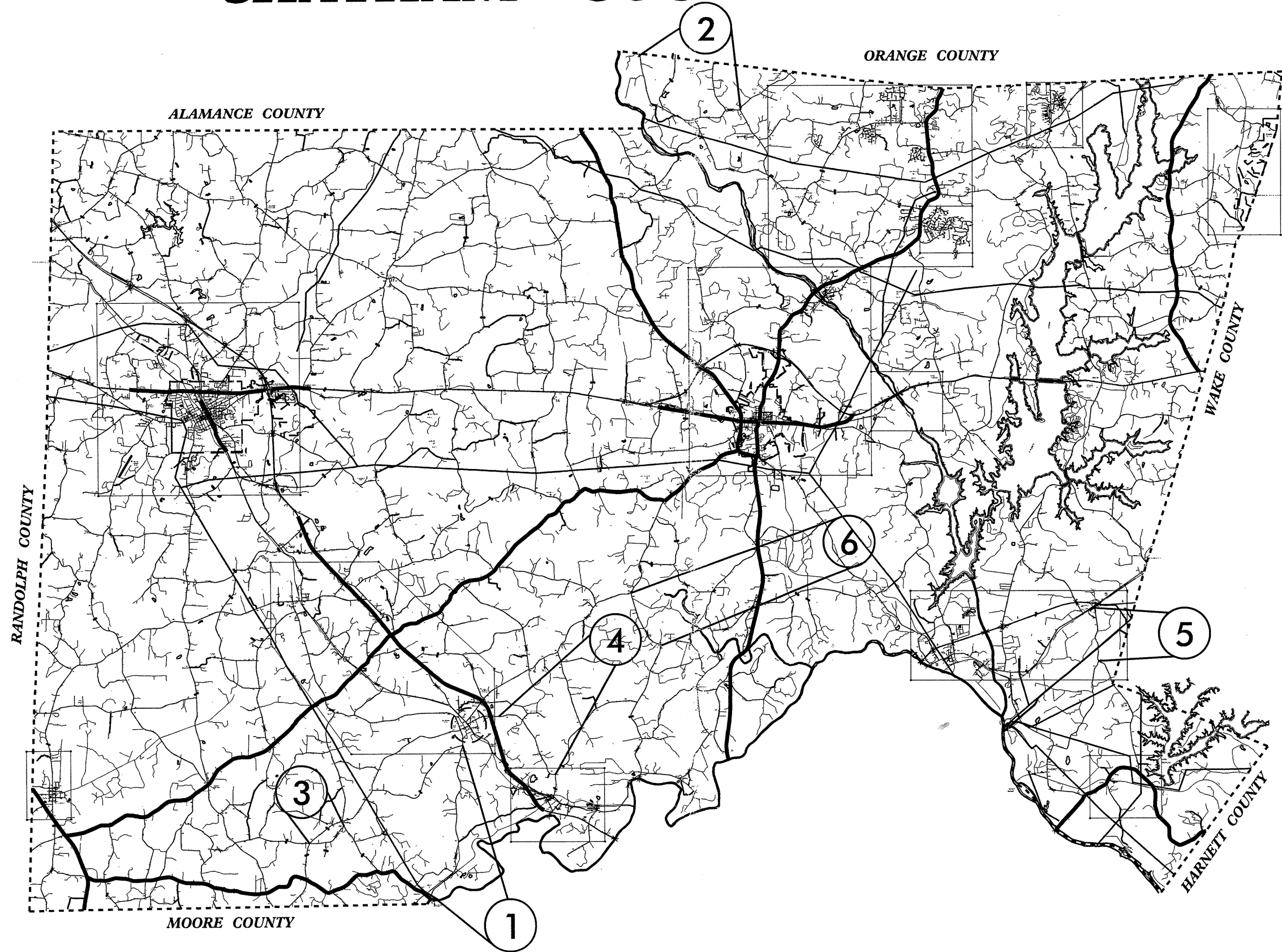


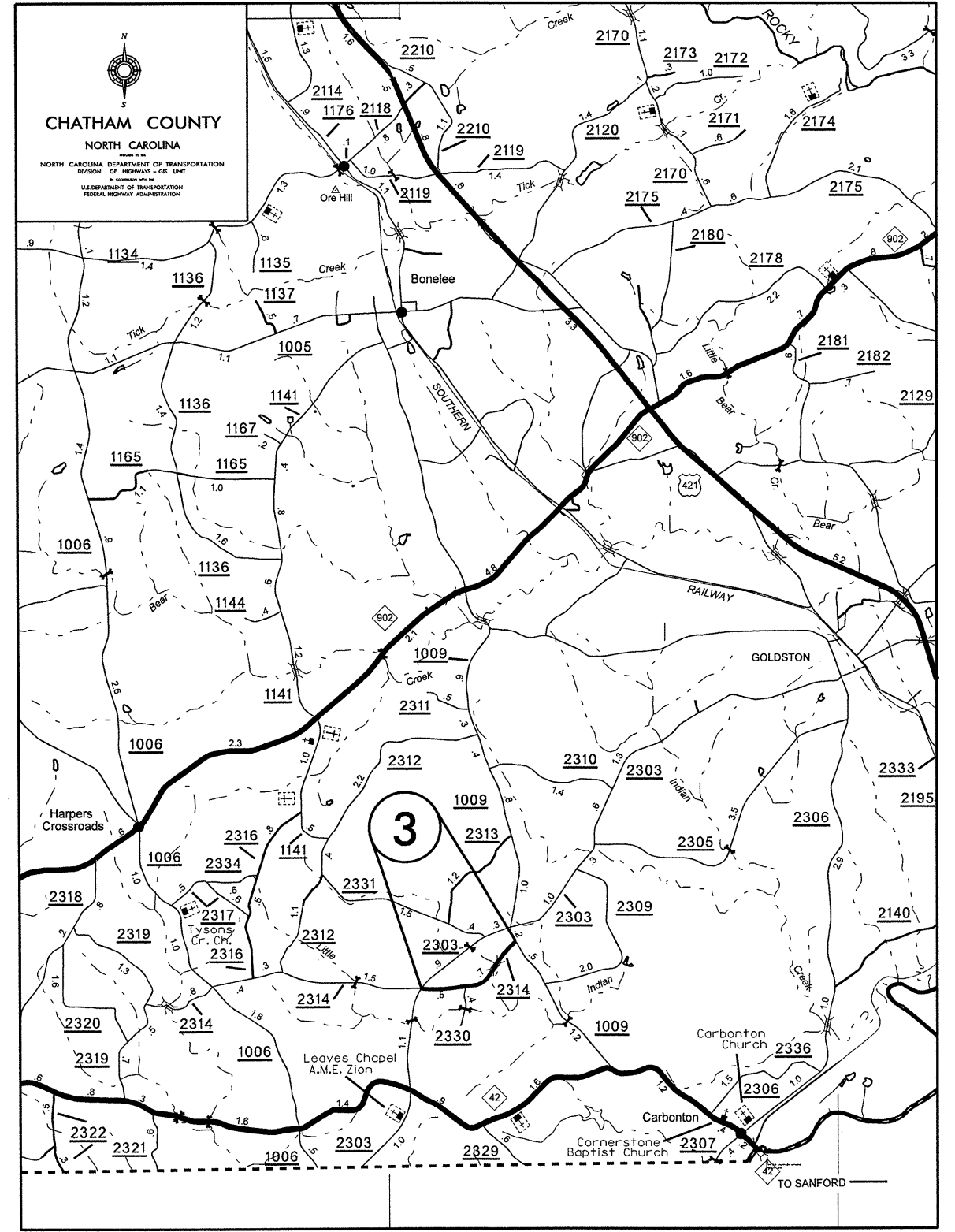
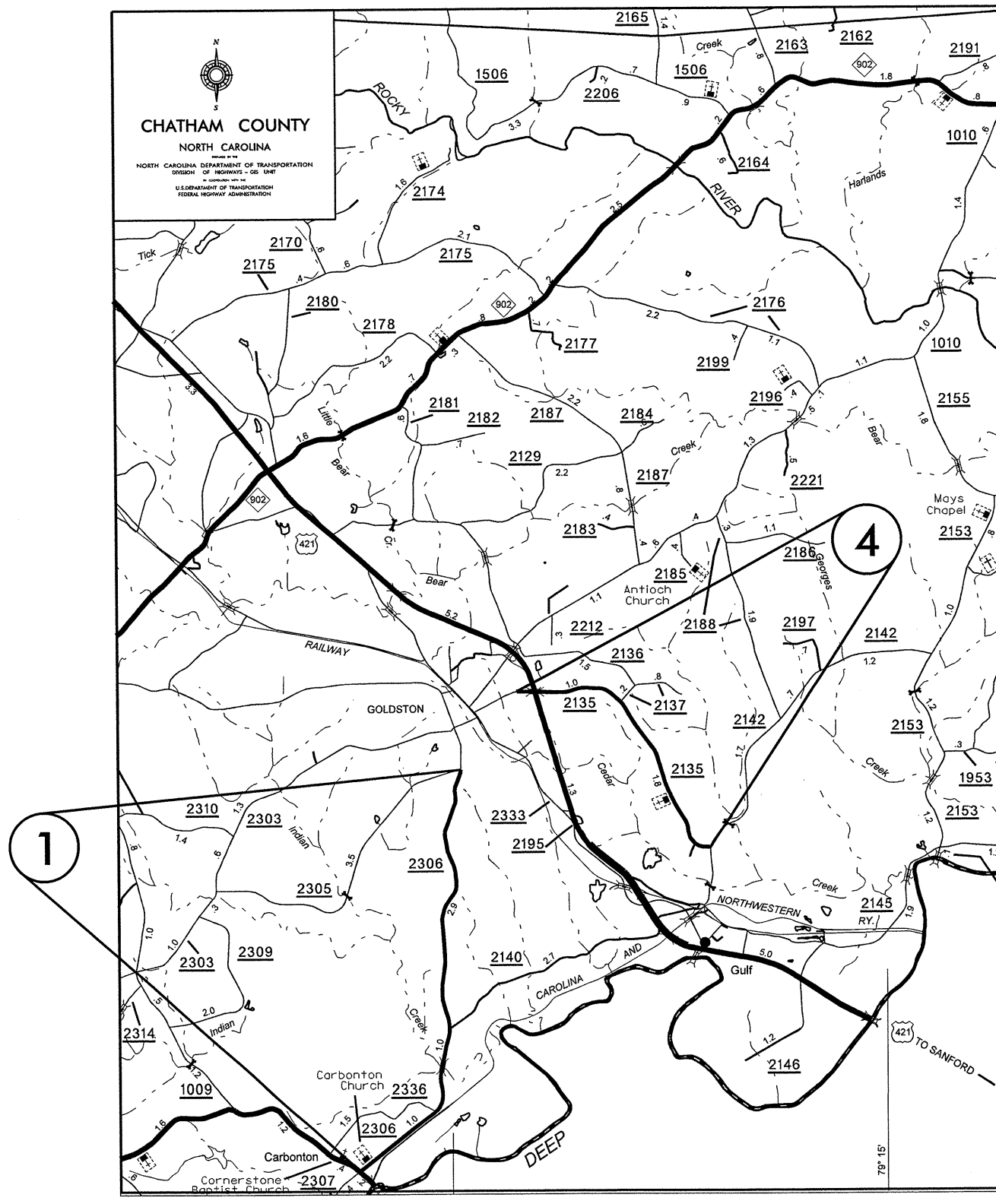
# CHATHAM COUNTY

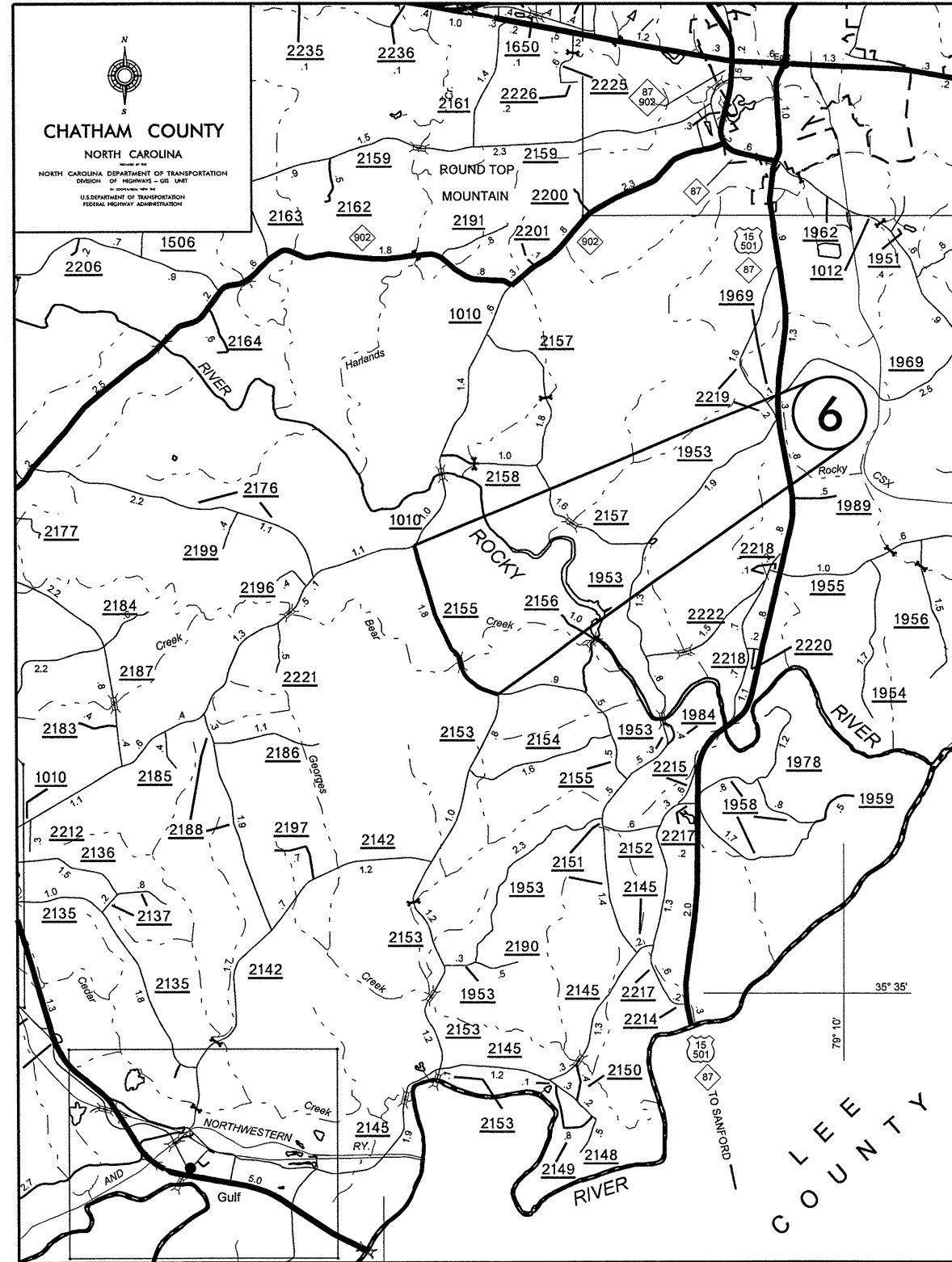
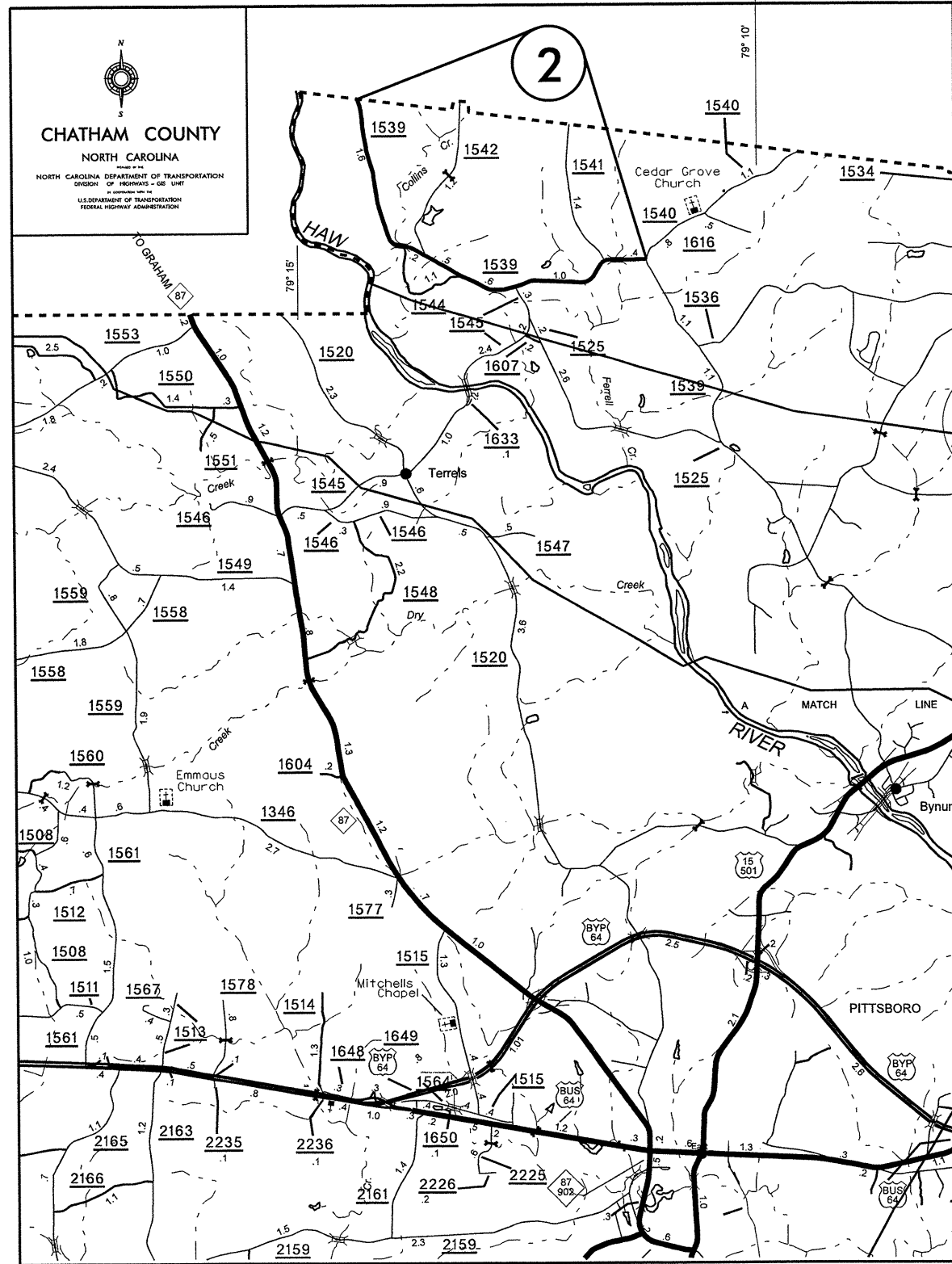
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8CR.20191.17	1

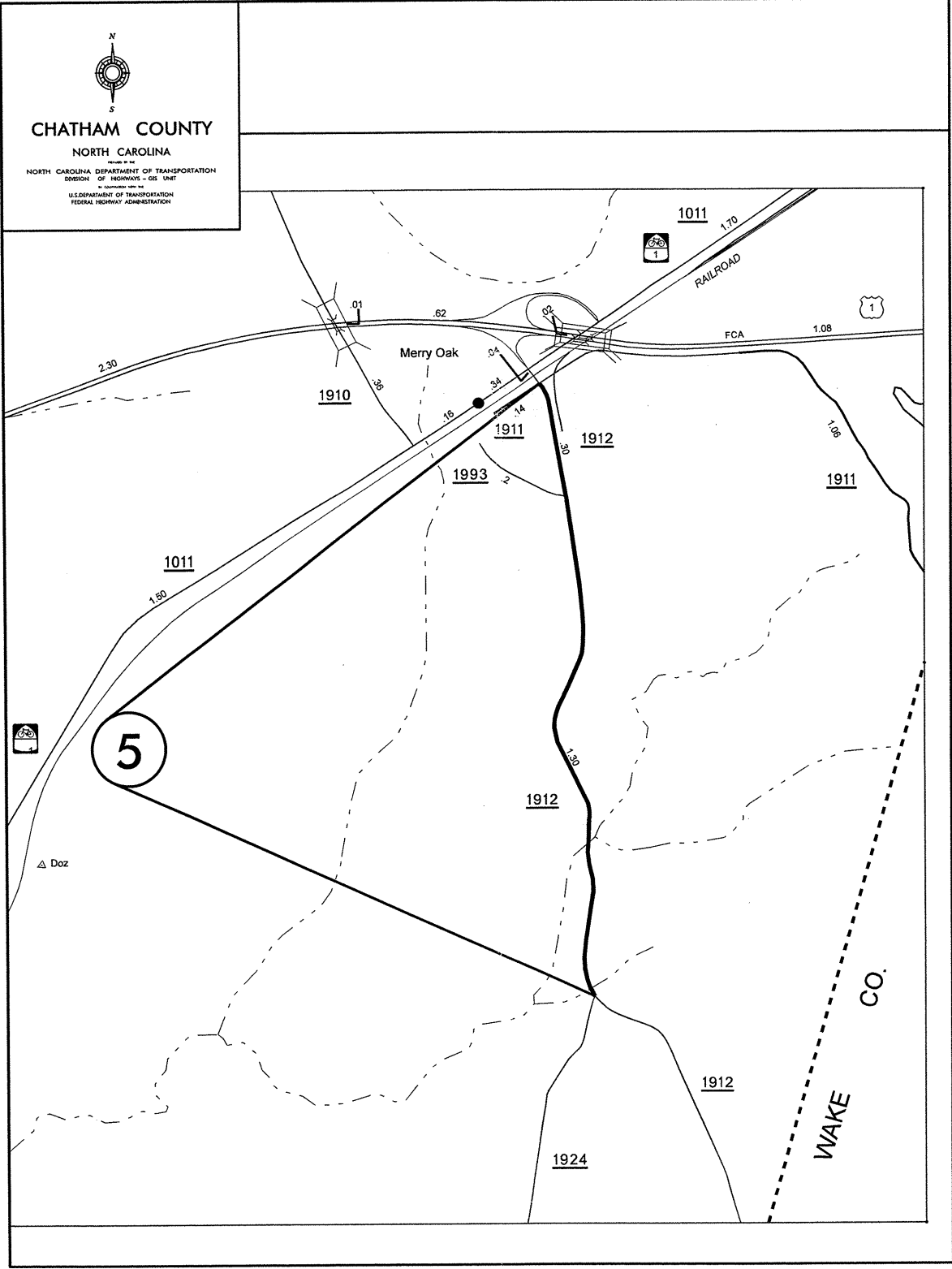


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8CR.20191.17	2

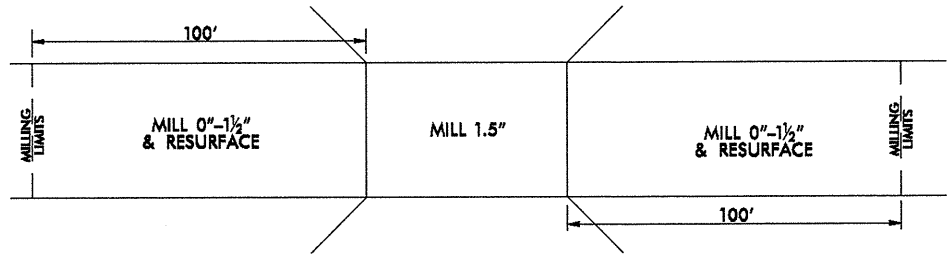
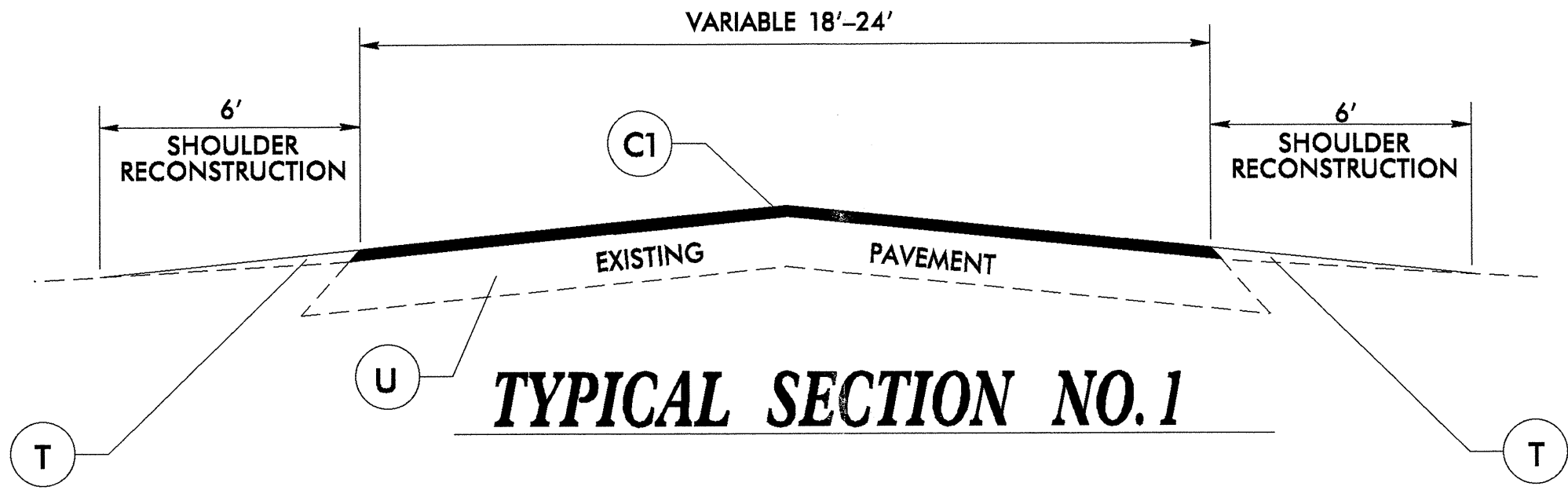






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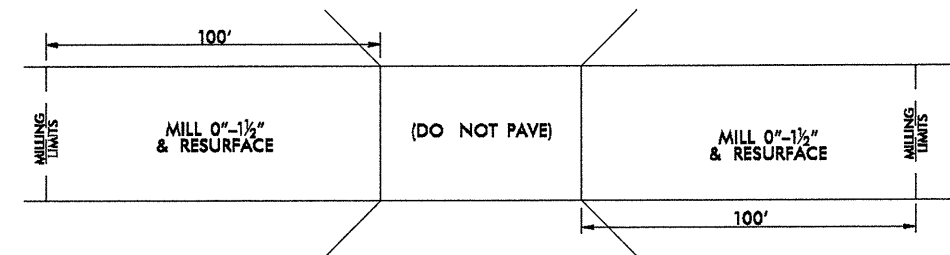
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\*\* - MILLING TO BE PAID AS INCIDENTAL MILLING

**BRIDGE MILLING**

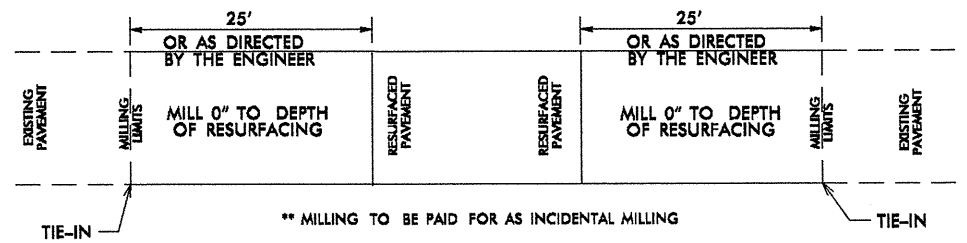
MAP NO. 1 (BRIDGE #201) & NO. 2 (BRIDGE #89)



\*\* - MILLING AT APPROACHES TO BE PAID AS INCIDENTAL MILLING

**BRIDGE MILLING**

MAP NO. 3 (BRIDGE #217) & NO. 6 (BRIDGE #149)



\*\* MILLING TO BE PAID FOR AS INCIDENTAL MILLING

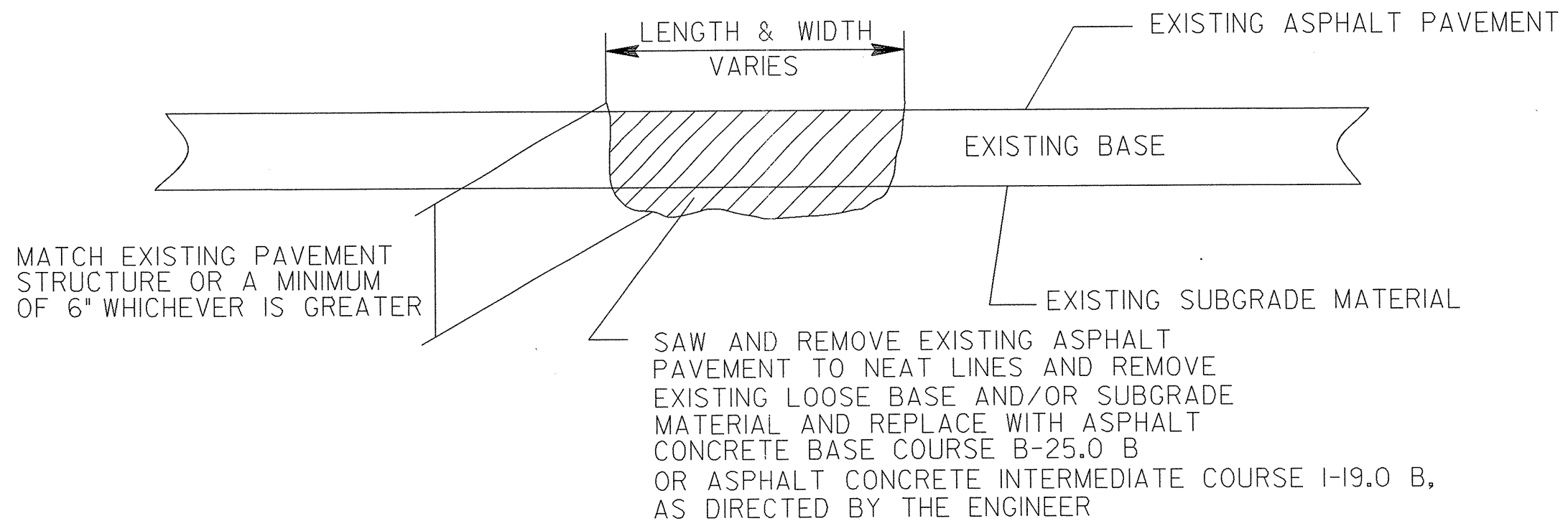
**PAVEMENT TIE-IN DETAIL**

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT.

040397  
22-Jul-2011 15:08  
Division of Transportation  
Chatham-Submittal\2011-fall-dv8-dist1-res-tyr-chat-mp.dgn

## DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING

### DETAIL



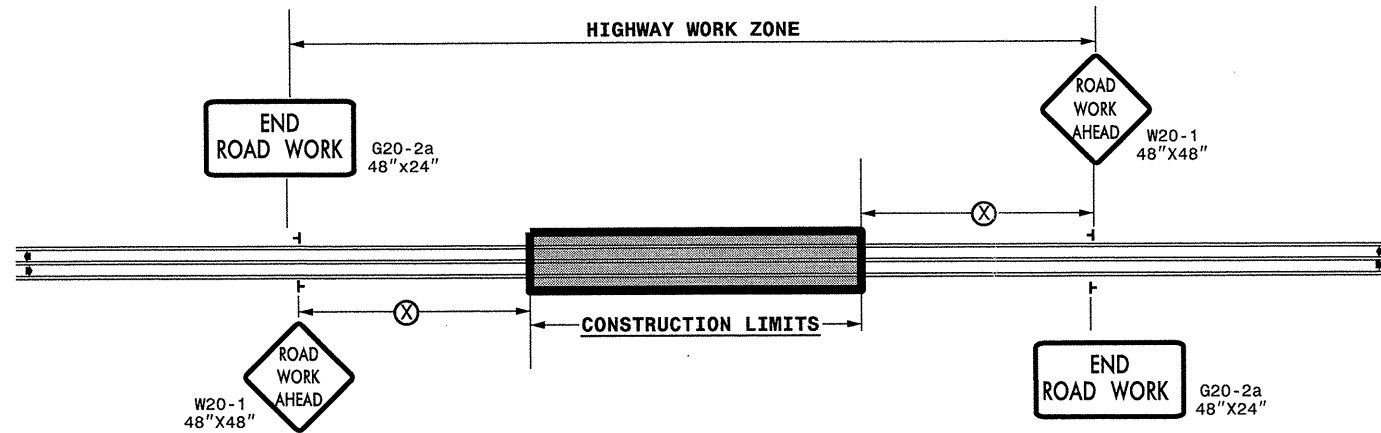
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PROJECT NO.	SHEET NO.	TOTAL NO.
8CR.20191.17	7	

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	GENERIC GRADING ITEM - AGGREGATE SHOULDER BORROW TON	SHOULDER RECONSTRUCTION SMI	INCIDENTAL MILLING SY	SURFACE COURSE, SF9.5A TON	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	4589000000-N	4705000000-E	4710000000-E	4721000000-E	4810000000-E	4810000000-E		
															GENERIC TRAFFIC CONTROL ITEM - TRAFFIC CONTROL LS	16" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO RXR 120 M EA	4" WHITE PAINT LF	4" YELLOW PAINT LF		
8CR.20191.17	Chatham	1	SR 2306	FROM NC 42 TO SR 2305	1	NO	4.81	20	1,202.5	9.62	389	4,975	323	25	*					103,511	103,511	
<b>TOTAL FOR MAP NO. 1</b>							<b>4.81</b>		<b>1,202.5</b>	<b>9.62</b>	<b>389</b>	<b>4,975</b>	<b>323</b>	<b>25</b>						<b>103,511</b>	<b>103,511</b>	
																			<b>207,022</b>			
	Chatham	2	SR 1539	FROM ORANGE CO TO SR 1540	1	NO	4.35	20	1,087.5	8.70	722	4,580	298	550	*					93,612	93,612	
<b>TOTAL FOR MAP NO. 2</b>							<b>4.35</b>		<b>1,087.5</b>	<b>8.70</b>	<b>722</b>	<b>4,580</b>	<b>298</b>	<b>550</b>						<b>93,612</b>	<b>93,612</b>	
																			<b>187,224</b>			
	Chatham	3	SR 2314	FROM SR 2303 TO SR 1009	1	NO	1.17	18	292.5	2.34	400	1,225	80	100	*					25,178	25,178	
<b>TOTAL FOR MAP NO. 3</b>							<b>1.17</b>		<b>292.5</b>	<b>2.34</b>	<b>400</b>	<b>1,225</b>	<b>80</b>	<b>100</b>						<b>25,178</b>	<b>25,178</b>	
																			<b>50,356</b>			
	Chatham	4	SR 2135	FROM SR 2142 TO GOLDSTON CL	1	NO	3.09	20	772.5	6.18	222	3,215	209	200	*					66,497	66,497	
<b>TOTAL FOR MAP NO. 4</b>							<b>3.09</b>		<b>772.5</b>	<b>6.18</b>	<b>222</b>	<b>3,215</b>	<b>209</b>	<b>200</b>							<b>66,497</b>	<b>66,497</b>
																			<b>132,994</b>			
	Chatham	5	SR 1912	FROM SR 1924 TO SR 1911	1	NO	1.6	20	400	3.20	200	1,665	108	175	*	50	36	2		34,432	34,432	
<b>TOTAL FOR MAP NO. 5</b>							<b>1.6</b>		<b>400</b>	<b>3.20</b>	<b>200</b>	<b>1,665</b>	<b>108</b>	<b>175</b>		<b>50</b>	<b>36</b>	<b>2</b>		<b>34,432</b>	<b>34,432</b>	
																			<b>68,864</b>			
	Chatham	6	SR 2155	FROM SR 2153 TO SR 1010	1	NO	1.8	20	450	3.60	444	1,870	122	150	*					38,736	38,736	
<b>TOTAL FOR MAP NO. 6</b>							<b>1.8</b>		<b>450</b>	<b>3.60</b>	<b>444</b>	<b>1,870</b>	<b>122</b>	<b>150</b>							<b>38,736</b>	<b>38,736</b>
																			<b>77,472</b>			
<b>TOTAL FOR PROJ NO. 8CR.20191.17</b>							<b>16.82</b>		<b>4,205</b>	<b>33.64</b>	<b>2,377</b>	<b>17,530</b>	<b>1,140</b>	<b>1,200</b>		<b>1</b>	<b>50</b>	<b>36</b>	<b>2</b>		<b>361,966</b>	<b>361,966</b>
																			<b>723,932</b>			
<b>GRAND TOTAL</b>							<b>16.82</b>		<b>4,205</b>	<b>33.64</b>	<b>2,377</b>	<b>17,530</b>	<b>1,140</b>	<b>1,200</b>		<b>1</b>	<b>50</b>	<b>36</b>	<b>2</b>		<b>361,966</b>	<b>361,966</b>
																			<b>723,932</b>			

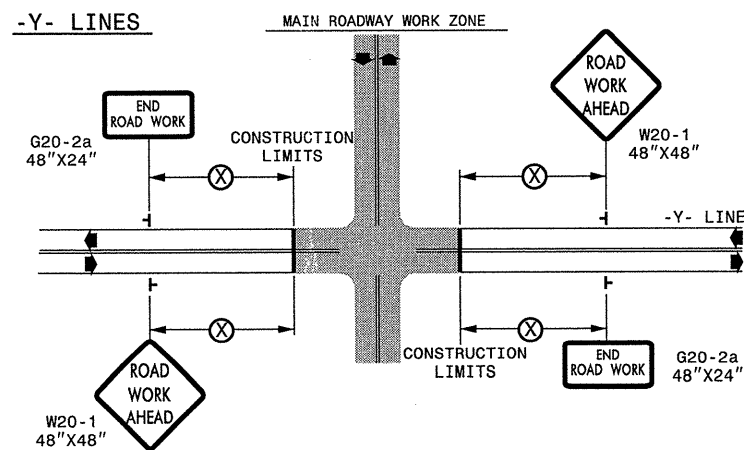
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

┆ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____ DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS		SCALE: NONE
SEAL			REVISIONS
			DATE: 7-98 10/01 DWG. BY: 10-98 03/04 DESIGN BY: 01/01 11/04 REVIEWED BY:

I9-JUL-2011 14:27 T:\N\GROUPS-WZ\TCCC\TMUN\WZTC\Resur\facimg\2011\Centr\al\2011.DIV08\C202834\_8CR.20191.17.Chatham-US42L.SRs.\jww\lxl17-c202834RW\_8CR.20191.17.2way\_Undiv\_&\_Urban\_Frwwys\_stationary.dgn  
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