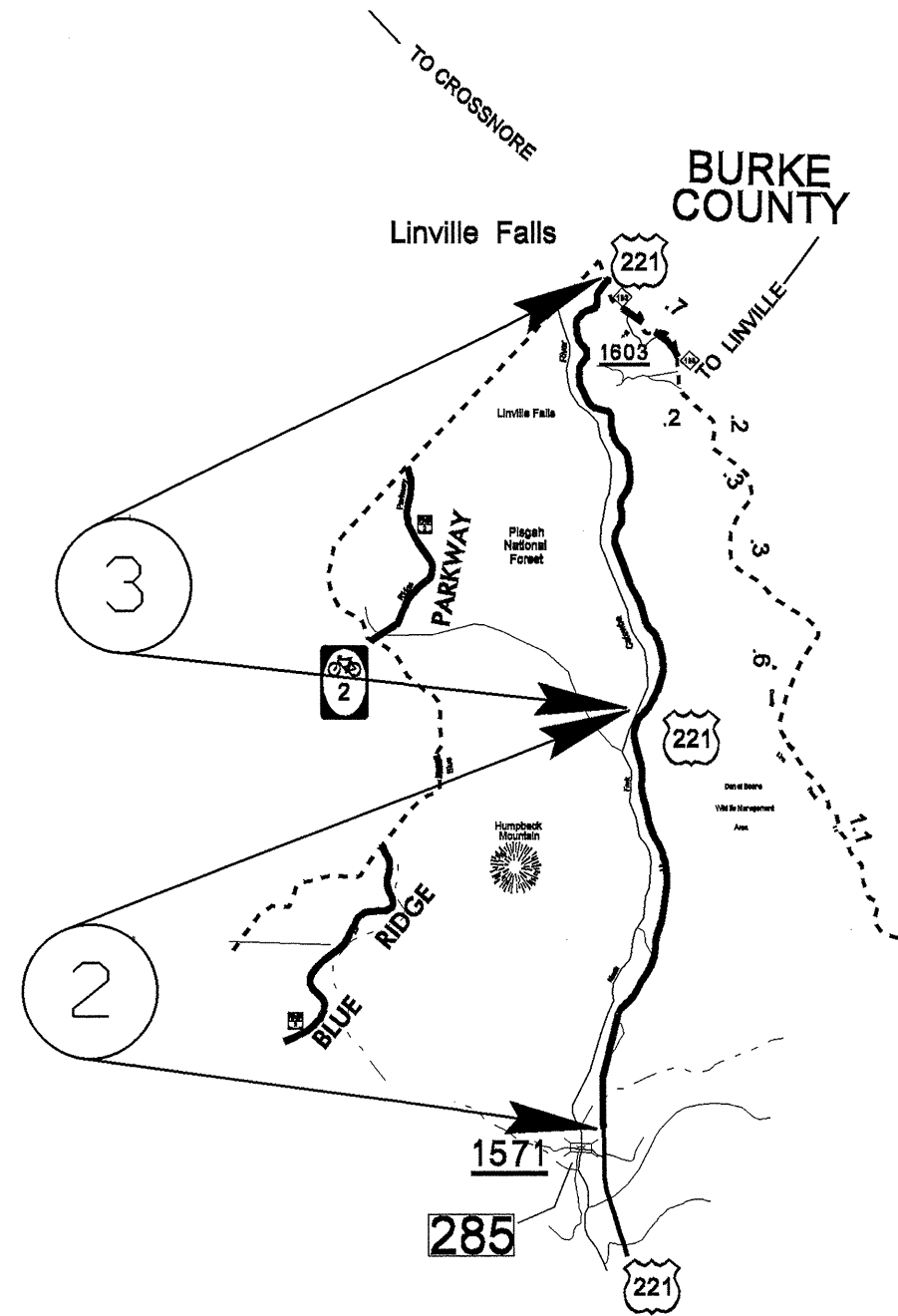
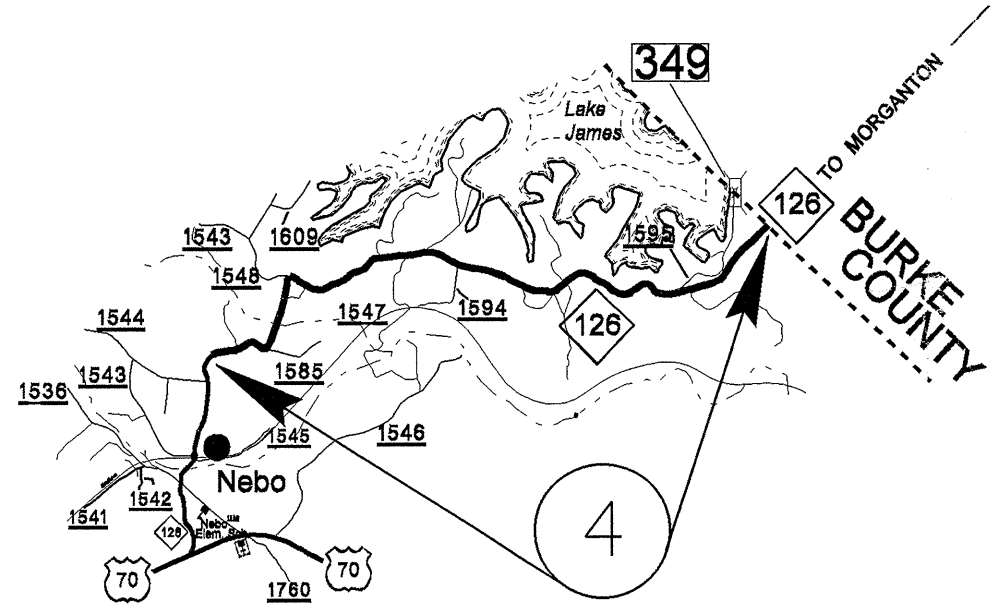
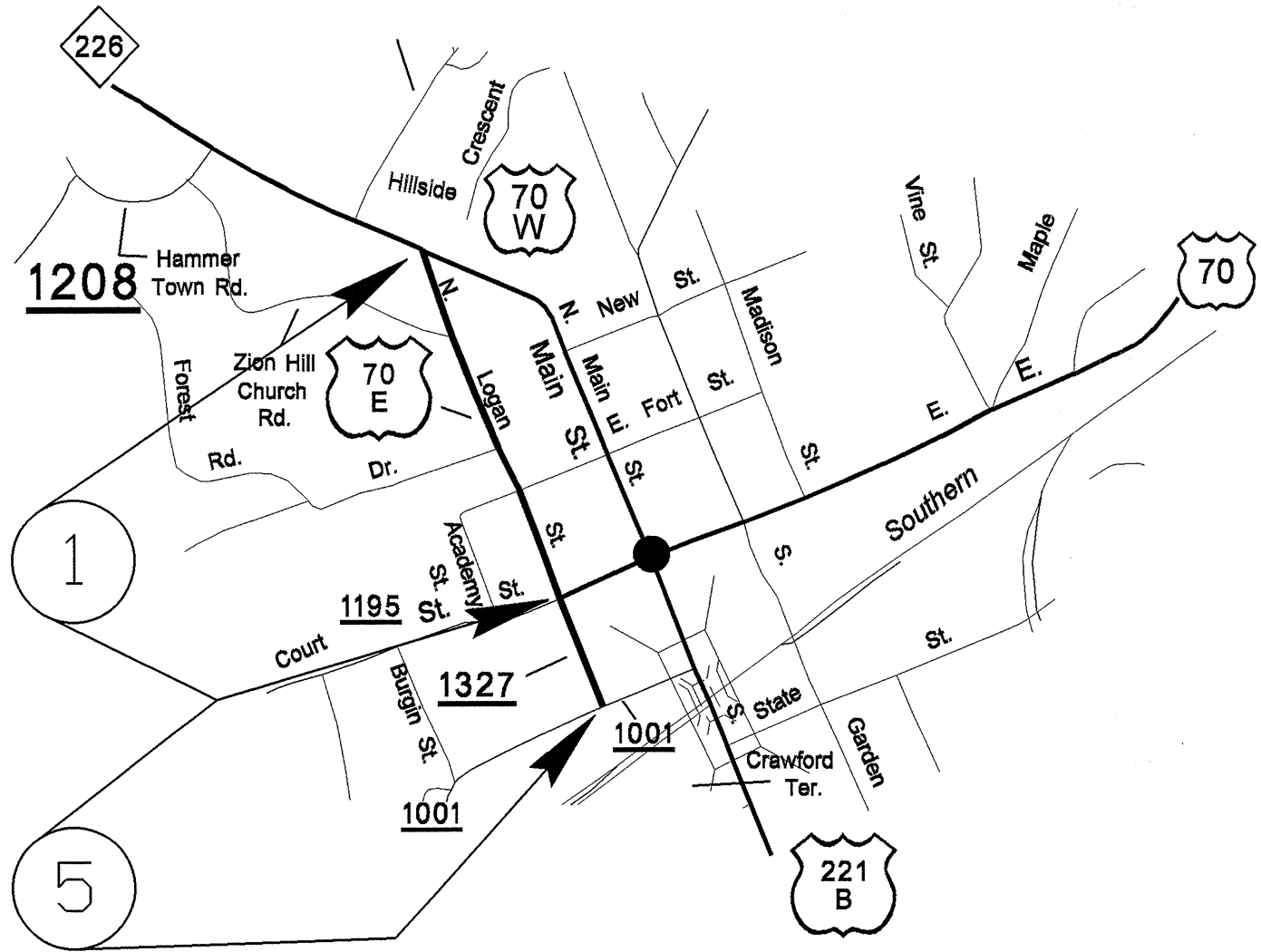
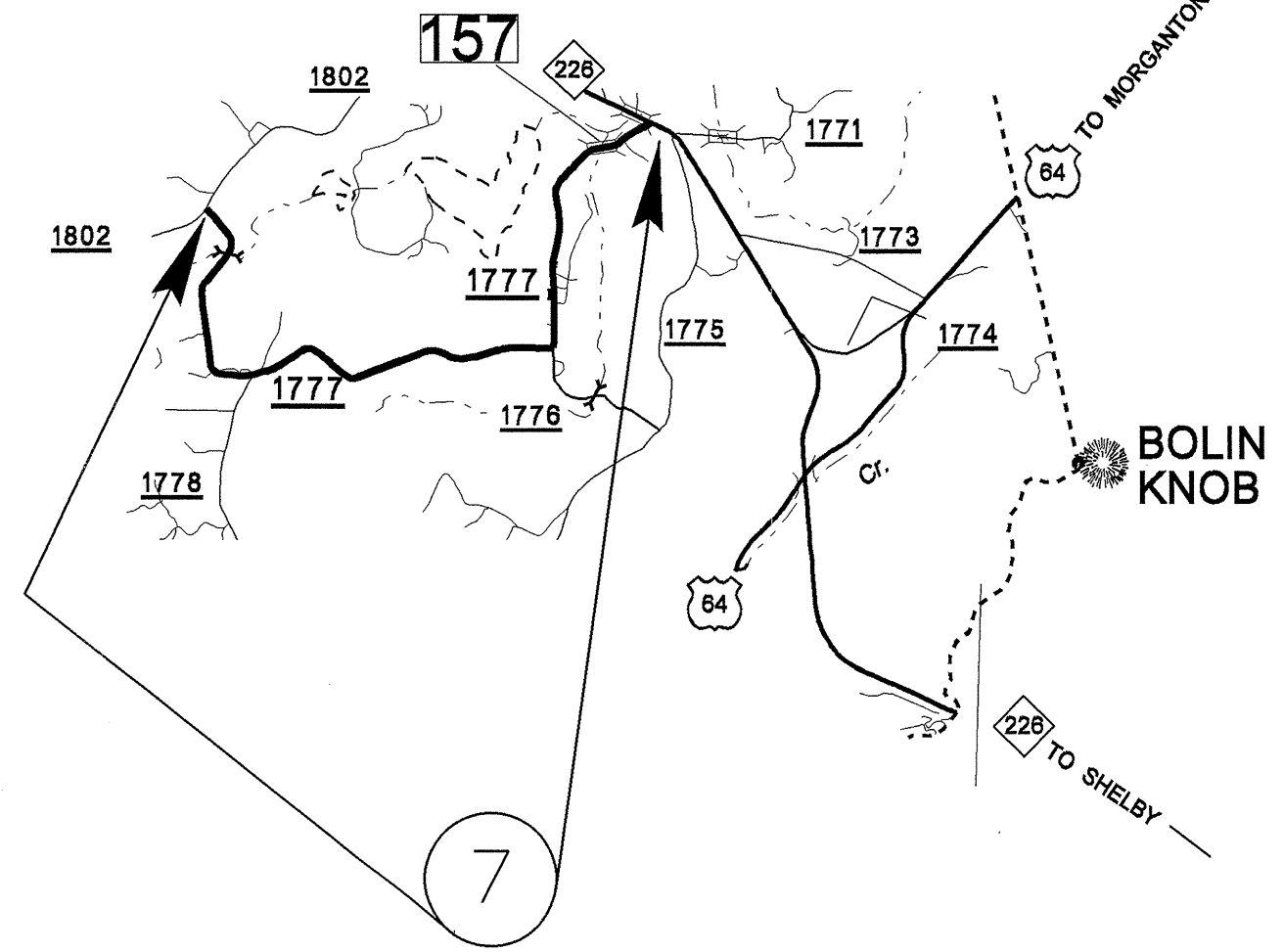
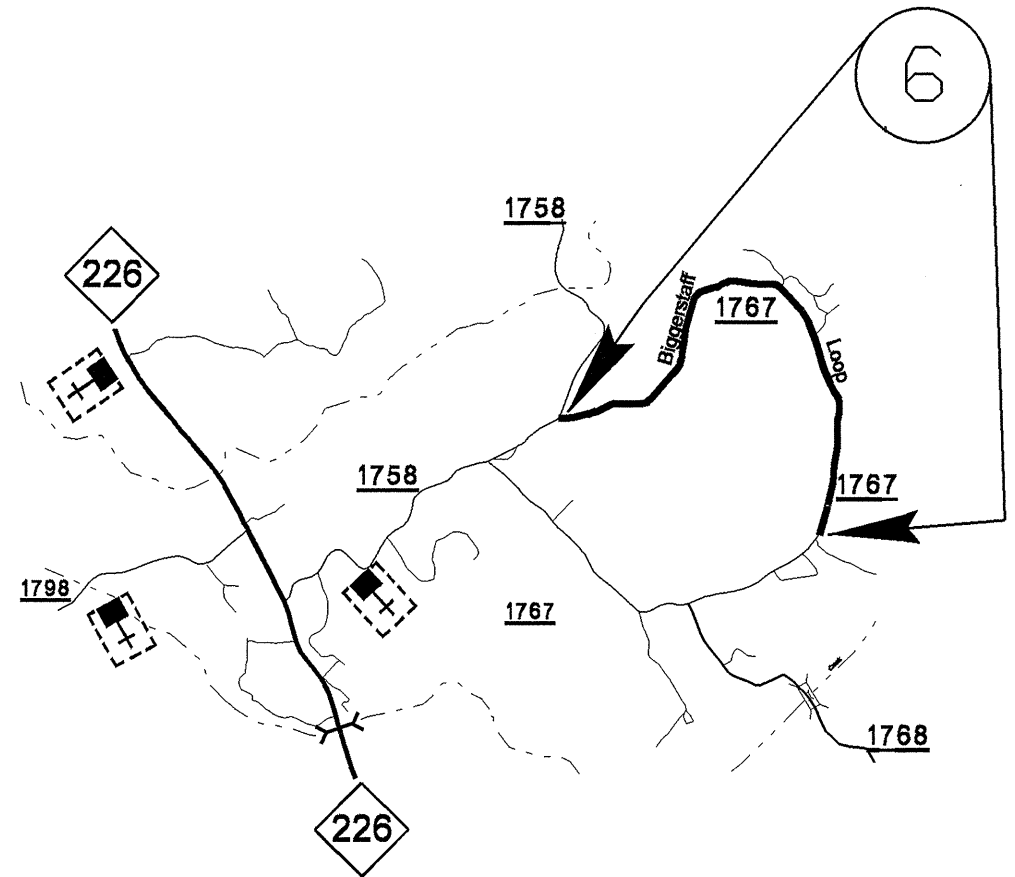


PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10 13CR.20591.9	1	13



**McDOWELL COUNTY**      **BURKE COUNTY**

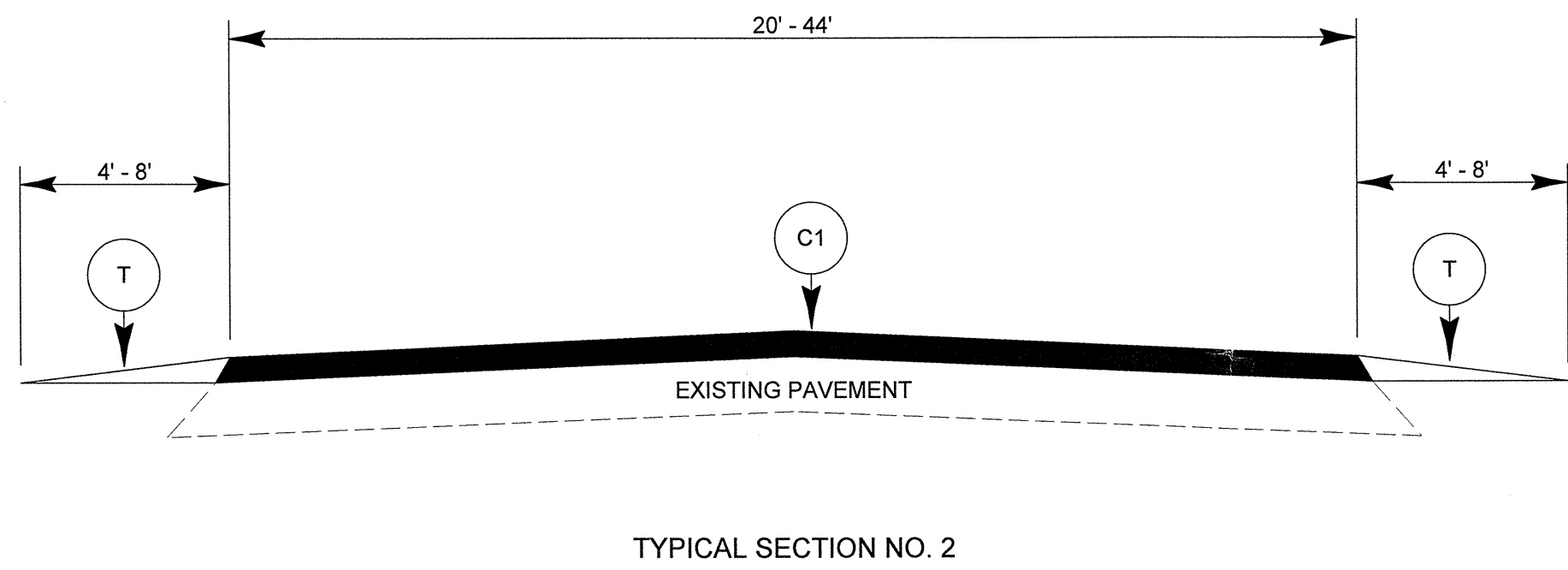
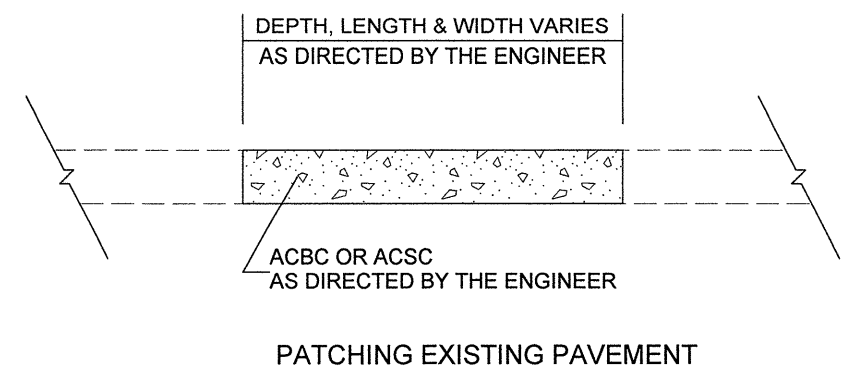
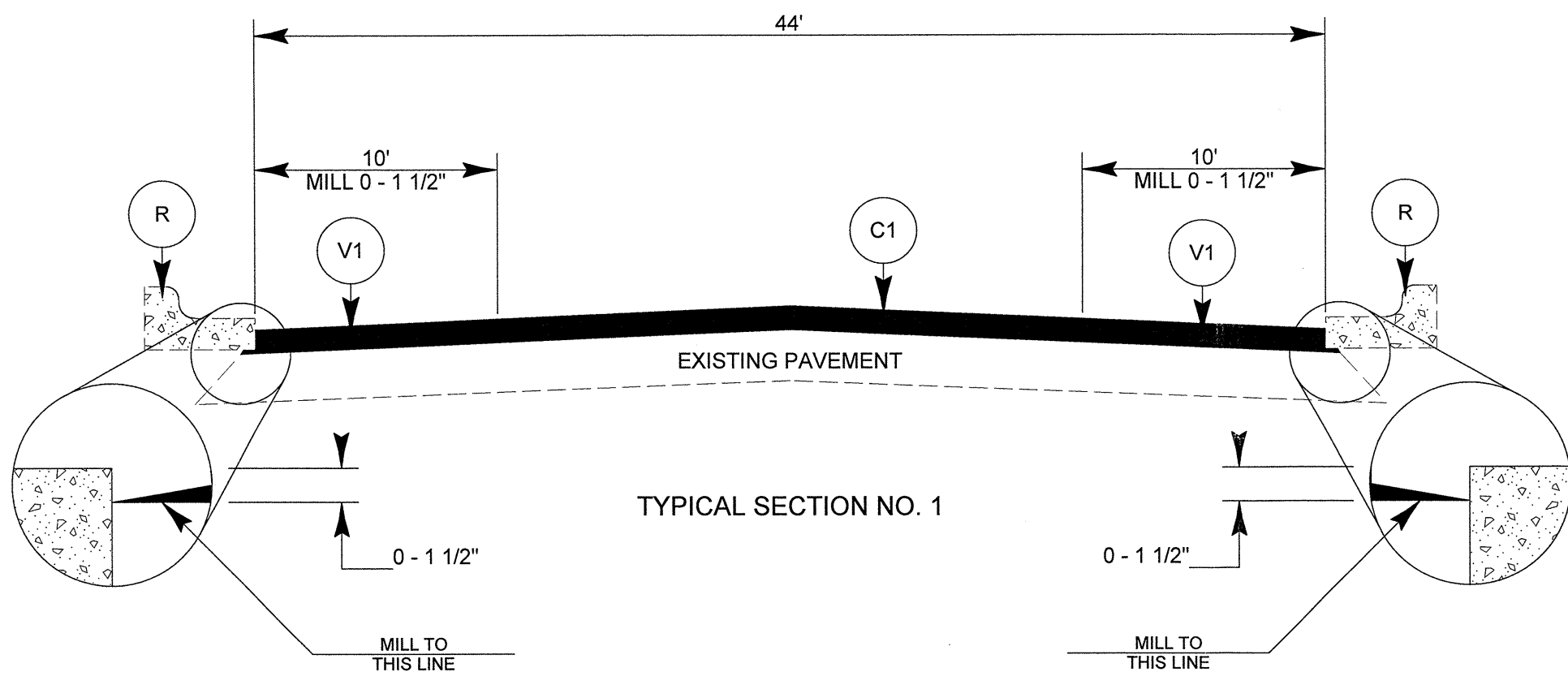
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13CR.10591.10 13CR.20591.9	2	13



**McDOWELL COUNTY**

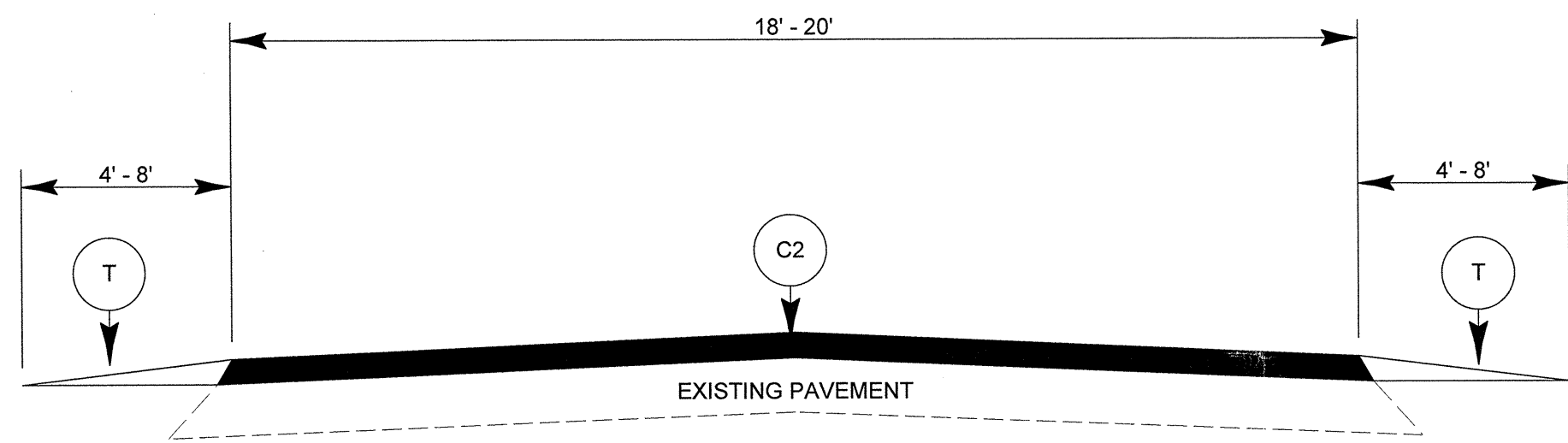
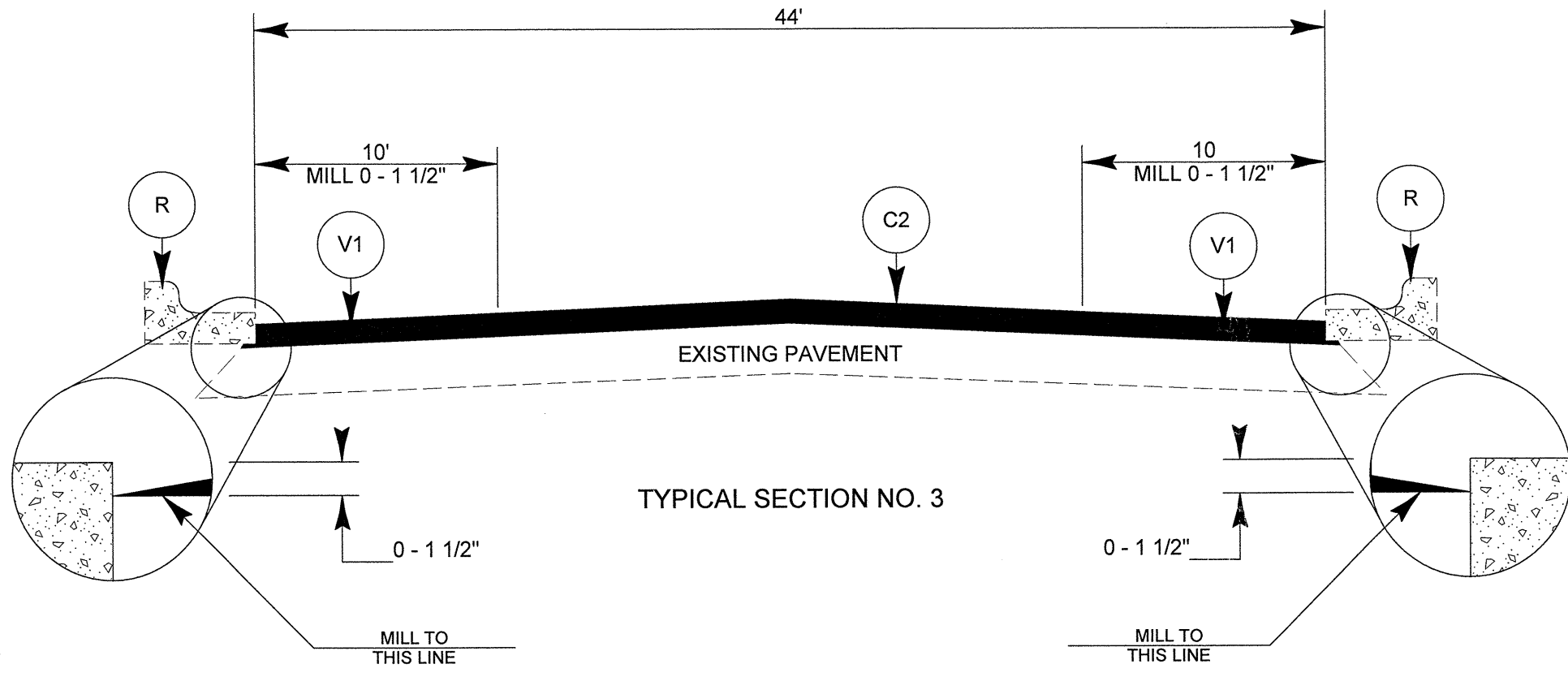


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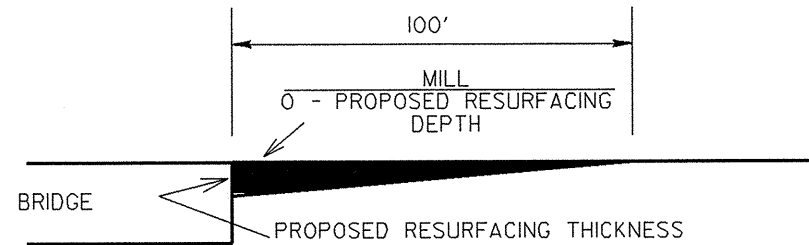
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
R	EXISTING CONCRETE CURB OR CONCRETE ISLAND
T	SHOULDER RECONSTRUCTION
V1	MILL 0 - 1 1/2"
V2	MILL 1 1/2"

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10, 13CR.20591.9	5	13

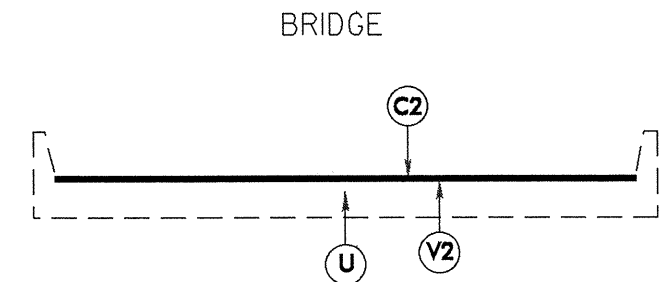


# McDOWELL COUNTY

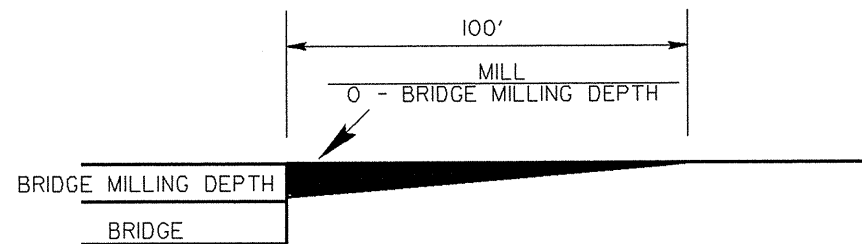
<b>PROJECT NO.</b>	<b>SHEET NO.</b>	<b>TOTAL SHEETS</b>
13CR.10591.10 & 13CR.20591.9	6	13



**MILLING DETAIL AT BRIDGE APPROACHES**  
**WHERE BRIDGES WILL NOT BE RESURFACED**  
**COST OF MILLING IS INCIDENTAL TO OTHER ITEMS**



**BRIDGE DETAIL**  
**BRIDGE # 157 MAP # 7**  
**MILL 1½" OFF EXISTING PAVEMENT**  
**SEE MAPS FOR BRIDGE LOCATION**

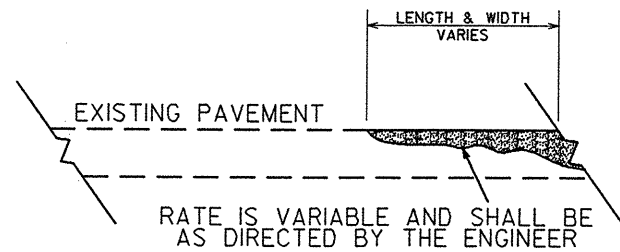
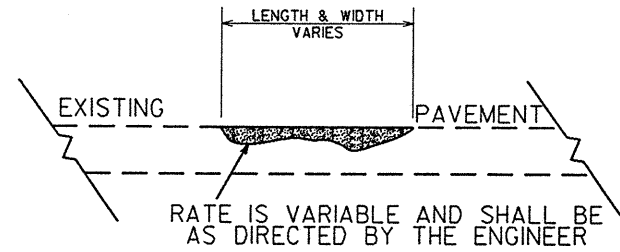


**MILLING DETAIL AT BRIDGE APPROACHES**  
**WHERE BRIDGES WILL BE MILLED THEN RESURFACED**  
**COST OF MILLING IS INCIDENTAL TO OTHER ITEMS**

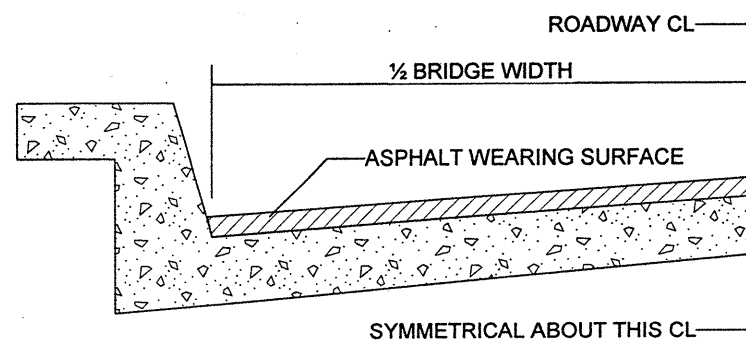
PAVEMENT SCHEDULE	
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.6A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
U	EXISTING PAVEMENT
V2	MILL 1½"

# McDOWELL COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10 & 13CR.20591.9	7	13



## DETAIL SHOWING METHOD OF WEDGING



### BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1/2", SF9.5A 1.0", S9.5X 1.5", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/2", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2". THE MAXIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1.0", SF9.5A 1.5", S9.5X 2.0", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/2", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2".

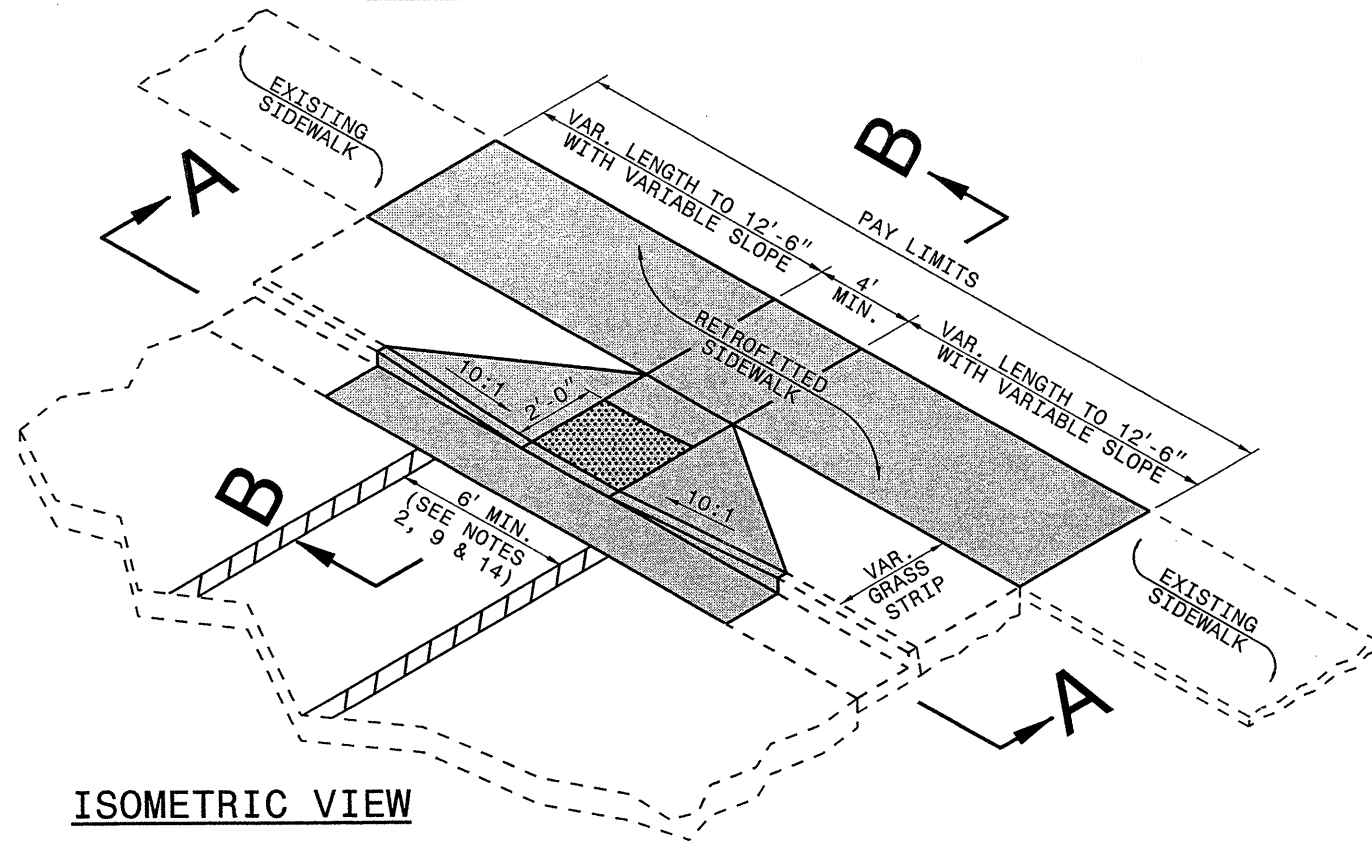
### NOTES

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.  
 ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.  
 EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.  
 SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.  
 BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

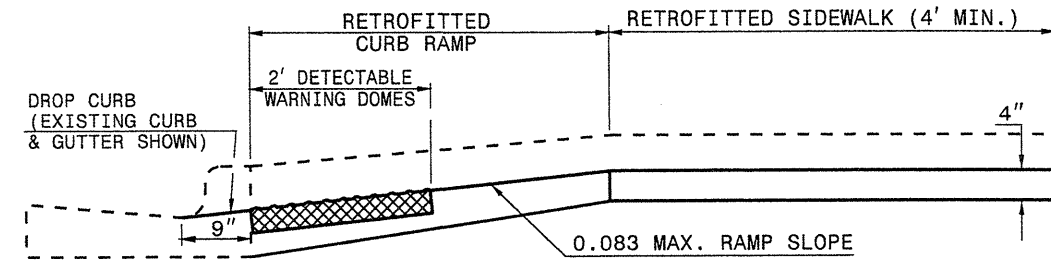
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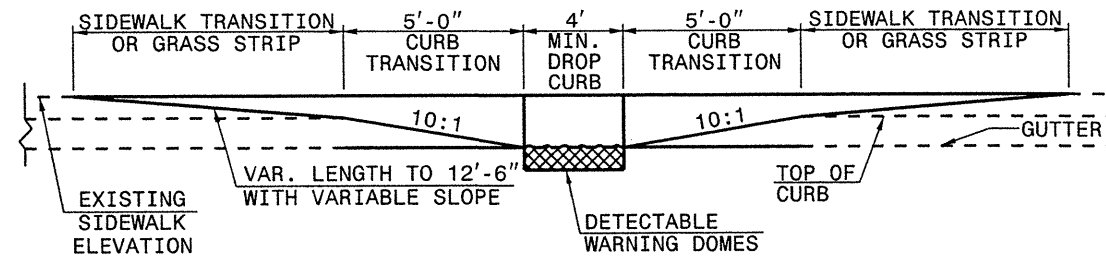
**CURB RAMP AND EXISTING SIDEWALK WITH GRASS STRIP**



**ISOMETRIC VIEW**

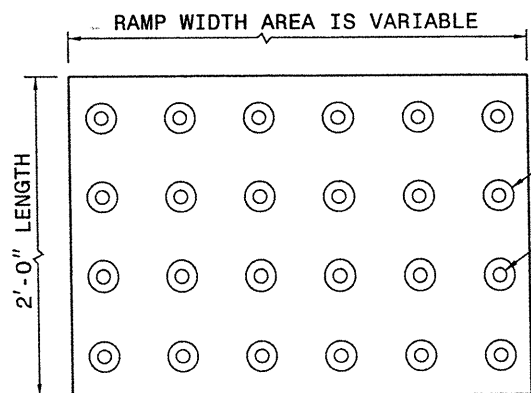


**SECTION B-B**

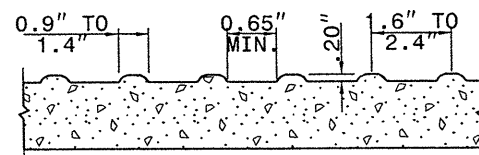


**SECTION A-A**

PAY LIMITS OF RETROFIT CURB RAMP

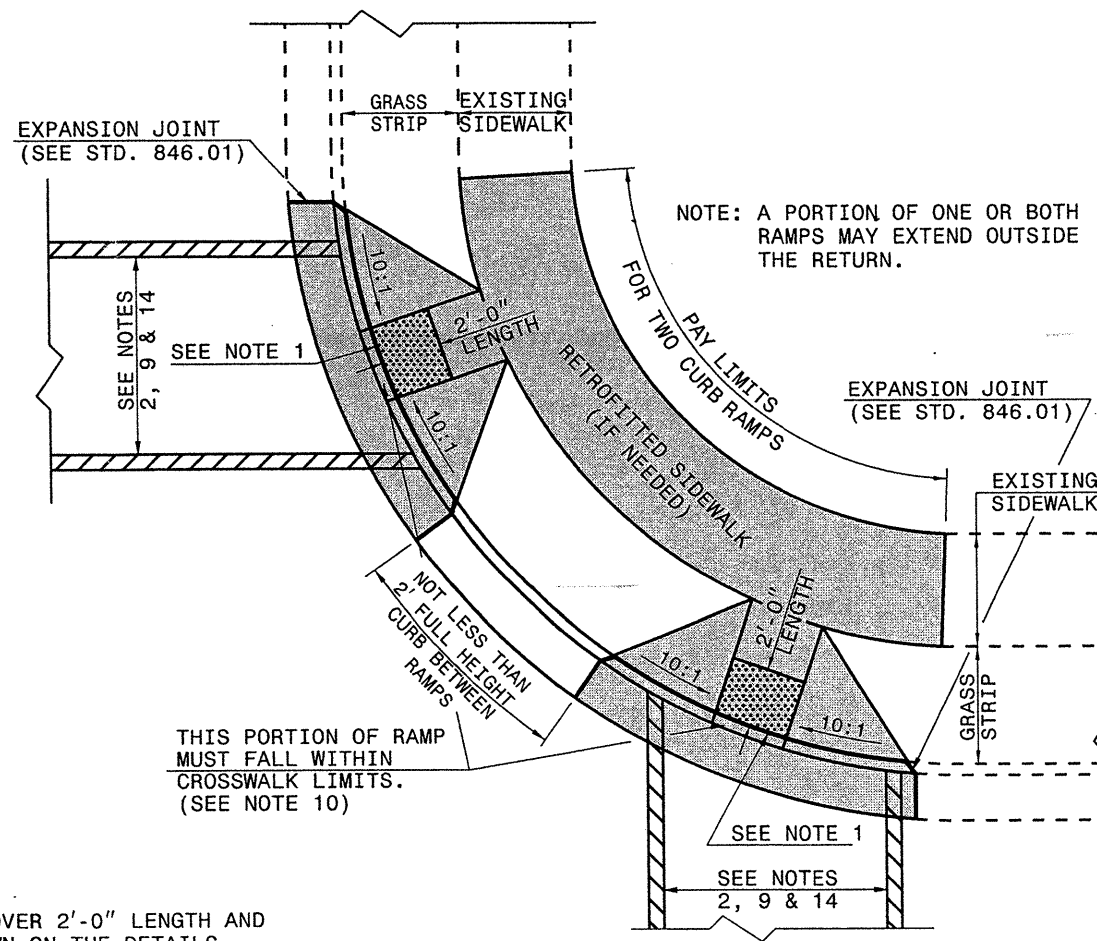


BASE DIAMETER  
0.90\"R TO 1.40\"R  
TOP DIAMETER OF NO LESS  
THAN 50% TO NO MORE  
THAN 65% OF THE BASE  
DIAMETER



**DETECTABLE WARNING DOMES**

- NOTES:
1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
  2. OBTAIN VISIBLE CONTRAST WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



**PLAN VIEW**

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

EXISTING CURB AND GUTTER  
**CURB RAMP**  
ENGLISH DETAIL DRAWING FOR

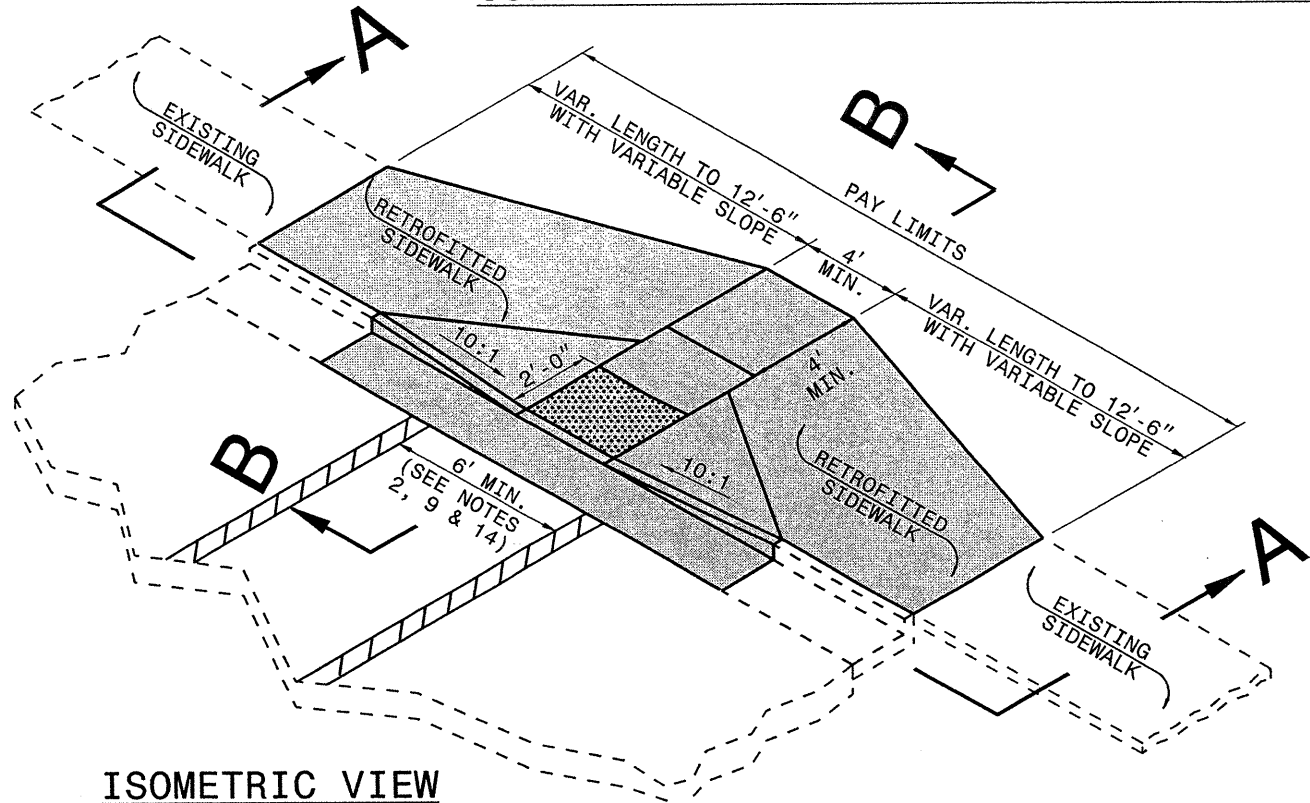
ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER



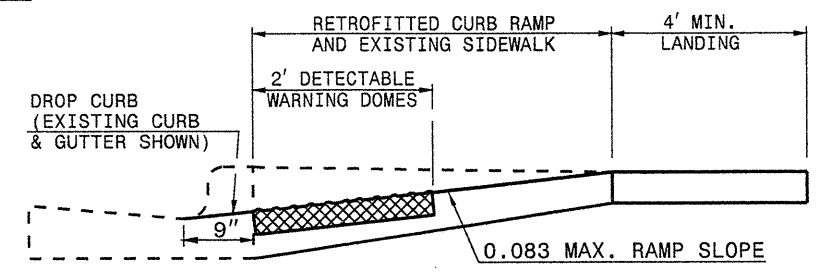
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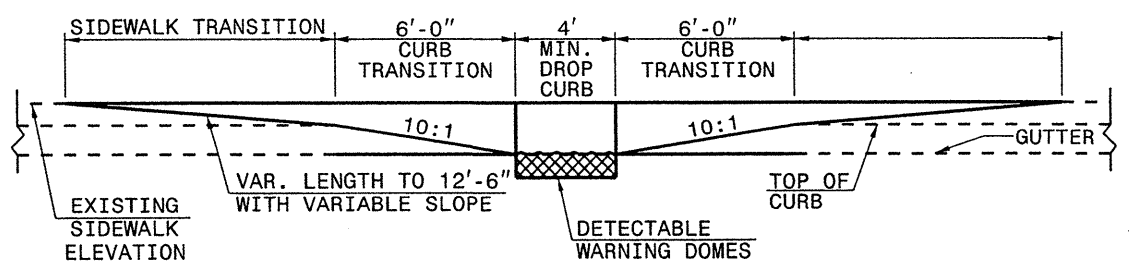
**CURB RAMPS AND EXISTING SIDEWALK ADJACENT TO CURB**



**ISOMETRIC VIEW**



**SECTION B-B**

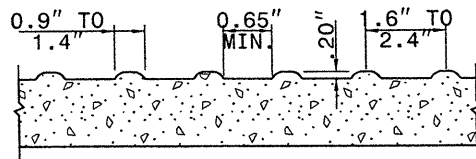
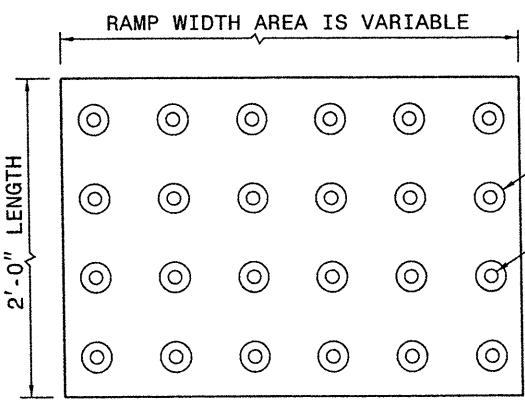


**SECTION A-A**

EXISTING CURB AND GUTTER

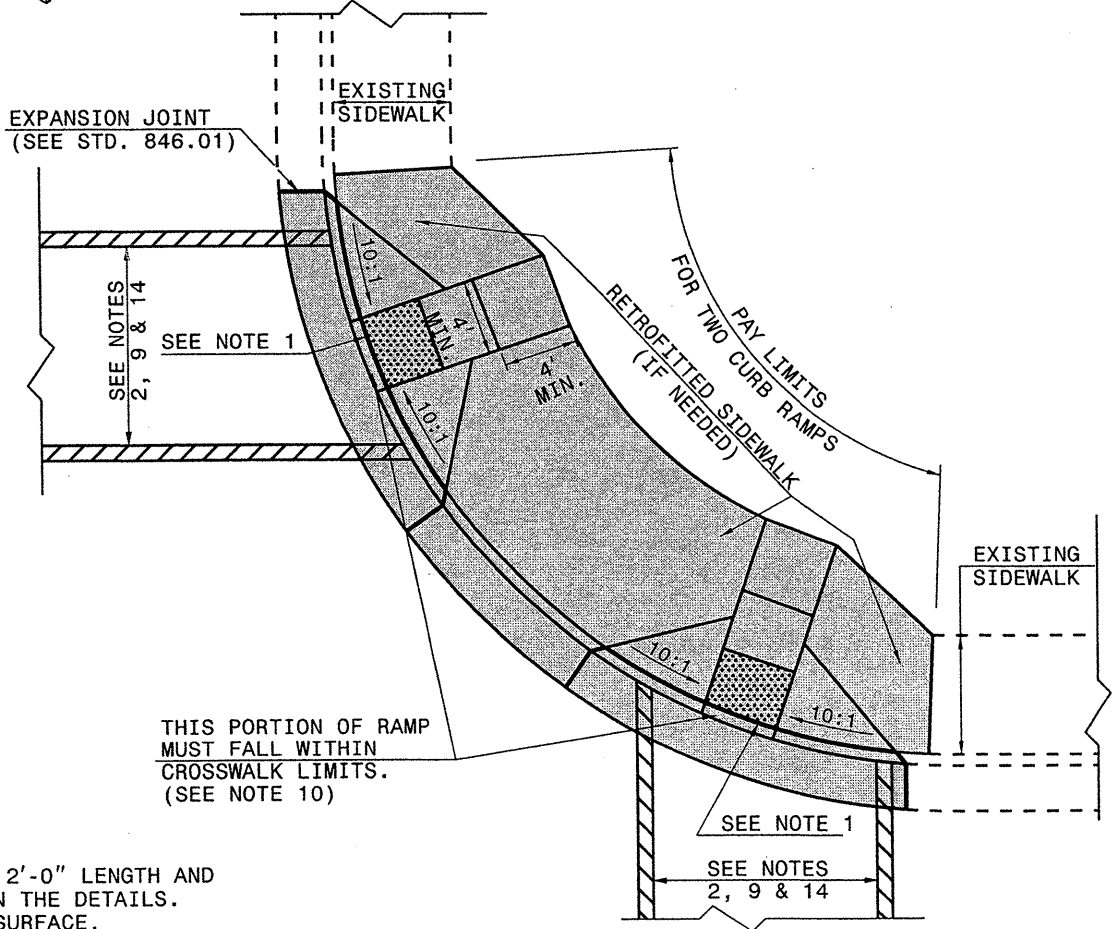
EXISTING CURB AND GUTTER

PAY LIMITS OF CURB RAMP



**DETECTABLE WARNING DOMES**

- NOTES:
1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
  2. OBTAIN VISIBLE CONTRAST WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



**PLAN VIEW**

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

ENGLISH DETAIL DRAWING FOR

**CURB RAMP**

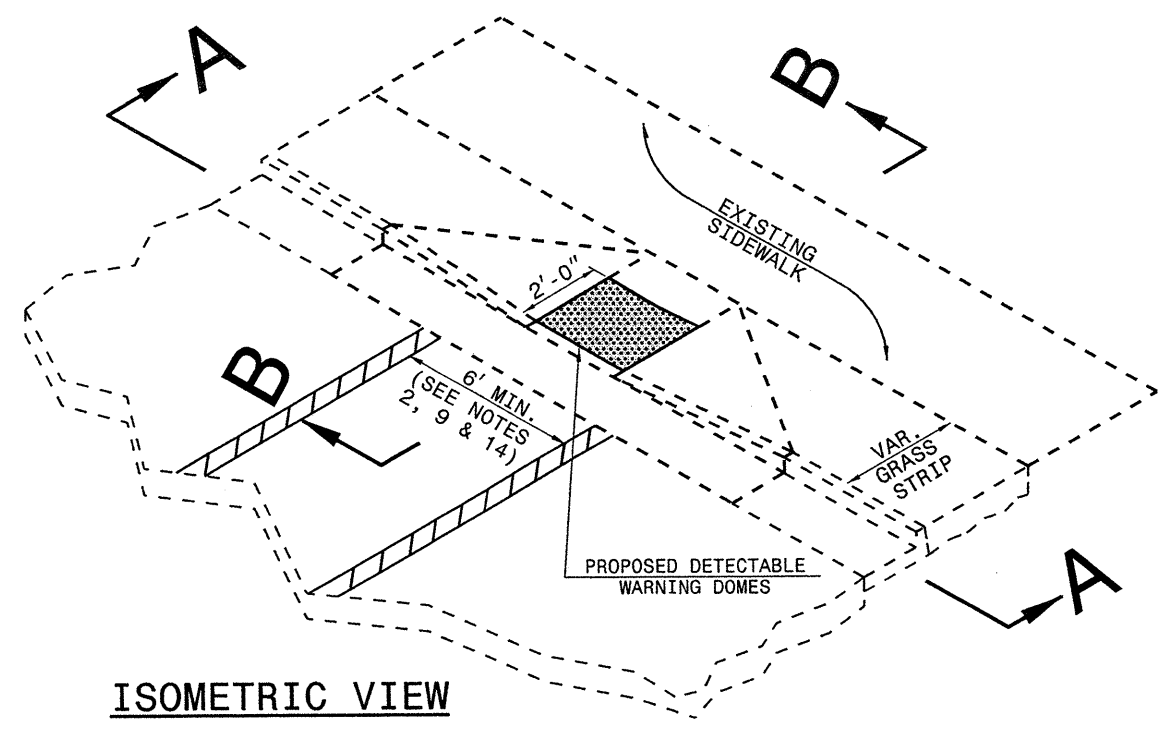
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**RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING CURB RAMP**

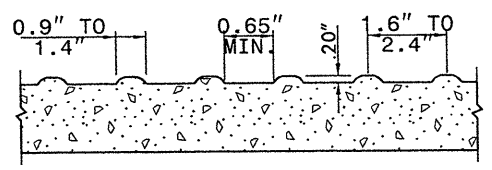
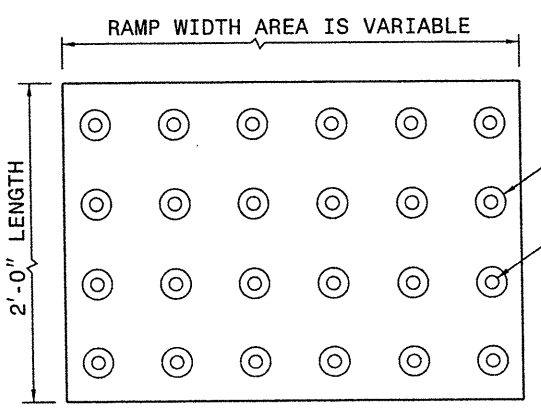
ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER



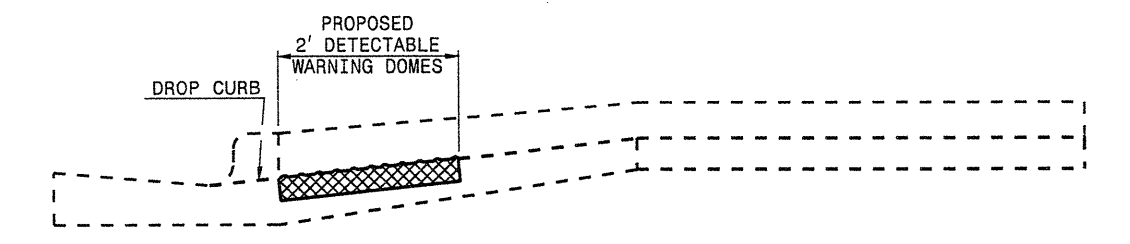
**ISOMETRIC VIEW**

PAY LIMITS OF RETROFIT CURB RAMP

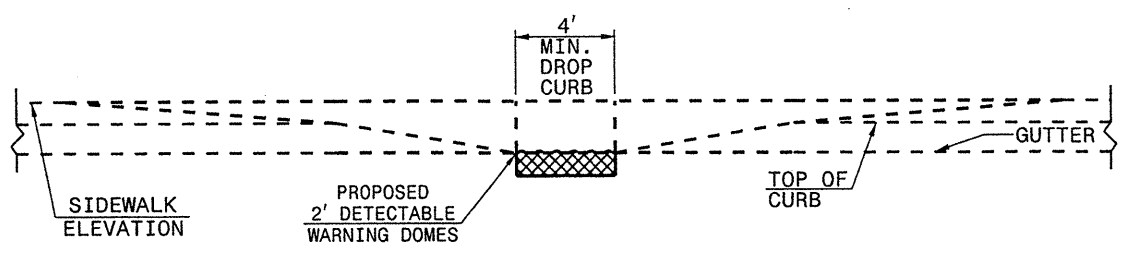


**DETECTABLE WARNING DOMES**

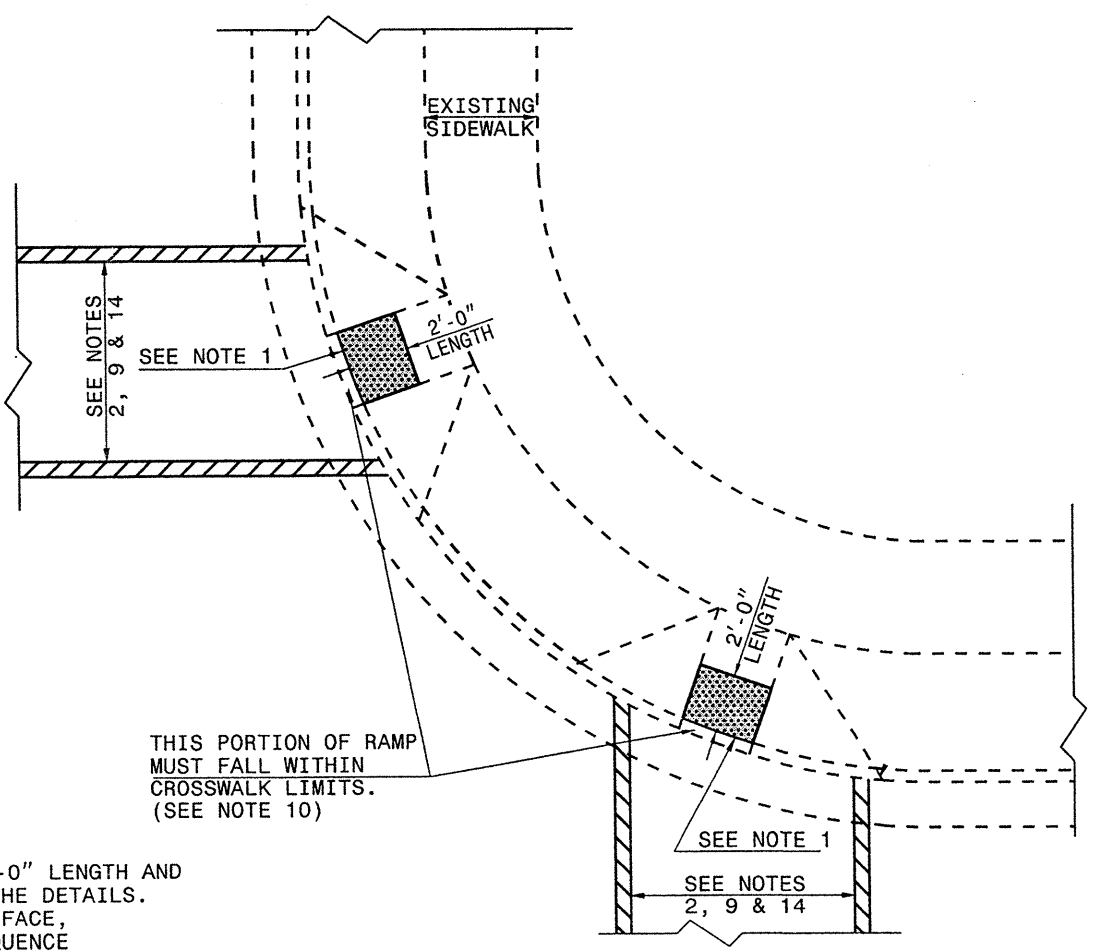
- NOTES:
1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
  2. OBTAIN VISIBLE CONTRAST WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



**SECTION B-B**



**SECTION A-A**



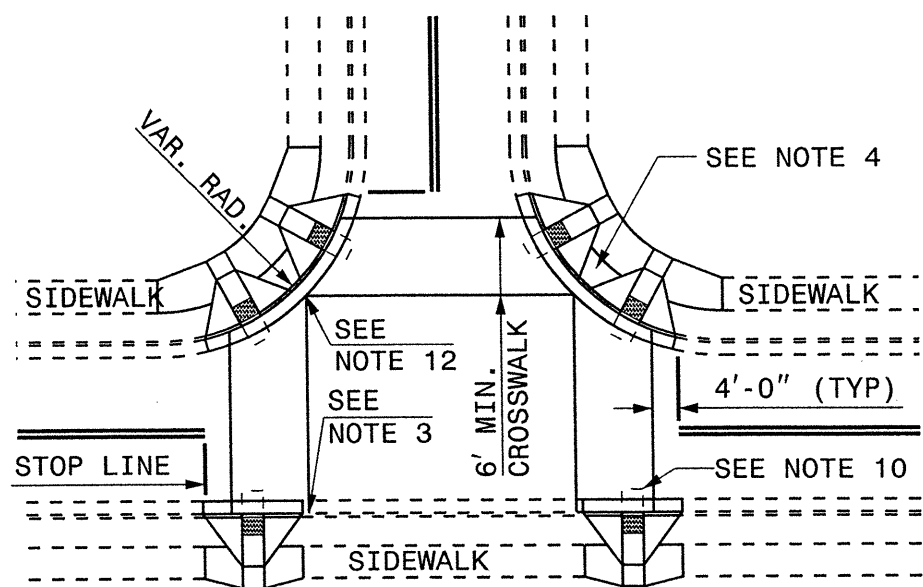
**PLAN VIEW**

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

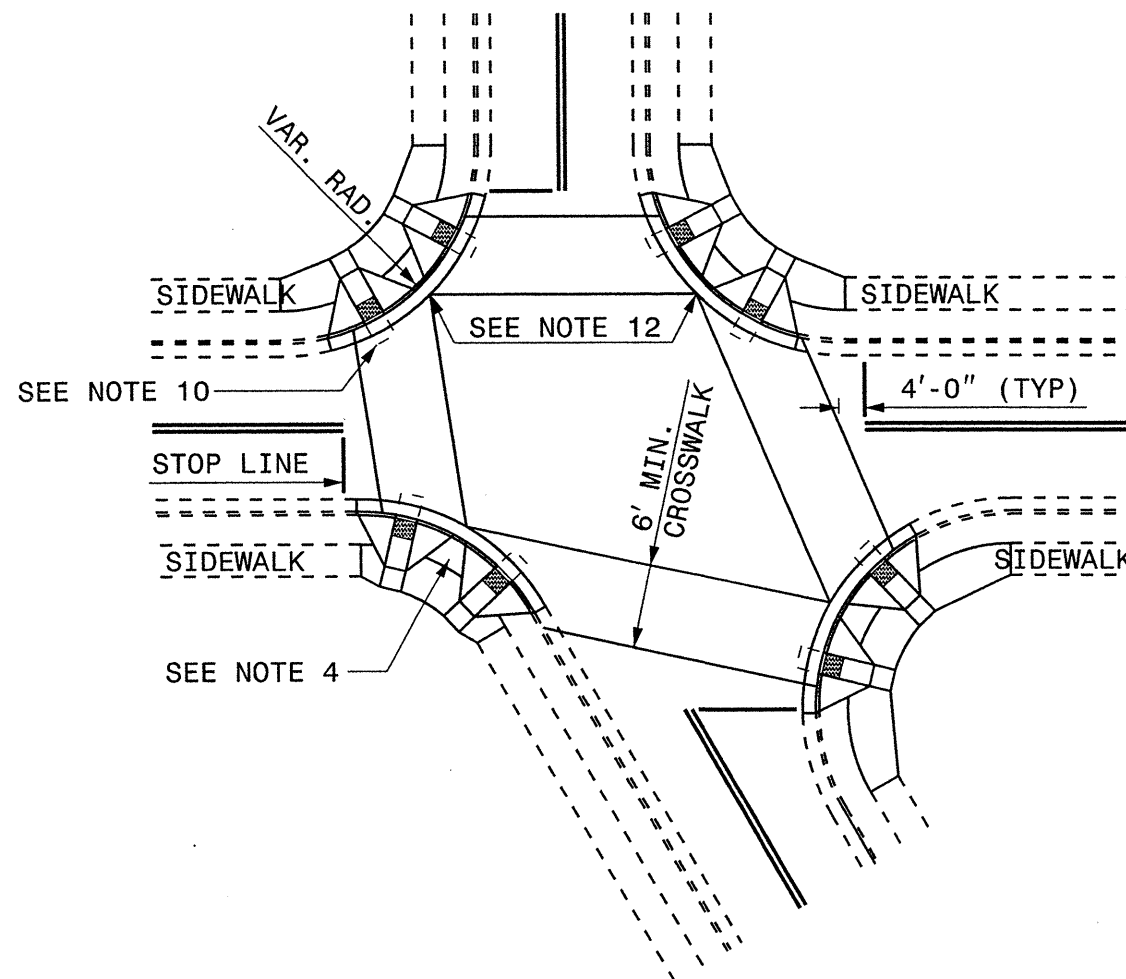
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**CURB RAMPS AND EXISTING SIDEWALK**



DETAIL SHOWING TYPICAL LOCATION OF CURB RAMPS,  
PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS

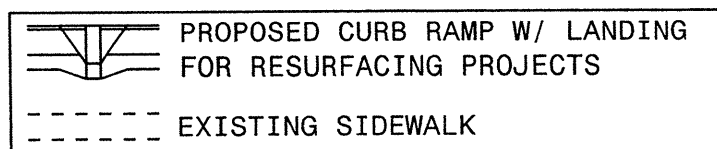


DETAIL SHOWING TYPICAL LOCATION OF CURB  
RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER

**RESURFACING PROJECTS**



ALLOWABLE LOCATIONS  
.....ANY

**CURB RAMP AND EXISTING SIDEWALK**

NOTES:

1. CONSTRUCT THE RAMP SURFACE TO BE STABLE, FIRM, AND SLIP RESISTANT. CONSTRUCT THE CURB RAMP TYPE AS SHOWN IN THE PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER.
2. LOCATE CURB RAMPS AND PLACE PEDESTRIAN CROSSWALK MARKINGS AS SHOWN IN THE PAVEMENT MARKING PLANS. WHEN FIELD ADJUSTMENTS REQUIRE MOVING CURB RAMPS OR MARKINGS AS SHOWN, CONTACT THE SIGNING AND DELINEATION UNIT OR LOCATE AS DIRECTED BY THE ENGINEER.
3. COORDINATE THE CURB RAMP AND THE PEDESTRIAN CROSSWALK MARKINGS SO A 4'x4' CLEAR SPACE AT THE BASE OF THE CURB RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES.
4. SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL LANE IS 4' MINIMUM.
5. REFER TO THE PAVEMENT MARKING PLANS FOR STOP BAR LOCATIONS AT SIGNALIZED INTERSECTIONS. IF A PAVEMENT MARKING PLAN IS NOT PROVIDED, CONTACT THE SIGNAL DESIGN SECTION FOR THE STOP BAR LOCATIONS OR LOCATE AS DIRECTED BY THE ENGINEER.
6. TERMINATE PARKING A MINIMUM OF 20' BACK OF A PEDESTRIAN CROSSWALK.
7. CONSTRUCT CURB RAMPS A MINIMUM OF 4' WIDE.
8. CONSTRUCT THE RUNNING SLOPE OF THE RAMP 8.33% MAXIMUM.
9. ALLOWABLE CROSS SLOPE ON SIDEWALKS AND CURB RAMPS WILL BE 2% MAXIMUM.
10. CONSTRUCT THE SIDE FLARE SLOPE A MAXIMUM OF 10% MEASURED ALONG THE CURB LINE.
11. CONSTRUCT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE BASE OF THE CURB RAMP A MAXIMUM OF 5% AND MAINTAIN A SMOOTH TRANSITION.
12. CONSTRUCT LANDINGS FOR SIDEWALK A MINIMUM OF 4'x4' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION. CONSTRUCT LANDINGS FOR MEDIAN ISLANDS A MINIMUM OF 5'x5' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION.
13. TO USE A MEDIAN ISLAND AS A PEDESTRIAN REFUGE AREA, MEDIAN ISLANDS WILL BE A MINIMUM OF 6' WIDE. CONSTRUCT MEDIAN ISLANDS TO PROVIDE PASSAGE OVER OR THROUGH THE ISLAND.
14. SMALL CHANNELIZATION ISLANDS THAT CAN NOT PROVIDE A 5'x5' LANDING AT THE TOP OF A RAMPS, WILL BE CUT THROUGH LEVEL WITH THE SURFACE STREET.
15. CURB RAMPS WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED.
16. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE CURB RAMP JOINS THE CURB AS SHOWN IN ROADWAY STANDARD DRAWING 848.01
17. PLACE ALL PEDESTRIAN PUSH BUTTON ACTUATORS AND CROSSING SIGNALS AS SHOWN IN THE PLANS OR AS SHOWN IN THE MUTCD.
18. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS. CONTACT THE CONTRACT STANDARDS AND DEVELOPMENT UNIT FOR THE DETAILS OR FOR A SPECIAL DESIGN.

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ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER

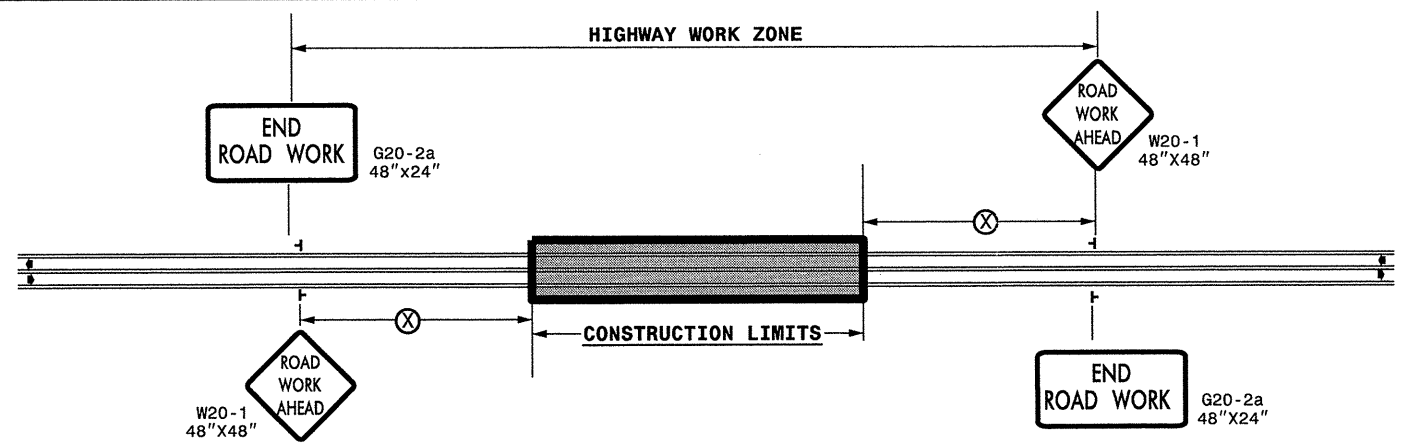
ENGLISH DETAIL DRAWING FOR  
**CURB RAMP**  
EXISTING CURB AND GUTTER



27-OCT-2011 12:45  
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PROJ. REFERENCE NO.	SHEET NO.
13CR.10591.10	TCP-1
13CR.20591.9	

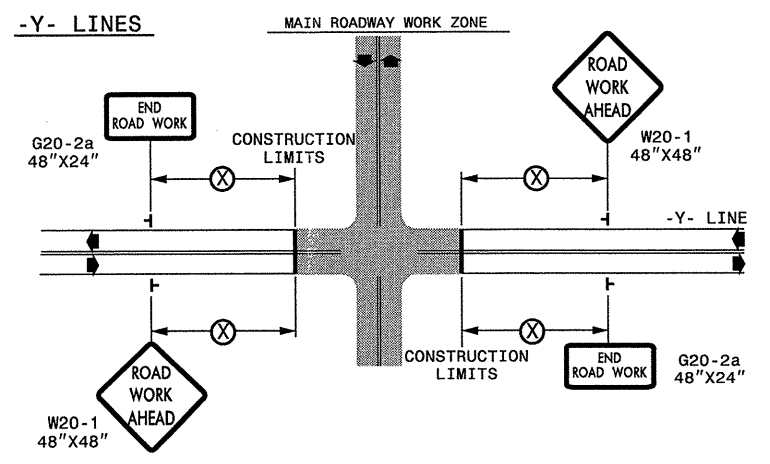
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

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**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

┆ STATIONARY SIGN  
 ◀ DIRECTION OF TRAFFIC FLOW

**DETAIL DRAWING FOR  
 TWO-WAY UNDIVIDED  
 WORK ZONE WARNING SIGNS**

SHEET 1 OF 1

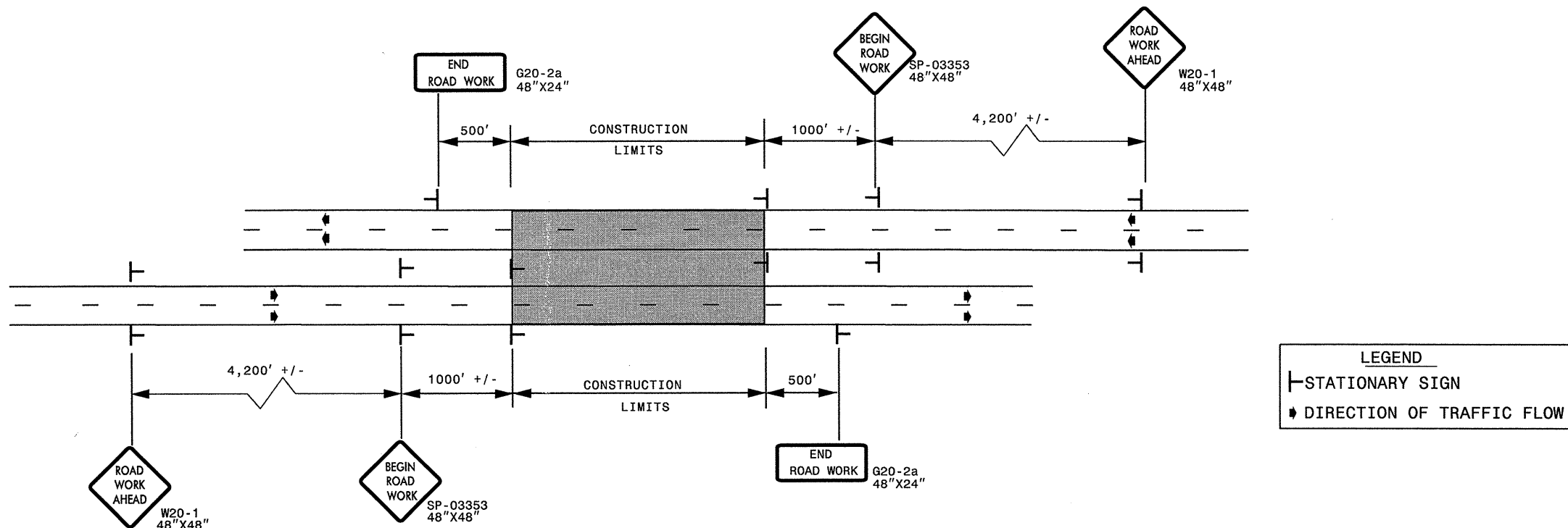
APPROVED: _____	DATE: _____
SEAL	
DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
SCALE: NONE	REVISIONS
DATE: _____	7-98 10/01
DWG. BY: _____	10-98 03/04
DESIGN BY: _____	01/01 11/04
REVIEWED BY: _____	CADD FILE



# ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

PROJ. REFERENCE NO.	SHEET NO.
13CR.10591.10 13CR.20591.9	TCP-2

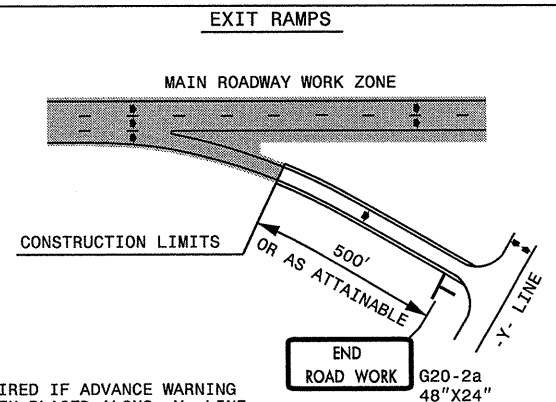
## DETAIL A



LEGEND	
	STATIONARY SIGN
◆	DIRECTION OF TRAFFIC FLOW

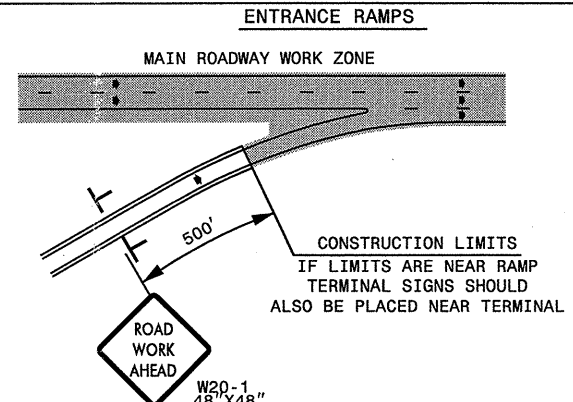
\* USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.

## DETAIL B



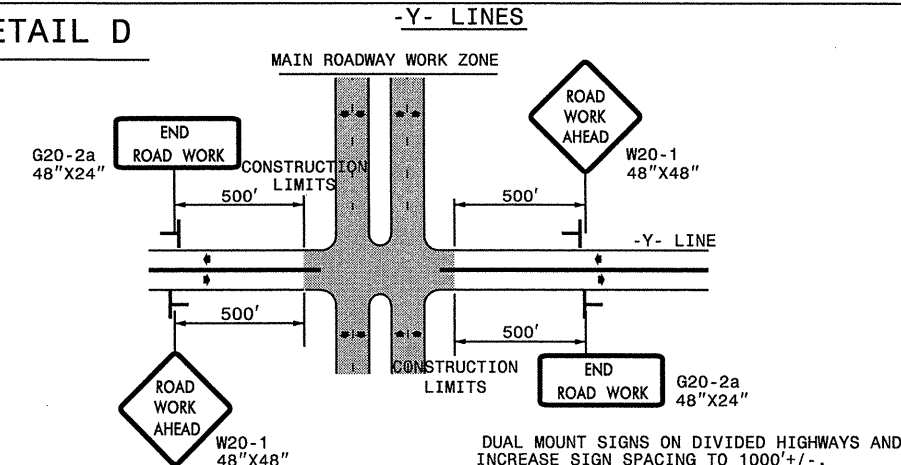
NOTE: SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

## DETAIL C



CONSTRUCTION LIMITS IF LIMITS ARE NEAR RAMP TERMINAL SIGNS SHOULD ALSO BE PLACED NEAR TERMINAL

## DETAIL D

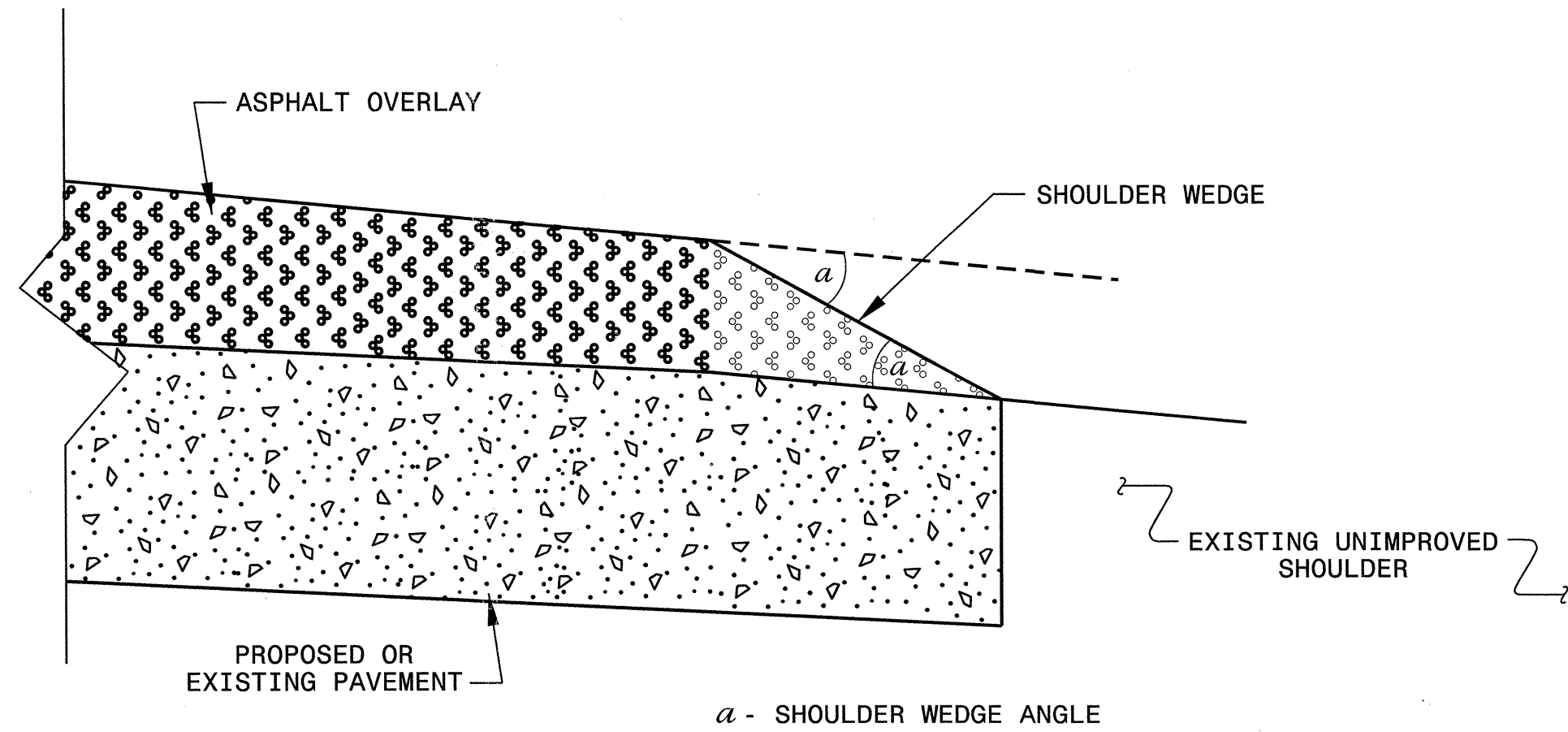


## GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

APPROVED: _____	DATE: _____	ADVANCED WORK ZONE WARNING SIGNS FOR FREEWAYS (4 LANES OR GREATER)	
	SCALE: NONE		REVISIONS
	DATE: 8/03		03/04
	DWG. BY: JI		
	DESIGN BY: JI		
REVIEWED BY: _____		CADD FILE	

27-OCT-2011 12:46 \\D01ADF5R001\GROUPS-WZTC\TCCC\TMU\WZTC\Resurfacing\2011\Western\2011\Div13\C202904A-B\13CR.0591.10x2.McDowell\_freeways\_4lanes\_or\_greater\_stationary.dgn



# SHOULDER WEDGE DETAIL

04-OCT-2011 11:22  
 S:\Contracts\Special Details\howerton\shoulderwedgedetail.dgn  
 \$\$\$USERNAME\$\$\$

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950	FAX 919-250-4119
<b>SHOULDER WEDGE DETAIL</b>	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn	