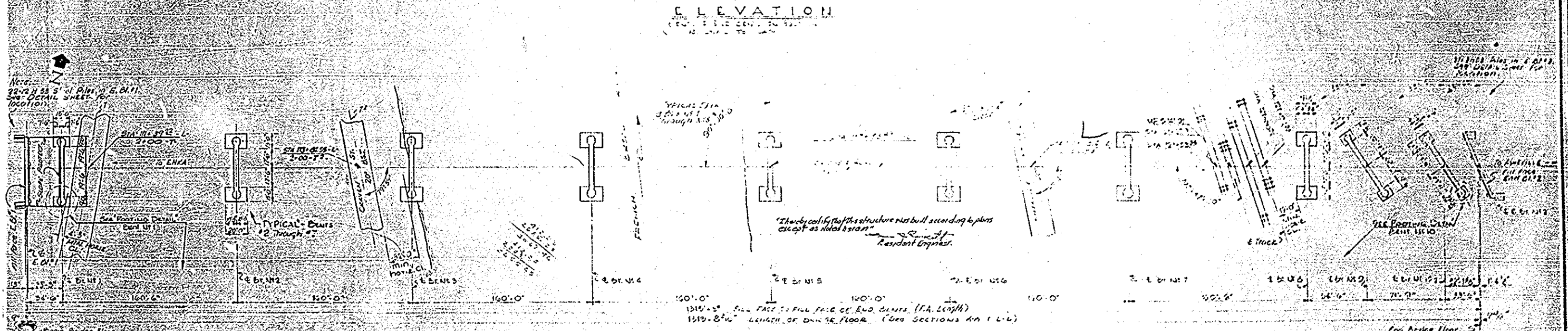
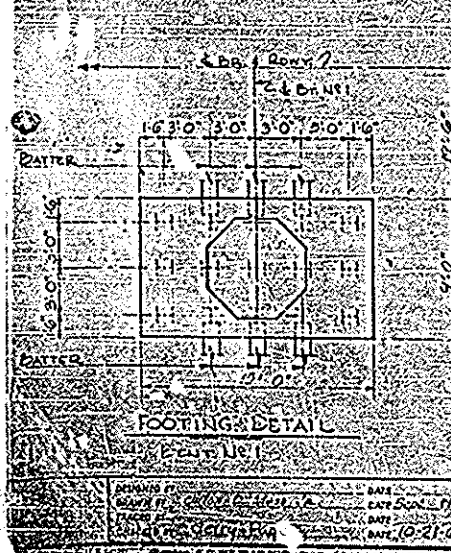


**ELEVATION**

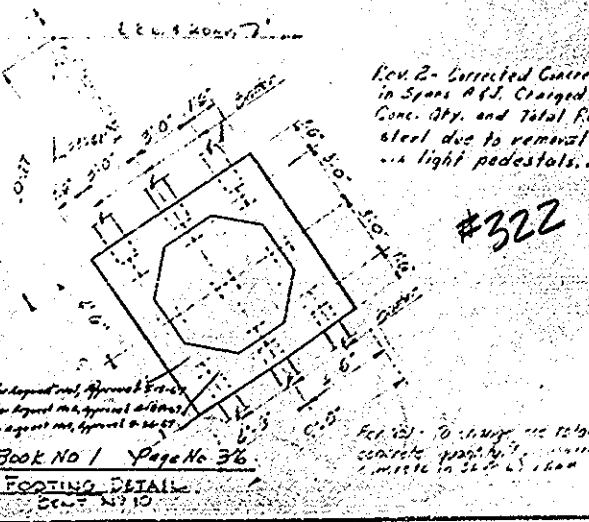


**PLAN**



\* For additional bid items & quantities covering junction boxes, catch basins, curbs & flow poles, see next sheet. See also Special Provisions.

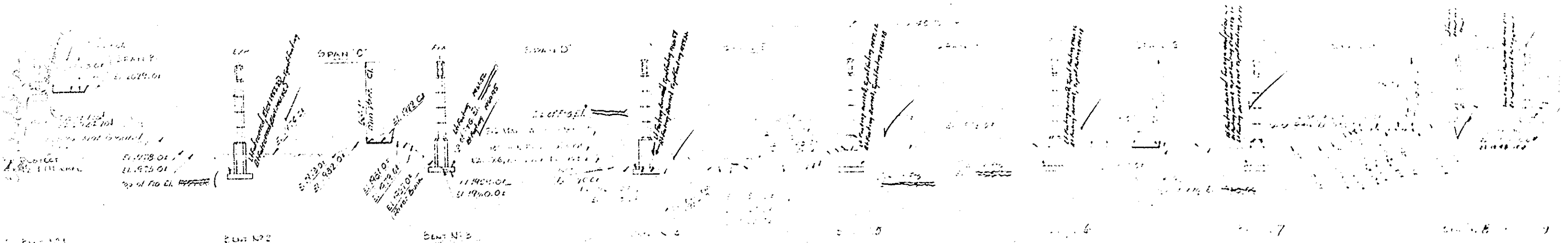
ITEM	CLASS. A		CONCRETE	EXCAVATION	2" DIA. METAL RAIL	4" CONC. CURB PART. 6" x 6" DIA.	12" x 12" CONCRETE	12" x 12" STEEL PILES	4" x 4" STEEL PILES	METHOD 'A' MATERIAL PRODUCE	LADDER & CLIMBING ASSISTANCE	REVISIONS
	STEEL	STEEL										
<b>SUPERSTRUCTURE</b>												
SPANS A, C, E, G, H	342.7	79,826.1	2,611,200		42.23							
SPANS B, D, F, G, H	341.4	437,622.4	2,541,200		2243.00	100.00		445.16				
END BENT 101	51.4	5,109.1						22	390			
BENT 102	209.7	22,275.1						30	751.06	19	Revised	
BENT 103		8,025.3										
BENT 104		8,313.6										
BENT 105		8,129.9										
BENT 106		8,129.9										
BENT 107		8,129.9										
BENT 108		8,129.9										
BENT 109		8,129.9										
BENT 110		8,129.9										
<b>TOTAL</b>	618.2	108,758.5	2,735,100		464.23	100.00		116.16	390	19		



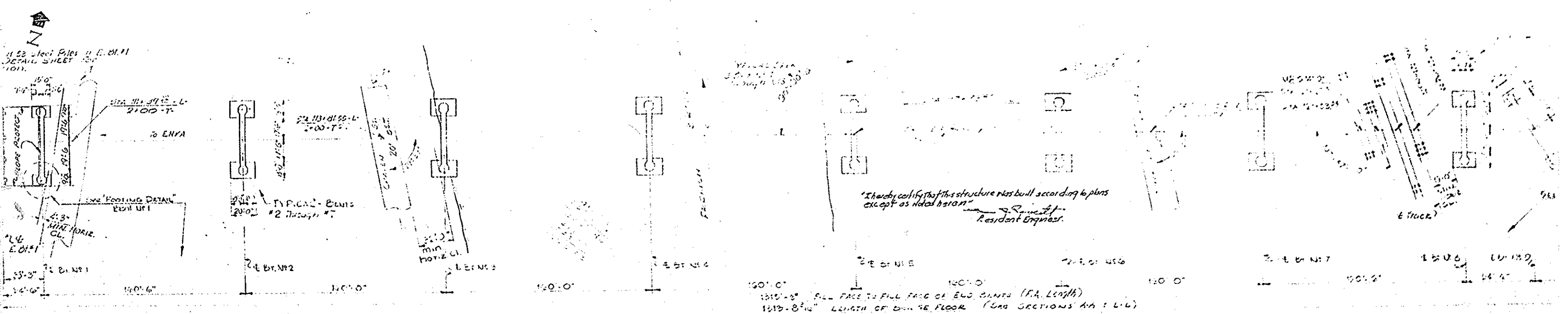
Rev. 2 - Corrected Concrete Qty in Spans A & C. Changed Total Conc. Qty. and Total Rein. steel due to removal of light pedestals. **PROJECT NO. 8,5023209**  
**BUNCOMBE COUNTY**  
**STATION: 117+53.41**

STATE OF NORTH CAROLINA  
**STATE HIGHWAY COMMISSION**  
 GENERAL DRAWING  
 For  
**BRIDGE OVER FRENCH BROAD RIVER, ON US 19 & 23 BETWEEN ASHEVILLE, WEST ASHEVILLE & ENKA.**  
 Sheet 1066 3 of 9

NO.	BY	DATE	NO.	BY	DATE
1	YH	1-20-68	2		
2	GAH	7-20-68	3		



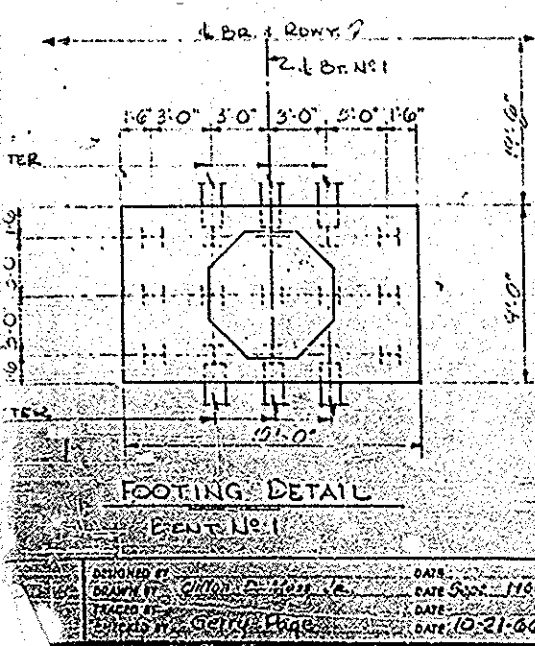
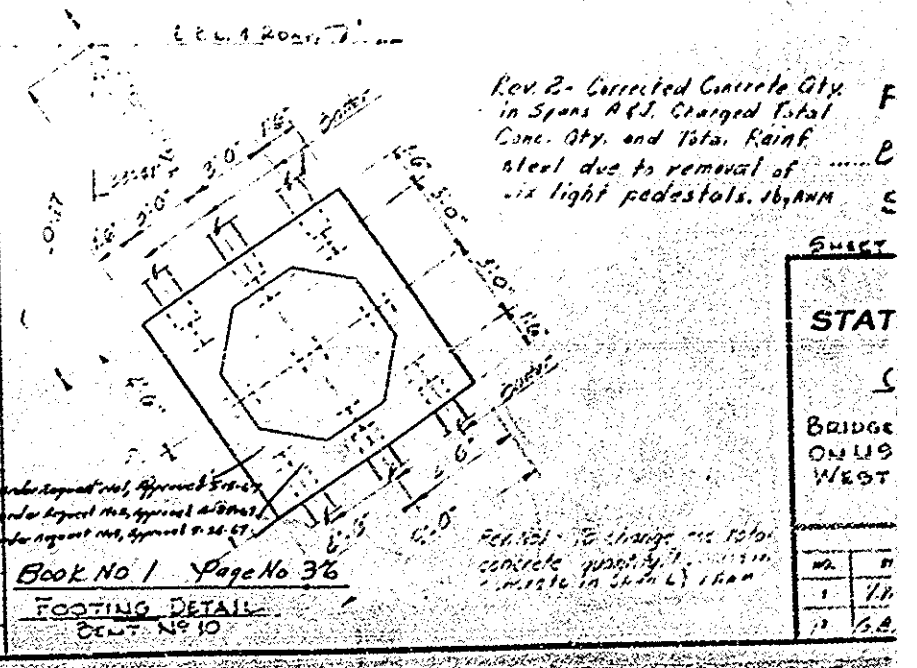
ELEVATION



PLAN

\* For additional bid items & quantities covering junction boxes, catch basins, pipes & flap gates, see next sheet. See also Special Provisions.

CLASS	* LENO.	STEEL	STRUCT. S-TEEL	EXCAVATION		2-DAB METAL RAIL	4 SQU. FLUTE POST. SPAN AT OWN.	CONCRETE SYSTEM	12453 STEEL PILES	4H78 STEEL PILE	METHOD "A" WATER PROOFING	LADDER	PIVOTAL
				MLT	CLY								
SUPERSTRUCTURE						42.23							
SPANS A, J, K, & L	342.7	79,246	241,200										
SPANS B, C, D, E, F, G, H	1841.4	437,600	2,541,900			2243.00	300.10		22	64547			
END BENT No. 1	51.4	5,109											
BENT No. 1	209.7	22,275											
BENT No. 2		64,603											
BENT No. 3		63,362											
BENT No. 4		61,293											
BENT No. 5		63,276											
BENT No. 6		61,293											
BENT No. 7		61,293											
BENT No. 8		61,293											
BENT No. 9		61,293											
BENT No. 10		61,293											
TOTALS	6118.7	1,027,598	2,735,106			2,655.23	300.10		21	1,027,598			

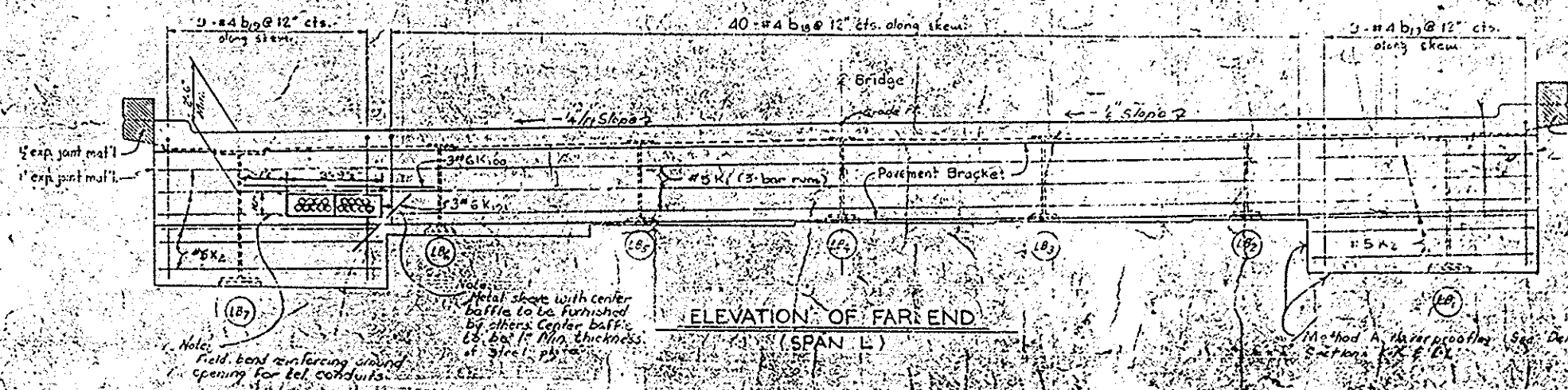
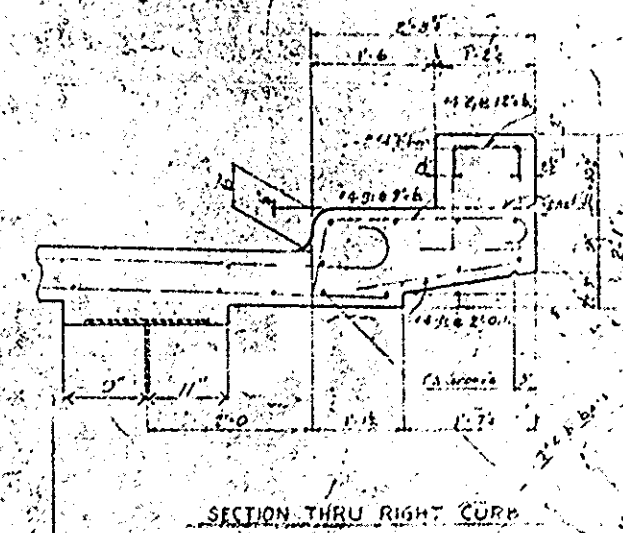
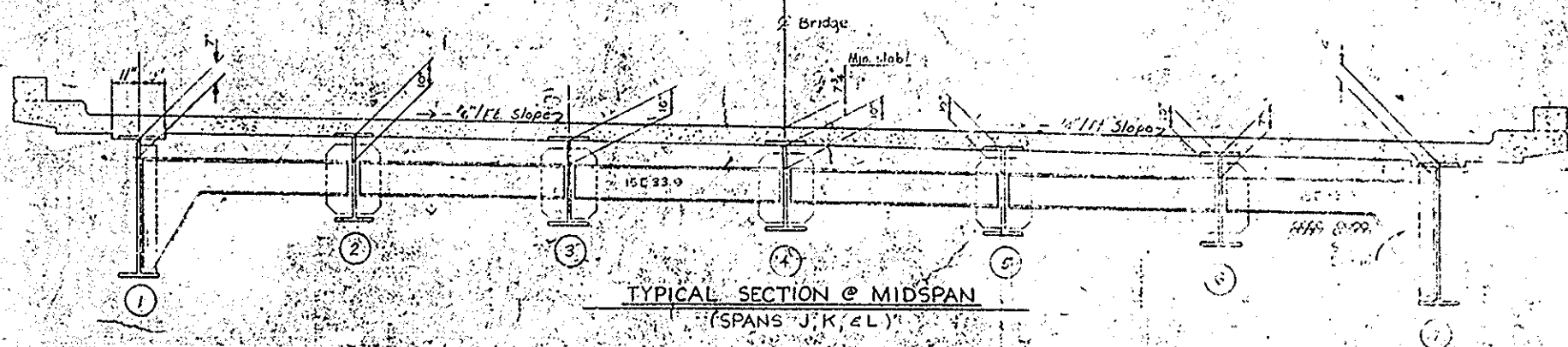
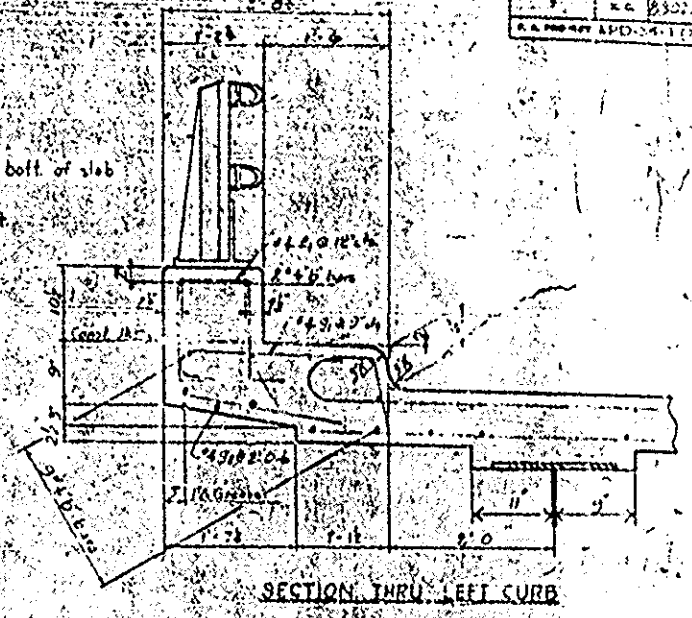
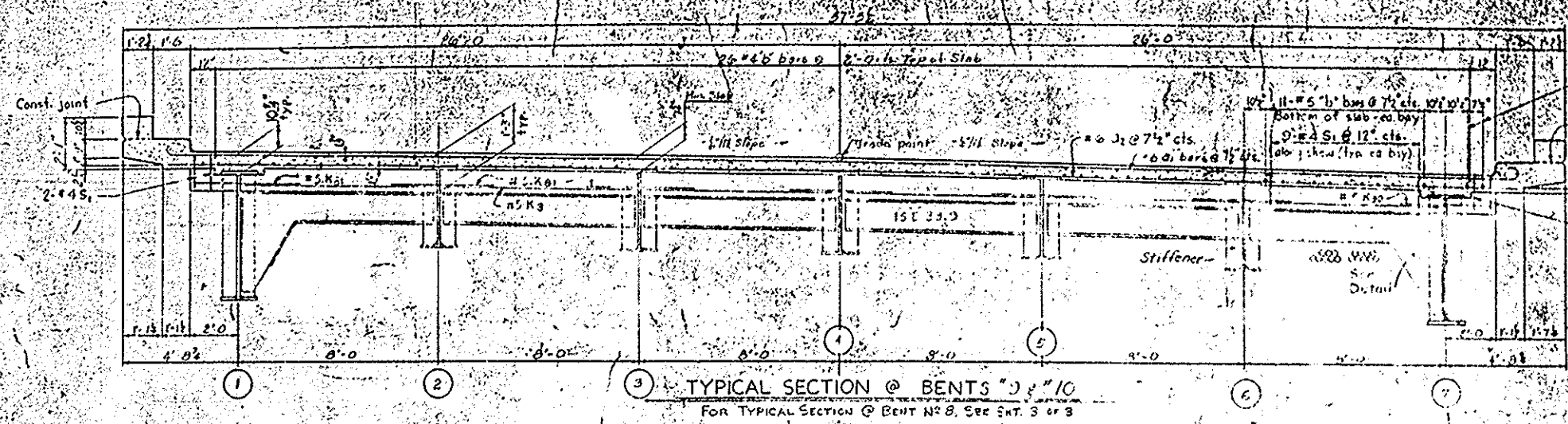


DESIGNED BY: GUYTON, D. J. 11/06  
 DRAWN BY: GUYTON, D. J. 11/06  
 CHECKED BY: GUYTON, D. J. 11/06  
 DATE: 10-21-00

STAT  
 BRIDGE ON US WEST

BOOK No 1 Page No 36  
 FOOTING DETAIL  
 BENT No 10

FILE NO.	DATE	PROJECT NO.
8.302320	8/1/59	8.302320
S.A. PROJECT APP. 3-1-57		



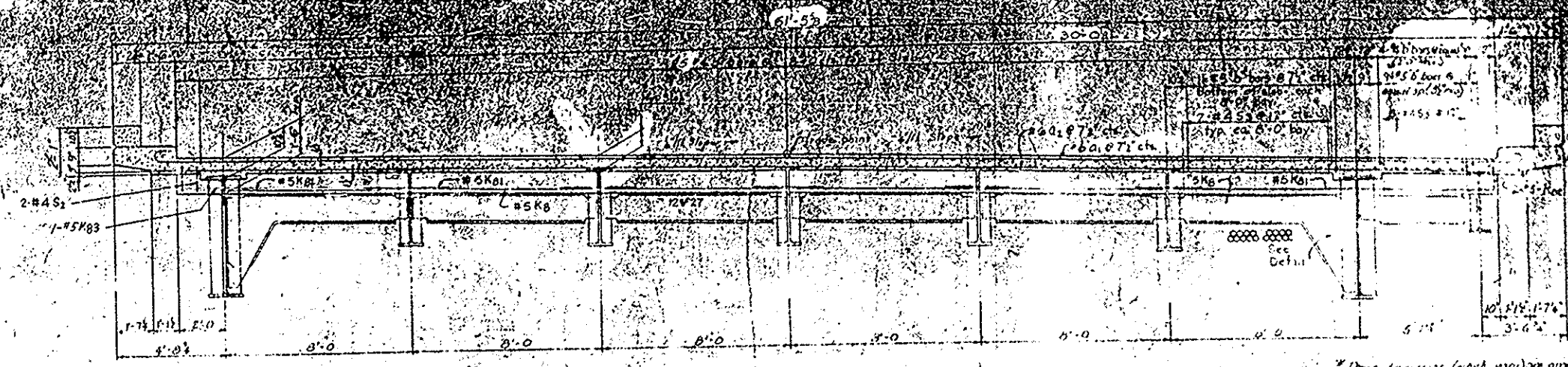
PROJECT NO. 8.302320  
 BUNCOMBE COUNTY  
 STATION 117+53.49

STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISSION  
 Raleigh

TYPICAL SECT. SPANS J, K, L

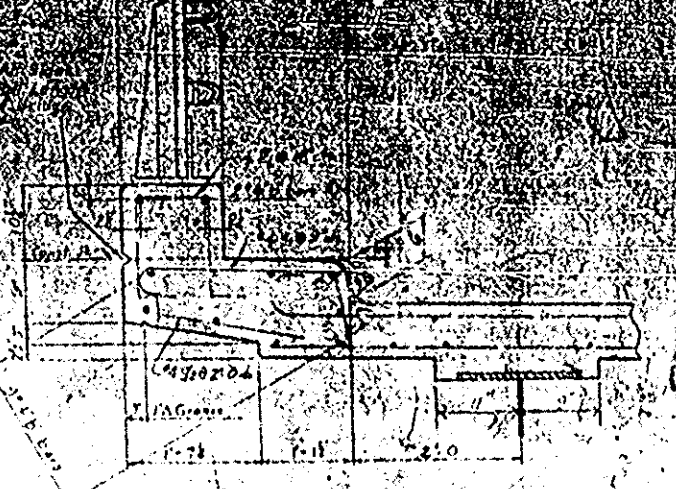
NO.	BY	DATE	NO.	BY	DATE
1	H. C. Stanley, Jr.	8/1/59	2		
2			3		
3			4		
4			5		

DRAWN BY H. C. Stanley, Jr. DATE 8/1/59  
 CHECKED BY [Signature] DATE 8/1/59

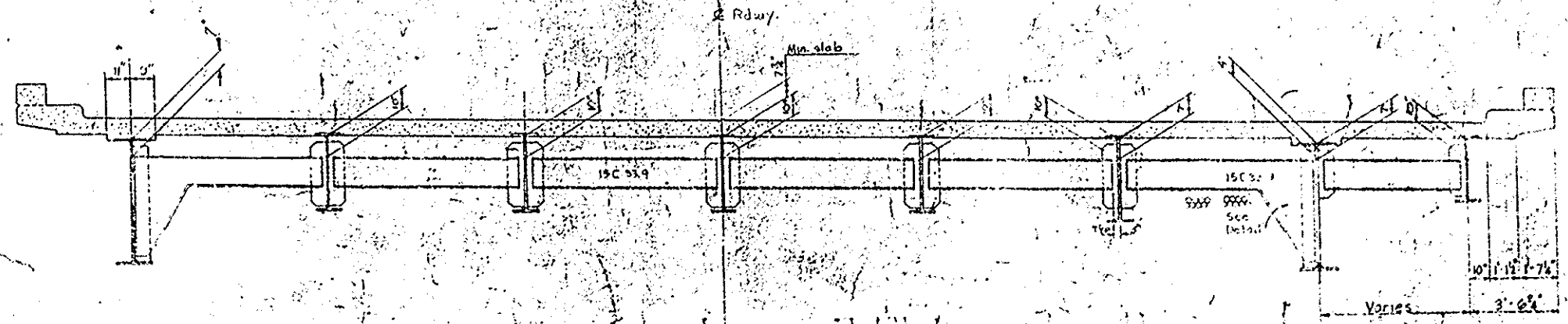


TYPICAL SECTION OF BENT NO. 1

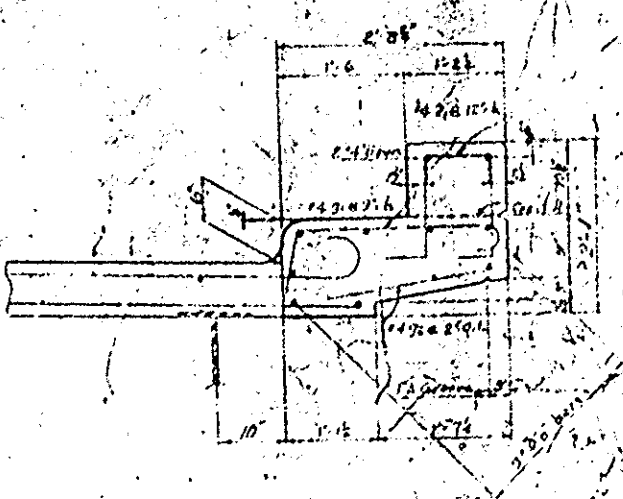
\*These dimensions (which vary) are given @ Span A. Area of 8" open joint, 33'2" from full face of End Curbside Wall.



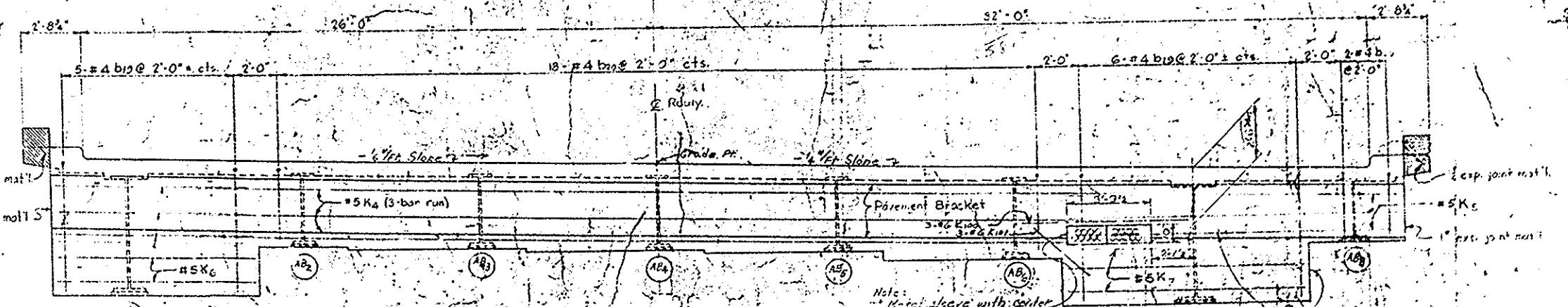
SECTION THRU LEFT CURB



TYPICAL SECTION @ MIDSPAN - SPAN A



SECTION THRU RIGHT CURB



ELEVATION OF NEAR END

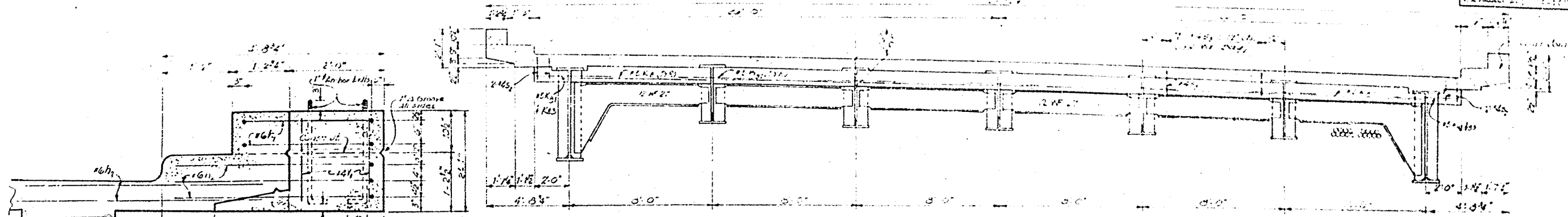
Note: Metal slabs with center bolts to be furnished by others. Center bolts have 1" Min. thickness (steel P).

Note: 2' old land remaining around opening for ice conduits.

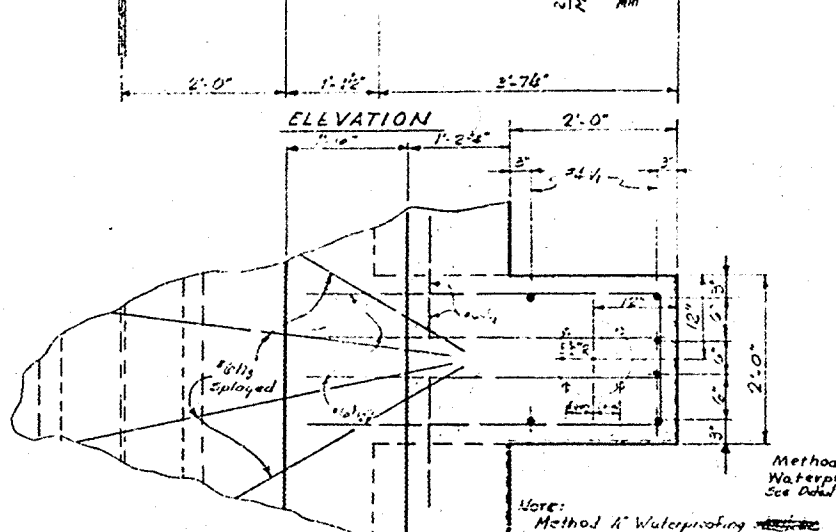
Note: Method of Water finding See Detail Section 2 of E&L

PROJECT No. 83023209  
 BUNCOMBE COUNTY  
 STATION 117+53.49  
 Sheet No. 1 of 3

STATE OF NORTH CAROLINA  
**STATE HIGHWAY COMMISSION**  
 Raleigh, N.C.  
 TYPICAL SECTION - SPAN A



TYPICAL SECTION E BENT NO'S  
SPAN J

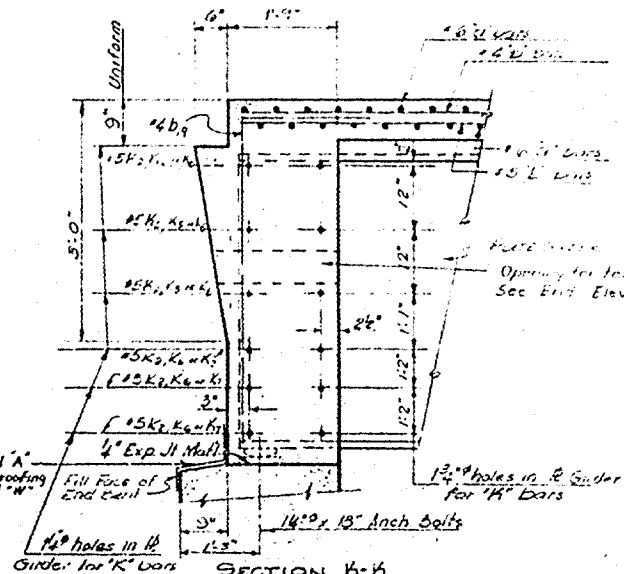


ELEVATION

PART PLAN

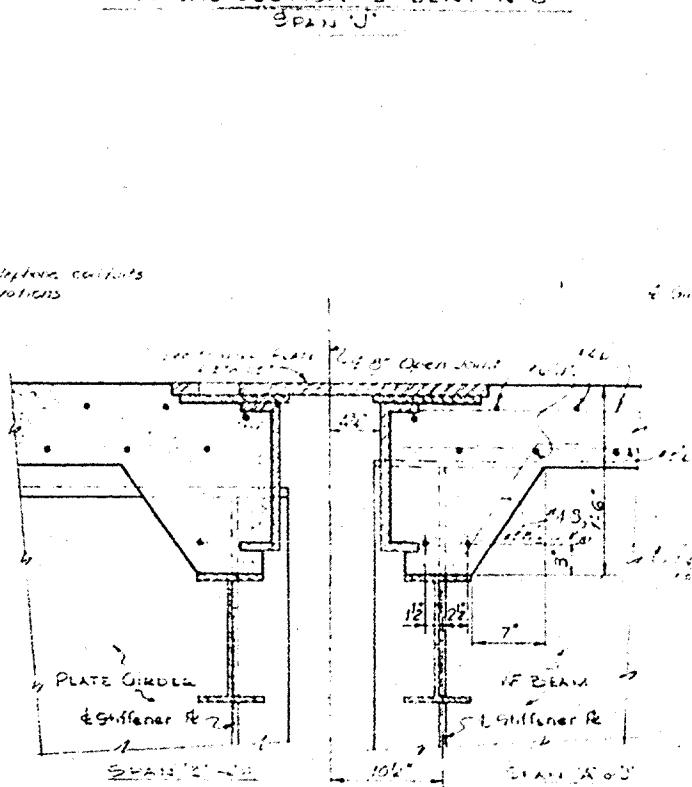
LIGHT PEDESTAL DETAILS

Note:  
Method A Waterproofing  
placed as shown below for hor-  
izontal joints and sym about  
all vertical joints between  
substructure & superstr.

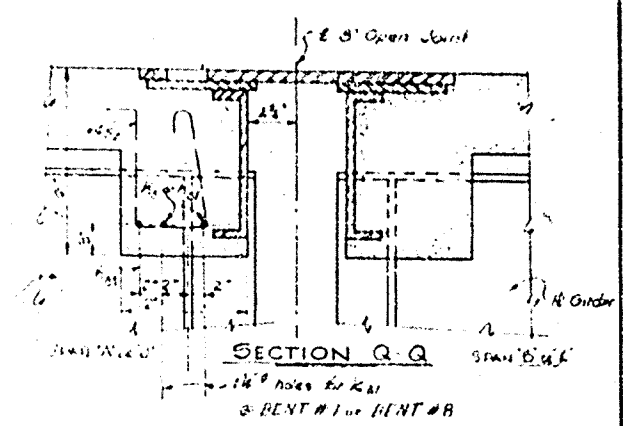


SECTION K-K

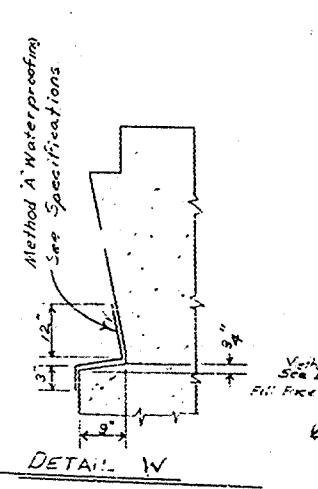
Seal with 1/2" APS Asphalt Cement  
Exp. Jt. Matl. Above  
Exp. Jt. Matl. Below



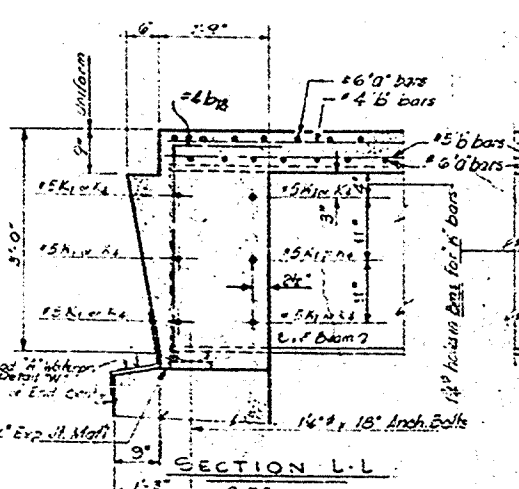
SECTION N-N



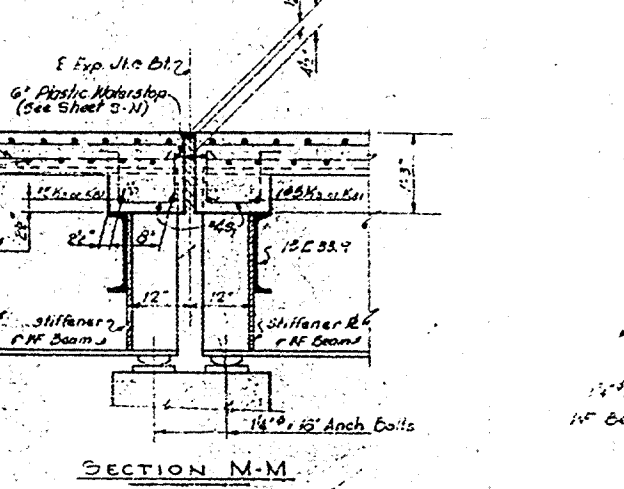
SECTION Q-Q



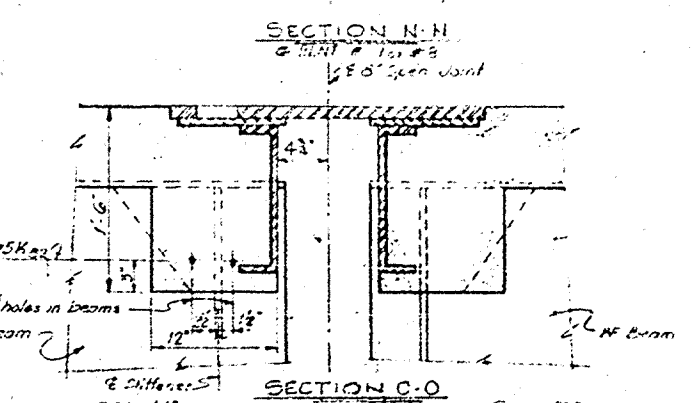
DETAIL W



SECTION L-L



SECTION M-M



SECTION C-C

PROJECT NO. D-3023209  
DUNCOMBE COUNTY  
STATION 117-53-62

SHEET 3 OF 3

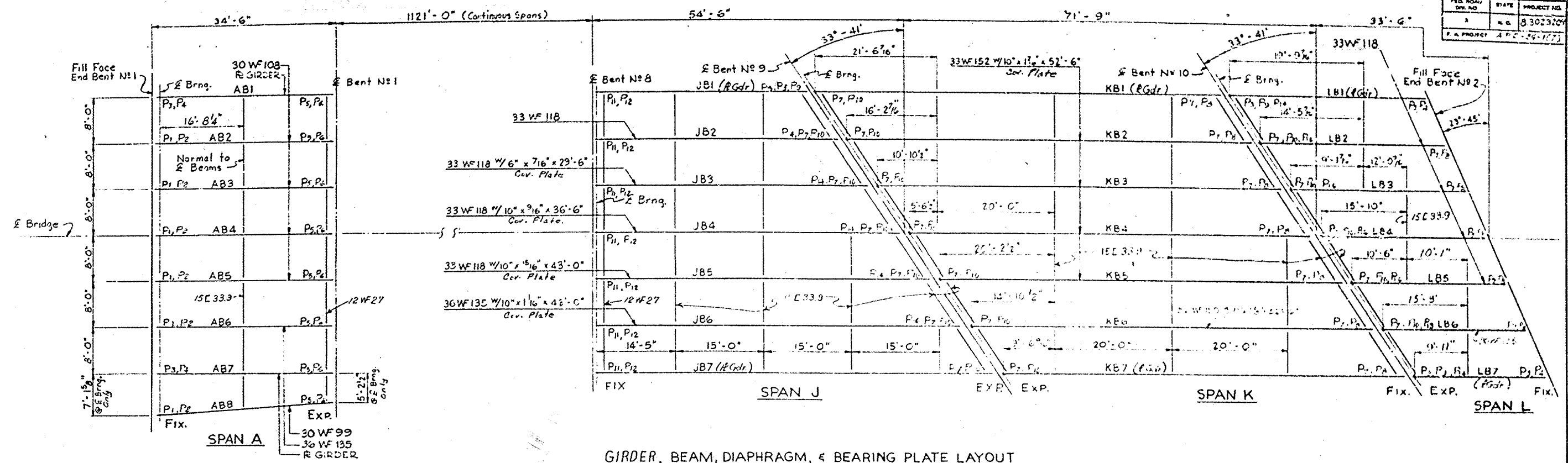
STATE OF NORTH CAROLINA  
STATE HIGHWAY COMMISSION  
RALEIGH

TYPICAL SECTIONS  
SPANS 'K' 'J' 'A' 'C'

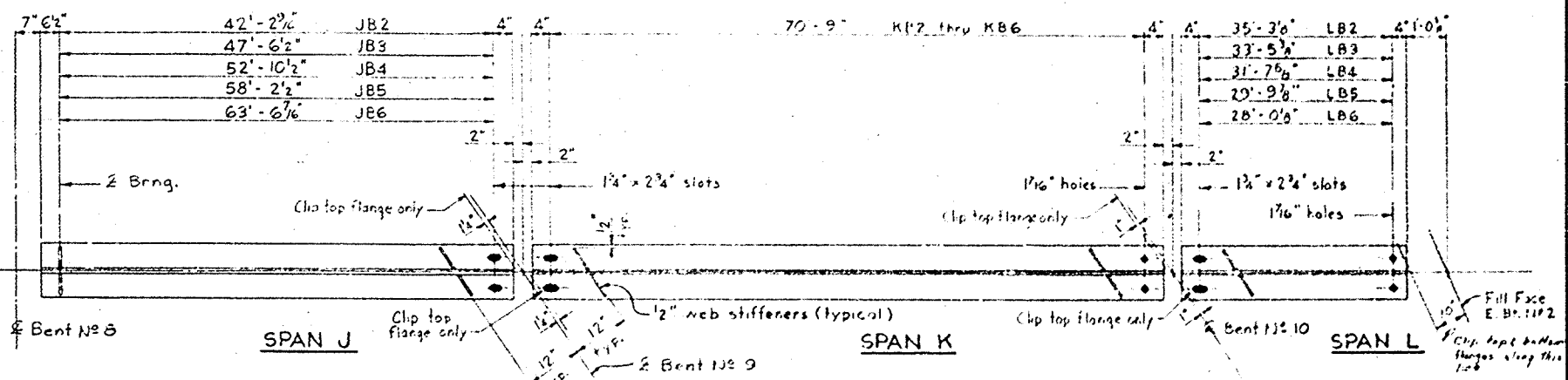
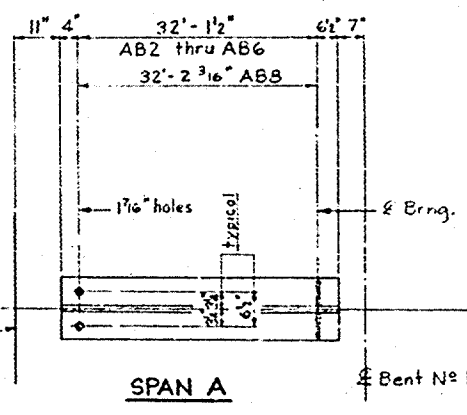
REVISIONS					
NO.	BY	DATE	NO.	BY	DATE
1					
2					
3					

DATE 9/1/91  
SCALE 5/8  
TOTAL SHEETS 66

DRAWN BY: J. D. WOOD  
CHECKED BY: G. W. PAGE  
DATE: 1-17-66

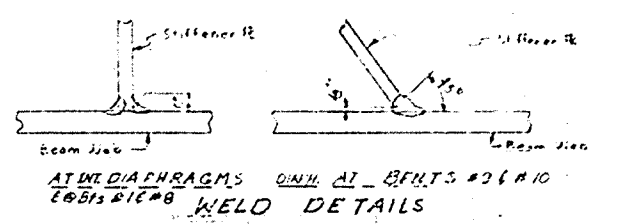


**GIRDER, BEAM, DIAPHRAGM, & BEARING PLATE LAYOUT**



**BOTTOM FLANGE DETAIL**  
(FOR ROLLED BEAMS ONLY)

PROJECT No. 8.3023207  
 BUNCOMBE COUNTY  
 STATION: 117+53.49



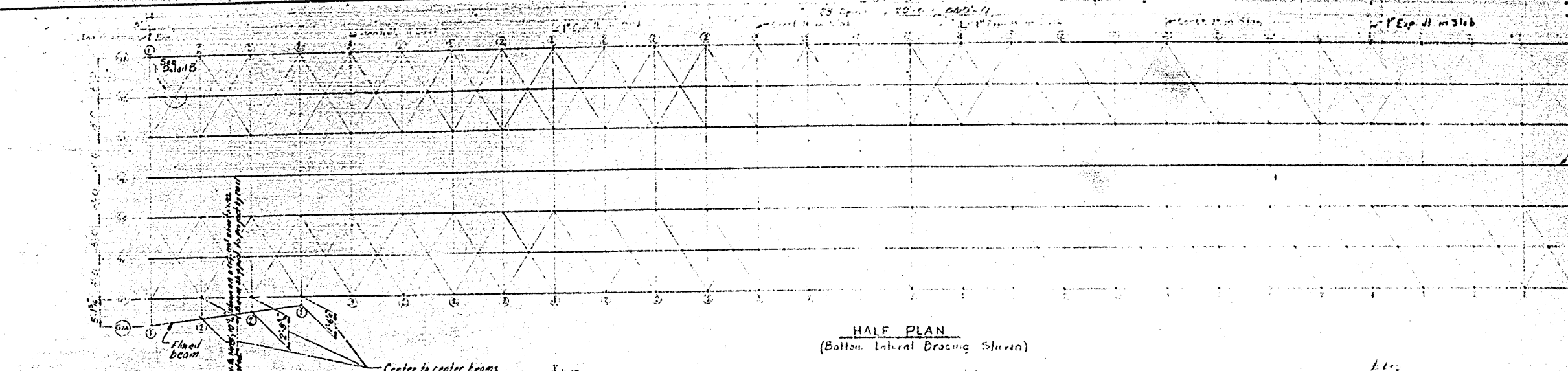
STATE OF NORTH CAROLINA  
**STATE HIGHWAY COMMISSION**  
 RALEIGH

STRUCTURAL STEEL  
 SPANS A, J, K, L  
 JUL 17 1966 10 OF 11

NO.	BY	DATE	REV.	DATE
1				

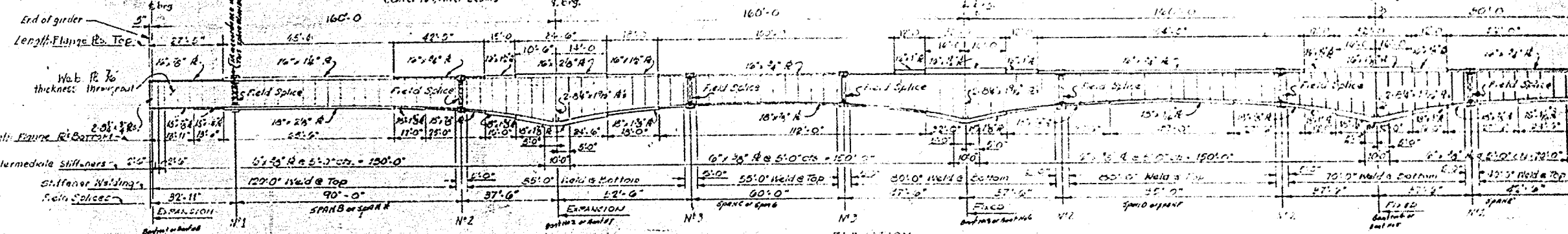
PROJECT NO. 8.3023207  
 SHEET NO. 5-3  
 TOTAL SHEETS 6-8



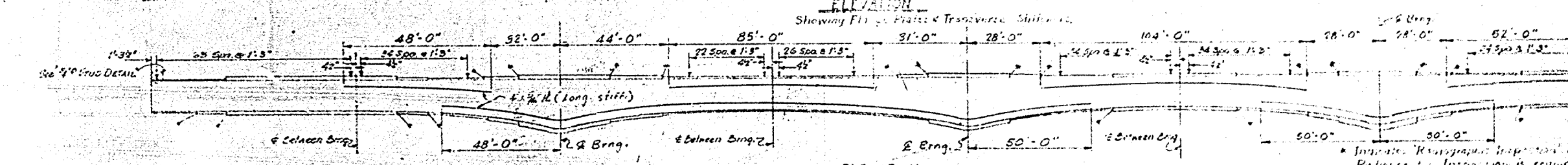


Const. in Slab  
 2 Symm. about  
 except for  
 flange beam  
 on S.W. corner  
 of bridge

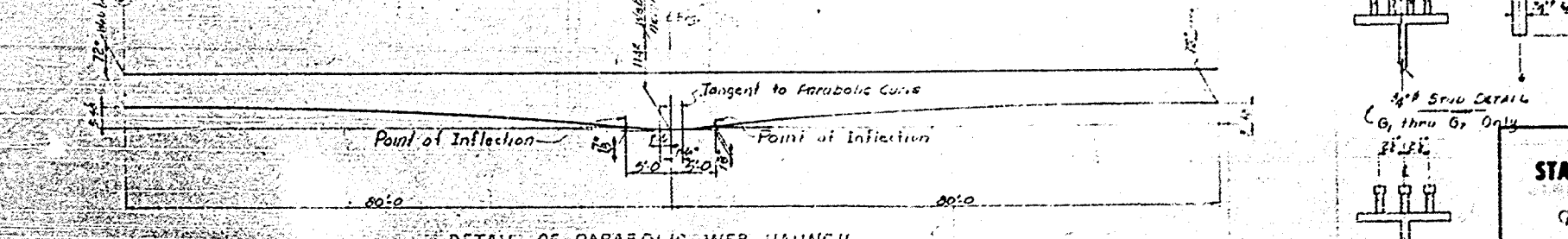
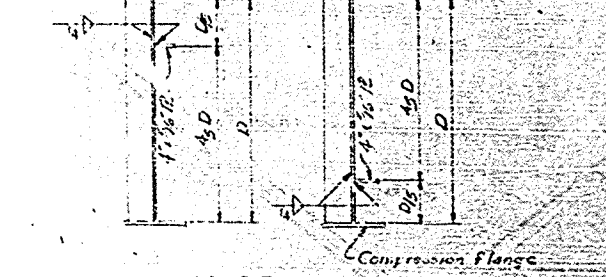
**NOTES**  
 Structural Steel Stress in ex-  
 treme fiber of structural  
 steel shall be 20,000 psi.  
 All structural steel shall be  
 A36 grade with the  
 exception of expansion plate  
 assemblies & bearing stiffeners.  
 Ends of girders, web splices  
 and all vert. stiffeners shall  
 be normal to the top flanges.  
 Field splices shall be  
 pinned & bolted before  
 tightening bolts; see specifications.



Girder makeup  
 Symm. about L.  
 Int. stiffeners shall be placed on  
 only one side of each girder  
 web for inside of G1-G3 &  
 G4-G6 on either side of  
 web except at cross frames  
 where they are required  
 on both sides of all interior  
 girders for connection pur-  
 poses.  
 All bearing stiffeners shall  
 be in pairs, one on each  
 side of the web of all girders.



**ELEVATION**  
 (Showing Longitudinal Stiffeners & Stud Spacing)  
 Note: For flange to web welds, see "R. Girder Det."  
 Note: Longitudinal stiffeners are on exterior face of girders only.

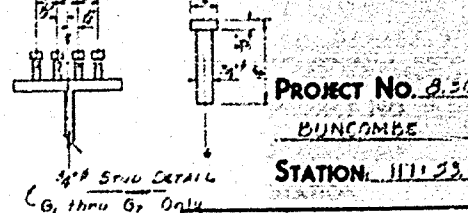


**CONCRETE POUR SCHEDULE**

SPAN 'B'	SPAN 'C'	SPAN 'D'	SPAN 'E'	SPAN 'F'	SPAN 'G'	SPAN 'H'	SPAN 'I'
(14)	(13)	(12)	(11)	(10)	(9)	(8)	(7)

NOTE: Bridge deck sections shall be poured in the order shown.

Indicate Reinforcement Inspection of flange full welds.  
 Reinforcement Inspection is required at all built  
 welds in tension flanges. See Special Provisions.



PROJECT No. 8.5023209  
 BUNCOMBE COUNTY  
 STATION 1111.33.37

STATE OF NORTH CAROLINA  
**STATE HIGHWAY COMMISSION**  
 RALEIGH  
**GIRDER LAYOUT & DETAILS**

JUL 14 1968 21 of 99

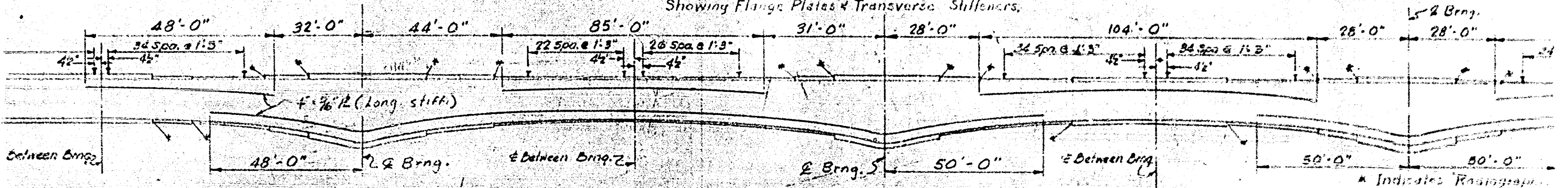
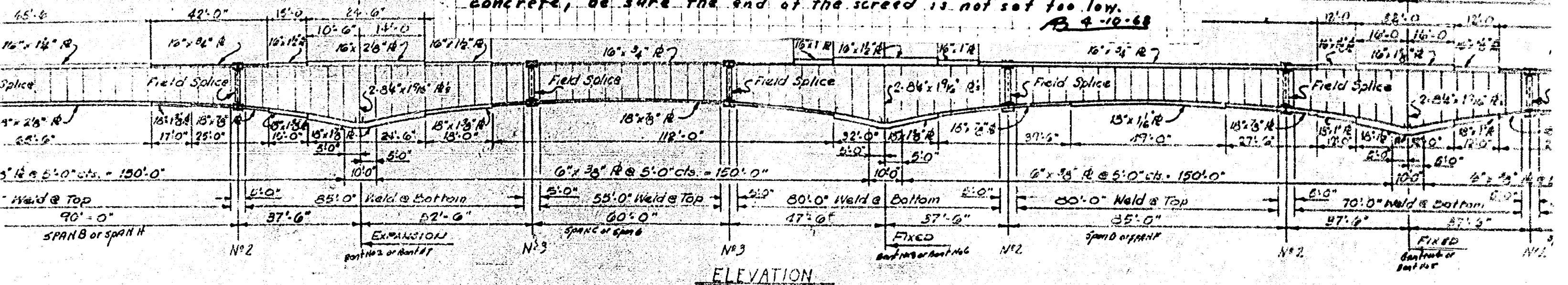
NO.	BY	DATE	NO.	BY	DATE
1			2		
3			4		

SHEET NO. 66



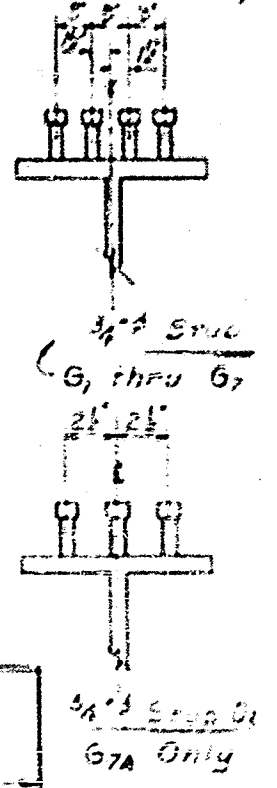
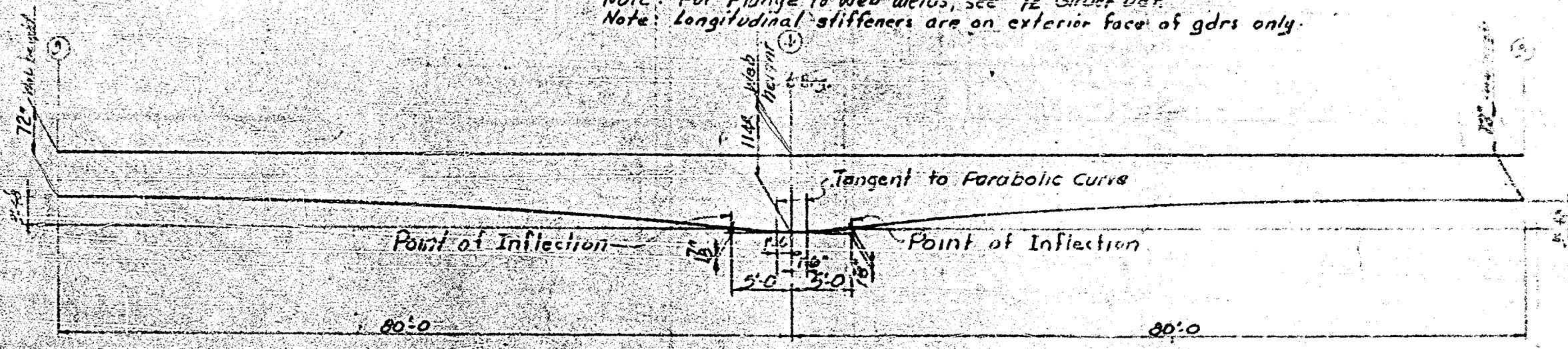
160'-0"

and make it smooth. Where end of screed rides on cured concrete, be sure the end of the screed is not set too low. B 4-10-68



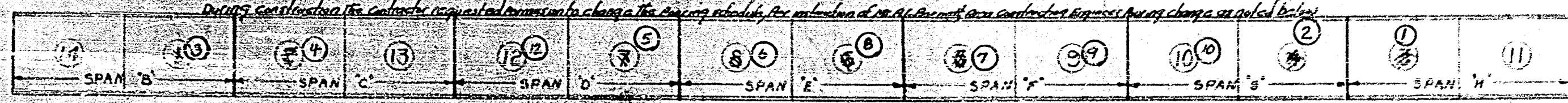
Note: For Flange to Web welds, see "R Girder Det."  
 Note: Longitudinal stiffeners are on exterior face of gdrs only.

\* Indicates Radiographic Radiographic Inspection welds in tension flang



ion Flange  
ENER

Span mark  
sections  
poured in the  
order shown.



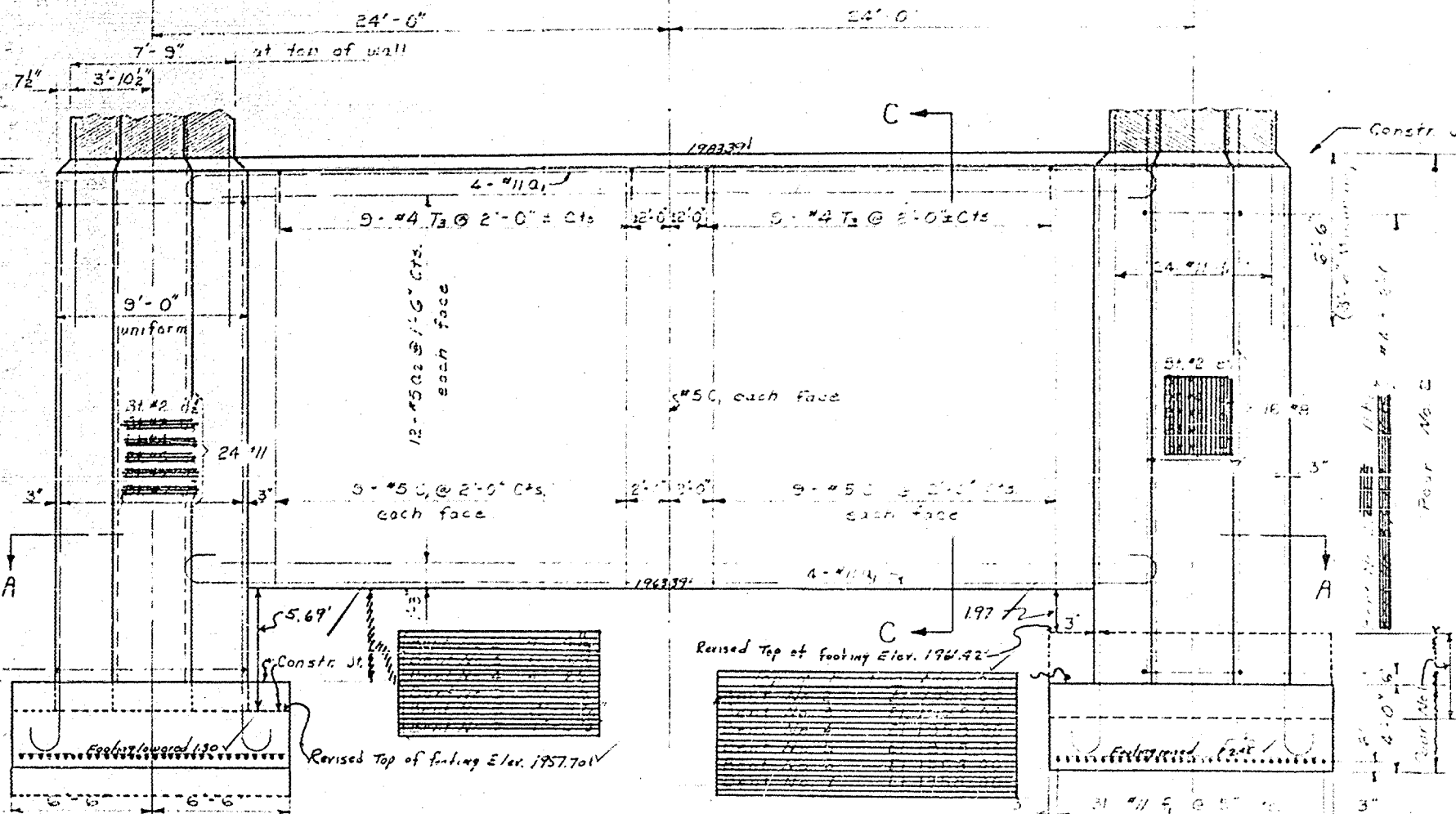
**CONCRETE POUR SCHEDULE**



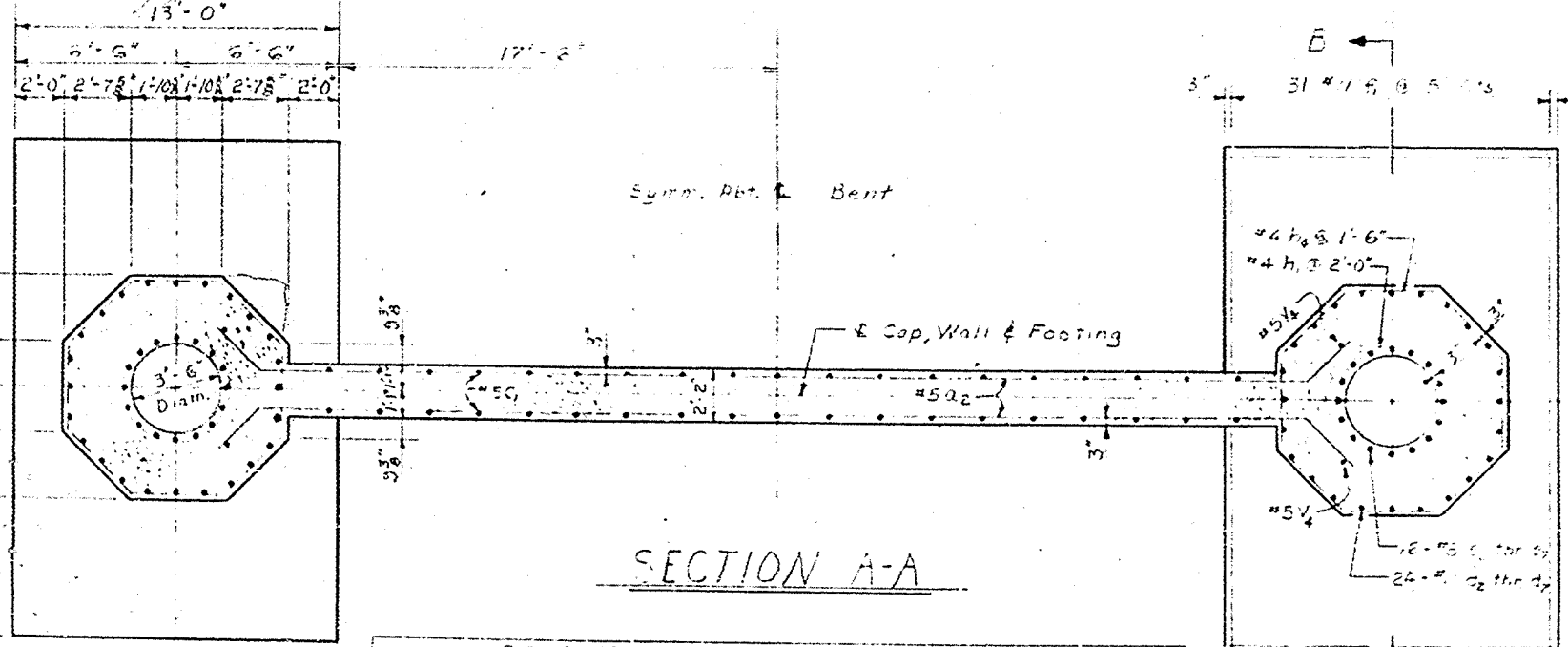




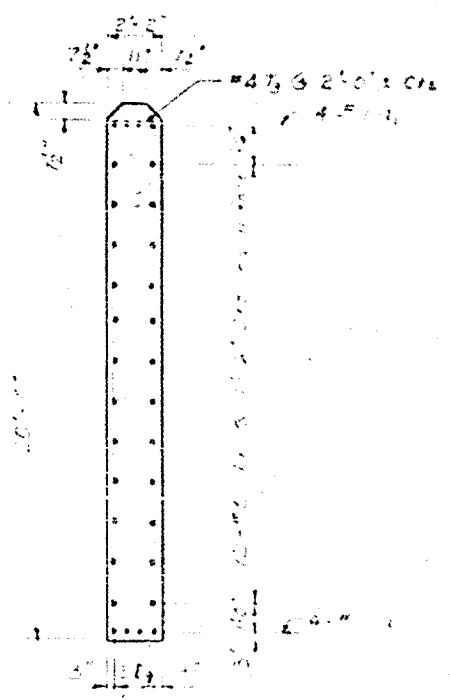




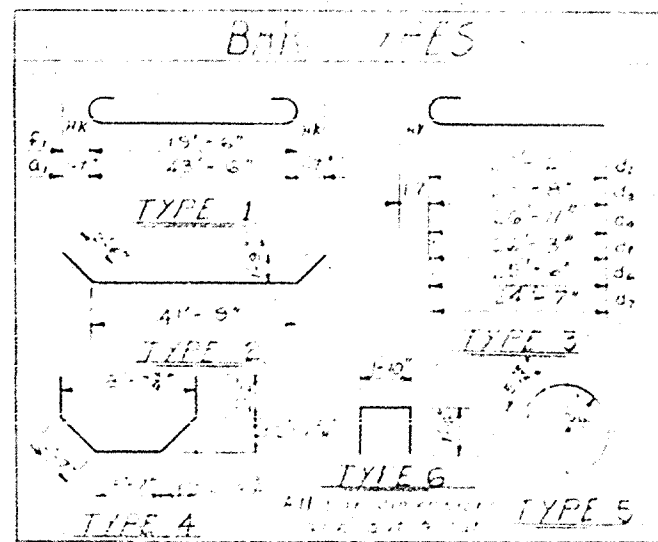
PART ELEVATION



SECTION A-A



SECTION C-C



SECTION B-B

BILL OF MATERIAL BENT PEDESTAL						BILL OF MATERIAL BENT PEDESTAL					
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
BENT NO 2						BENT NO 5					
1	10	#3	3	8'-0"	31	1	10	#3	3	8'-0"	31
2	11	#4	4	10'-0"	112	2	11	#4	4	10'-0"	112
3	12	#5	5	10'-0"	120	3	12	#5	5	10'-0"	120
4	13	#5	5	10'-0"	120	4	13	#5	5	10'-0"	120
5	14	#5	5	10'-0"	120	5	14	#5	5	10'-0"	120
6	15	#5	5	10'-0"	120	6	15	#5	5	10'-0"	120
7	16	#5	5	10'-0"	120	7	16	#5	5	10'-0"	120
8	17	#5	5	10'-0"	120	8	17	#5	5	10'-0"	120
9	18	#5	5	10'-0"	120	9	18	#5	5	10'-0"	120
10	19	#5	5	10'-0"	120	10	19	#5	5	10'-0"	120
11	20	#5	5	10'-0"	120	11	20	#5	5	10'-0"	120
12	21	#5	5	10'-0"	120	12	21	#5	5	10'-0"	120
13	22	#5	5	10'-0"	120	13	22	#5	5	10'-0"	120
14	23	#5	5	10'-0"	120	14	23	#5	5	10'-0"	120
15	24	#5	5	10'-0"	120	15	24	#5	5	10'-0"	120
16	25	#5	5	10'-0"	120	16	25	#5	5	10'-0"	120
17	26	#5	5	10'-0"	120	17	26	#5	5	10'-0"	120
18	27	#5	5	10'-0"	120	18	27	#5	5	10'-0"	120
19	28	#5	5	10'-0"	120	19	28	#5	5	10'-0"	120
20	29	#5	5	10'-0"	120	20	29	#5	5	10'-0"	120
21	30	#5	5	10'-0"	120	21	30	#5	5	10'-0"	120
22	31	#5	5	10'-0"	120	22	31	#5	5	10'-0"	120
23	32	#5	5	10'-0"	120	23	32	#5	5	10'-0"	120
24	33	#5	5	10'-0"	120	24	33	#5	5	10'-0"	120
25	34	#5	5	10'-0"	120	25	34	#5	5	10'-0"	120
26	35	#5	5	10'-0"	120	26	35	#5	5	10'-0"	120
27	36	#5	5	10'-0"	120	27	36	#5	5	10'-0"	120
28	37	#5	5	10'-0"	120	28	37	#5	5	10'-0"	120
29	38	#5	5	10'-0"	120	29	38	#5	5	10'-0"	120
30	39	#5	5	10'-0"	120	30	39	#5	5	10'-0"	120
31	40	#5	5	10'-0"	120	31	40	#5	5	10'-0"	120
32	41	#5	5	10'-0"	120	32	41	#5	5	10'-0"	120
33	42	#5	5	10'-0"	120	33	42	#5	5	10'-0"	120
34	43	#5	5	10'-0"	120	34	43	#5	5	10'-0"	120
35	44	#5	5	10'-0"	120	35	44	#5	5	10'-0"	120
36	45	#5	5	10'-0"	120	36	45	#5	5	10'-0"	120
37	46	#5	5	10'-0"	120	37	46	#5	5	10'-0"	120
38	47	#5	5	10'-0"	120	38	47	#5	5	10'-0"	120
39	48	#5	5	10'-0"	120	39	48	#5	5	10'-0"	120
40	49	#5	5	10'-0"	120	40	49	#5	5	10'-0"	120
41	50	#5	5	10'-0"	120	41	50	#5	5	10'-0"	120
42	51	#5	5	10'-0"	120	42	51	#5	5	10'-0"	120
43	52	#5	5	10'-0"	120	43	52	#5	5	10'-0"	120
44	53	#5	5	10'-0"	120	44	53	#5	5	10'-0"	120
45	54	#5	5	10'-0"	120	45	54	#5	5	10'-0"	120
46	55	#5	5	10'-0"	120	46	55	#5	5	10'-0"	120
47	56	#5	5	10'-0"	120	47	56	#5	5	10'-0"	120
48	57	#5	5	10'-0"	120	48	57	#5	5	10'-0"	120
49	58	#5	5	10'-0"	120	49	58	#5	5	10'-0"	120
50	59	#5	5	10'-0"	120	50	59	#5	5	10'-0"	120
51	60	#5	5	10'-0"	120	51	60	#5	5	10'-0"	120
52	61	#5	5	10'-0"	120	52	61	#5	5	10'-0"	120
53	62	#5	5	10'-0"	120	53	62	#5	5	10'-0"	120
54	63	#5	5	10'-0"	120	54	63	#5	5	10'-0"	120
55	64	#5	5	10'-0"	120	55	64	#5	5	10'-0"	120
56	65	#5	5	10'-0"	120	56	65	#5	5	10'-0"	120
57	66	#5	5	10'-0"	120	57	66	#5	5	10'-0"	120
58	67	#5	5	10'-0"	120	58	67	#5	5	10'-0"	120
59	68	#5	5	10'-0"	120	59	68	#5	5	10'-0"	120
60	69	#5	5	10'-0"	120	60	69	#5	5	10'-0"	120
61	70	#5	5	10'-0"	120	61	70	#5	5	10'-0"	120
62	71	#5	5	10'-0"	120	62	71	#5	5	10'-0"	120
63	72	#5	5	10'-0"	120	63	72	#5	5	10'-0"	120
64	73	#5	5	10'-0"	120	64	73	#5	5	10'-0"	120
65	74	#5	5	10'-0"	120	65	74	#5	5	10'-0"	120
66	75	#5	5	10'-0"	120	66	75	#5	5	10'-0"	120
67	76	#5	5	10'-0"	120	67	76	#5	5	10'-0"	120
68	77	#5	5	10'-0"	120	68	77	#5	5	10'-0"	120
69	78	#5	5	10'-0"	120	69	78	#5	5	10'-0"	120
70	79	#5	5	10'-0"	120	70	79	#5	5	10'-0"	120
71	80	#5	5	10'-0"	120	71	80	#5	5	10'-0"	120
72	81	#5	5	10'-0"	120	72	81	#5	5	10'-0"	120
73	82	#5	5	10'-0"	120	73	82	#5	5	10'-0"	120
74	83	#5	5	10'-0"	120	74	83	#5	5	10'-0"	120
75	84	#5	5	10'-0"	120	75	84	#5	5	10'-0"	120
76	85	#5	5	10'-0"	120	76	85	#5	5	10'-0"	120
77	86	#5	5	10'-0"	120	77	86	#5	5	10'-0"	120
78	87	#5	5	10'-0"	120	78	87	#5	5	10'-0"	120
79	88	#5	5	10'-0"	120	79	88	#5	5	10'-0"	120
80	89	#5	5	10'-0"	120	80	89	#5	5	10'-0"	120
81	90	#5	5	10'-0"	120	81	90	#5	5	10'-0"	120
82	91	#5	5	10'-0"	120	82	91	#5	5	10'-0"	120
83	92	#5	5	10'-0"	120	83	92	#5	5	10'-0"	120
84	93	#5	5	10'-0"	120	84	93	#5	5	10'-0"	120
85	94	#5	5	10'-0"	120	85	94	#5	5	10'-0"	120
86	95	#5	5	10'-0"	120	86	95	#5	5	10'-0"	120
87	96	#5	5	10'-0"	120	87	96	#5	5	10'-0"	120
88	97	#5	5	10'-0"	120	88	97	#5	5	10'-0"	120
89	98	#5	5	10'-0"	120	89	98	#5	5	10'-0"	120
90	99	#5	5	10'-0"	120	90	99	#5	5	10'-0"	120
91	100	#5	5	10'-0"	120	91	100	#5	5	10'-0"	120

No change in Reinforcement used for concrete in footings

Handwritten notes: 4/11/44, 4/31/44, 4/31/44, 2/12/44, 1/17/44

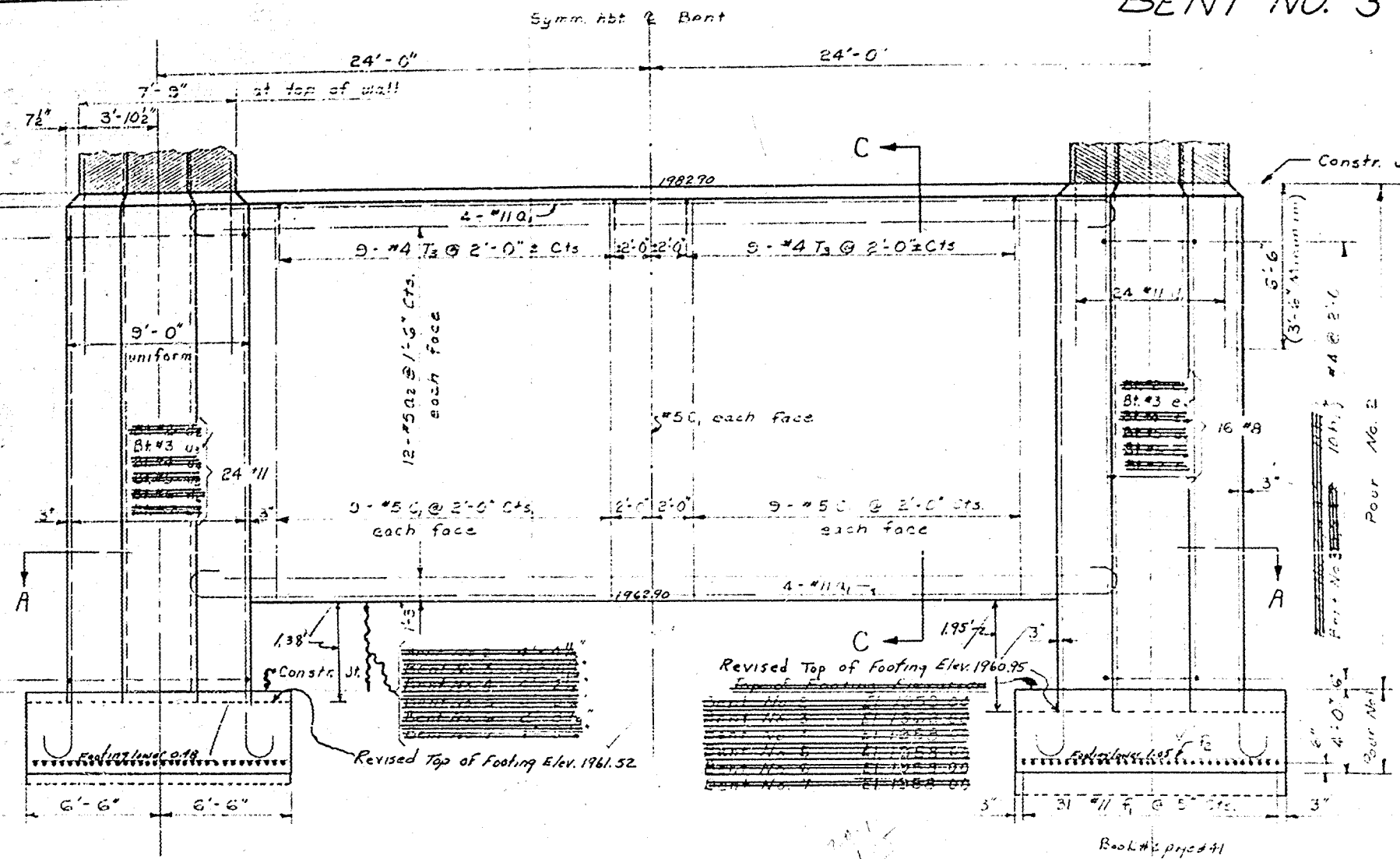
CONCRETE BY POURS	
BENT NO.	2
Pour No. 1	9289

For completion of quantities, see sheet No. 50 and 51.  
TOTAL BILL OF MATERIAL

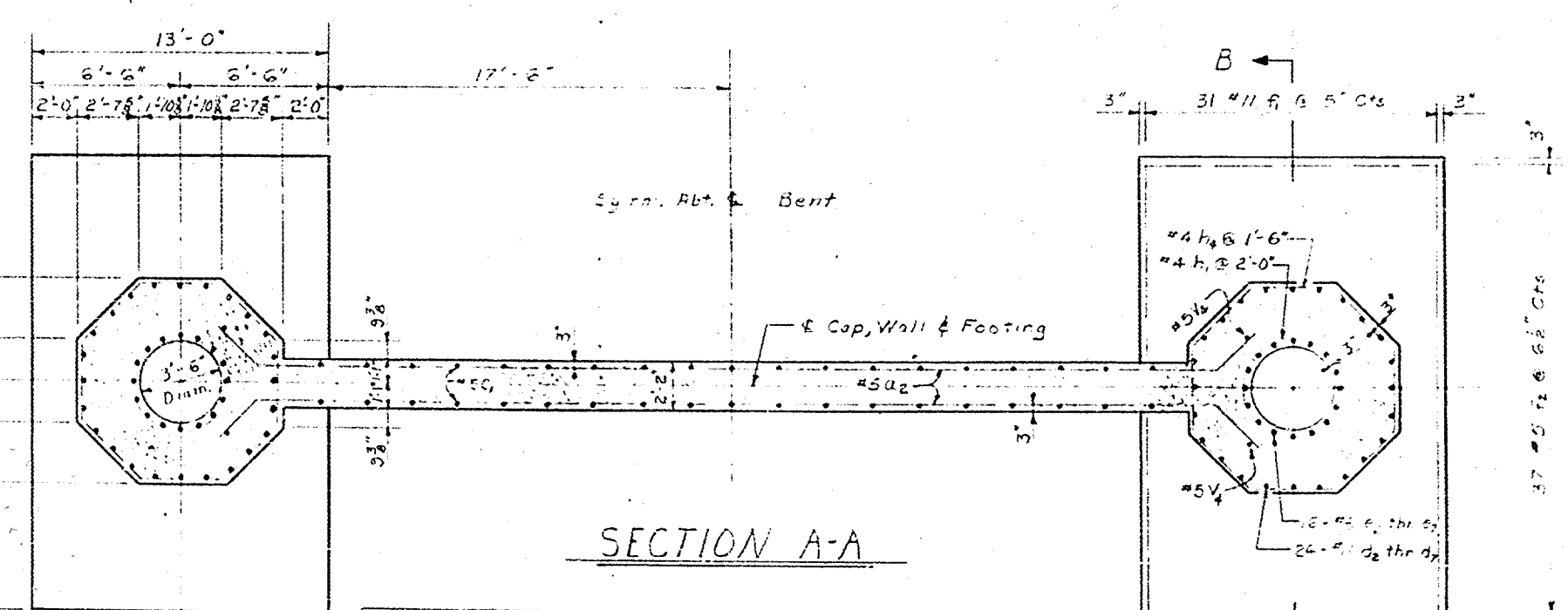
PROJECT No. 8302  
 BLANTON CO. N. C.  
 STATION: 10+00  
 STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISSION  
 RALEIGH

# BENT NO. 3

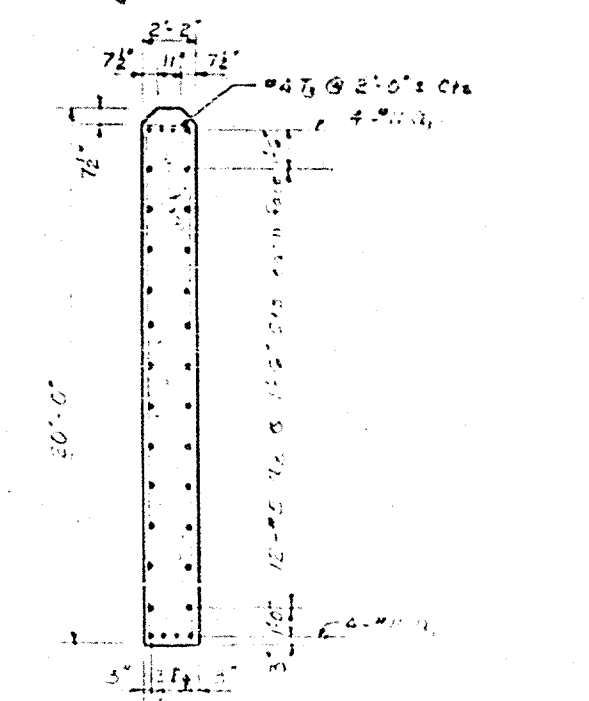
FIG. READ	DATE
REV. NO.	
BY	K.C.
P. A. PROJECT APP-3	



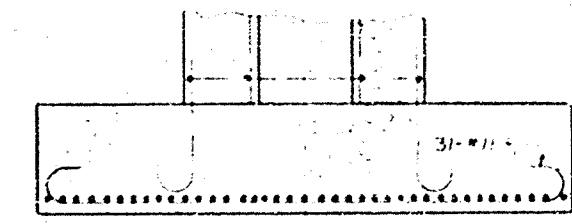
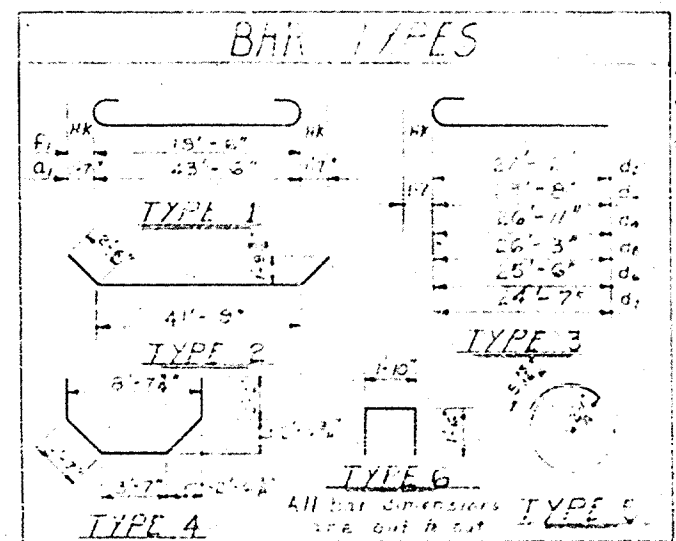
PART ELEVATION



SECTION A-A



SECTION C-C



SECTION B-B

BILL OF MATERIAL BENT PEDESTAL					BILL OF MATERIAL BENT PEDESTAL						
BAR NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR NO.	SIZE	TYPE	LENGTH	WEIGHT		
BENT NO. 2											
19	#4	6	4'-10"	61	19	#4	6	4'-10"	61		
20	#11	1	26'-0"	1083	20	#11	1	26'-0"	1083		
21	#4	5	2	46'-0"	170	21	#4	5	2	46'-0"	170
22	#5	11	11	12'-0"	256	22	#5	11	11	12'-0"	256
23	#5	11	11	12'-0"	256	23	#5	11	11	12'-0"	256
24	#11	1	10'-0"	236	24	#11	1	10'-0"	236		
25	#11	3	28'-3"	733	25	#11	3	28'-3"	733		
26	#8	1	28'-3"	500	26	#8	1	28'-3"	500		
27	#11	1	28'-3"	500	27	#11	1	28'-3"	500		
28	#5	1	28'-3"	500	28	#5	1	28'-3"	500		
29	#5	1	28'-3"	500	29	#5	1	28'-3"	500		
30	#4	5	3'-7"	200	30	#4	5	3'-7"	200		
31	#4	4	1'-11"	64	31	#4	4	1'-11"	64		
32	#5	1	22'-0"	93	32	#5	1	22'-0"	93		
Reinforcing Steel - 125,500					Reinforcing Steel - 125,500						

BILL OF MATERIAL BENT NO. 3					BILL OF MATERIAL BENT NO. 6						
BAR NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR NO.	SIZE	TYPE	LENGTH	WEIGHT		
BENT NO. 3											
19	#4	6	4'-10"	61	19	#4	6	4'-10"	61		
20	#11	1	26'-0"	1083	20	#11	1	26'-0"	1083		
21	#4	5	2	46'-0"	170	21	#4	5	2	46'-0"	170
22	#5	11	11	12'-0"	256	22	#5	11	11	12'-0"	256
23	#5	11	11	12'-0"	256	23	#5	11	11	12'-0"	256
24	#11	1	10'-0"	236	24	#11	1	10'-0"	236		
25	#11	3	28'-3"	733	25	#11	3	28'-3"	733		
26	#8	1	28'-3"	500	26	#8	1	28'-3"	500		
27	#11	1	28'-3"	500	27	#11	1	28'-3"	500		
28	#5	1	28'-3"	500	28	#5	1	28'-3"	500		
29	#5	1	28'-3"	500	29	#5	1	28'-3"	500		
30	#4	5	3'-7"	200	30	#4	5	3'-7"	200		
31	#4	4	1'-11"	64	31	#4	4	1'-11"	64		
32	#5	1	22'-0"	93	32	#5	1	22'-0"	93		
Reinforcing Steel - 125,500					Reinforcing Steel - 125,500						

BILL OF MATERIAL BENT NO. 4					BILL OF MATERIAL BENT NO. 7						
BAR NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR NO.	SIZE	TYPE	LENGTH	WEIGHT		
BENT NO. 4											
19	#4	6	4'-10"	61	19	#4	6	4'-10"	61		
20	#11	1	26'-0"	1083	20	#11	1	26'-0"	1083		
21	#4	5	2	46'-0"	170	21	#4	5	2	46'-0"	170
22	#5	11	11	12'-0"	256	22	#5	11	11	12'-0"	256
23	#5	11	11	12'-0"	256	23	#5	11	11	12'-0"	256
24	#11	1	10'-0"	236	24	#11	1	10'-0"	236		
25	#11	3	28'-3"	733	25	#11	3	28'-3"	733		
26	#8	1	28'-3"	500	26	#8	1	28'-3"	500		
27	#11	1	28'-3"	500	27	#11	1	28'-3"	500		
28	#5	1	28'-3"	500	28	#5	1	28'-3"	500		
29	#5	1	28'-3"	500	29	#5	1	28'-3"	500		
30	#4	5	3'-7"	200	30	#4	5	3'-7"	200		
31	#4	4	1'-11"	64	31	#4	4	1'-11"	64		
32	#5	1	22'-0"	93	32	#5	1	22'-0"	93		
Reinforcing Steel - 125,500					Reinforcing Steel - 125,500						

CONCRETE BY POURS		
BENT NO.	3	
Four No. 1	85.83	

TOTAL BILL OF MATERIAL		
BENT NO. 3		
BENT NO. 4		
BENT NO. 6		
BENT NO. 7		

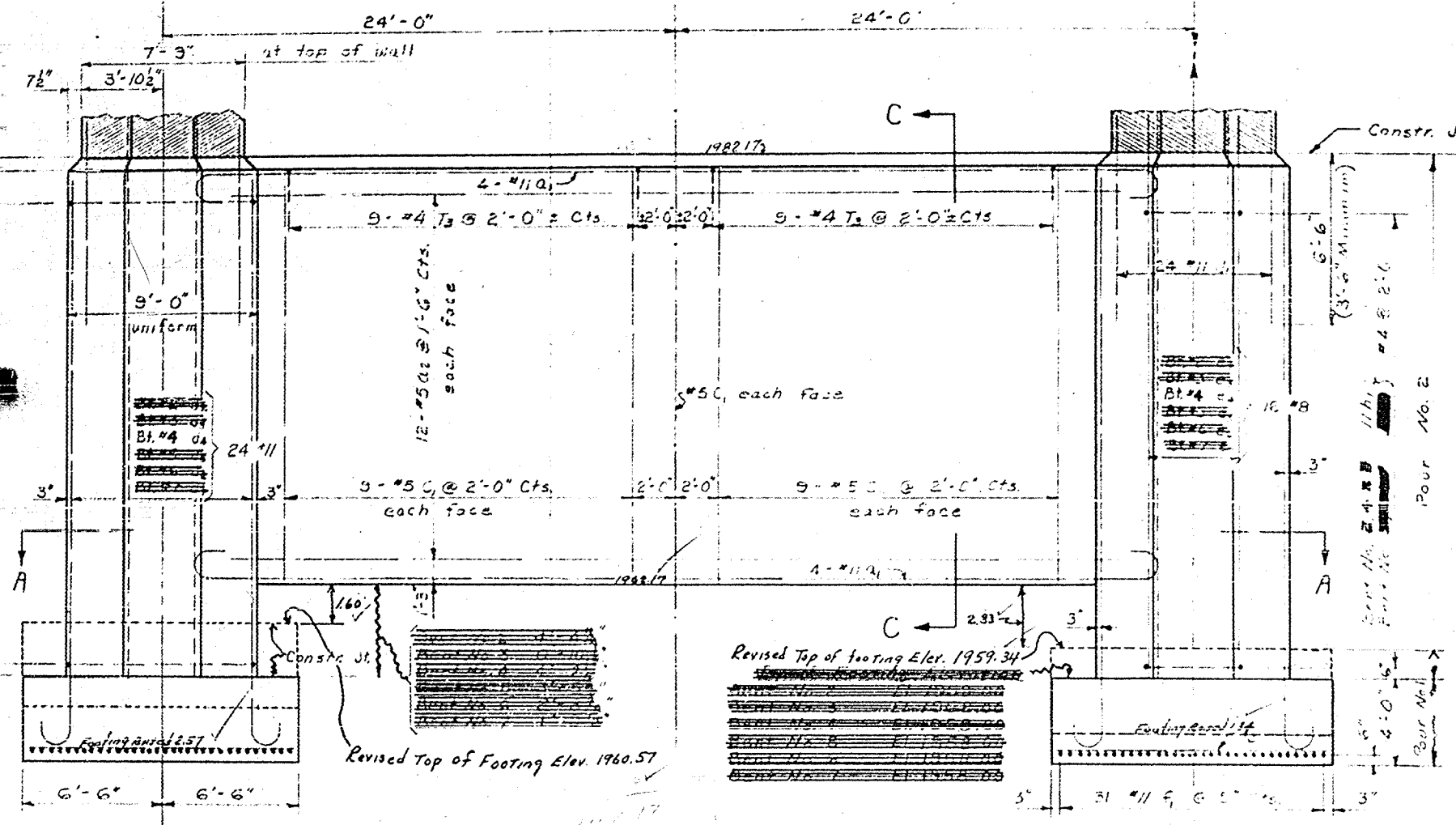
PROJECT No. 8-3024  
 BUNNOMEE Co  
 STATION 17+53

STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISSION  
 Raleigh

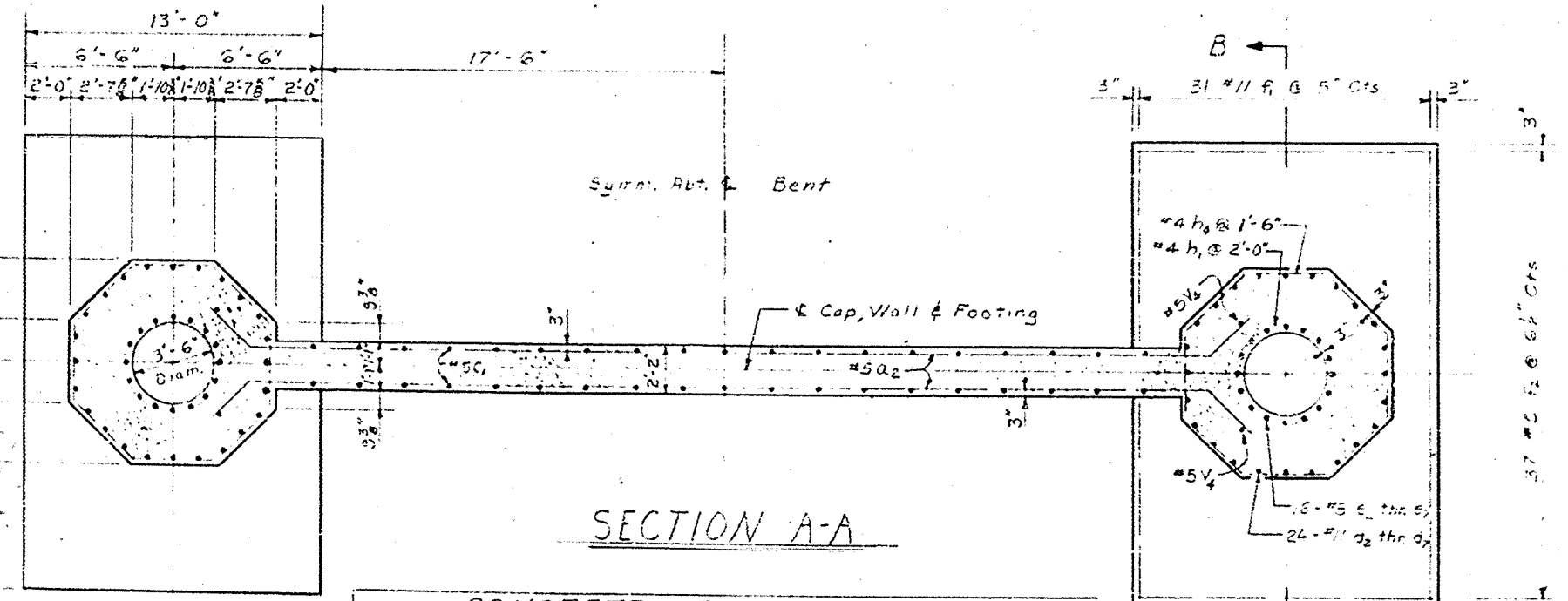
BENT NO. 3

# BENT NO. 4

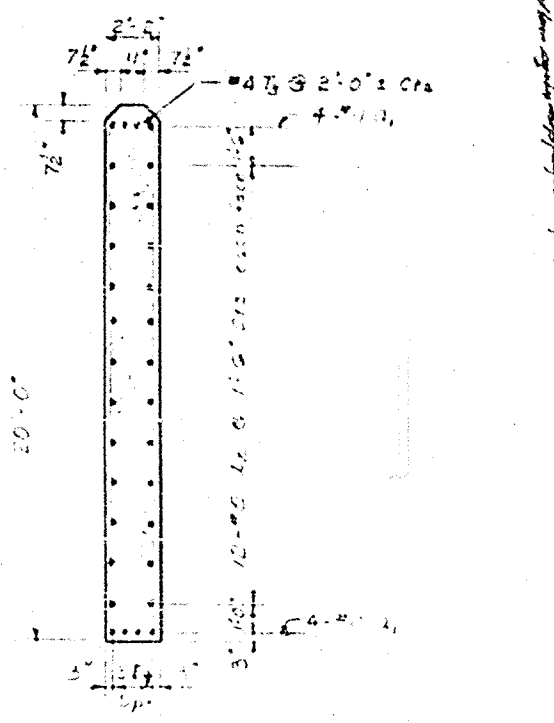
FED. ROAD DIV. NO.	STATE
8	N.C.
F.A. PROJECT APP. NO.	



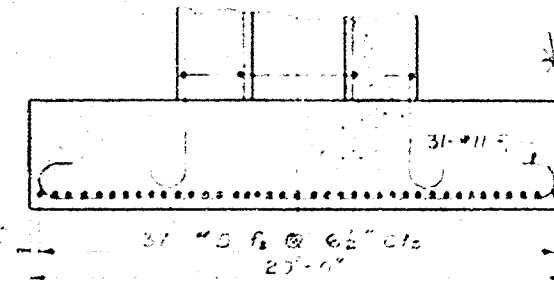
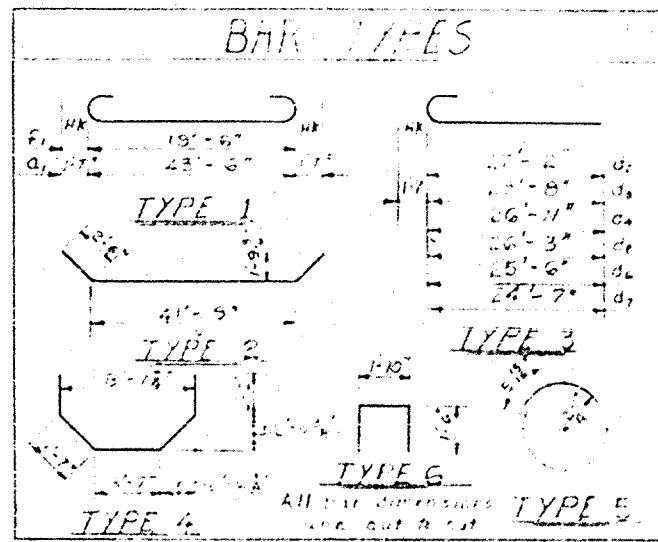
PART ELEVATION



SECTION A-A



SECTION C-C



SECTION B-B

BILL OF MATERIAL BENT PEDESTAL					BILL OF MATERIAL BENT PEDES						
BAR NO.	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR NO.	NO.	SIZE	TYPE	LENGTH	WEIGHT
BENT NO. 4					BENT NO. 7						
a1	19	#4	6	4'-10"	61	a1	19	#4	6	4'-10"	61
a2	8	#11	1	36'-8"	1982	a2	8	#11	1	36'-8"	1982
a3	24	#5	2	48'-0"	1170	a3	24	#5	2	48'-0"	1170
a4	38	#5	5	19'-0"	756	a4	38	#5	5	19'-0"	756
a5	48	#11	3	28'-0"	2520	a5	48	#11	3	28'-0"	2520
a6	22	#8	1	22'-0"	704	a6	22	#8	1	22'-0"	704
a7	24	#5	1	12'-6"	568	a7	24	#5	1	12'-6"	568
a8	25	#4	5	9'-7"	800	a8	25	#4	5	9'-7"	800
a9	64	#4	4	15'-1"	640	a9	64	#4	4	15'-1"	640
a10	4	#5	1	22'-4"	93	a10	4	#5	1	22'-4"	93
Reinforcing Steel-lbs. 25,526					Reinforcing Steel-lbs. 25,526						
Class #7 Concrete-cu yds. 4					Class #7 Concrete-cu yds. 4						

CONCRETE BY POURS		
BENT NO.	4	
Pour No. 1	8643	
Pour No. 2		

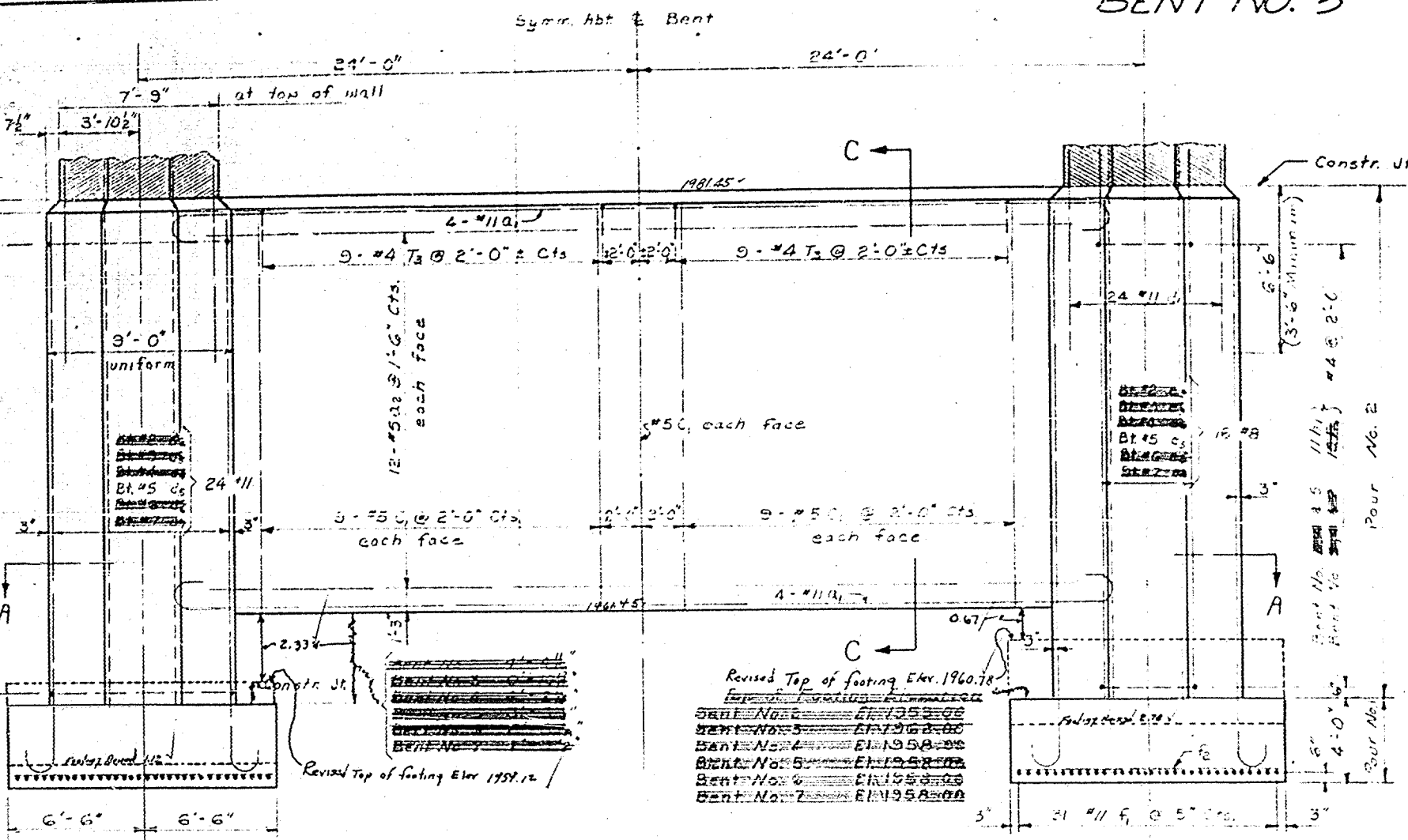
TOTAL BILL OF MATERIAL		
BENT	NO. 4	

PROJECT No. 8-3003  
 BUNNOMARE Co  
 STATION: 117-03  
 STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISS  
 REVISION

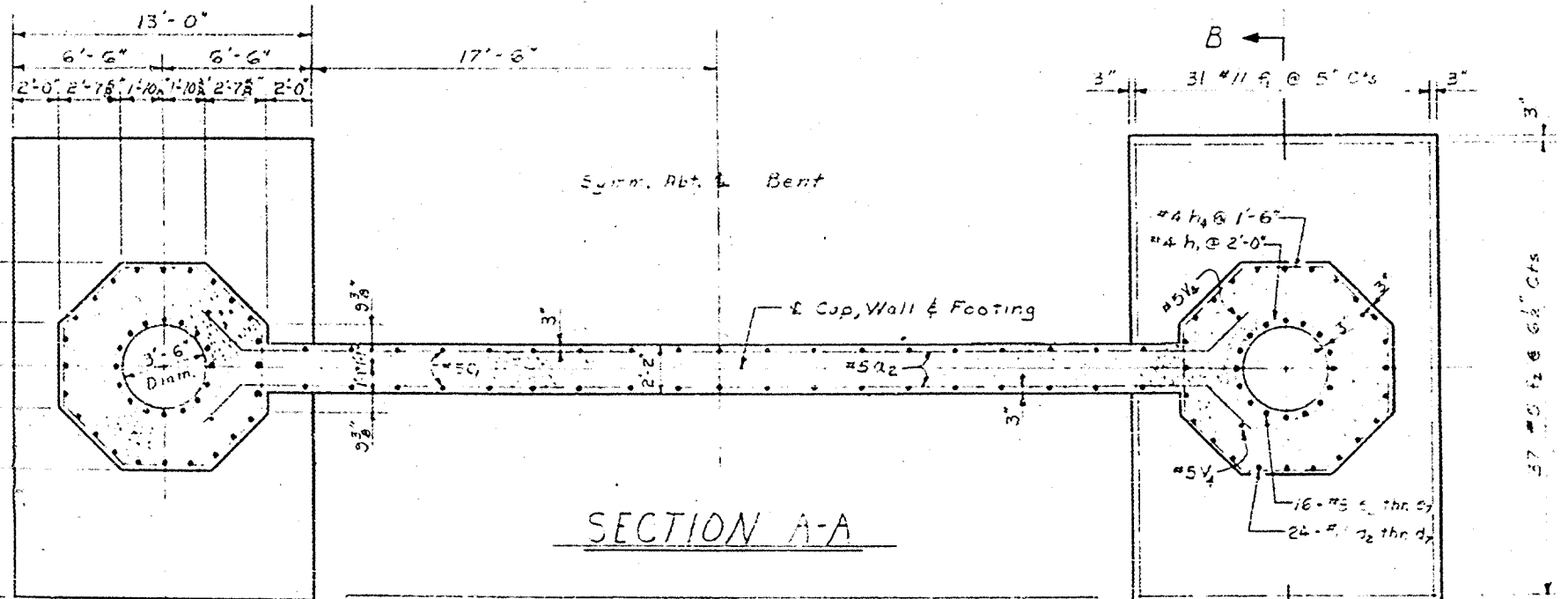


# BENT NO. 5

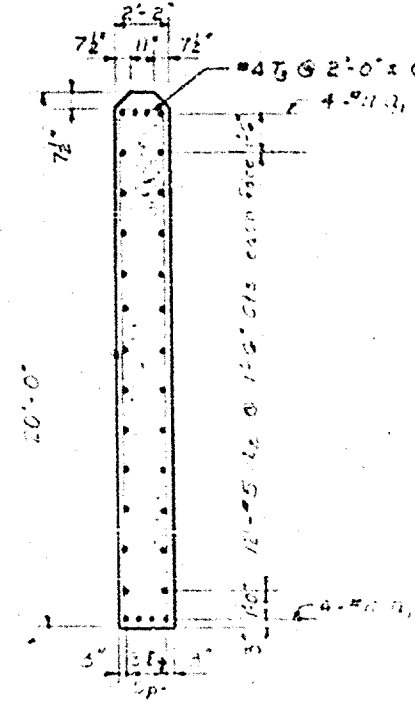
FED. ROAD DIST. NO.	STATE	PROJ.
6	N.C.	117-53.4
P. A. PROJECT APP-34-		



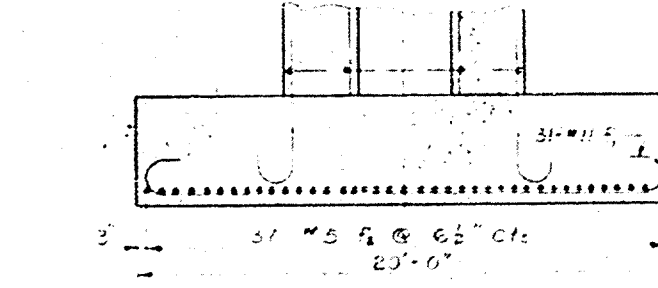
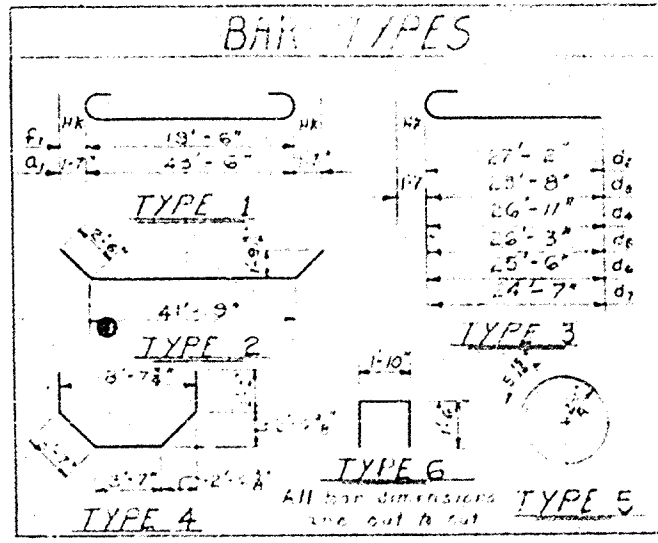
PART ELEVATION



SECTION A-A



SECTION C-C



SECTION B-B

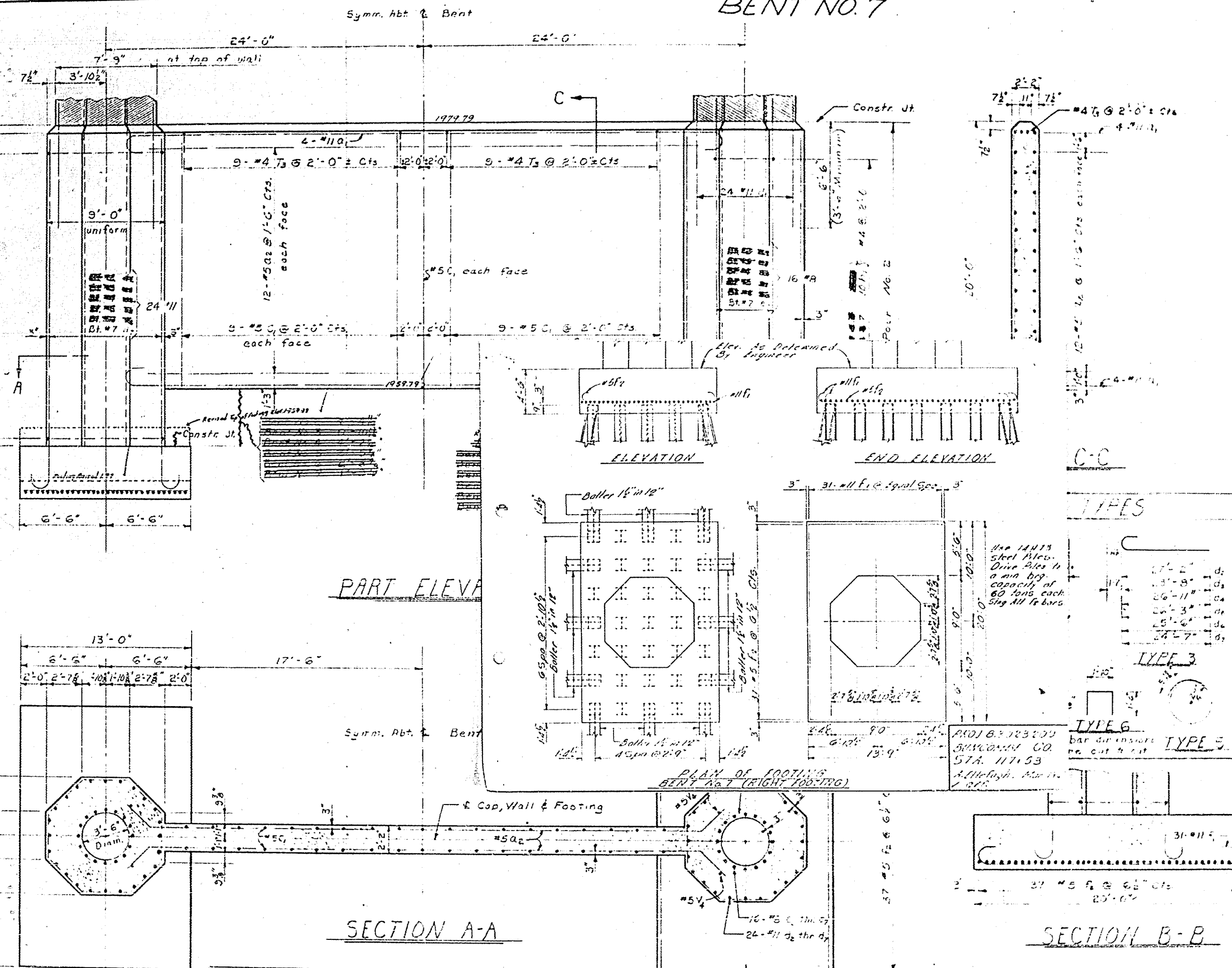
BILL OF MATERIAL					BILL OF MATERIAL				
BENT NO. 5					BENT NO. 5				
BAR NO.	SIZE	TYPE	LENGTH	QTY	BAR NO.	SIZE	TYPE	LENGTH	QTY
1	#11	1	43'-6"	1	1	#11	1	43'-6"	1
2	#11	2	27'-2"	1	2	#11	2	27'-2"	1
3	#11	3	23'-8"	1	3	#11	3	23'-8"	1
4	#11	4	26'-11"	1	4	#11	4	26'-11"	1
5	#11	5	28'-3"	1	5	#11	5	28'-3"	1
6	#11	6	25'-6"	1	6	#11	6	25'-6"	1
7	#11	7	24'-7"	1	7	#11	7	24'-7"	1
8	#11	8	41'-9"	1	8	#11	8	41'-9"	1
9	#11	9	8'-7 1/2"	1	9	#11	9	8'-7 1/2"	1
10	#11	10	1'-10"	1	10	#11	10	1'-10"	1
11	#11	11	1'-5 1/2"	1	11	#11	11	1'-5 1/2"	1
12	#11	12	1'-10"	1	12	#11	12	1'-10"	1
13	#11	13	8'-7 1/2"	1	13	#11	13	8'-7 1/2"	1
14	#11	14	1'-10"	1	14	#11	14	1'-10"	1
15	#11	15	1'-5 1/2"	1	15	#11	15	1'-5 1/2"	1
16	#11	16	1'-10"	1	16	#11	16	1'-10"	1
17	#11	17	8'-7 1/2"	1	17	#11	17	8'-7 1/2"	1
18	#11	18	1'-10"	1	18	#11	18	1'-10"	1
19	#11	19	1'-5 1/2"	1	19	#11	19	1'-5 1/2"	1
20	#11	20	1'-10"	1	20	#11	20	1'-10"	1
21	#11	21	8'-7 1/2"	1	21	#11	21	8'-7 1/2"	1
22	#11	22	1'-10"	1	22	#11	22	1'-10"	1
23	#11	23	1'-5 1/2"	1	23	#11	23	1'-5 1/2"	1
24	#11	24	1'-10"	1	24	#11	24	1'-10"	1
25	#11	25	8'-7 1/2"	1	25	#11	25	8'-7 1/2"	1
26	#11	26	1'-10"	1	26	#11	26	1'-10"	1
27	#11	27	1'-5 1/2"	1	27	#11	27	1'-5 1/2"	1
28	#11	28	1'-10"	1	28	#11	28	1'-10"	1
29	#11	29	8'-7 1/2"	1	29	#11	29	8'-7 1/2"	1
30	#11	30	1'-10"	1	30	#11	30	1'-10"	1
31	#11	31	1'-5 1/2"	1	31	#11	31	1'-5 1/2"	1
32	#11	32	1'-10"	1	32	#11	32	1'-10"	1
33	#11	33	8'-7 1/2"	1	33	#11	33	8'-7 1/2"	1
34	#11	34	1'-10"	1	34	#11	34	1'-10"	1
35	#11	35	1'-5 1/2"	1	35	#11	35	1'-5 1/2"	1
36	#11	36	1'-10"	1	36	#11	36	1'-10"	1
37	#11	37	8'-7 1/2"	1	37	#11	37	8'-7 1/2"	1
38	#11	38	1'-10"	1	38	#11	38	1'-10"	1
39	#11	39	1'-5 1/2"	1	39	#11	39	1'-5 1/2"	1
40	#11	40	1'-10"	1	40	#11	40	1'-10"	1
41	#11	41	8'-7 1/2"	1	41	#11	41	8'-7 1/2"	1
42	#11	42	1'-10"	1	42	#11	42	1'-10"	1
43	#11	43	1'-5 1/2"	1	43	#11	43	1'-5 1/2"	1
44	#11	44	1'-10"	1	44	#11	44	1'-10"	1
45	#11	45	8'-7 1/2"	1	45	#11	45	8'-7 1/2"	1
46	#11	46	1'-10"	1	46	#11	46	1'-10"	1
47	#11	47	1'-5 1/2"	1	47	#11	47	1'-5 1/2"	1
48	#11	48	1'-10"	1	48	#11	48	1'-10"	1
49	#11	49	8'-7 1/2"	1	49	#11	49	8'-7 1/2"	1
50	#11	50	1'-10"	1	50	#11	50	1'-10"	1
51	#11	51	1'-5 1/2"	1	51	#11	51	1'-5 1/2"	1
52	#11	52	1'-10"	1	52	#11	52	1'-10"	1
53	#11	53	8'-7 1/2"	1	53	#11	53	8'-7 1/2"	1
54	#11	54	1'-10"	1	54	#11	54	1'-10"	1
55	#11	55	1'-5 1/2"	1	55	#11	55	1'-5 1/2"	1
56	#11	56	1'-10"	1	56	#11	56	1'-10"	1
57	#11	57	8'-7 1/2"	1	57	#11	57	8'-7 1/2"	1
58	#11	58	1'-10"	1	58	#11	58	1'-10"	1
59	#11	59	1'-5 1/2"	1	59	#11	59	1'-5 1/2"	1
60	#11	60	1'-10"	1	60	#11	60	1'-10"	1
61	#11	61	8'-7 1/2"	1	61	#11	61	8'-7 1/2"	1
62	#11	62	1'-10"	1	62	#11	62	1'-10"	1
63	#11	63	1'-5 1/2"	1	63	#11	63	1'-5 1/2"	1
64	#11	64	1'-10"	1	64	#11	64	1'-10"	1
65	#11	65	8'-7 1/2"	1	65	#11	65	8'-7 1/2"	1
66	#11	66	1'-10"	1	66	#11	66	1'-10"	1
67	#11	67	1'-5 1/2"	1	67	#11	67	1'-5 1/2"	1
68	#11	68	1'-10"	1	68	#11	68	1'-10"	1
69	#11	69	8'-7 1/2"	1	69	#11	69	8'-7 1/2"	1
70	#11	70	1'-10"	1	70	#11	70	1'-10"	1
71	#11	71	1'-5 1/2"	1	71	#11	71	1'-5 1/2"	1
72	#11	72	1'-10"	1	72	#11	72	1'-10"	1
73	#11	73	8'-7 1/2"	1	73	#11	73	8'-7 1/2"	1
74	#11	74	1'-10"	1	74	#11	74	1'-10"	1
75	#11	75	1'-5 1/2"	1	75	#11	75	1'-5 1/2"	1
76	#11	76	1'-10"	1	76	#11	76	1'-10"	1
77	#11	77	8'-7 1/2"	1	77	#11	77	8'-7 1/2"	1
78	#11	78	1'-10"	1	78	#11	78	1'-10"	1
79	#11	79	1'-5 1/2"	1	79	#11	79	1'-5 1/2"	1
80	#11	80	1'-10"	1	80	#11	80	1'-10"	1
81	#11	81	8'-7 1/2"	1	81	#11	81	8'-7 1/2"	1
82	#11	82	1'-10"	1	82	#11	82	1'-10"	1
83	#11	83	1'-5 1/2"	1	83	#11	83	1'-5 1/2"	1
84	#11	84	1'-10"	1	84	#11	84	1'-10"	1
85	#11	85	8'-7 1/2"	1	85	#11	85	8'-7 1/2"	1
86	#11	86	1'-10"	1	86	#11	86	1'-10"	1
87	#11	87	1'-5 1/2"	1	87	#11	87	1'-5 1/2"	1
88	#11	88	1'-10"	1	88	#11	88	1'-10"	1
89	#11	89	8'-7 1/2"	1	89	#11	89	8'-7 1/2"	1
90	#11	90	1'-10"	1	90	#11	90	1'-10"	1
91	#11	91	1'-5 1/2"	1	91	#11	91	1'-5 1/2"	1
92	#11	92	1'-10"	1	92	#11	92	1'-10"	1
93	#11	93	8'-7 1/2"	1	93	#11	93	8'-7 1/2"	1
94	#11	94	1'-10"	1	94	#11	94	1'-10"	1
95	#11	95	1'-5 1/2"	1	95	#11	95	1'-5 1/2"	1
96	#11	96	1'-10"	1	96	#11	96	1'-10"	1
97	#11	97	8'-7 1/2"	1	97	#11	97	8'-7 1/2"	1
98	#11	98	1'-10"	1	98	#11	98	1'-10"	1
99	#11	99	1'-5 1/2"	1	99	#11	99	1'-5 1/2"	1
100	#11	100	1'-10"	1	100	#11	100	1'-10"	1

BILL OF MATERIAL					BILL OF MATERIAL				
BENT NO. 5					BENT NO. 6				
BAR NO.	SIZE	TYPE	LENGTH	QTY	BAR NO.	SIZE	TYPE	LENGTH	QTY
1	#11	1	43'-6"	1	1	#11	1	43'-6"	1
2	#11	2	27'-2"	1	2	#11	2	27'-2"	1
3	#11	3	23'-8"	1	3	#11	3	23'-8"	1
4	#11	4	26'-11"	1	4	#11	4	26'-11"	1
5	#11	5	28'-3"	1	5	#11	5	28'-3"	1
6	#11	6	25'-6"	1	6	#11	6	25'-6"	1
7	#11	7	24'-7"	1	7	#11	7	24'-7"	1
8	#11	8	41'-9"	1	8	#11	8	41'-9"	1
9	#11	9	8'-7 1/2"	1	9	#11	9	8'-7 1/2"	1
10	#11	10	1'-10"	1	10	#11	10	1'-10"	1
11	#11	11	1'-5 1/2"	1	11	#11	11	1'-5 1/2"	1
12	#11	12	1'-10"	1	12	#11	12	1'-10"	1
13	#11	13	8'-7 1/2"	1	13	#11	13	8'-7 1/2"	1
14	#11	14	1'-10"	1	14	#11	14	1'-10"	1
15	#11	15	1'-5 1/2"	1	15	#11	15	1'-5 1/2"	1
16	#11	16	1'-10"	1	16	#11	16	1'-10"	1
17	#11	17	8'-7 1/2"	1	17	#11	17	8'-7 1/2"	1
18	#11	18	1'-10"	1	18	#11	18	1'-10"	1
19	#11	19	1'-5 1/2"	1	19	#11	19	1'-5 1/2"	1
20	#11	20	1'-10"	1	20	#11	20	1'-10"	1
21	#11	21	8'-7 1/2"	1	2				



# BENT NO. 7

REG. NO. 10	STATE
D.Y. NO.	N. C.
3	
A. PROJECT APP. 3	



BILL OF MATERIAL BENT PEDESTAL					BILL OF MATERIAL BENT PEDES				
BAR	NO.	SIZE	TYPE	LENGTH	BAR	NO.	SIZE	TYPE	LENGTH
<b>BENT NO. 2</b>					<b>BENT NO. 6</b>				
A	19	#4	G	4'-10"	A	19	#4	G	4'-10"
B	8	#11	L	46'-0"	B	8	#11	L	46'-0"
C	24	#5	S	46'-0"	C	24	#5	S	46'-0"
D	38	#5	L	19'-0"	D	38	#5	L	19'-0"
E	48	#11	L	17'-0"	E	48	#11	L	17'-0"
F	48	#11	L	28'-9"	F	48	#11	L	28'-9"
G	32	#8	S	28'-9"	G	32	#8	S	28'-9"
H	62	#11	L	28'-9"	H	62	#11	L	28'-9"
I	74	#5	L	18'-6"	I	74	#5	L	18'-6"
J	22	#4	S	4'-7"	J	22	#4	S	4'-7"
K	64	#4	L	10'-1"	K	64	#4	L	10'-1"
L	4	#5	L	22'-4"	L	4	#5	L	22'-4"
Reinforcing Steel - lbs. = 28,522					Reinforcing Steel - lbs. = 28,522				
Concrete - cu. yd. = 11.5					Concrete - cu. yd. = 11.5				
<b>BENT NO. 3</b>					<b>BENT NO. 6</b>				
A	19	#4	G	4'-10"	A	19	#4	G	4'-10"
B	8	#11	L	46'-0"	B	8	#11	L	46'-0"
C	24	#5	S	46'-0"	C	24	#5	S	46'-0"
D	38	#5	L	19'-0"	D	38	#5	L	19'-0"
E	48	#11	L	17'-0"	E	48	#11	L	17'-0"
F	48	#11	L	28'-9"	F	48	#11	L	28'-9"
G	32	#8	S	28'-9"	G	32	#8	S	28'-9"
H	62	#11	L	28'-9"	H	62	#11	L	28'-9"
I	74	#5	L	18'-6"	I	74	#5	L	18'-6"
J	22	#4	S	4'-7"	J	22	#4	S	4'-7"
K	64	#4	L	10'-1"	K	64	#4	L	10'-1"
L	4	#5	L	22'-4"	L	4	#5	L	22'-4"
Reinforcing Steel - lbs. = 28,522					Reinforcing Steel - lbs. = 28,522				
Concrete - cu. yd. = 11.5					Concrete - cu. yd. = 11.5				
<b>BENT NO. 4</b>					<b>BENT NO. 7</b>				
A	19	#4	G	4'-10"	A	19	#4	G	4'-10"
B	8	#11	L	46'-0"	B	8	#11	L	46'-0"
C	24	#5	S	46'-0"	C	24	#5	S	46'-0"
D	38	#5	L	19'-0"	D	38	#5	L	19'-0"
E	48	#11	L	17'-0"	E	48	#11	L	17'-0"
F	48	#11	L	28'-9"	F	48	#11	L	28'-9"
G	32	#8	S	28'-9"	G	32	#8	S	28'-9"
H	62	#11	L	28'-9"	H	62	#11	L	28'-9"
I	74	#5	L	18'-6"	I	74	#5	L	18'-6"
J	22	#4	S	4'-7"	J	22	#4	S	4'-7"
K	64	#4	L	10'-1"	K	64	#4	L	10'-1"
L	4	#5	L	22'-4"	L	4	#5	L	22'-4"
Reinforcing Steel - lbs. = 28,522					Reinforcing Steel - lbs. = 28,522				
Concrete - cu. yd. = 11.5					Concrete - cu. yd. = 11.5				

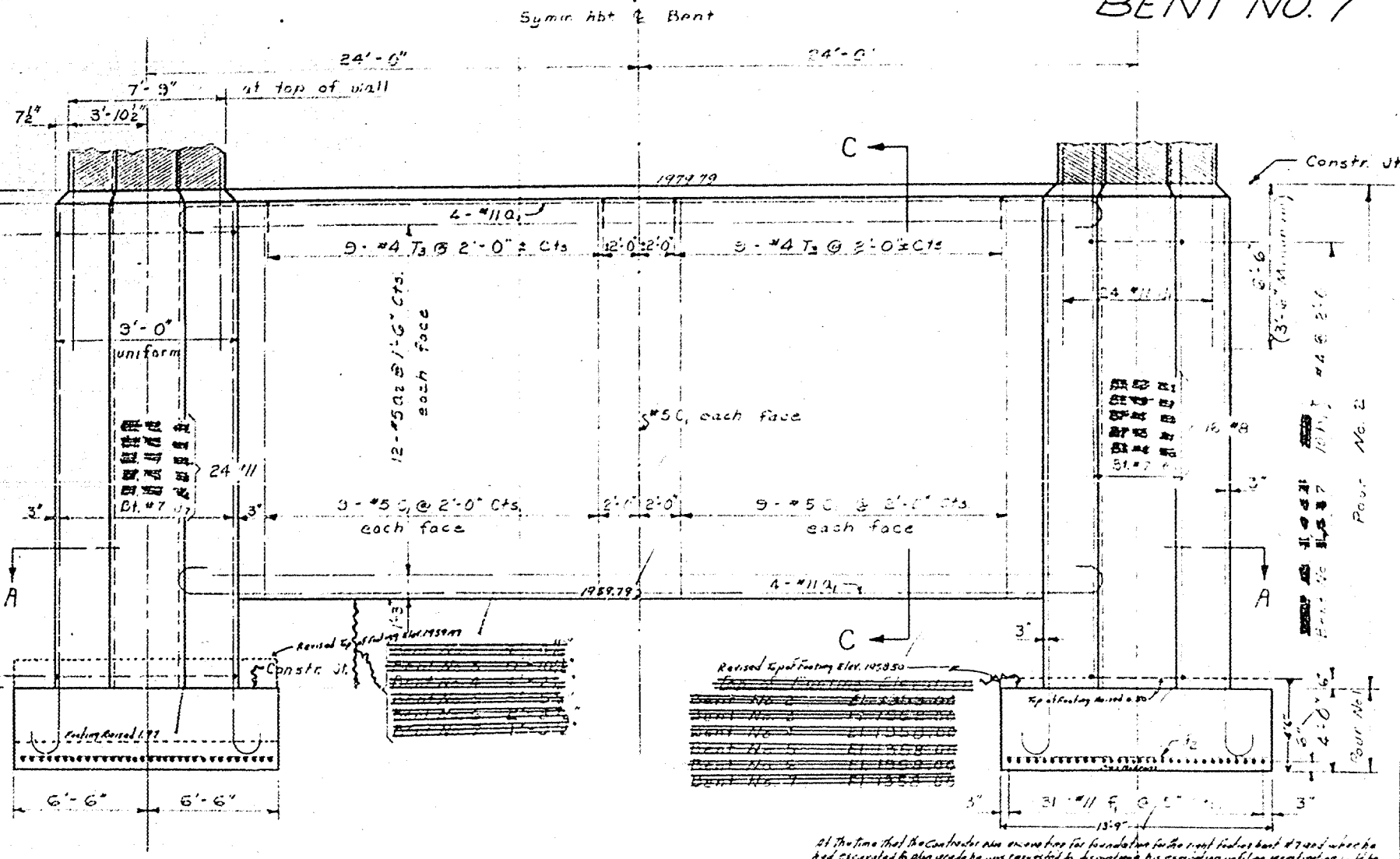
CONCRETE BY POURS

TOTAL BILL OF MATERIAL

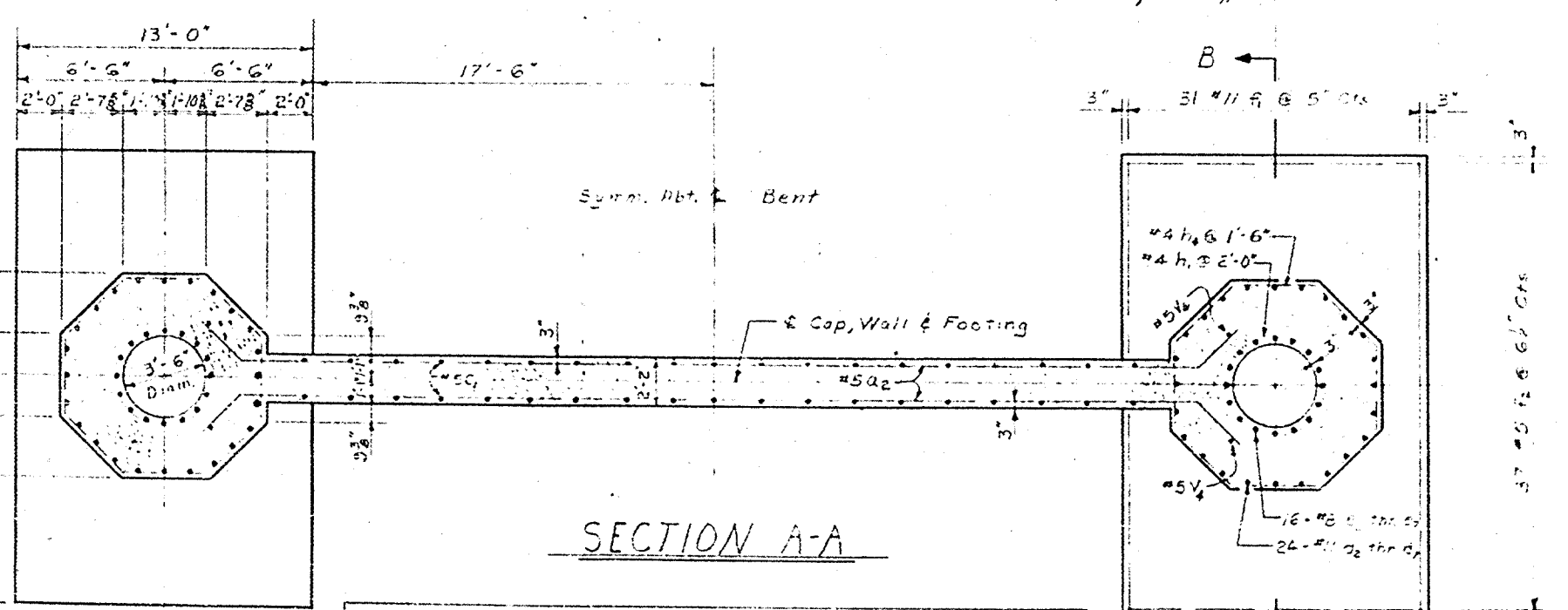
PROJECT No. B. 3042  
 BUNCOMBE Co.  
 STATION: 117.53  
 STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISSION  
 RALEIGH

# BENT NO. 7

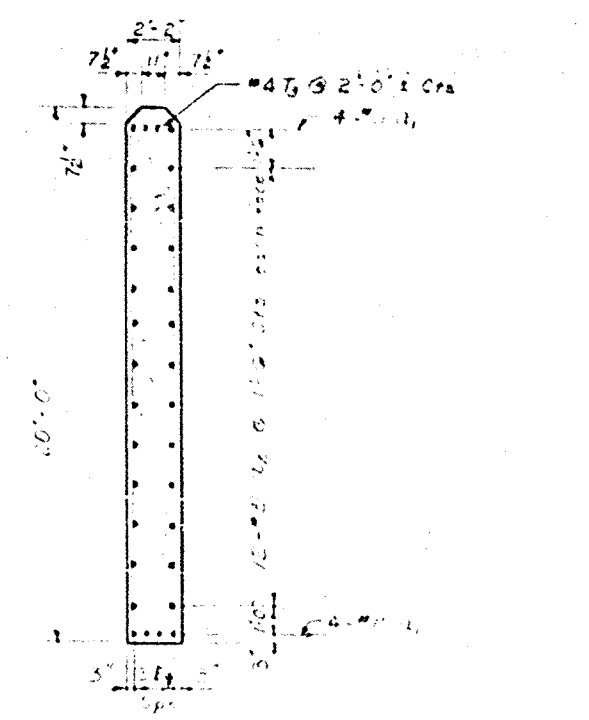
FED. ROAD DIST. NO.	DATE	PK.
1	N.C.	12
D. A. PROJECT APP-34		



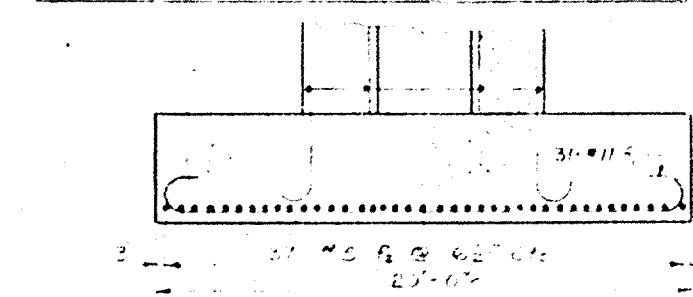
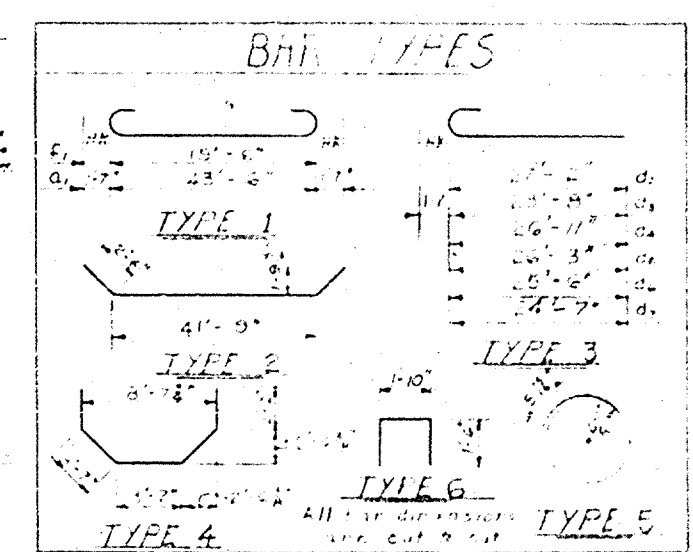
PART ELEVATION



SECTION A-A



SECTION C-C



SECTION B-B

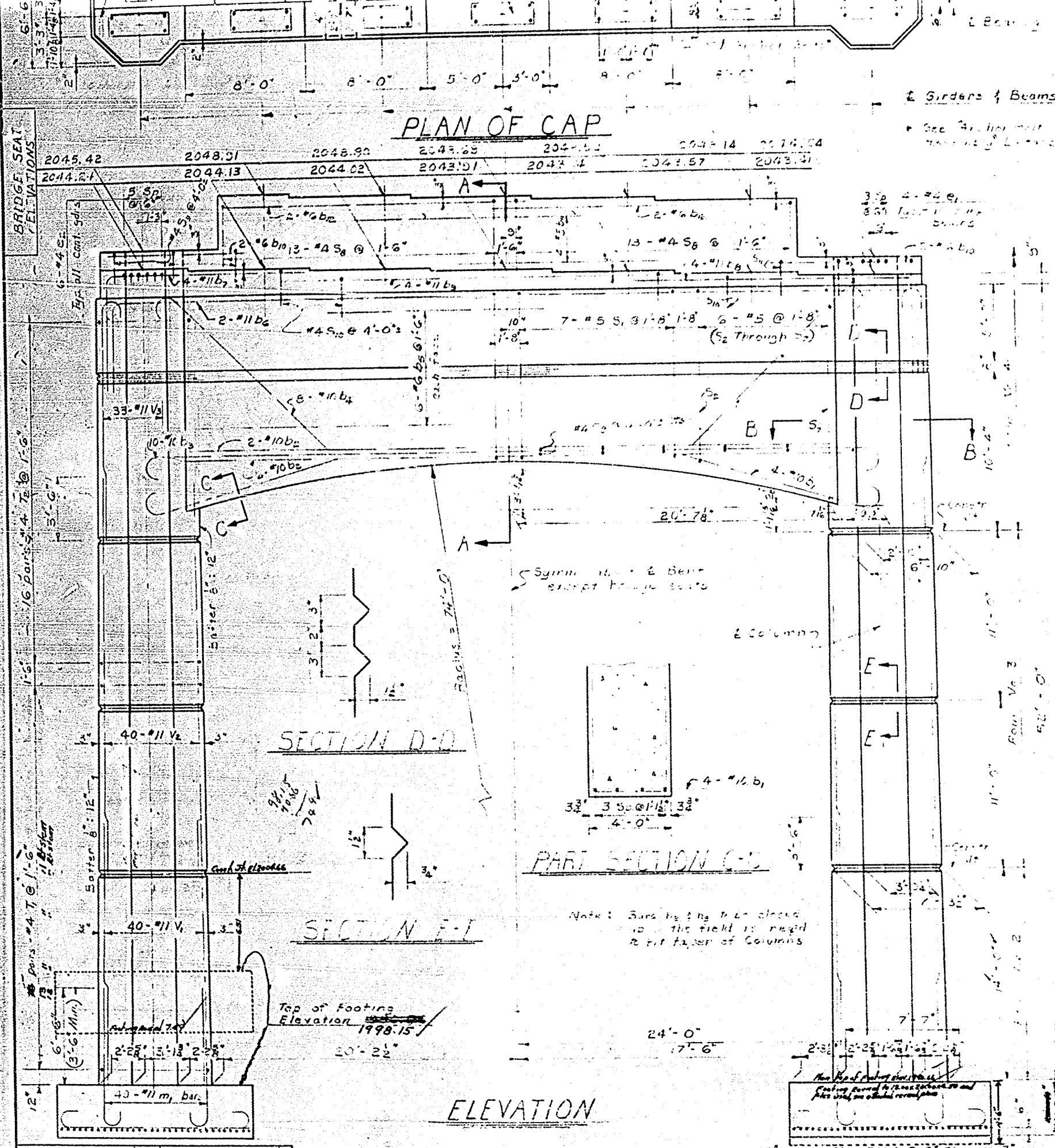
BILL OF MATERIAL BENT PEDESTAL					BILL OF MATERIAL BENT PEDESTAL				
BAR	NO.	SIZE	TYPE	LENGTH	BAR	NO.	SIZE	TYPE	LENGTH
BENT NO. 2					BENT NO. 5				
a	19	#4	G	4'-10"	a	19	#4	G	4'-10"
b	8	#11	L	4'-10"	b	8	#11	L	4'-10"
c	4	#5	S	4'-10"	c	4	#5	S	4'-10"
d	48	#11	S	17'-0"	d	48	#11	S	17'-0"
e	48	#11	S	17'-0"	e	48	#11	S	17'-0"
f	48	#11	S	17'-0"	f	48	#11	S	17'-0"
g	48	#11	S	17'-0"	g	48	#11	S	17'-0"
h	24	#5	S	12'-6"	h	24	#5	S	12'-6"
i	24	#5	S	12'-6"	i	24	#5	S	12'-6"
j	24	#5	S	12'-6"	j	24	#5	S	12'-6"
k	24	#5	S	12'-6"	k	24	#5	S	12'-6"
l	24	#5	S	12'-6"	l	24	#5	S	12'-6"
m	24	#5	S	12'-6"	m	24	#5	S	12'-6"
n	24	#5	S	12'-6"	n	24	#5	S	12'-6"
o	24	#5	S	12'-6"	o	24	#5	S	12'-6"
p	24	#5	S	12'-6"	p	24	#5	S	12'-6"
q	24	#5	S	12'-6"	q	24	#5	S	12'-6"
r	24	#5	S	12'-6"	r	24	#5	S	12'-6"
s	24	#5	S	12'-6"	s	24	#5	S	12'-6"
t	24	#5	S	12'-6"	t	24	#5	S	12'-6"
u	24	#5	S	12'-6"	u	24	#5	S	12'-6"
v	24	#5	S	12'-6"	v	24	#5	S	12'-6"
Reinforcing Steel lbs	25,621				Reinforcing Steel lbs	25,621			

CONCRETE BY POURS	
7	9000

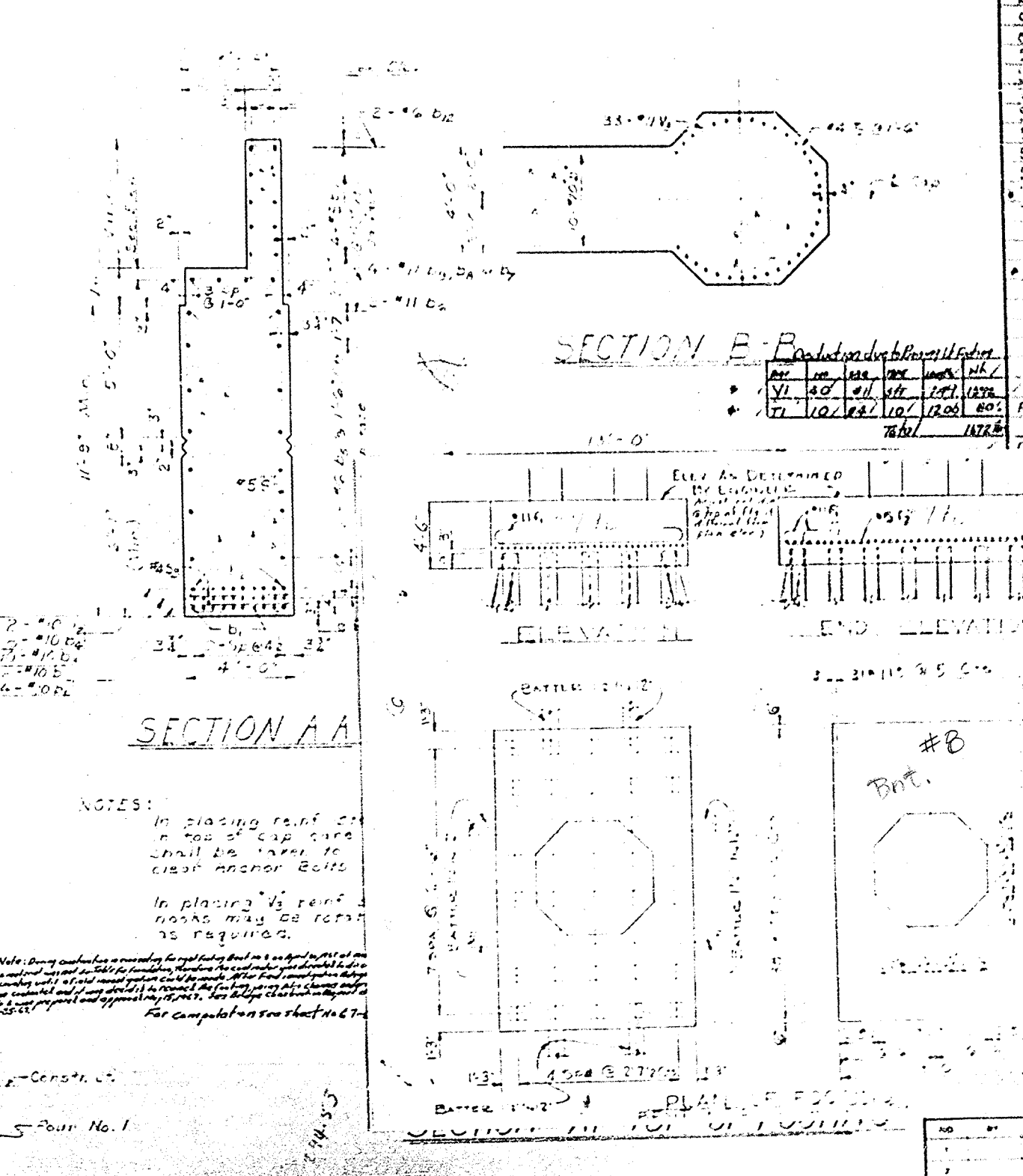
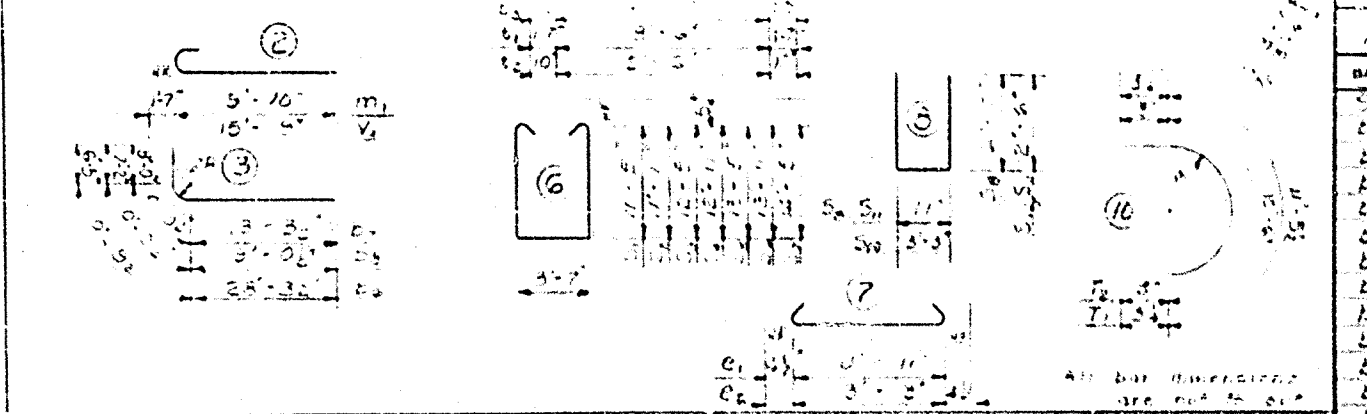
TOTAL BILL OF MATERIAL	
7	9000

PROJECT No. 8-30-17  
 BUNTYPE COU  
 STATION 17-23-6

STATE OF NORTH CAROLINA  
**STATE HIGHWAY COMMISSION**  
 RALEIGH



DRAWN BY Don H. Smith DATE 12 Sept 64  
 CHECKED BY S. J. Hoke DATE Sept 12 64





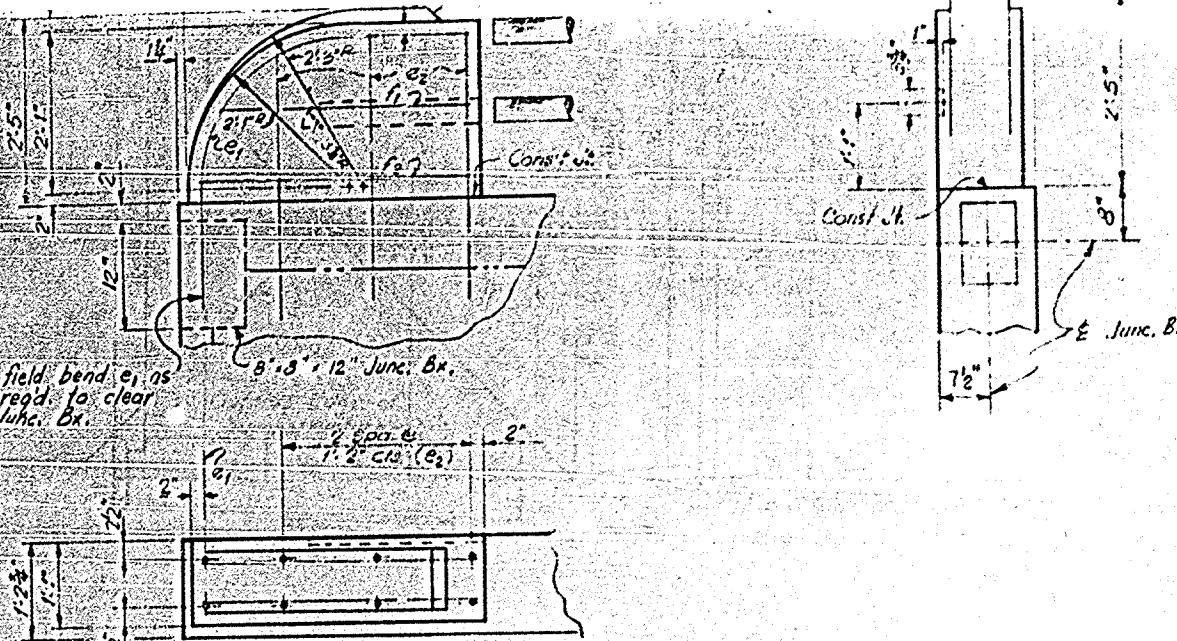




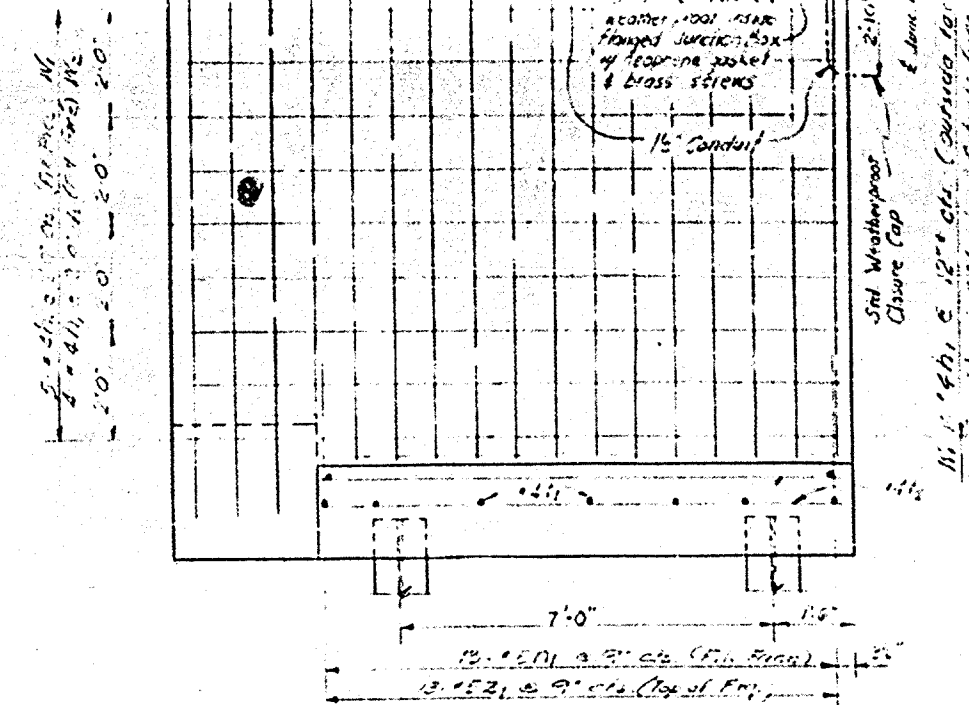




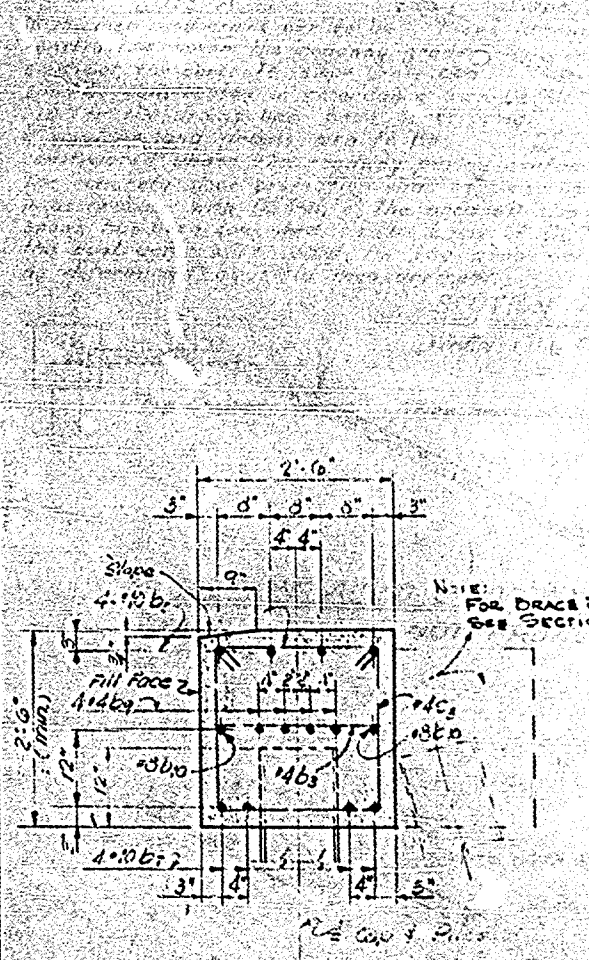




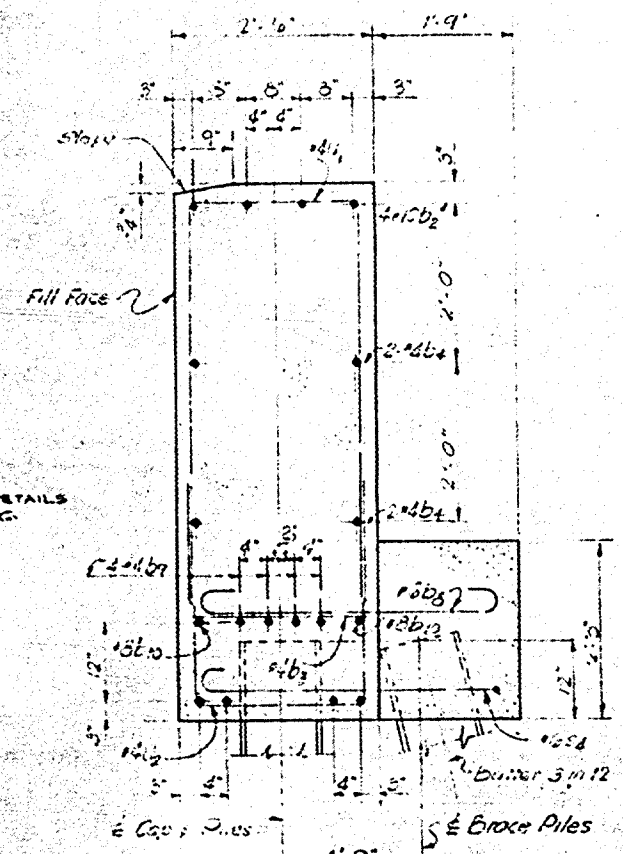
END POST DETAILS  
(SEE METAL PLAN)



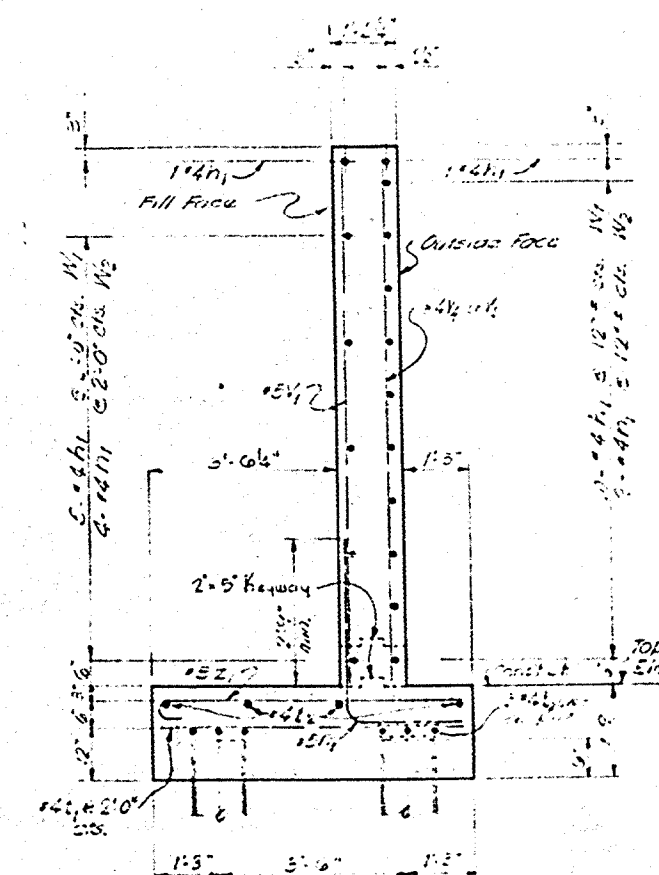
TYPICAL WING ELEVATION



SECTION C-C

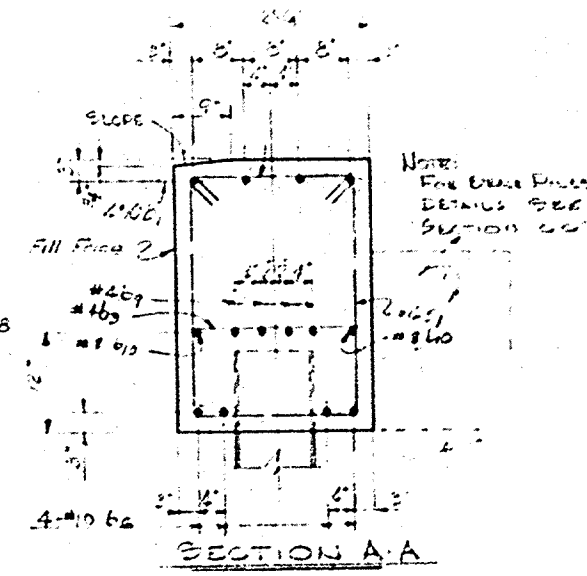


SECTION B-B



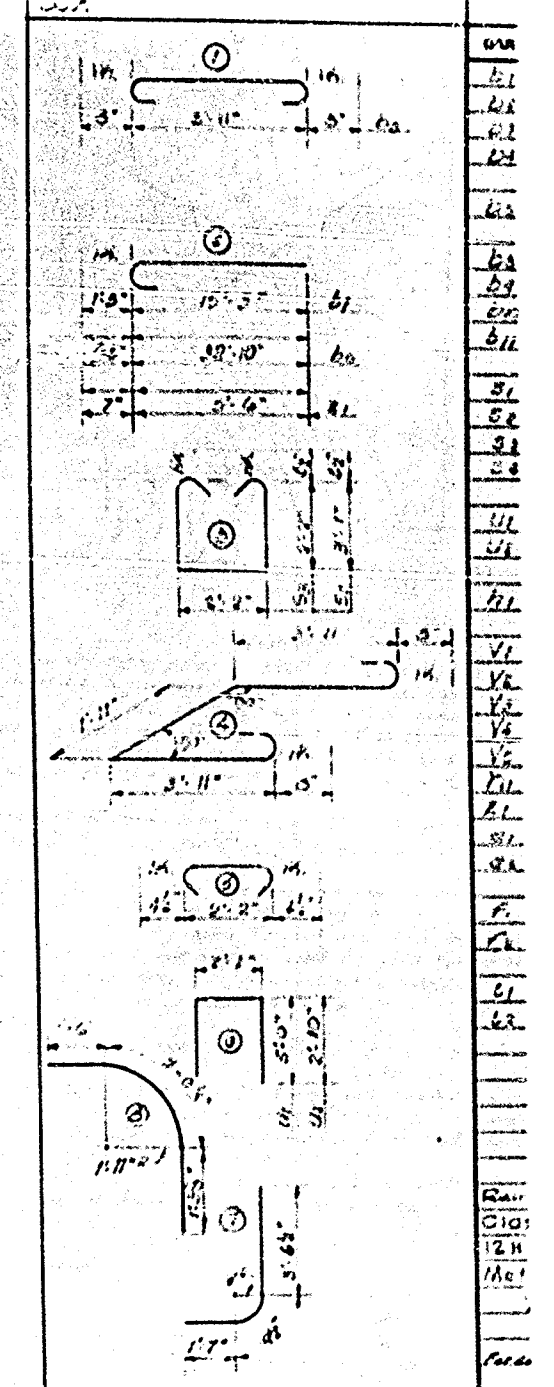
TYPICAL WING SECTION

Note: Built to Plans  
2, 3, 4



SECTION A-A

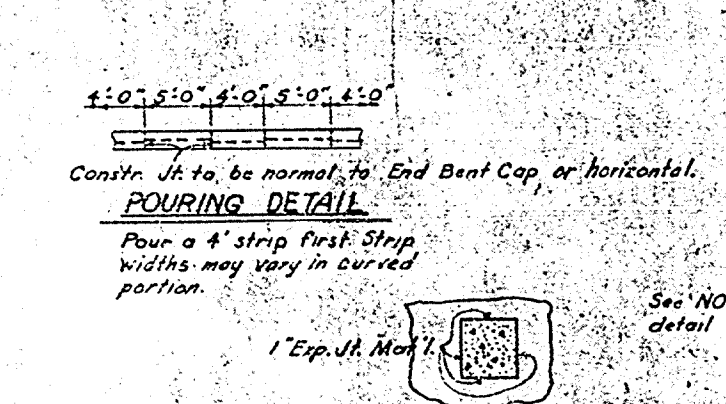
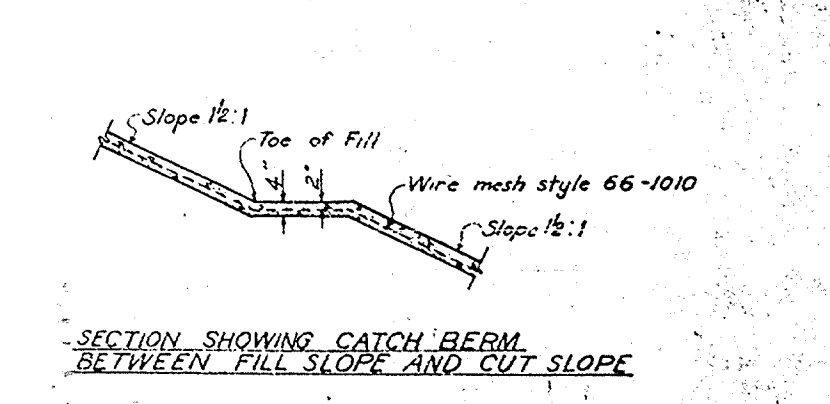
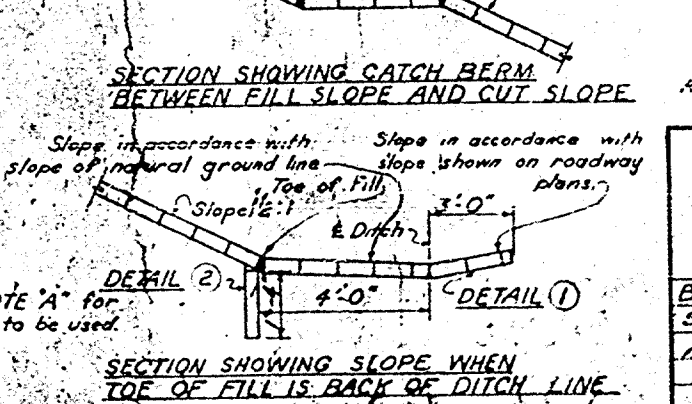
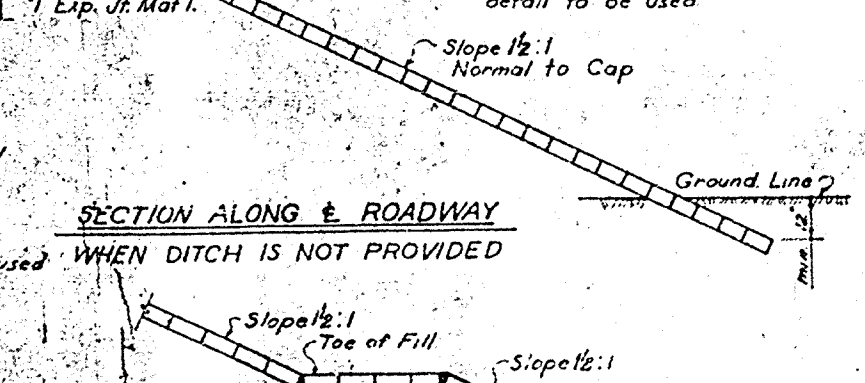
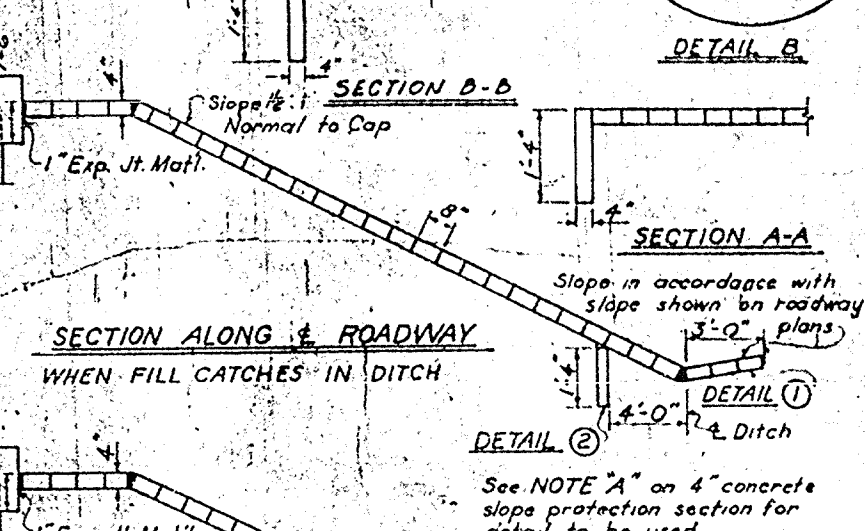
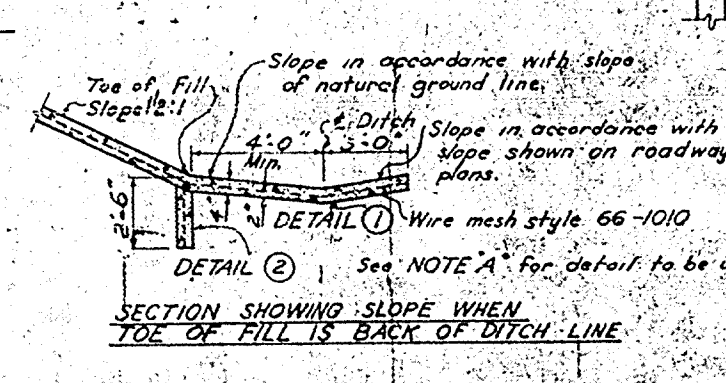
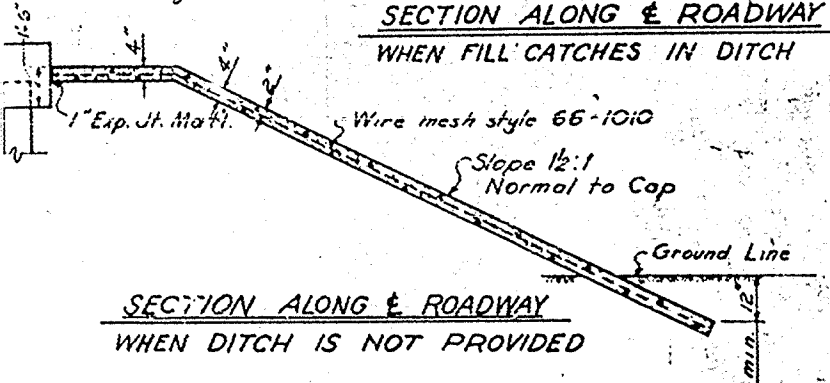
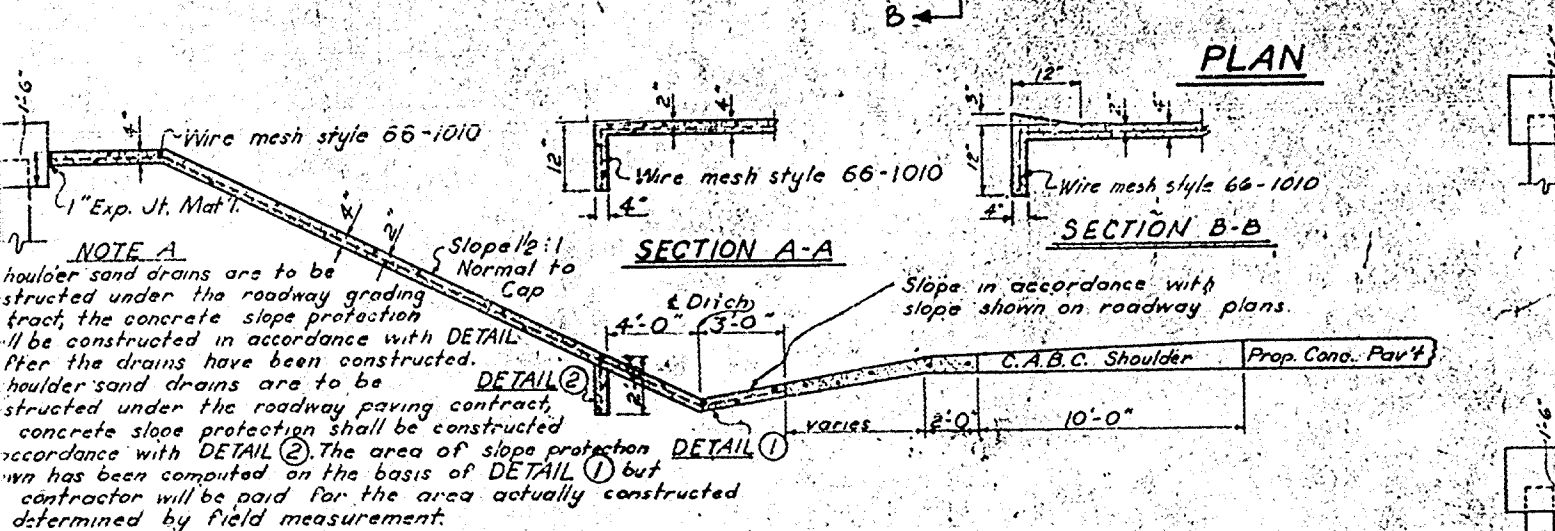
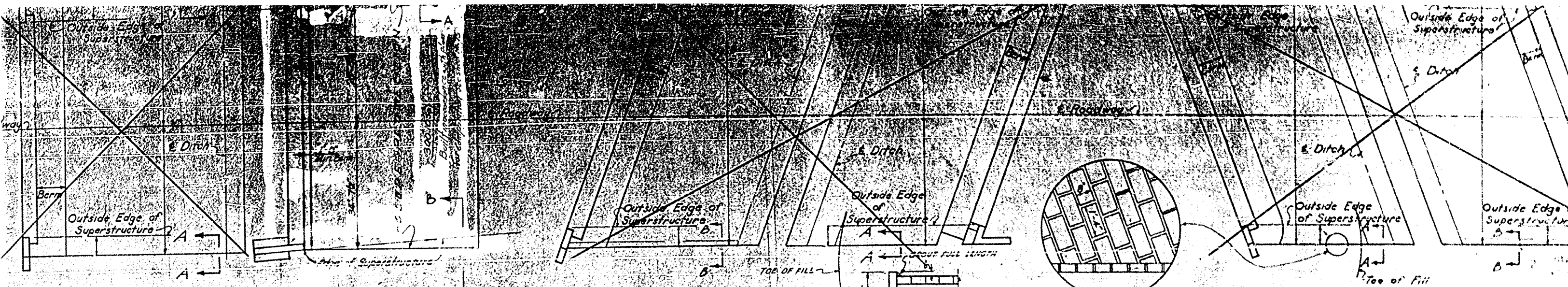
CLASS	AMOUNT	TOTAL
Post 1 - CAP, BRASS & FRG.	39.8	C.T.
Post 2 - FRG.	11.0	C.T.
Post 3 - EMB. POSTS (2)	6.0	C.T.
TOTALS	56.8	C.T.



NOTE:  
 1. The cap on the weather wing should be built to the same dimensions as the cap on the weather wing on the structure.  
 2. The cap on the weather wing should be built to the same dimensions as the cap on the structure.  
 For note regarding Method A waterproofing see Superstructure.

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DRAWN BY: G.P. MOSE DATE: 5/20/66  
 CHECKED BY: R.D. GRIFFIN DATE: 5/20/66



**DETAILS FOR ALTERNATE "A"**

**DETAILS FOR ALTERNATE "B"**

**NOTES:**

A 4" concrete slope protection paving shall be placed under the ends of the protection shall be as shown in the details. The contractor, at his option, either type "A" or "B", as described below immediately before his slope shall be properly shaped and firmly compacted so that it conforms to shown. The finished surface shall be reasonably smooth and uniform and lines, grades, and sections shown by more than 1/2" along a 10' straight edge.

**ALTERNATE "A"**

Alternate "A" shall consist of 4" poured in place concrete paving as shown. Concrete shall be Class B. The concrete surface shall be finished with a wire mesh.

The quantity to be paid for under this item shall be the number of square feet measured in place complete and accepted, including the area of the toe of protection. (For example 8" pay area for toe wall 1'-0" deep.)

The quantity measured as provided for above, shall be paid for at the contract yard for mesh excavation, backfilling, preparation of slopes, and all material tools and incidentals necessary to complete the work.

**ALTERNATE "B"**

Alternate "B" shall consist of solid concrete blocks 4" x 8" x 16" laid in horizontal in successive courses will break joints with units in the preceding one. Blocks with their long axis parallel to the end bent cap with graded joints preferred than 2" nor more than 1 1/2" wide between successive courses and ends of block by pouring a mixture of one part ball sand cement to three parts sand mixed to enable the mixture to be poured through a spout.

The concrete blocks shall be cast to accurate dimensions, shall have uniform and shall be manufactured of materials to produce a compressive strength of 2800 psi at age of 28 days. No broken blocks shall be used except in corners along each side of the paving down the slope. Care shall be taken to treat a uniform workmanlike joint and surface.

Method of measurement and basis of payment shall be as prescribed above.

**ALTERNATE "A"** wire mesh reinforcing to be style 66-1010 60" wide, mesh to lap at least 6". Slope Protection to be poured in alternate 4" x 3" Pouring Detail. The cost of wire mesh to be included in the contract unit yard for 4" concrete slope protection.

The same type of slope protection shall be used under both ends of any one bridge.

**PROJECT**  
**BULLCOMBE**  
**STATION:**

Res. No. 1. To remove that portion of the note covering for Standard 2.4.2. Number 3 Coarse aggregate, by N.H.S. 7-1-55, 2-5-55

SHEET 1 OF 2

BRIDGE @	S.Y.		Approx. L.F.
	E.B. 1	E.B. 2	
Sta. 117 + 53.8	285		370
Calculation by Sta. 117 + 53.8			
Checked by Sta. 117 + 53.8			

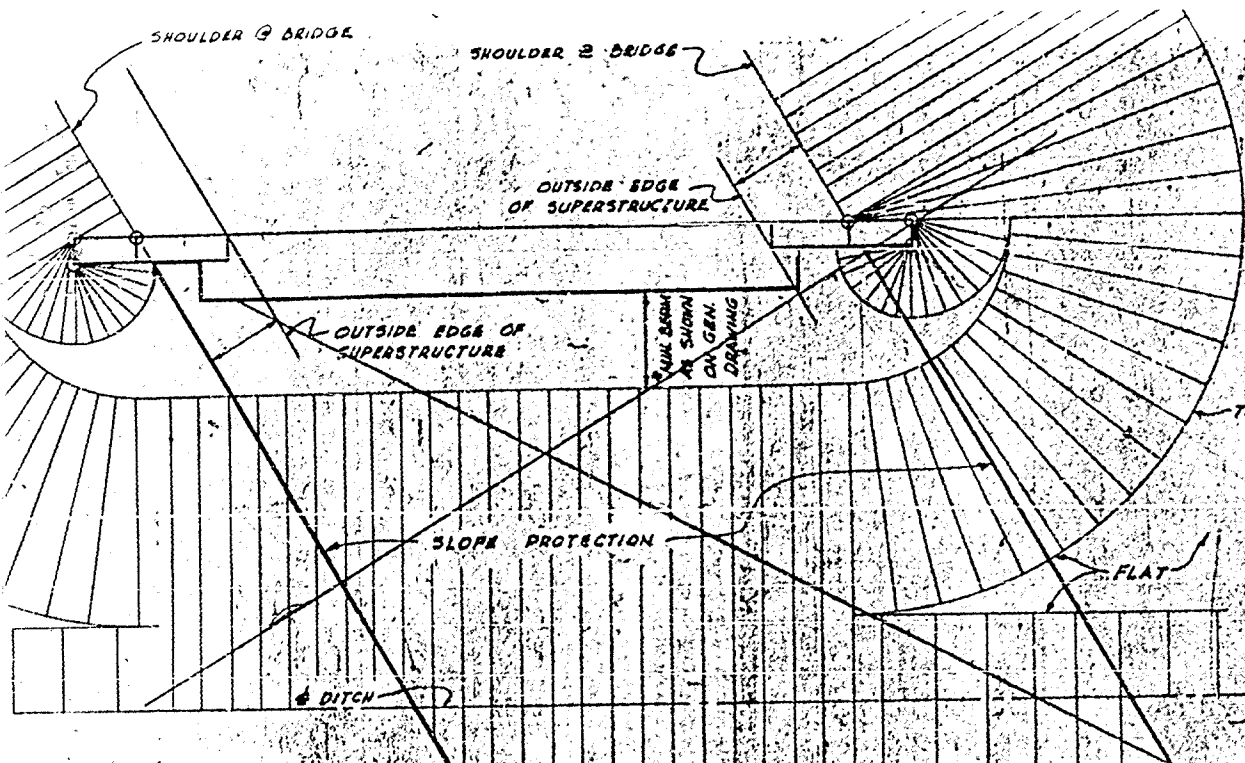
STATE OF NORTH CAROLINA  
STATE HIGHWAY  
STATION  
SLOPE PROTECTION  
March 50.2

BY C. D. MOSS, JR. DATE Sept. 26, 64  
BY B. P. JEWELL DATE Oct. 17, 66  
BY W. J. BALDWIN DATE Mar. 9, 64  
BY G. T. PHILLIPS DATE MAR. 10, 64

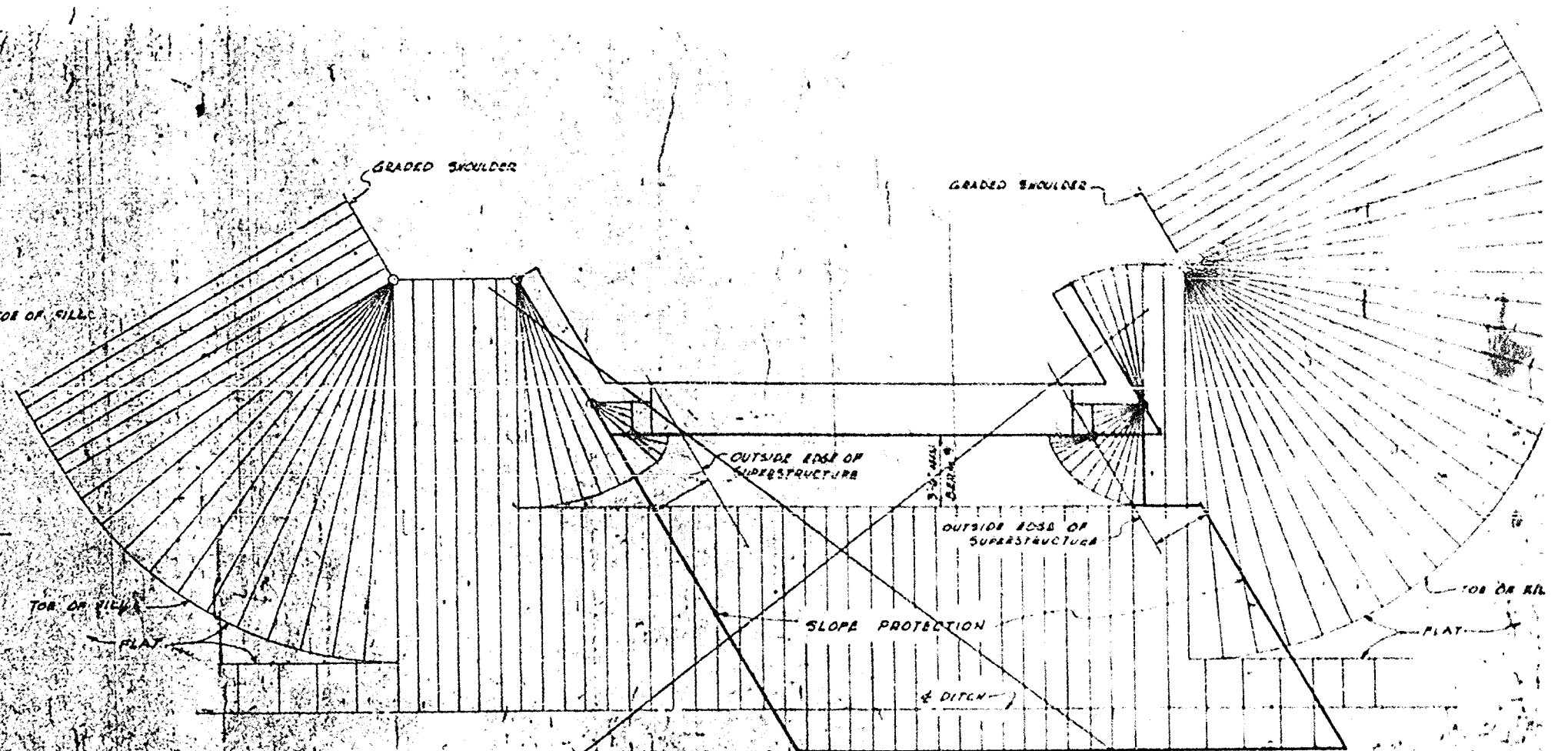
REVISIONS TO ELIMINATE 90° CORNER AT TOE OF SLOPE FOR EXCESS VERTICAL CURVES, AND TO ALLOW ALTERNATE "A" FOR CONSTRUCTION JTS. A BENT COLUMN.

PLAN WHERE CONC. OR CONC. BLOCK SLOPE PROTECT. MUST BE PLACED AROUND A BENT COLUMN.

REVISIONS TO TAKE OUT DIMENSIONS FROM OUTSIDE EDGE OF SUPERSTRUCTURE TO OUTSIDE SLOPE PROTECTION. REVISIONS TO SHOW CORNER 1'-0" ABOVE TOE OF CAP & BERM.

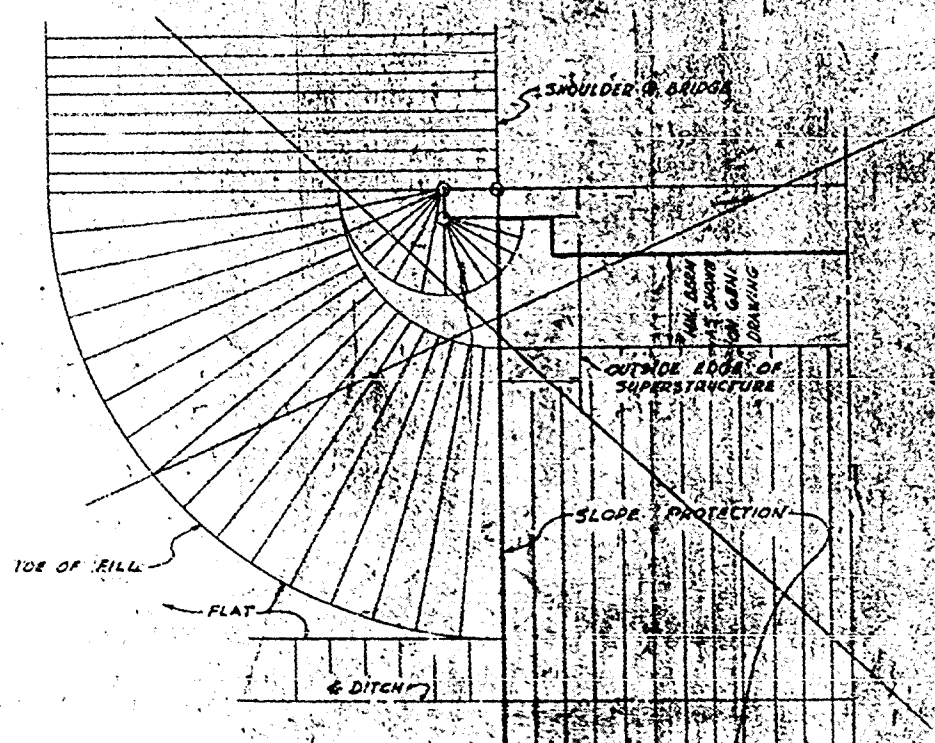


END BENT WITH EAR WALLS - SKEWED



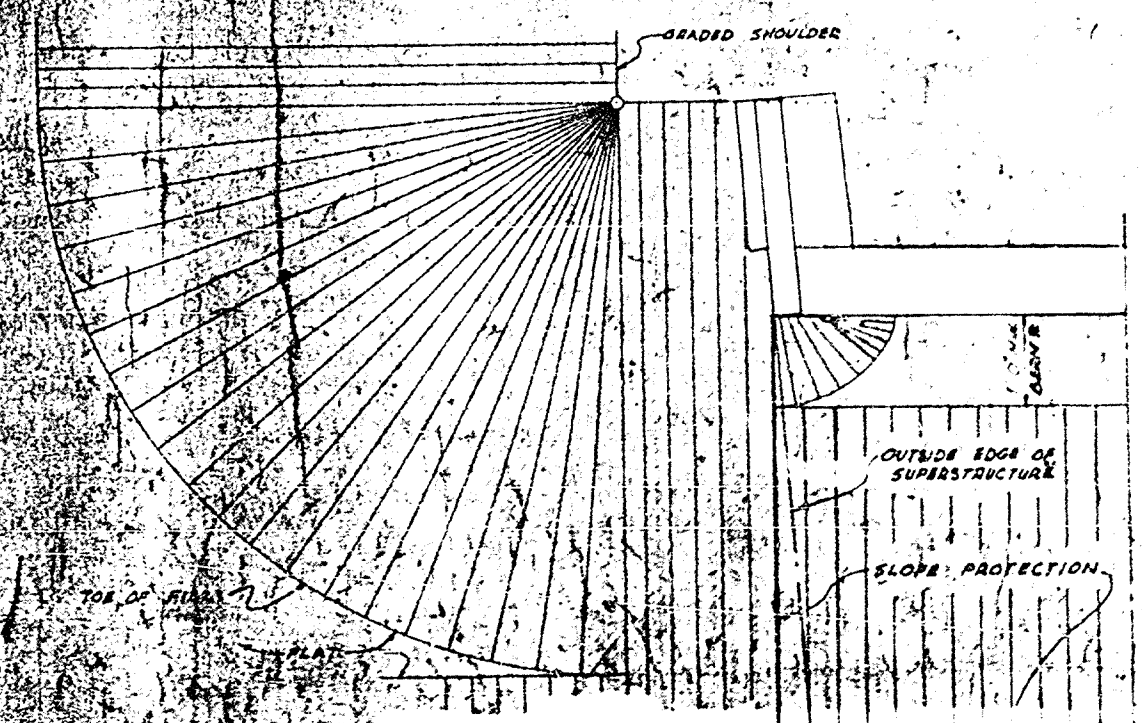
END BENT WITH SWEEP BACK WINGS - SKEWED

\* NOTE VARY BERM WIDTH AS NECESSARY TO FIT DITCH ALIGNMENT.



HALF PLAN END BENT WITH EAR WALLS - 90°

NOTE: OTHER SIDE SIMILAR.



HALF PLAN END BENT WITH SWEEP BACK WINGS - 90°

NOTE: OTHER WINGS @ 90° SLOPE PROTECT. SIMILAR AS SHOWN

PROJECT NO. B 30132  
 BUNCOMBE COUNTY  
 STATION: 117+53.43

SHEET 2 OF 2

STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISSION  
 BALDWIN  
 STANDARD  
 SLOPE PROTECTION PAV  
 DETAILS  
 FEBRUARY 1964 51069

DATE OCT 1964  
 DATE OCT 1964  
 DATE FEB 64  
 DATE MARCH 64

REV. NO. 2 TO ELIMINATE 90° CORNER AT TOE OF SLOPE FOR EXTENDED LENGTHS OF  
 REV. NO. 1 TO TAKE CUT DIMENSIONS FROM OUTSIDE EDGE OF SUPERSTRUCTURE TO OUTSIDE SLOPE PROTECT.

UNCOMBE COUNTY  
 PROJECT 8.3023209  
 FEDERAL AID  
 1968

SECTION SHOWING CORRECT WORDING  
 NAME PLATE USING FOUR LINES

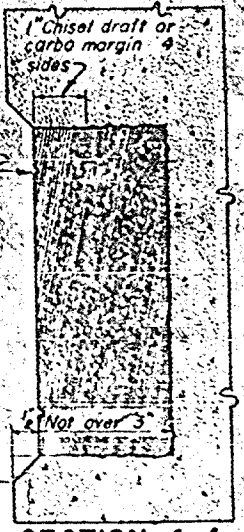
SECTION SHOWING CORRECT WORDING  
 NAME PLATE USING THREE LINES

**GENERAL NOTES**

1. There will be two-way traffic, two name plates for each bridge. For bridges on which one-way traffic, one name plate is required for each side. Name plates to be placed on the right hand end post of bridge, and shall be placed parallel to grade as shown in SECTION A-A.

2. Name plates to be made of granite. Granite shall be light colored, sound in quality and free from cracks, stains or other defects. Exposed face of plates shall be finished with a sandblast finish.

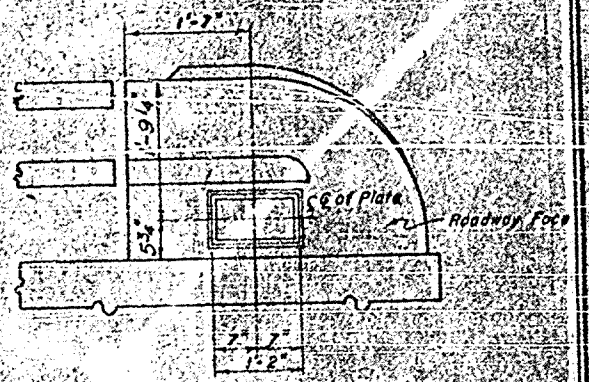
3. Name plates, complete in place, shall be included in the unit price bid for Class "A" Concrete.



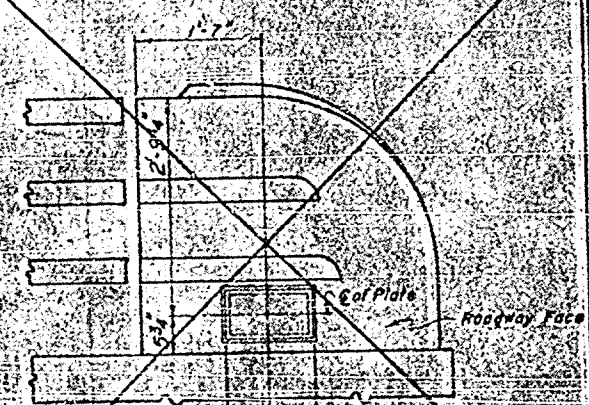
SECTION A-A

**NAME PLATES**

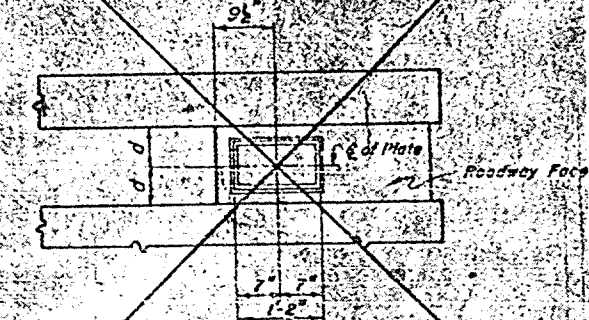
FOR ONE BAR METAL RAIL



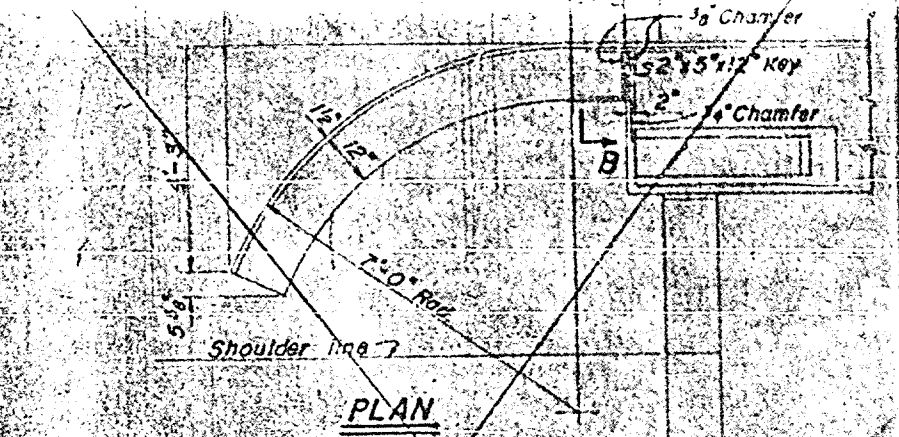
FOR TWO BAR METAL RAIL



FOR THREE BAR METAL RAIL

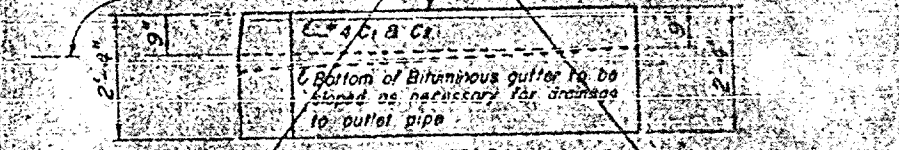


FOR CONCRETE BAR TYPE RAIL



PLAN

Approach curb to be constructed on vertical curve or grade conforming to that of Bridge curb.

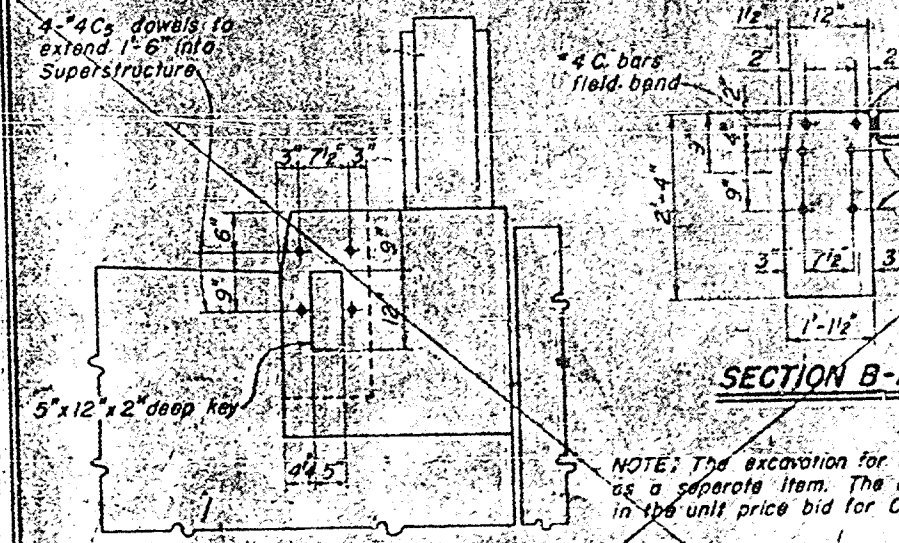


ELEVATION

BILL OF MATERIAL FOR 4" CURBS

Bar No.	Size	Length	Weight
C1	4" x 4"	7'-5"	21
C2	4" x 4"	8'-6"	23
C3	16" x 4"	9'-0"	32
Reinforcing Steel			76 Lbs.
Class "A" Concrete			3.2 CY.

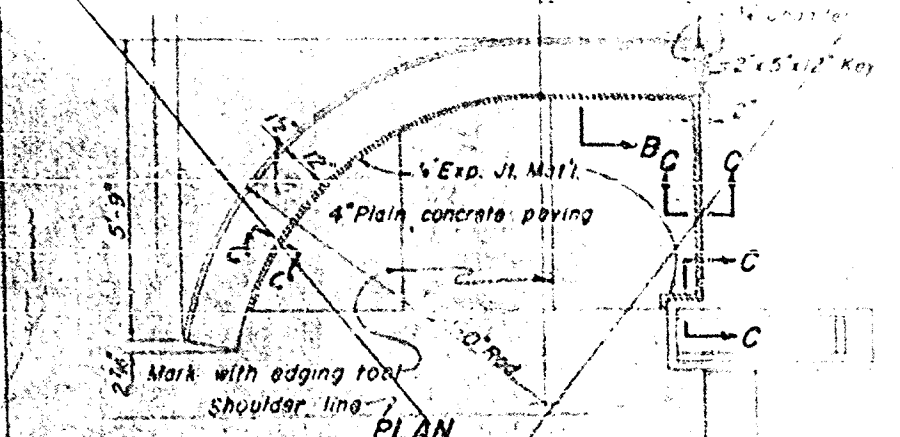
TO BE USED WITH 18" CURBS



END ELEVATION SHOWING KEY IN SUPERSTRUCTURE

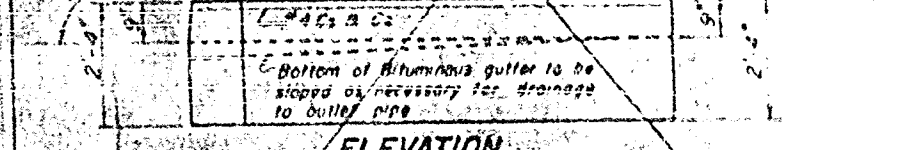
SUPERSTRUCTURE

**APPROACH CURBS**



PLAN

Approach curb to be constructed on vertical curve or grade conforming to that of Bridge curb.



ELEVATION

BILL OF MATERIAL FOR 4" CURBS

Bar No.	Size	Length	Weight
C1	4" x 4"	11'-0"	32
C2	4" x 4"	12'-0"	36
C3	16" x 4"	13'-0"	40
Reinforcing Steel			108 Lbs.
Class "A" Concrete			4.0 CY.

TO BE USED WITH 3' & 5' WALKS

SECTION C-C

PROJECT No. 8.3023.0  
 UNCOMBE COUNTY  
 STATION: 1176.5

STATE OF NORTH CAROLINA  
 STATE HIGHWAY COMMISSION  
 STANDARD  
 NAME PLATES  
 AND  
 APPROACH CURBS  
 AUGUST 1965