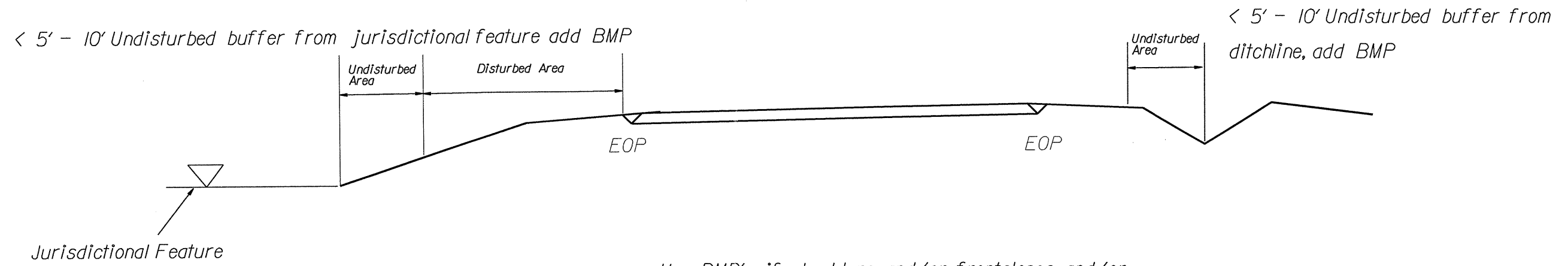
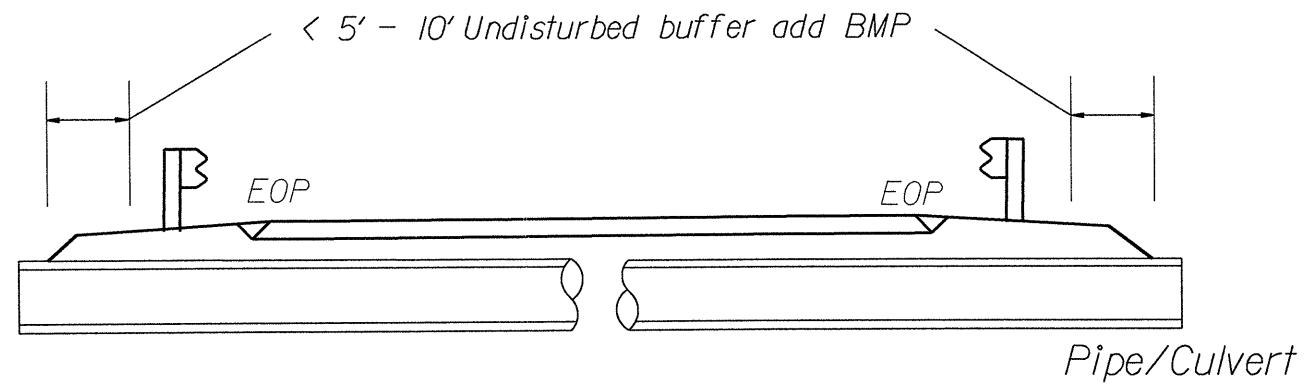
THERMOPLASTIC AND PAINT QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | LEN GTH | WIDTH | 4399000000-N | 4685000000-E | 4686000000-E | | | | 4695000000-E | | | | 4725000000-E | | | | 4810000000-E | | 4820000000-E | 4900000000-N | | 4770000000-E | 4850000000-E |
|--|---------|--------|-------------------|--|---------|-------|------------------------------|---------------------------|-----------------------------|----------------------------|----------------------------|-------------------------|-------------------------|--------------------------|-------------------------------|-------------------|--------------------|--------------------|--------------------------|----------------------------|--|----------------------------------|--------------|--------------|--|--------------|--------------|
| | | | | | | | TEMPORARY TRAFFIC CONTROL LS | 4" X 90 M WHITE THERMO LF | 4" X 120 M YELLOW THERMO LF | 4" X 120 M WHITE THERMO LF | 8" X 90 M YELLOW THERMO LF | THERMO RT ARROW 90 M EA | THERMO LT ARROW 90 M EA | THERMO STR ARROW 90 M EA | THERMO STR & RT ARROW 90 M EA | 4" WHITE PAINT LF | 4" YELLOW PAINT LF | 8" YELLOW PAINT LF | CRYSTAL & RED MARKERS EA | YELLOW & YELLOW MARKERS EA | COLD APPLIED PLASTIC PAVEMENT MARKING LINES, TYPE II (4") LF | PAVEMENT MARKING REMOVAL (4") LF | | | | | |
| 8CR.10191.18 | Chatham | 1 | US 15-501 / NC 87 | FROM SR 1989 TO WIDTH CHANGE 0.7 MI SOUTH OF SR 1012 | 2.8 | 31 | | 30,128 | 18,480 | 1,900 | 186 | 6 | 3 | 4 | | 27,000 | 27,000 | | | 80 | 215 | | | | | | |
| | | 2 | NC 751 | FROM US 64 TO JOINT 550' SOUTH OF SR 1931 | 9 | 24 | * | 97,206 | 90,340 | 800 | 575 | 6 | 6 | 4 | | 97,000 | 97,000 | | | 40 | 723 | 1,000 | 1,000 | | | | |
| TOTAL FOR PROJ NO. 8CR.10191.18 | | | | | | | 12 | | 127,334 | 108,820 | 2,700 | 761 | 6 | 9 | 4 | | 124,000 | 124,000 | | | 120 | 938 | 1,000 | 1,000 | | | |
| TOTAL FOR PROJ NO. 8CR.10191.18 | | | | | | | | | | 111,520 | | | | 19 | | | 248,000 | | | | 1,058 | | 1,000 | 1,000 | | | |
| 8CR.20191.18 | Chatham | 3 | SR 1716 | FROM US 64 TO SR 1717 | 6.3 | 20 | | | | | | 1 | 4 | 2 | 2 | 135,791 | 133,835 | 560 | 20 | 416 | | | | | | | |
| | | 4 | SR 1006 | FROM MOORE CO LINE TO NC 902 | 4.2 | 22 | * | | | | | | | | 91,137 | 89,824 | | | | 280 | | | | | | | |
| TOTAL FOR PROJ NO. 8CR.20191.18 | | | | | | | 11 | | 1 | | | | 1 | 4 | 2 | 2 | 226,928 | 223,659 | 560 | 20 | 696 | | | | | | |
| TOTAL FOR PROJ NO. 8CR.20191.18 | | | | | | | | | | | | | | 9 | | | 450,587 | | | | 716 | | | | | | |
| GRAND TOTAL | | | | | | | 22 | | 1 | 127,334 | 108,820 | 2,700 | 761 | 7 | 13 | 6 | 2 | 350,928 | 347,659 | 560 | 140 | 1,634 | 1,000 | 1,000 | | | |
| GRAND TOTAL | | | | | | | | | | 111,520 | | | | 28 | | | 698,587 | | | | 1,774 | | | | | | |

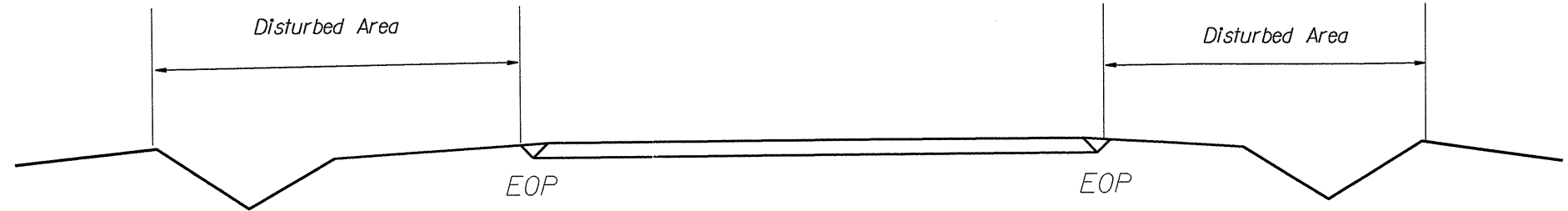
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

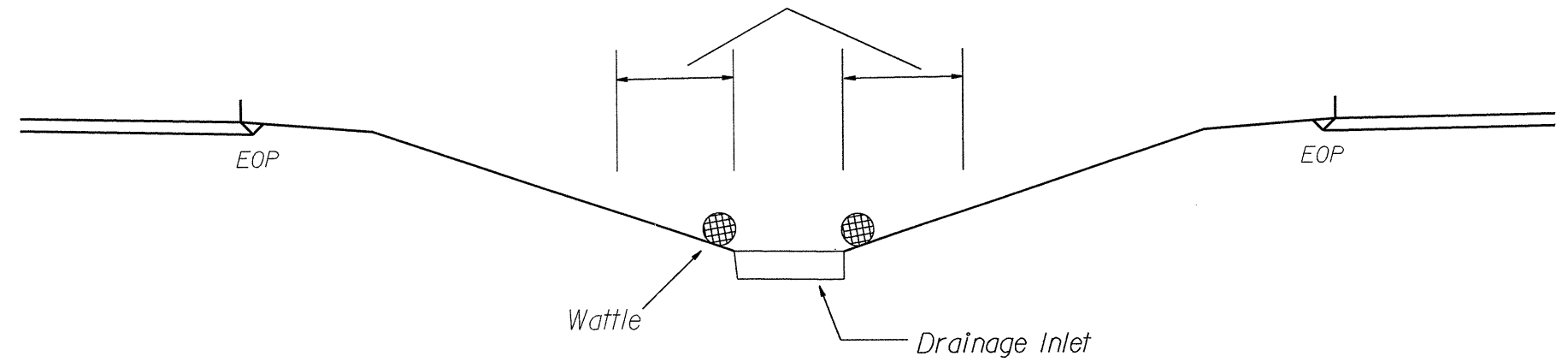
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or front slopes and/or ditchline and/or backslopes are disturbed

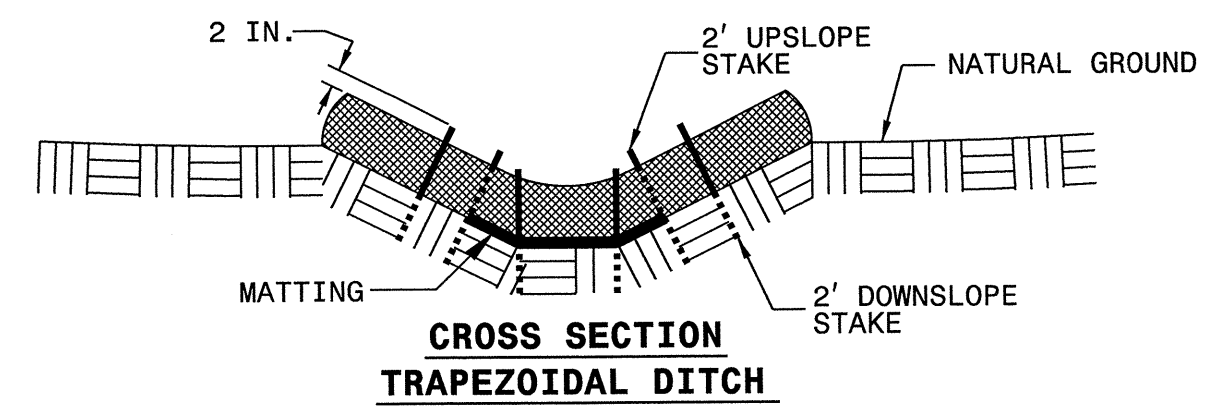
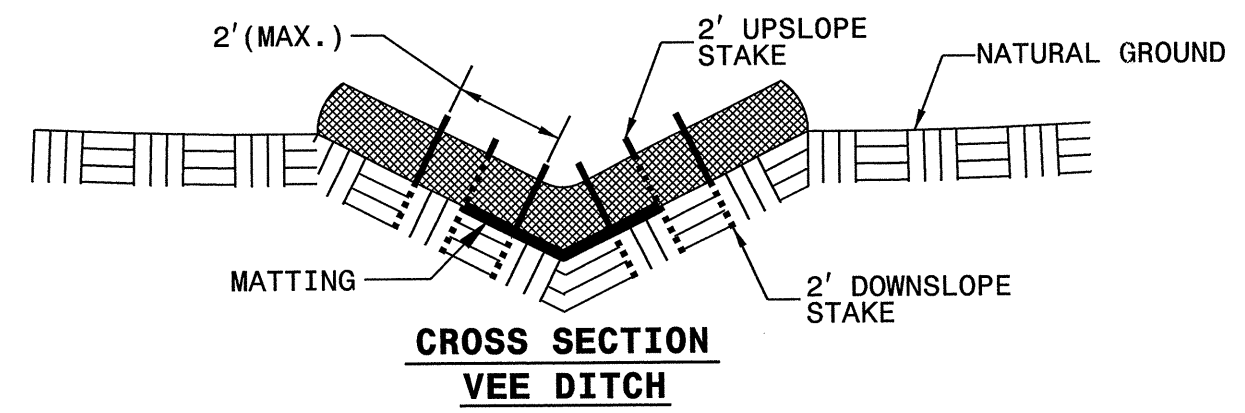
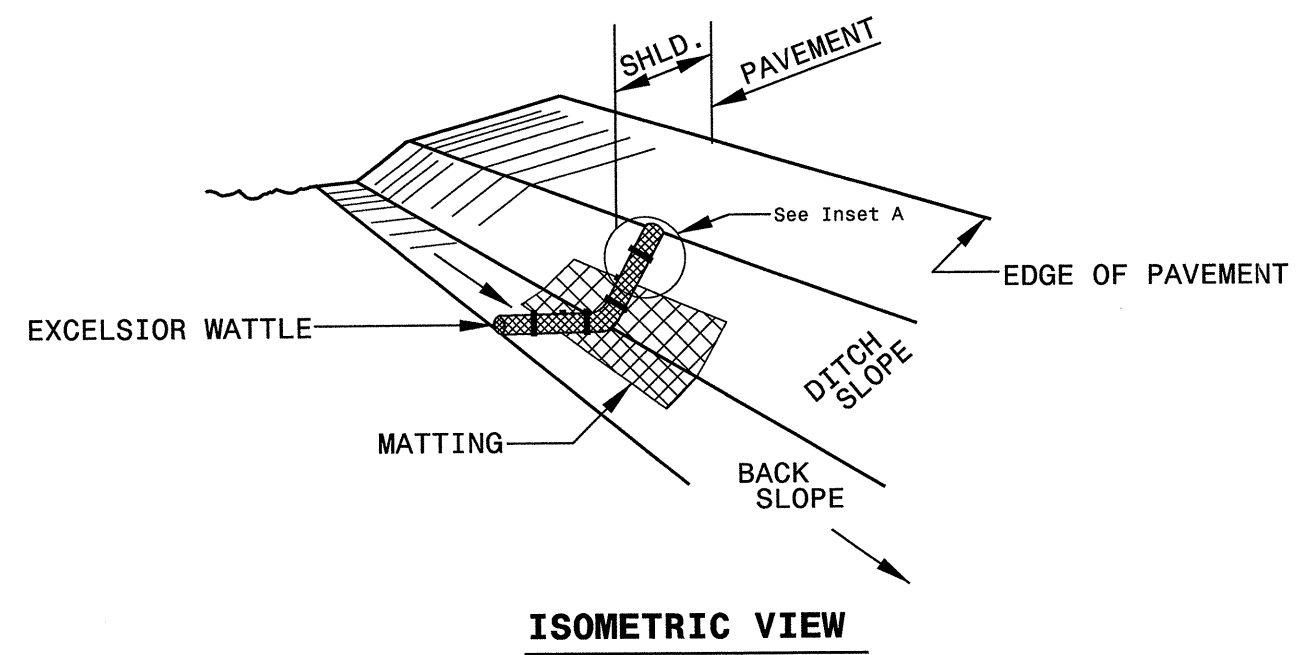


< 5' - 10' Undisturbed buffer from inlet, add wattle

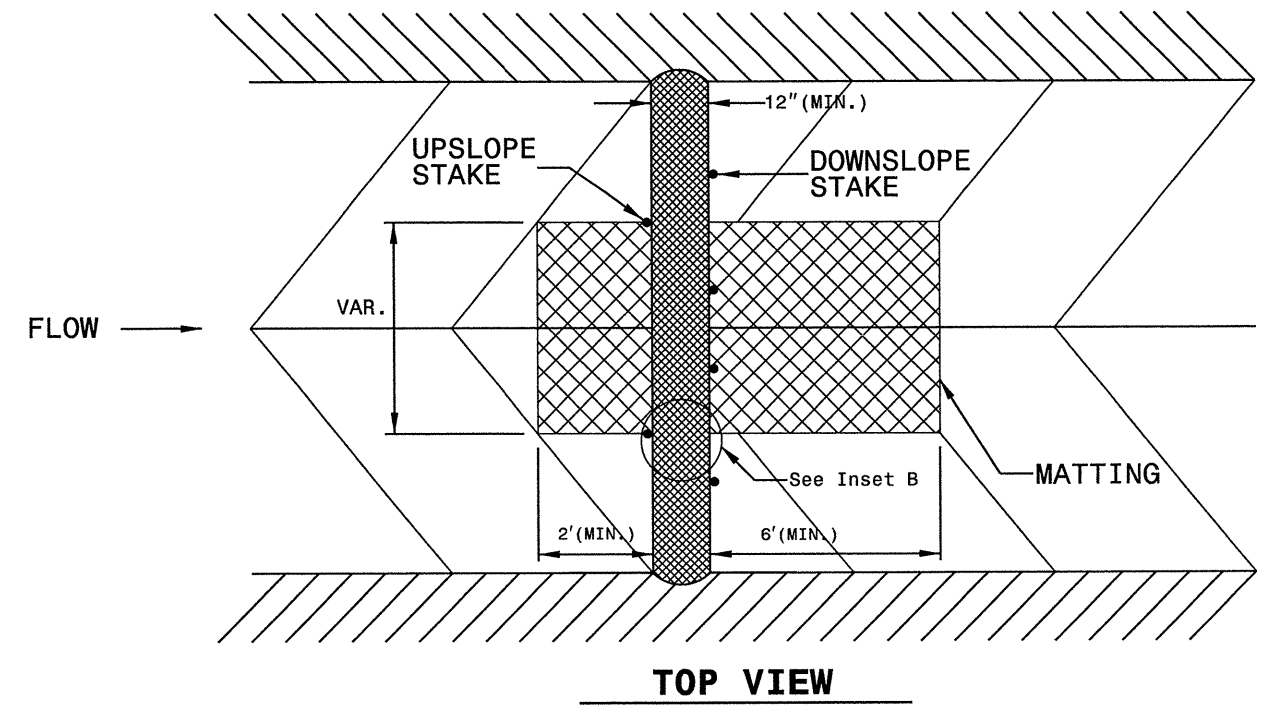
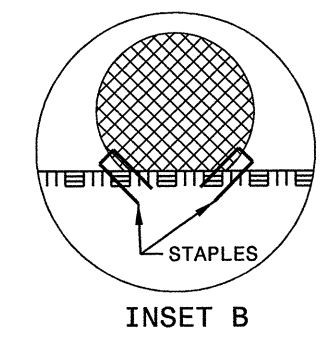
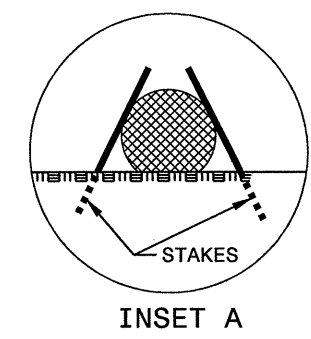


NOT TO SCALE

WATTLE DETAIL

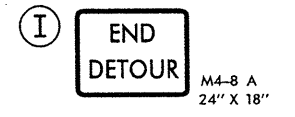
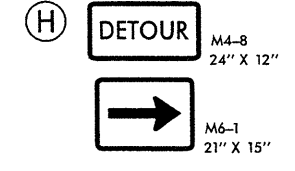
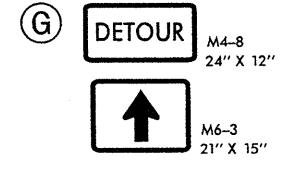
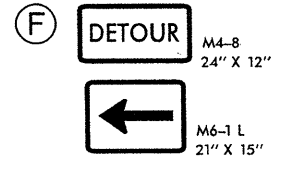
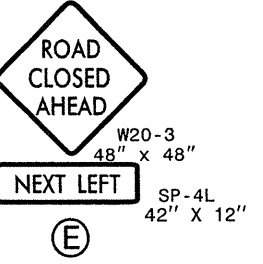
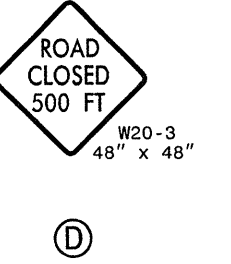
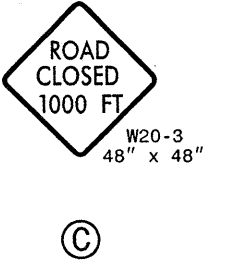
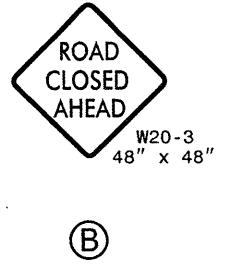
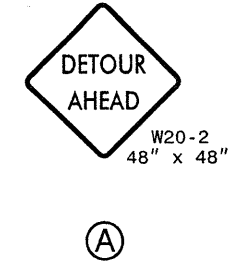
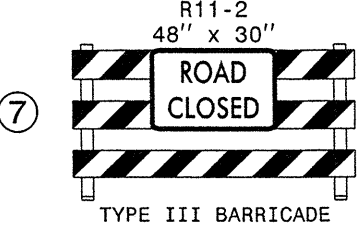
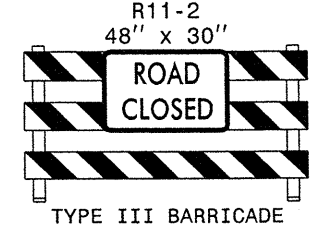
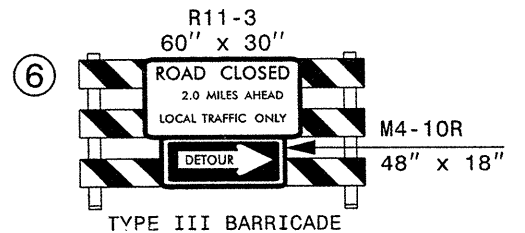
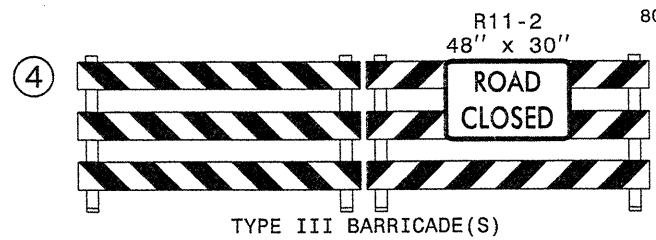
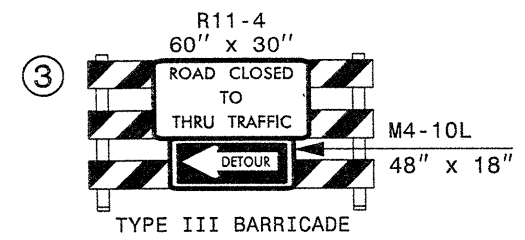
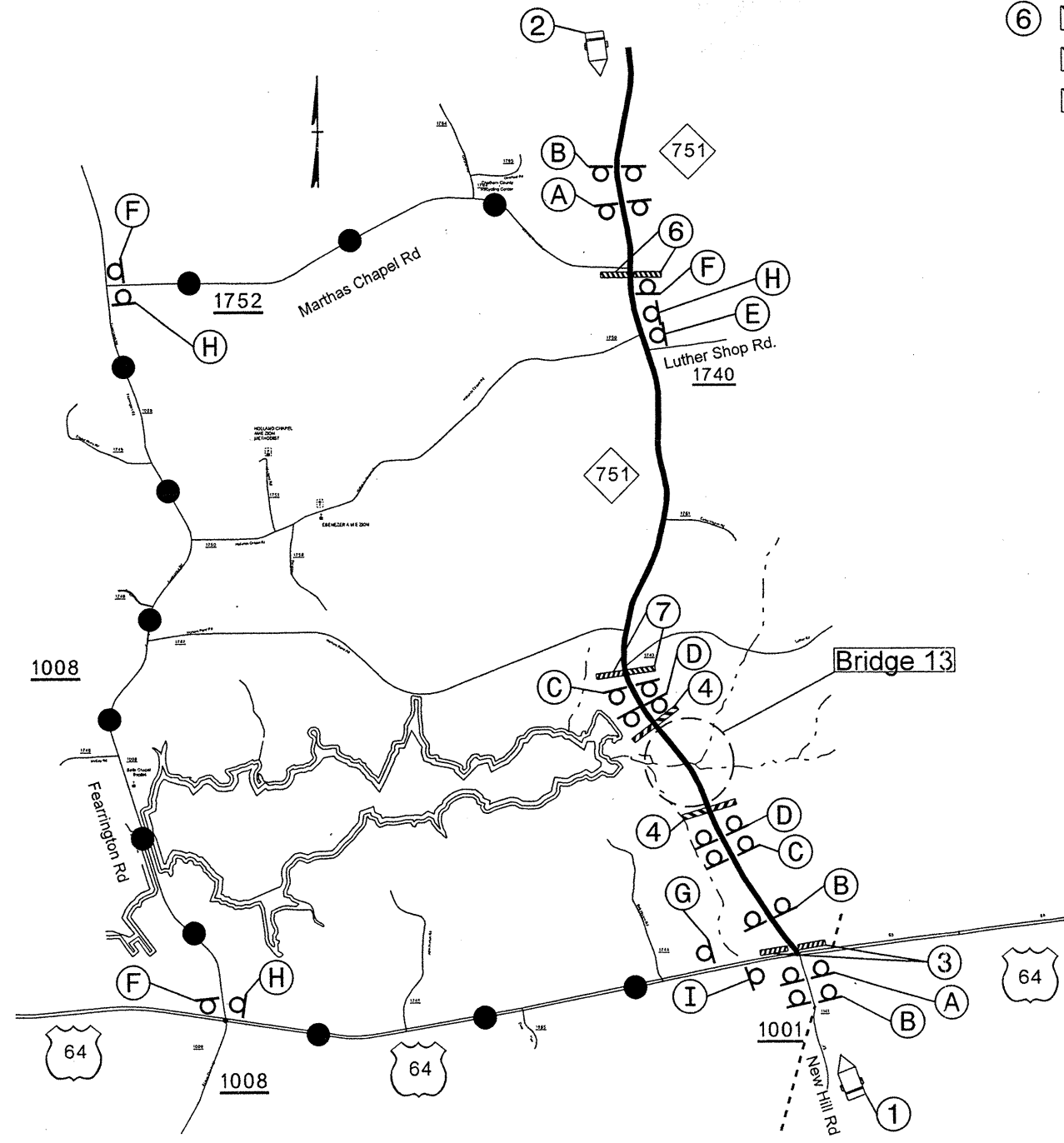


- NOTES:**
- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
 - USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
 - ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
 - INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
 - PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
 - INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
 - INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



ADVANCE NOTICE

| SUGGESTED MESSAGES | | SUGGESTED MESSAGES | | SUGGESTED MESSAGES | |
|---|--|--|-------------------------|--|---|
| ① | MESSAGE NO. 1 NC 751 WILL BE CLOSED | MESSAGE NO. 2 8 PM TO 6 AM | ① | MESSAGE NO. 1 NC 751 CLOSED | MESSAGE NO. 2 DETOUR LEFT ON US 64 |
| AND | CHANGEABLE MESSAGE SIGN | | CHANGEABLE MESSAGE SIGN | | ② |
| * INSTALL CMS 7 DAYS PRIOR TO CLOSURES TO PROVIDE ADVANCE NOTICE TO MOTORISTS. REMOVE/RELOCATE DURING ROAD CLOSURE. | | MESSAGE TO BE USED DURING ROAD CLOSURE | | MESSAGE TO BE USED DURING ROAD CLOSURE | |



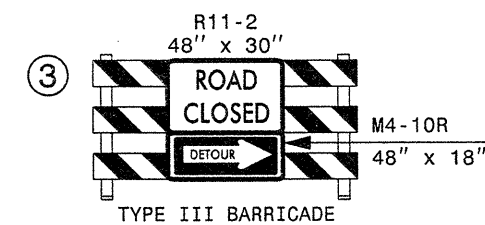
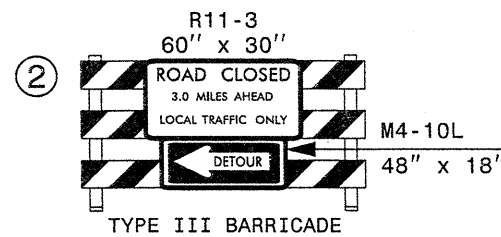
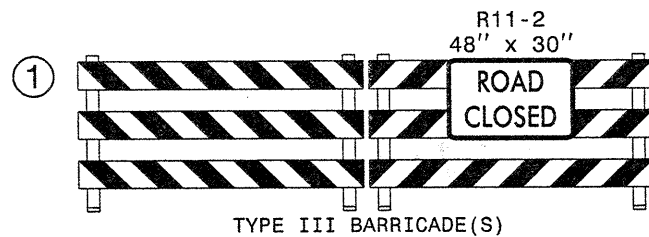
-INSTALL ALL DETOUR SIGNS AND CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.
-SEE INTERMEDIATE CONTRACT TIMES AND LIQUIDATED DAMAGES.

APPROVED: _____ DATE: _____
SEAL



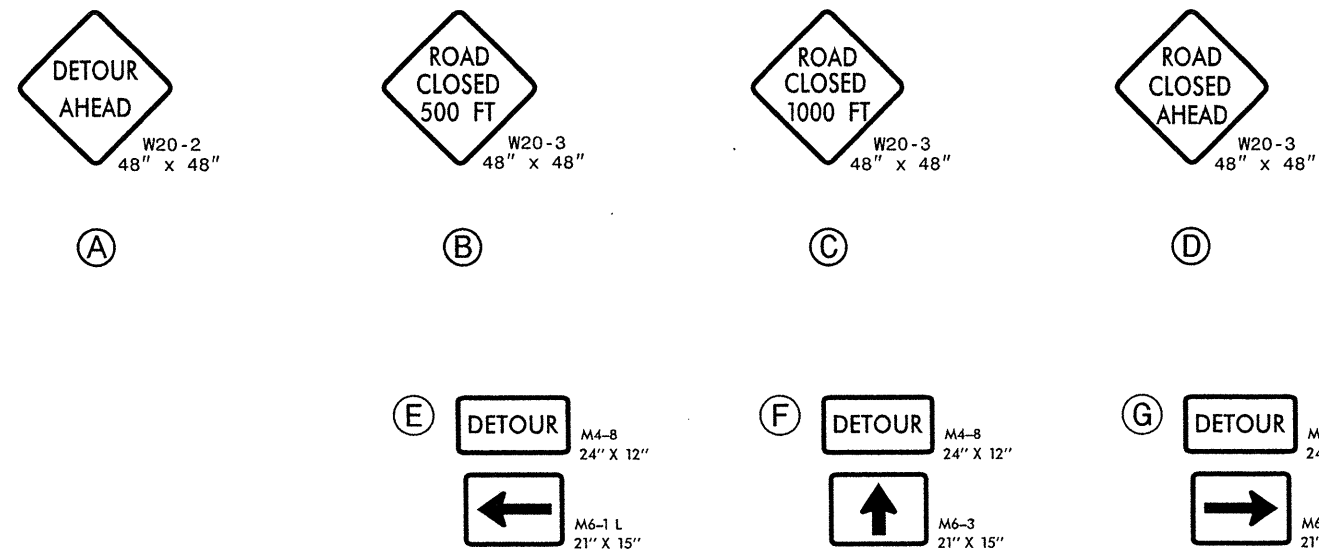
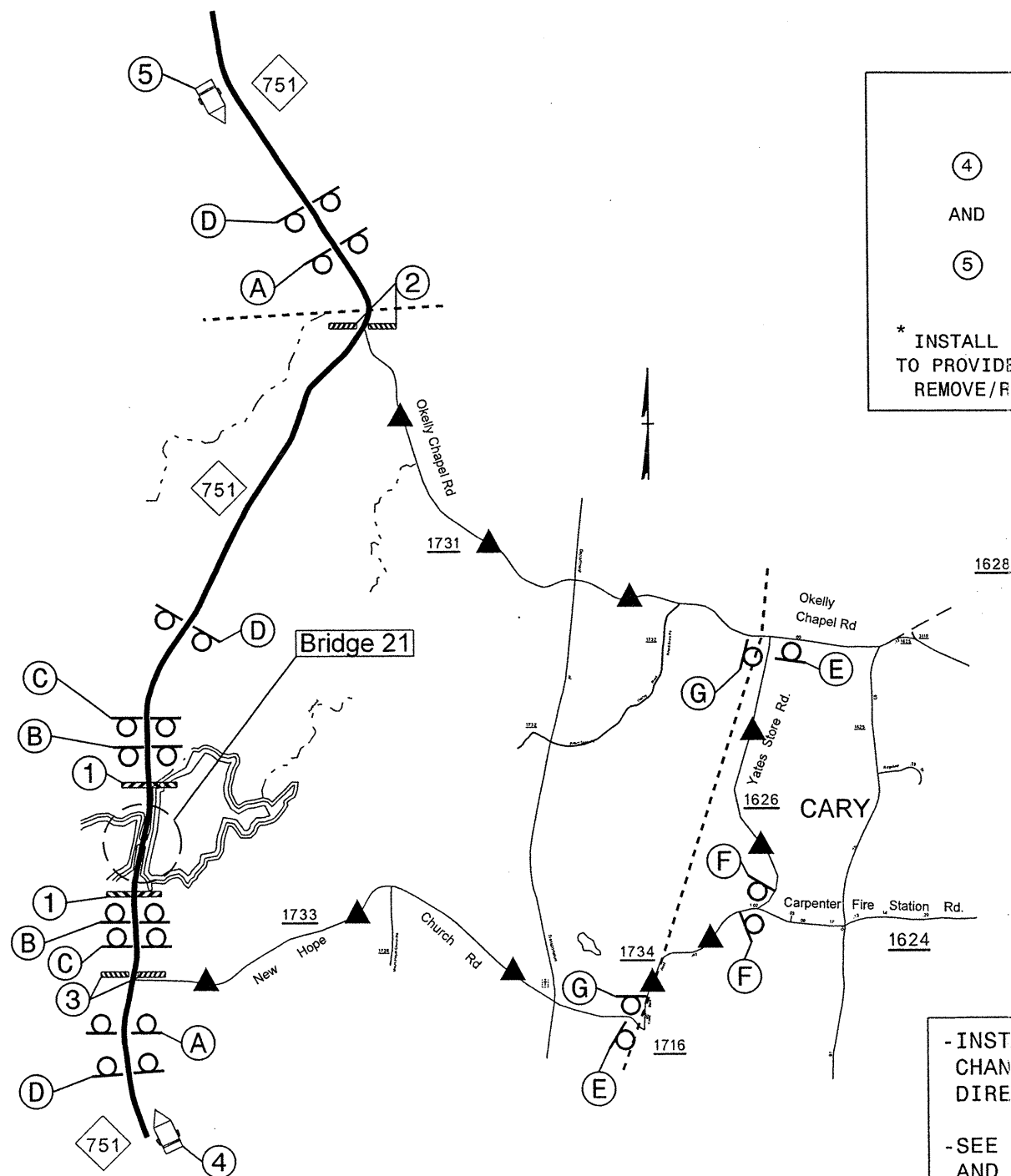
RESURFACING
OFFSITE DETOUR
BRIDGE NO. 13

06-JUN-2012 15:57 \\DOT\FSR\DOT\GROUPS-WZTCC\TMUN\WZTC\Resurfacing\2012\Centra\2012_Div08\20312A-B.CR.10191.18x2.Chotham.US15-501.m4.sh\Detour-Br13d013.dgn AT TE248373 shasson



ADVANCE NOTICE

| | SUGGESTED MESSAGES | SUGGESTED MESSAGES | SUGGESTED MESSAGES | | | | | | | | | | | | |
|-----------------------|--|--|--|-----------------------|--------------|---|---------------|---------------|---------------|-------------------------|--|---------------|---------------|---------------|----------------------|
| ④ | <table border="1"> <tr> <th>MESSAGE NO. 1</th> <th>MESSAGE NO. 2</th> </tr> <tr> <td>NC 751 WILL BE CLOSED</td> <td>8 PM TO 6 AM</td> </tr> </table> CHANGEABLE MESSAGE SIGN | MESSAGE NO. 1 | MESSAGE NO. 2 | NC 751 WILL BE CLOSED | 8 PM TO 6 AM | <table border="1"> <tr> <th>MESSAGE NO. 1</th> <th>MESSAGE NO. 2</th> </tr> <tr> <td>NC 751 CLOSED</td> <td>DETOUR NEW HOPE CH. RD.</td> </tr> </table> CHANGEABLE MESSAGE SIGN | MESSAGE NO. 1 | MESSAGE NO. 2 | NC 751 CLOSED | DETOUR NEW HOPE CH. RD. | <table border="1"> <tr> <th>MESSAGE NO. 1</th> <th>MESSAGE NO. 2</th> </tr> <tr> <td>NC 751 CLOSED</td> <td>DETOUR OKELLY CHAPEL</td> </tr> </table> CHANGEABLE MESSAGE SIGN | MESSAGE NO. 1 | MESSAGE NO. 2 | NC 751 CLOSED | DETOUR OKELLY CHAPEL |
| MESSAGE NO. 1 | MESSAGE NO. 2 | | | | | | | | | | | | | | |
| NC 751 WILL BE CLOSED | 8 PM TO 6 AM | | | | | | | | | | | | | | |
| MESSAGE NO. 1 | MESSAGE NO. 2 | | | | | | | | | | | | | | |
| NC 751 CLOSED | DETOUR NEW HOPE CH. RD. | | | | | | | | | | | | | | |
| MESSAGE NO. 1 | MESSAGE NO. 2 | | | | | | | | | | | | | | |
| NC 751 CLOSED | DETOUR OKELLY CHAPEL | | | | | | | | | | | | | | |
| AND | | | | | | | | | | | | | | | |
| ⑤ | | | | | | | | | | | | | | | |
| | * INSTALL CMS 7 DAYS PRIOR TO CLOSURES TO PROVIDE ADVANCE NOTICE TO MOTORISTS. REMOVE/RELOCATE DURING ROAD CLOSURE. | | | | | | | | | | | | | | |
| | | MESSAGE TO BE USED DURING ROAD CLOSURE | MESSAGE TO BE USED DURING ROAD CLOSURE | | | | | | | | | | | | |



-INSTALL ALL DETOUR SIGNS AND CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.
 -SEE INTERMEDIATE CONTRACT TIMES AND LIQUIDATED DAMAGES.

| | | |
|-----------------------------|--|---|
| APPROVED: _____ DATE: _____ | | RESURFACING OFFSITE DETOUR BRIDGE NO. 21 |
| SEAL | | |

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