

CATAWBA COUNTY

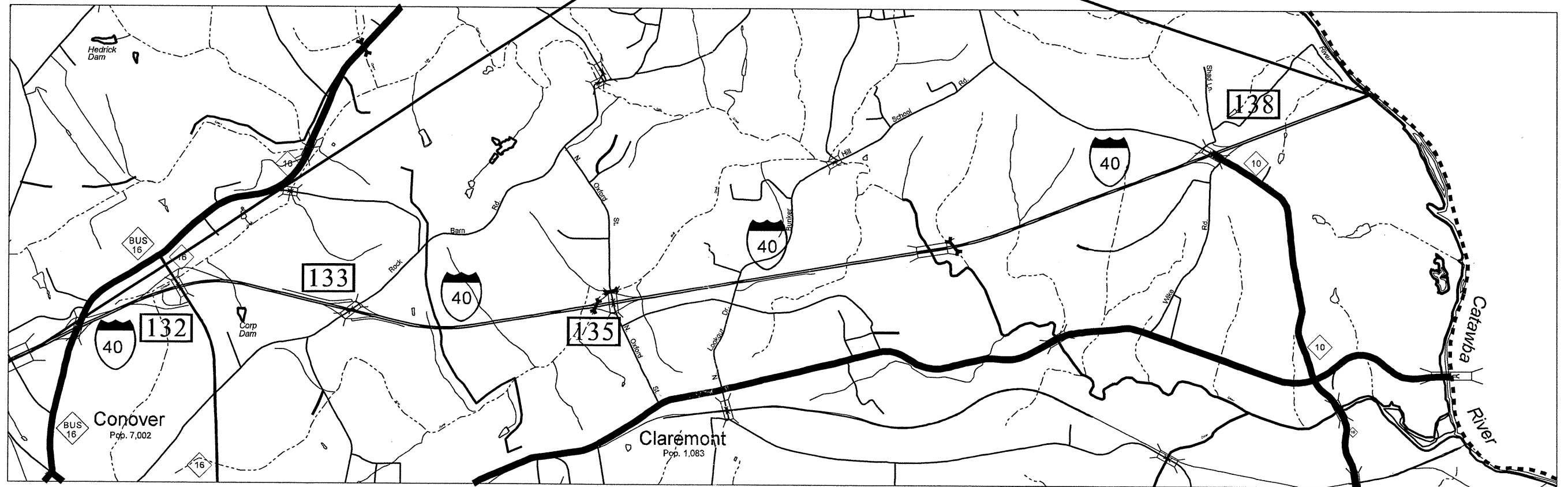
NORTH CAROLINA

MAP #1: I-40

8.7 MILES



1

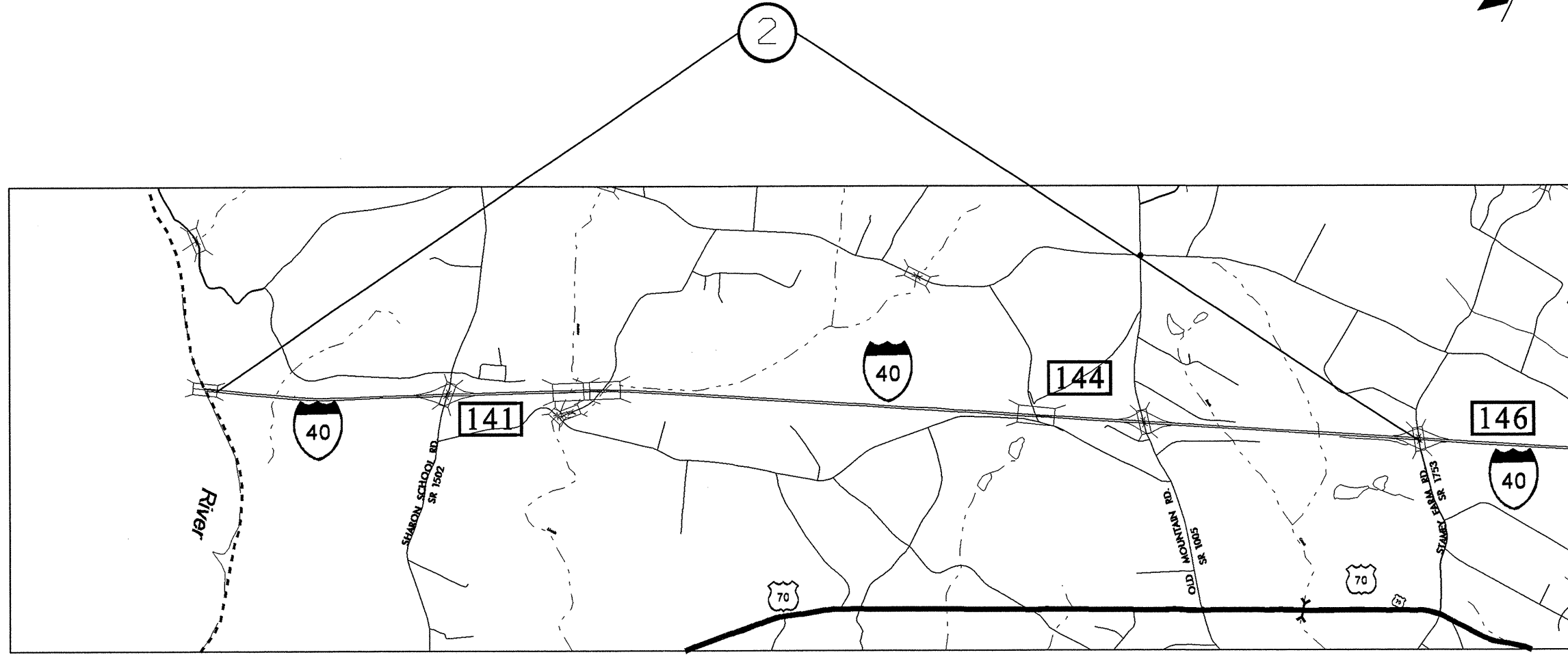
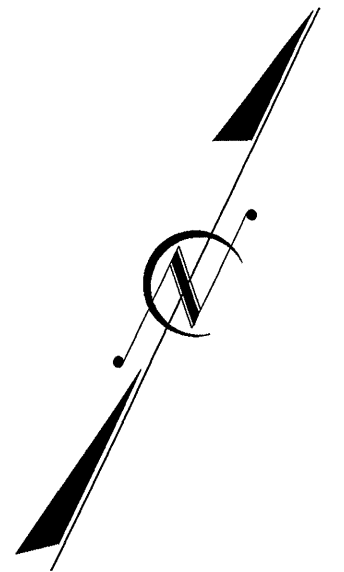


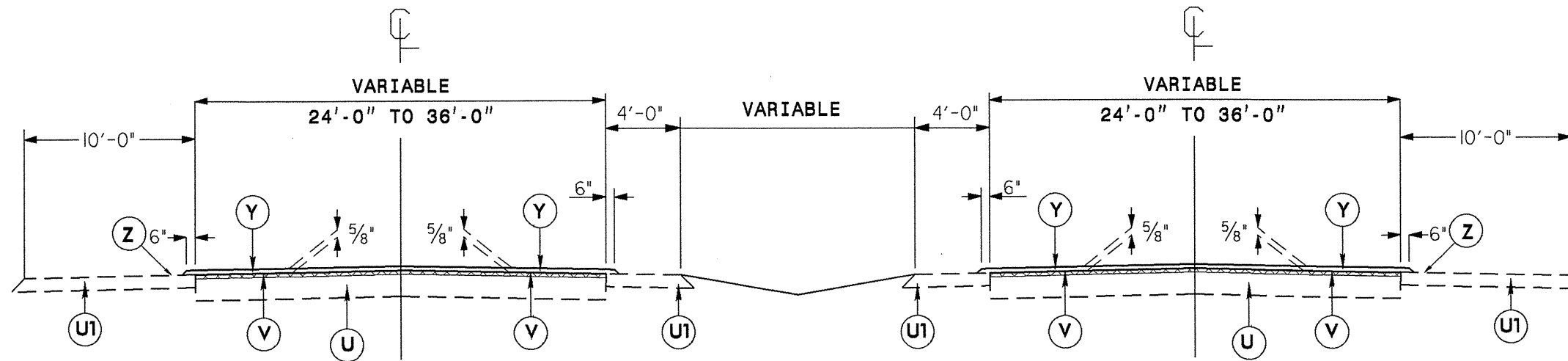
IREDELL COUNTY

NORTH CAROLINA

MAP #2: I-40

6.0 MILES



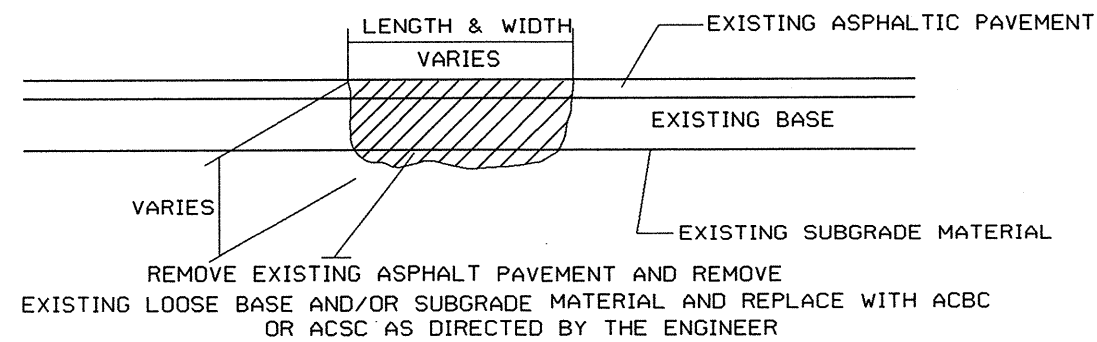


TYPICAL SECTION NO. 1

USE TYPICAL SECTION NO. 1 AS FOLLOWS

FROM END OF BRIDGES OVER 4TH AVENUE (BRIDGES #171 & #172)
TO BEGIN BRIDGE OVER CATAWBA RIVER AT COUNTY LINE (BRIDGES #6 EAST BOUND)
AND TO BEGINNING OF WEST BOUND OFF-RAMP AT EXIT #138.

DO NOT DIAMOND GRIND OR OVERLAY ANY BRIDGES ON THIS MAP



PATCHING EXISTING PAVEMENT

USE WITH TYPICAL NO. 1 WHERE SHOULDER PATCHING MAY BE NECESSARY

PAVEMENT SCHEDULE

U	EXISTING CONCRETE PAVEMENT.
U1	EXISTING ASPHALT PAVEMENT.
V	DIAMOND GRIND CONCRETE PAVEMENT
Y	PROP. APPROX. 5/8" ULTRA-THIN TYPE B BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH
Z	MILLED RUMBLE STRIPS

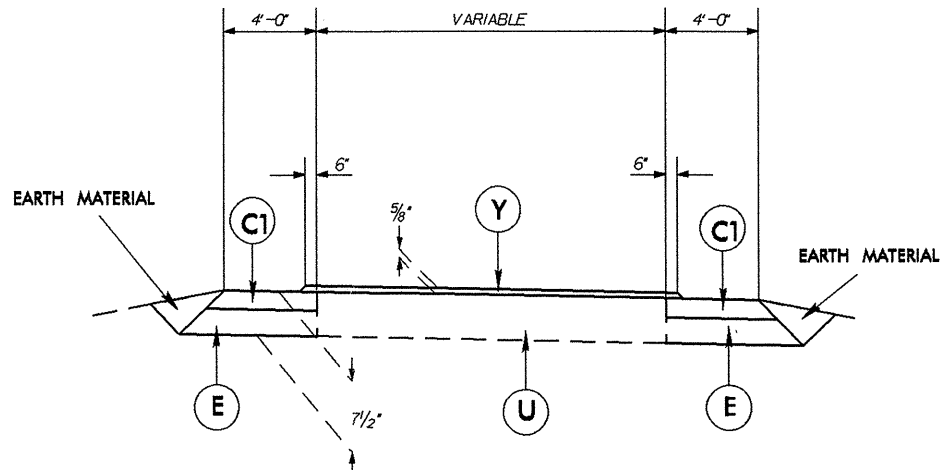
NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

5/14/99

*****SYSTEMS*****

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD
E	PROP. APPROX. 5 1/2" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YARD
U	EXISTING CONCRETE PAVEMENT.
V1	MILLING ASPHALT PAVEMENT 2" DEPTH
Y	PROP. APPROX. 5/8" ULTRA-THIN TYPE B BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH

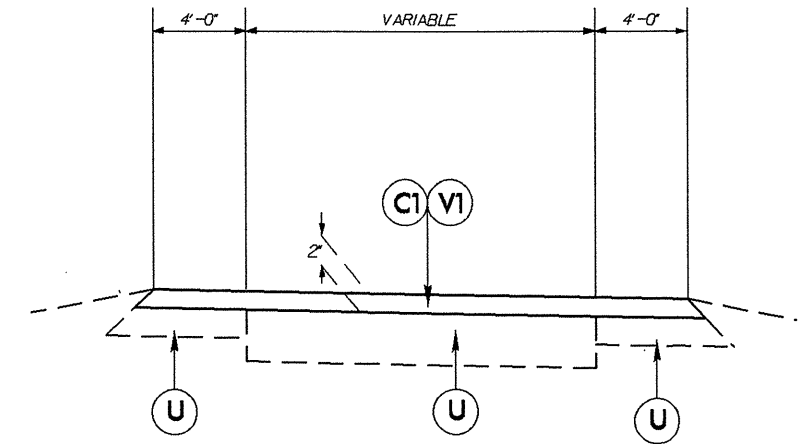
NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



TYPICAL SECTION NO. 1A

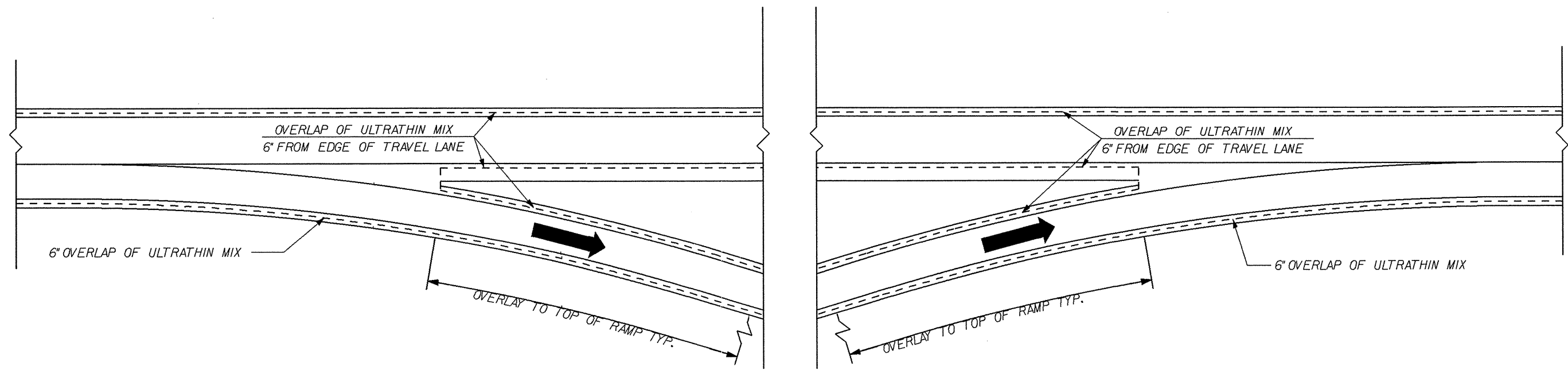
USE TYPICAL SECTION NO. 1A ON: RAMPS AND LOOPS
IN CONJUNCTION WITH TYPICAL NO. 1 AT EXITS #133, #135, & #138

DO NOT PLACE ULTRATHIN ON ANY RAMPS AT EXIT # 132 OR REST AREAS
SHOULDER REMOVAL TO BE PAID FOR AS UNCLASSIFIED EXCAVATION (SEE SUMMARY OF EARTHWORK).



TYPICAL SECTION NO. 1B

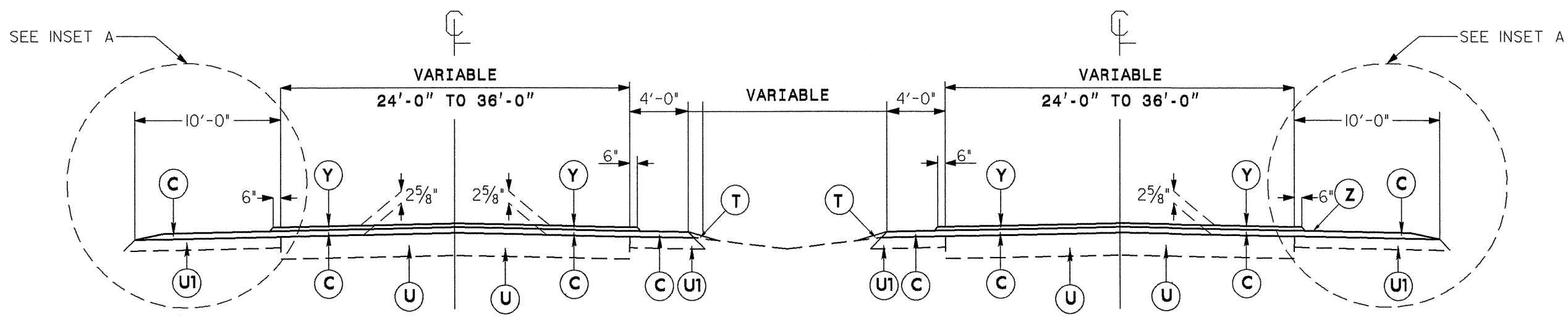
USE TYPICAL SECTION NO. 1B ON: RAMPS IN CONJUNCTION
WITH TYPICAL NO. 1 AT REST AREAS UP TO SPLIT FOR
TRUCK AND CAR PARKING



DETAIL GORE AND RAMP RESURFACING

USE DETAIL IN CONJUNCTION WITH TYPICAL SECTION NO. 1

5/14/99
DATE
TIME
LOCATION
BY
CHECKED
APPROVED



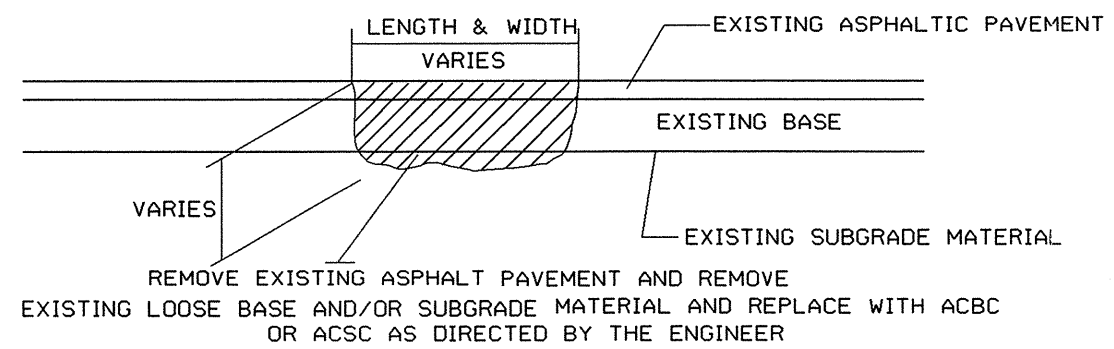
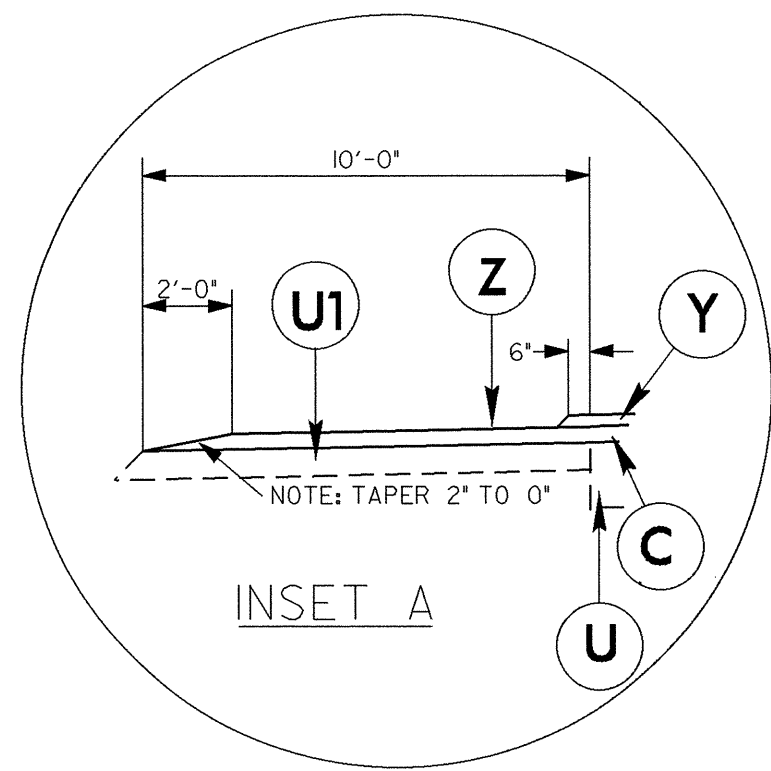
TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2 AS FOLLOWS

EAST BOUND FROM END OF BRIDGES OVER CATAWBA RIVER /COUNTY LINE (BRIDGE #6) TO END OF EXISTING CONCRETE PAVEMENT AT EXIT 146.

WEST BOUND FROM BEGINNING OF CONCRETE PAVEMENT AT EXIT 146 TO OFF-RAMP AT EXIT 138

*****DO NOT OVERLAY ANY BRIDGES ON THIS MAP***
SEE BRIDGE APPROACH AND DEPARTURE DETAIL FOR PAVEMENT TRANSITIONS AT BRIDGES**



PATCHING EXISTING PAVEMENT

USE WITH TYPICAL NO. 2 WHERE SHOULDER PATCHING MAY BE NECESSARY

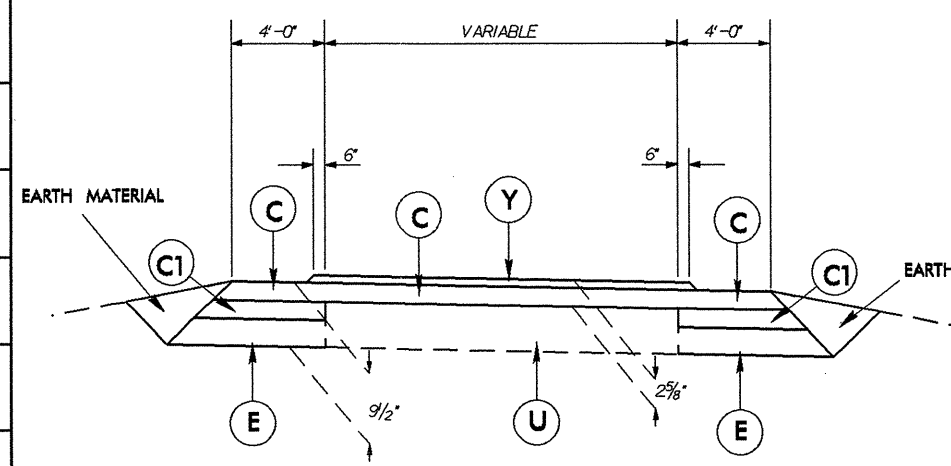
PAVEMENT SCHEDULE	
C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
U	EXISTING CONCRETE PAVEMENT.
U1	EXISTING ASPHALT PAVEMENT.
Y	PROP. APPROX. 5/8" ULTRA-THIN TYPE B BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH
Z	MILLED RUMBLE STRIPS

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

5/14/99 SYSTEMS

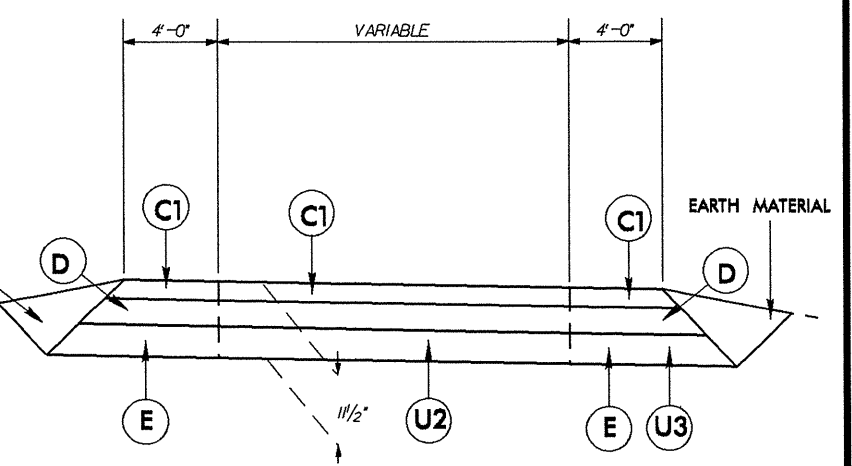
PAVEMENT SCHEDULE	
C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C1	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERM. COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YARD
E	PROP. APPROX. 5 1/2" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 827 LBS. PER SQ. YARD
U	EXISTING CONCRETE PAVEMENT.
U2	REMOVE EXISTING CONCRETE PAVEMENT.
U3	REMOVE EXISTING ASPHALT PAVEMENT.
Y	PROP. APPROX. 5/8" ULTRA-THIN TYPE B BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



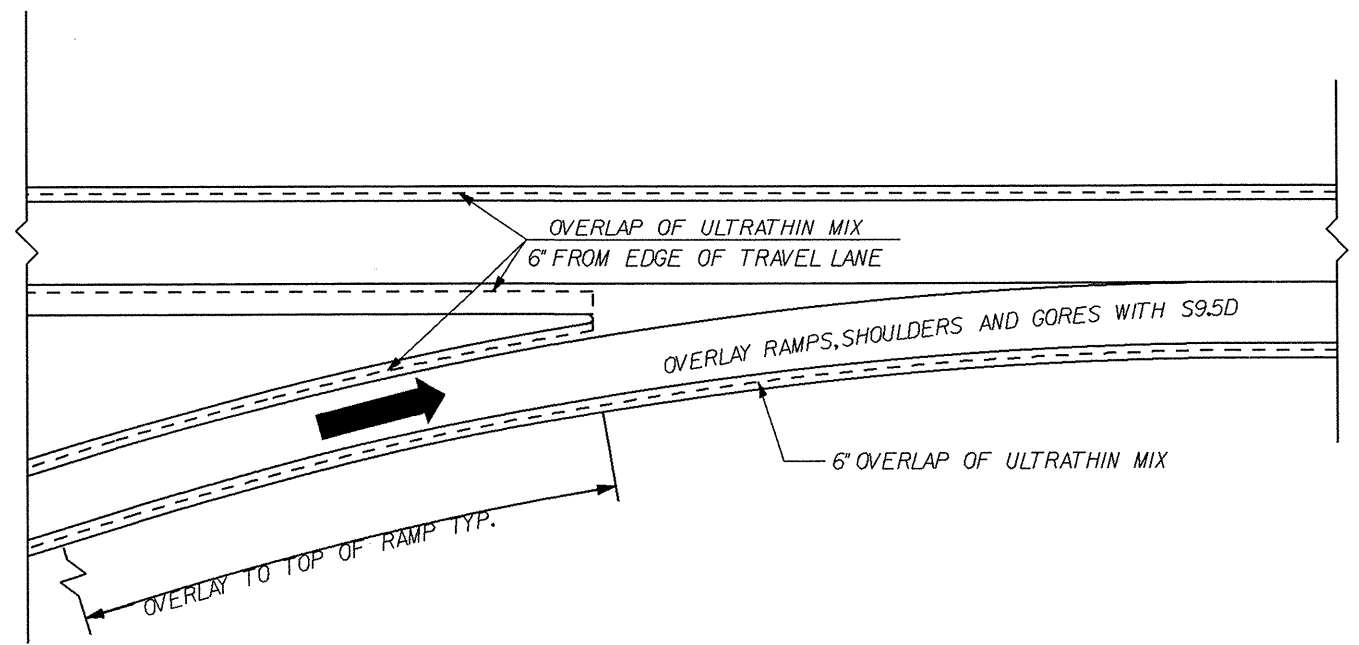
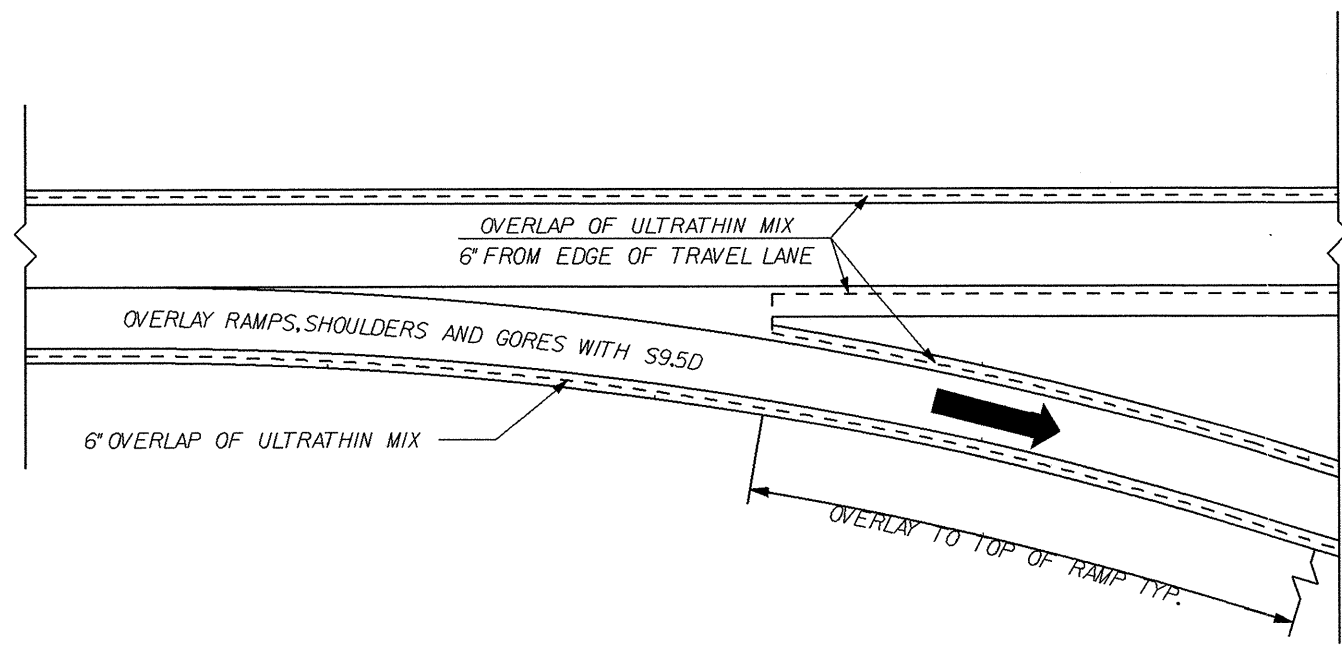
TYPICAL SECTION NO. 2A

USE TYPICAL SECTION NO. 2A ON: RAMPS AND LOOPS IN CONJUNCTION WITH TYPICAL NO. 2 AT EXITS 141 (EXCLUDING EXIT 141 WB OFF-RAMP), & EXIT 144. USE ON WB ON-RAMP AND EB OFF-RAMP ONLY AT EXIT 146.



TYPICAL SECTION NO. 2B

USE TYPICAL SECTION NO. 2B ON: EXIT 141 WB OFF-RAMP



DETAIL GORE AND RAMP RESURFACING

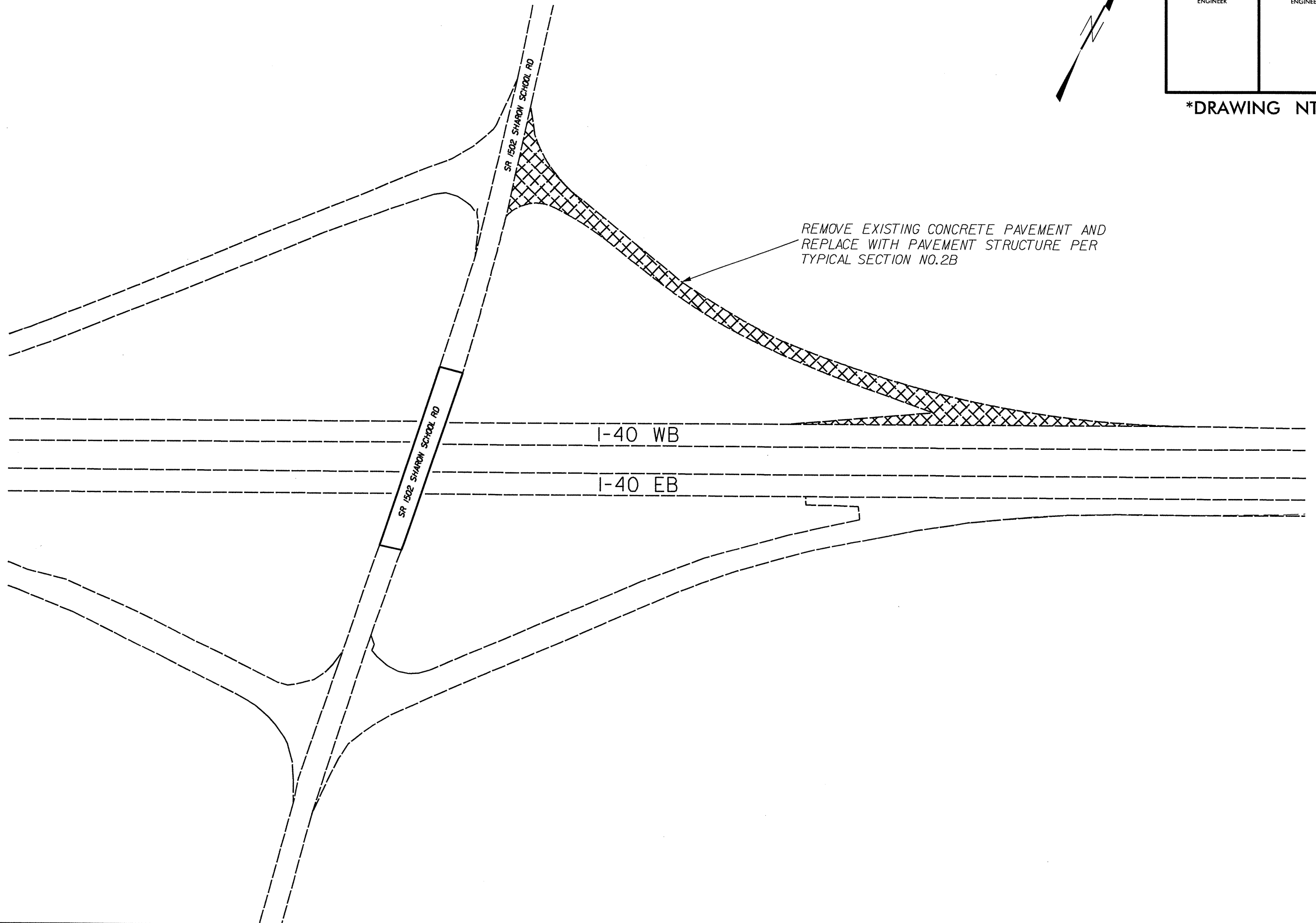
USE DETAIL IN CONJUNCTION WITH TYPICAL SECTION NO. 2 AT WEIGH STATION TAPER DOWN TO EXISTING GRADE BY END OF GORE

5/14/99 SYSTEMS DESIGN ON THE EDGE

EXIT 141 RAMP REPLACEMENT DETAIL

PROJECT REFERENCE NO. 1-5502	SHEET NO. 7
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

*DRAWING NTS



REMOVE EXISTING CONCRETE PAVEMENT AND REPLACE WITH PAVEMENT STRUCTURE PER TYPICAL SECTION NO.2B

I-40 WB

I-40 EB

SR 1502 SHARON SCHOOL RD

SR 1502 SHARON SCHOOL RD

8/17/99

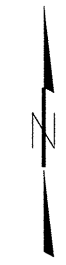
 SYSTEMS SECTION

B.17/99

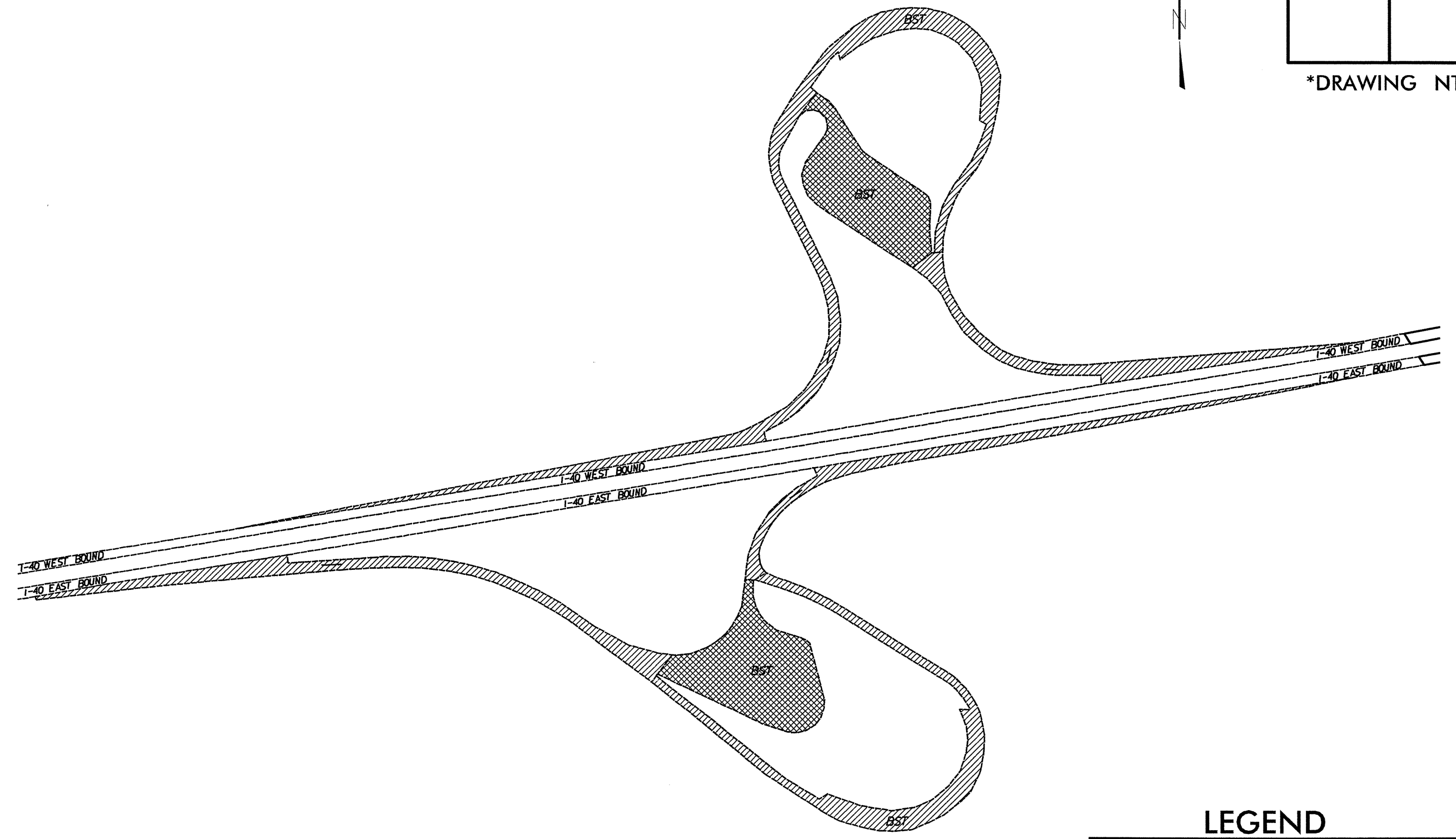
SYSTEMS

I-40 REST AREA DETAILS



PROJECT REFERENCE NO. 1-5502	SHEET NO. 8
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

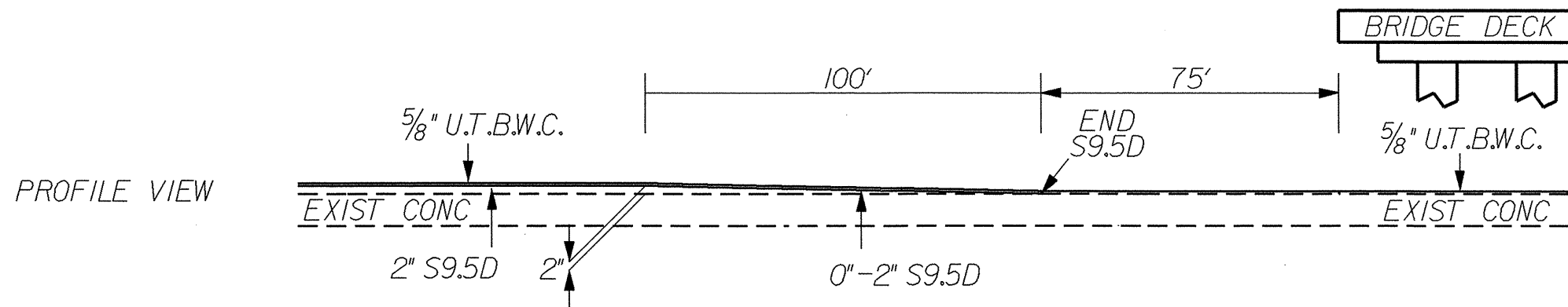
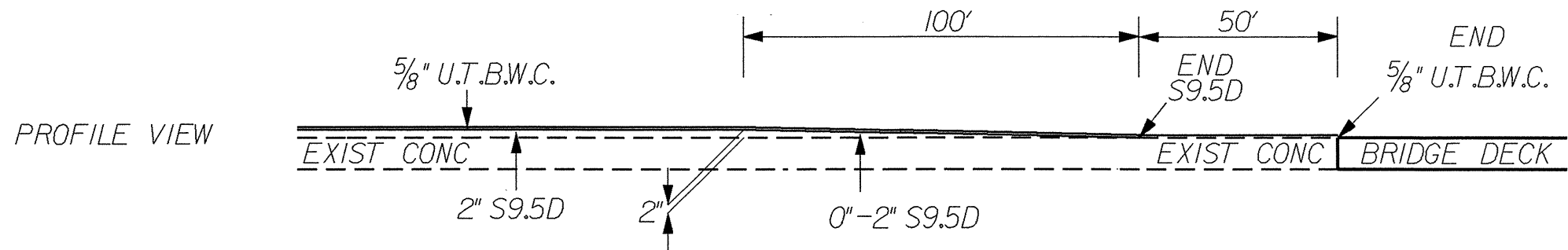


*DRAWING NTS



LEGEND

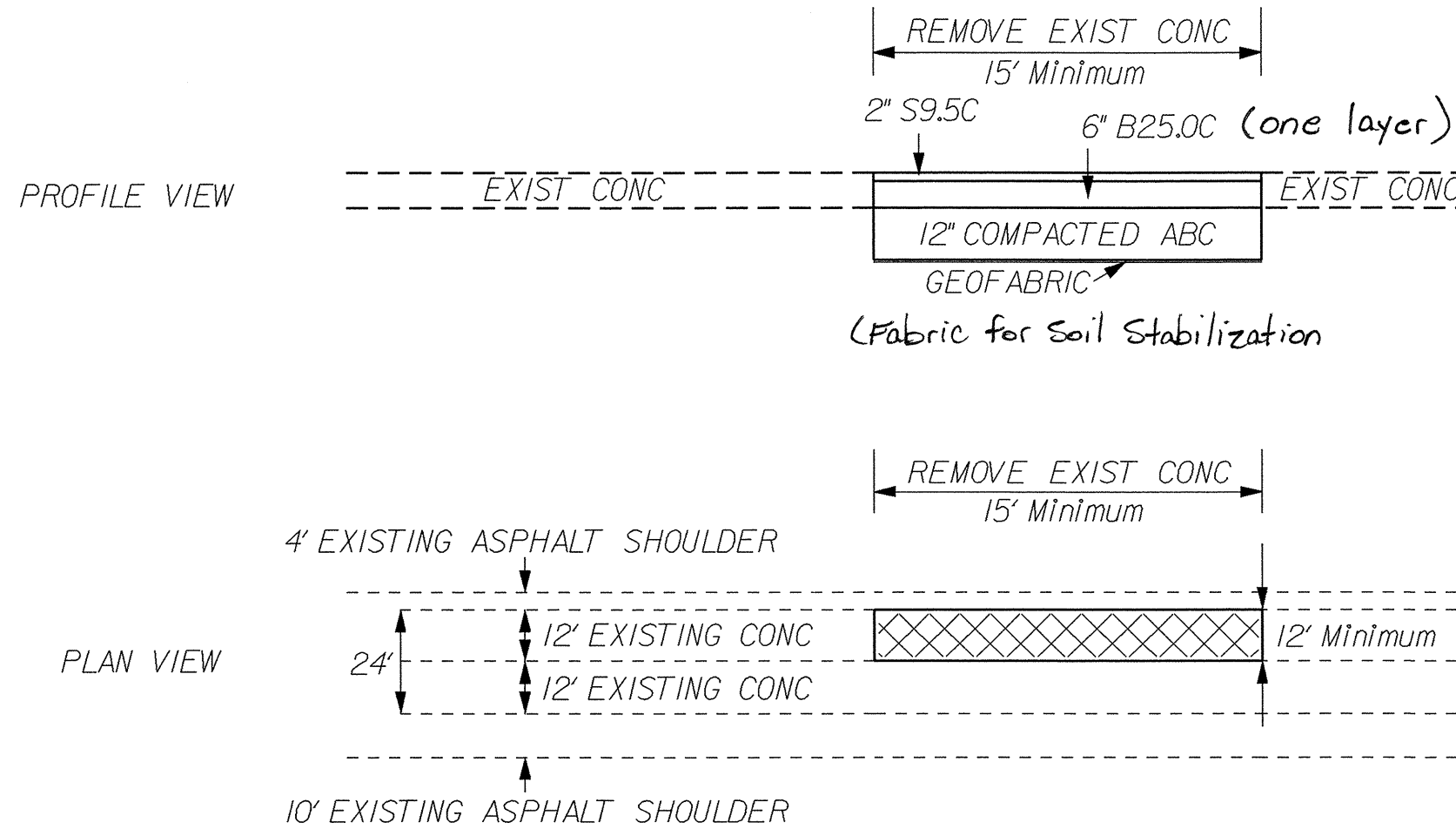
- 2" MILLING WITH 2" S9.5C ASPHALT FILL 
- 2" S9.5C OVERLAY 



Bridge Approach And Departure Detail

USE DETAIL AT THE FOLLOWING LOCATIONS

BEGINNING AT THE BRIDGES OVER THE CATAWBA RIVER
 AT THE CATAWBA /IREDELL COUNTY LINE TO STAMEY FARM ROAD



REPAIR OF EXISTING PAVEMENT

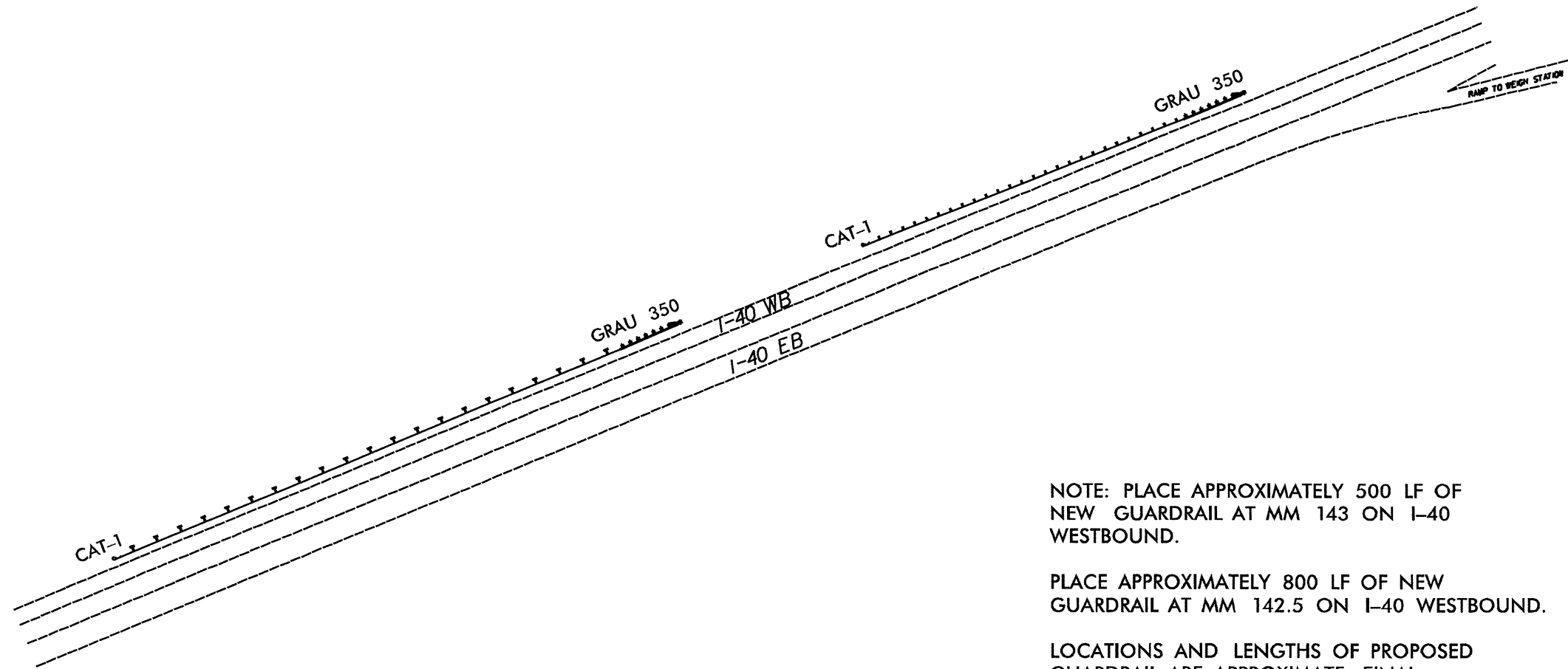
B/17/95

PROPOSED GUARDRAIL LOCATION



PROJECT REFERENCE NO. 1-5502	SHEET NO. 11
RAW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

***DRAWING NTS**



NOTE: PLACE APPROXIMATELY 500 LF OF NEW GUARDRAIL AT MM 143 ON I-40 WESTBOUND.

PLACE APPROXIMATELY 800 LF OF NEW GUARDRAIL AT MM 142.5 ON I-40 WESTBOUND.

LOCATIONS AND LENGTHS OF PROPOSED GUARDRAIL ARE APPROXIMATE. FINAL LENGTH AND LOCATION TO BE DETERMINED BY THE ENGINEER.

SYNTHETIC
 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63 64 65 66 67 68 69 70 71 72 73 74 75 76 77 78 79 80 81 82 83 84 85 86 87 88 89 90 91 92 93 94 95 96 97 98 99 100

PROJECT NO.	SHEET NO.	TOTAL NO.
I-5502	13	13

SUMMARY OF QUANTITIES

PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	CONSTRUCTION SURVEYING LS	DIAMOND GRINDING PCC PAVEMENT SY	CLEARING & GRUBBING .. ACRE(S) LS	SUPPLEMENTARY CLEARING & GRUB-BING ACR	UNCLASSIFIED EXCAVATION CY	UNDERCUT EXCAVATION CY	BORROW CY	REMOVAL OF EXISTING ASPHALT PAVEMENT SY	REMOVAL OF EXISTING CONCRETE PAVEMENT SY	REMOVAL OF EXISTING CONCRETE PAVEMENT SLABS SY	FABRIC FOR SOIL STABILIZATION SY	FINE GRADING LS	STABILIZER AGGREGATE TONS	AGGREGATE BASE COURSE TONS	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	2" MILLING SY	INCIDENTAL MILLING SY	
I-5502	Catawba	1	I-40	I-40 from NC 16 Bus. To the Iredell County Line	1, 1A, 1B, 2	NO	8.7	48		232,922			5,030	1,300				4,000	4,000			2,605	1,200		27,444	3,000	
TOTAL FOR MAP NO. 1							8.7			232,922			5,030	1,300				4,000	4,000			2,605	1,200		27,444	3,000	
I-5502	Iredell	2	I-40	I-40 from the Catawba County line to SR 1753	2, 2A, 2B	NO	6.0	48	1		1	1	3,670	1,300	2,500	350	1,700	4,000	4,000	1	200	2,605	1,200	12.00		1,000	
TOTAL FOR MAP NO. 2							6.0		1	0	1	1	3,670	1,300	2,500	350	1,700	4,000	4,000	1	200	2,605	1,200	12.00		1,000	
TOTAL FOR PROJ NO. I-5502							14.7		1	232,922	1	1	8,700	2,600	2,500	350	1,700	8,000	8,000	1	200	5,210	2,400	12.00		27,444	4,000
GRAND TOTAL							14.7		1	232,922	1	1	8,700	2,600	2,500	350	1,700	8,000	8,000	1	200	5,210	2,400	12.00		27,444	4,000

SUMMARY OF QUANTITIES

PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	BASE COURSE, B25.0C TONS	INTERMEDIATE COURSE, I19.0C TONS	SURFACE COURSE, S9.5C TONS	SURFACE COURSE, S9.5D TONS	ASPHALT BINDER FOR PLANT MIX TONS	POLYMER MODIFIED ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	PATCHING CONCRETE PAVEMENT SPALLS SF	ULTRATHIN HOT MIX, TYPE B TONS	APPLICATION OF ULTRATHIN HOT MIX SY	MILLED RUMBLE STRIPS (ASPHALT CEMENT CONCRETE) LF	STEEL BM GUARDRAIL LF	ADDITIONAL GUARDRAIL POSTS EA	GUARDRAIL ANCHOR UNITS, TYPE CAT-1 EA	GUARDRAIL ANCHOR UNITS, TYPE 350 EA	PORTABLE LIGHTING LS	TEMPORARY SILT FENCE LF	WATTLE LF		
I-5502	Catawba	1	I-40	I-40 from NC 16 Bus. To the Iredell County Line	1, 1A, 1B, 2	NO	8.7	48	8,940		7,920	2,114	861	672	1,000	550	10,613	303,229	183,744			9	19	LS		500		
TOTAL FOR MAP NO. 1							8.7		8,940		7,920	2,114	861	672	1,000	550	10,613	303,229	183,744					9	19	LS		500
I-5502	Iredell	2	I-40	I-40 from the Catawba County line to SR 1753	2, 2A, 2B	NO	6	48	6,370	390	5,110	34,435	600	2,349	1,000	550	7,432	212,343	126,720	1,300	5	2	7	LS	1,200	500		
TOTAL FOR MAP NO. 2							6		6,370	390	5,110	34,435	600	2,349	1,000	550	7,432	212,343	126,720	1,300	5	2	7	LS	1,200	500		
TOTAL FOR PROJ NO. I-5502							14.7		15,310	390	13,030	36,549	1,461	3,021	2,000	1,100	18,045	515,572	310,464	1,300	5	11	26	1	1,200	1,000		
GRAND TOTAL							14.7		15,310	390	13,030	36,549	1,461	3,021	2,000	1,100	18,045	515,572	310,464	1,300	5	11	26	1	1,200	1,000		

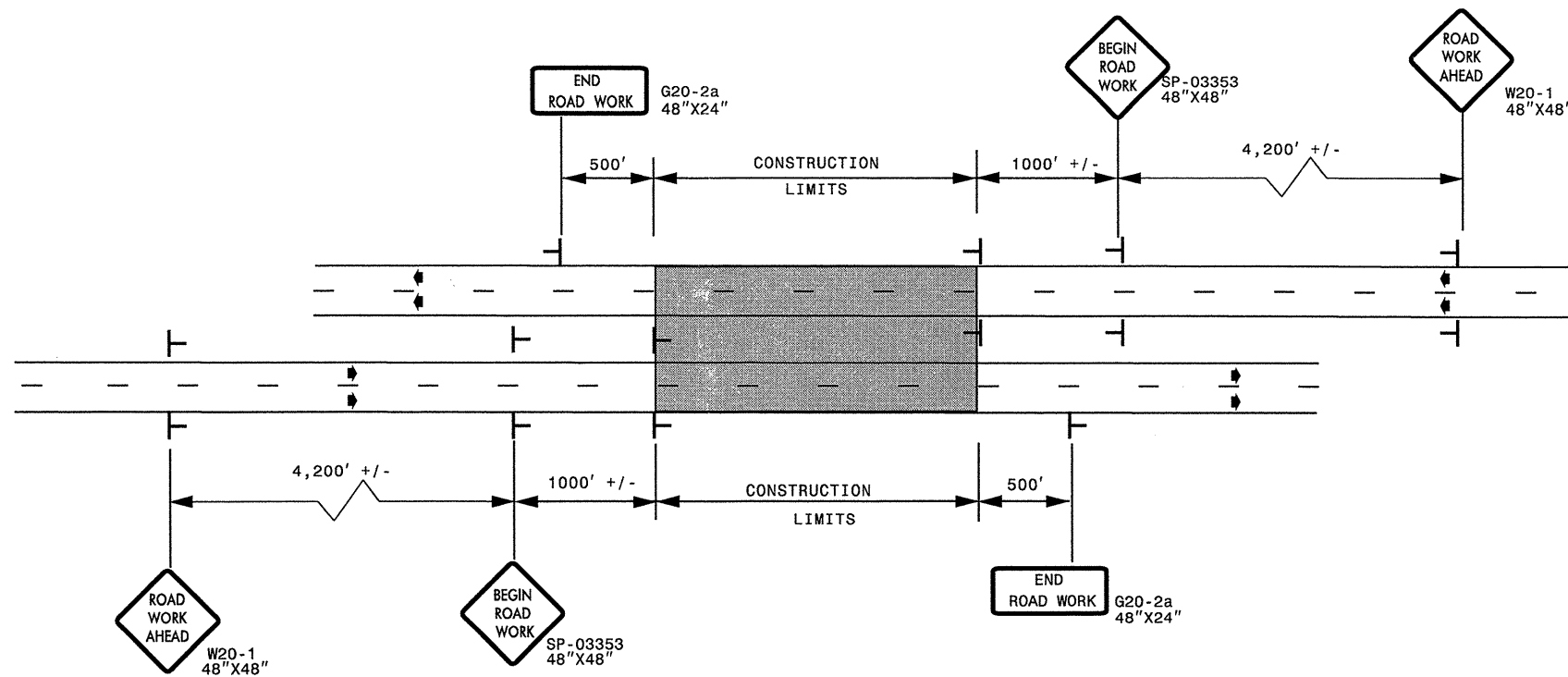
THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	LENGTH	WIDTH	4589000000-N		4710000000-E				4725000000-E				4815000000-E		4825000000-E		4835000000-E		4845000000-N		4847100000-E		4847120000-E		4905000000-N	
							TRAFFIC CONTROL LS	24" X 120 M WHITE THERMO LF	THERMO STR ARROW 90 M EA	THERMO RT ARROW 90 M EA	THERMO LT ARROW 90 M EA	THERMO STR & LT ARROW 90 M EA	6" WHITE PAINT LF	6" YELLOW PAINT LF	12" WHITE PAINT LF	24" WHITE PAINT LF	PAINT STR ARROW EA	PAINT RT ARROW EA	6" WHITE POLYUREA (Highly Reflective Elements) LF	6" YELLOW POLYUREA (Highly Reflective Elements) LF	12" WHITE POLYUREA (Highly Reflective Elements) LF	SNOW PLOWABLE MARKERS EA								
I-5502	Catawba	1	I-40	I-40 from NC 16 Bus. To the Iredell County Line	8.7	48	LS	160	2	1					275,000	225,000	20,000	320	4	2			126,529	110,422	9,150	2,300				
TOTAL FOR MAP NO. 1							8.7	160	2	1					275,000	225,000	20,000	320	4	2					126,529	110,422	9,150	2,300		
I-5502	Iredell	2	I-40	I-40 from the Catawba County line to SR 1753	6	48	LS				3	3			180,000	150,000	15,100						90,197	71,360	7,550	800				
TOTAL FOR MAP NO. 2							6					3	3			180,000	150,000	15,100								90,197	71,360	7,550	800	
TOTAL FOR PROJ NO. I-5502							14.7	160	2	1		3	3			455,000	375,000	35,100	320	4	2					216,726	181,782	16,700	3,100	
GRAND TOTAL							14.7	160	2	1		3	3			830,000	630,000	50,200	320	4	2					398,508	281,782	26,050	3,100	

ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

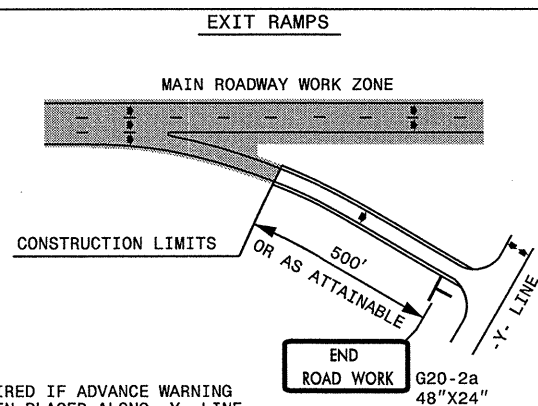
PROJ. REFERENCE NO.	SHEET NO.
12CR.10491.10, 12CR.20491.11	TCP-2

DETAIL A



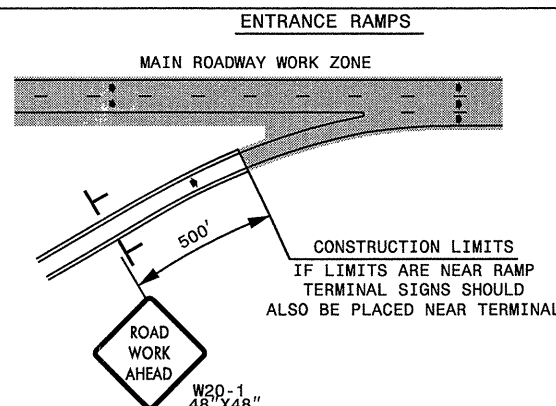
* USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.

DETAIL B

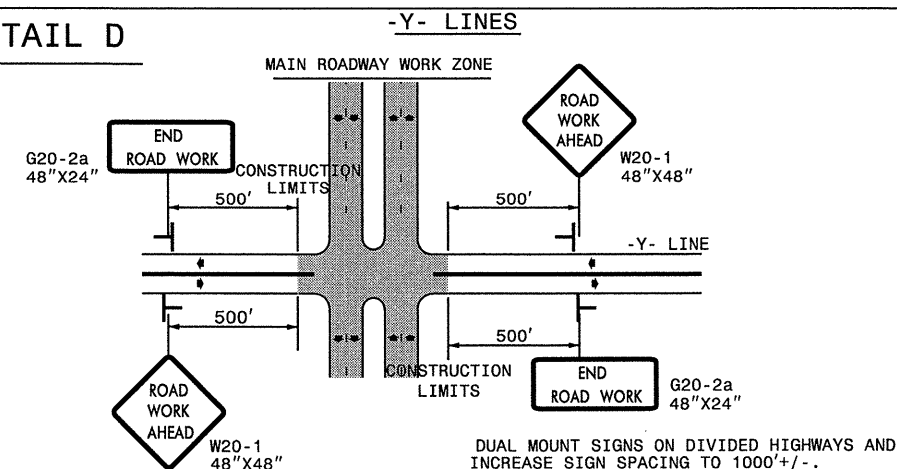


NOTE: SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

DETAIL C



DETAIL D

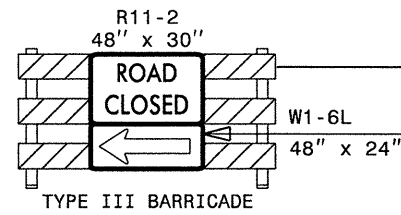
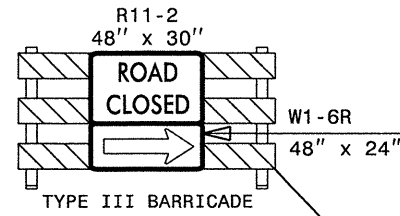


GENERAL NOTES

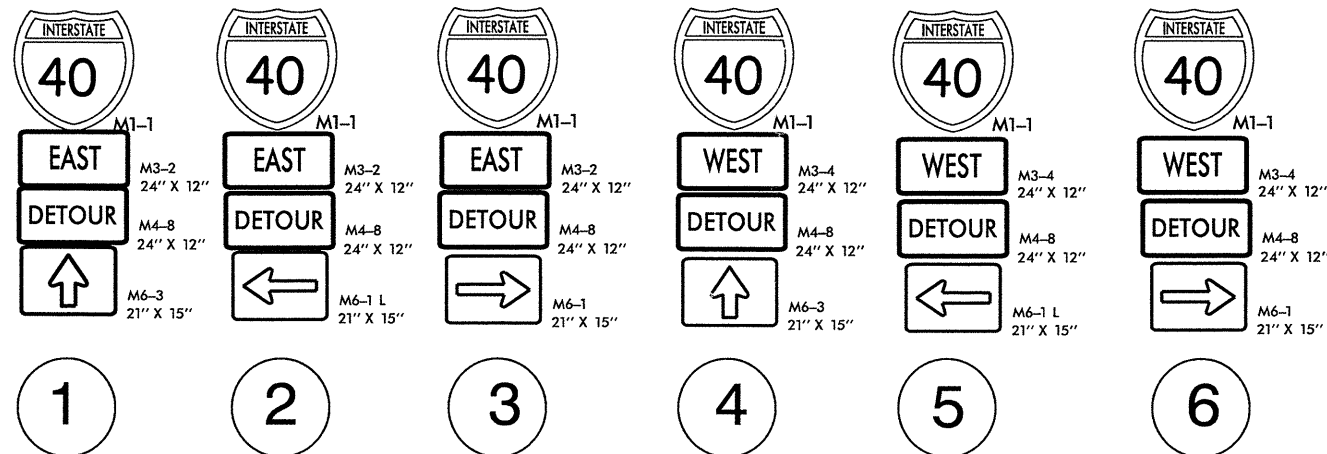
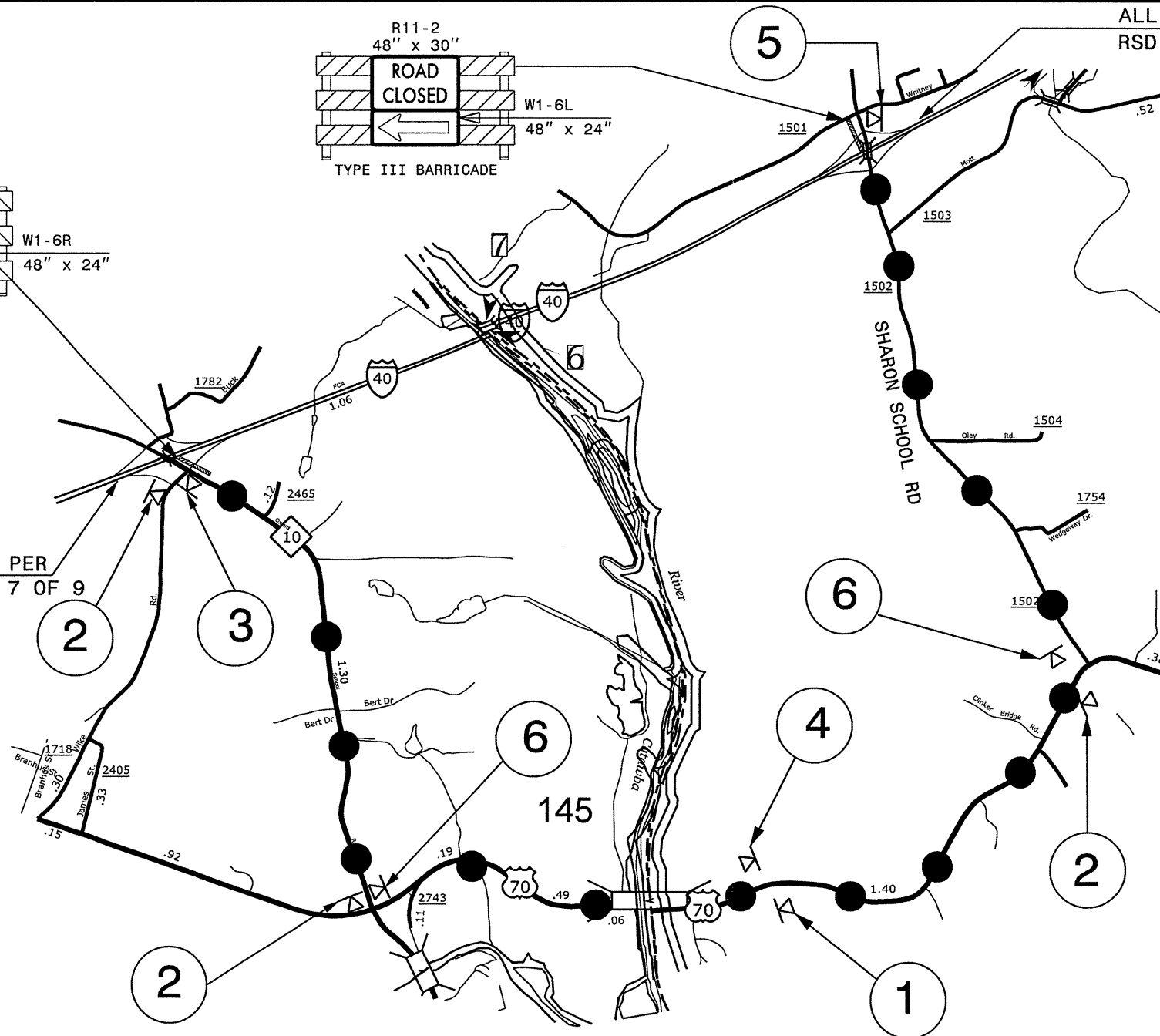
- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

APPROVED: _____ DATE: _____	ADVANCED WORK ZONE WARNING SIGNS FOR FREEWAYS (4 LANES OR GREATER)	
SEAL		
	DATE: 8/03	REVISIONS
	DWG. BY: JI	03/04
	DESIGN BY: JI	
	REVIEWED BY:	

I:\LAN-20110810\GROUPS\WZTC\TMU\WZTC\Resur-facing\2011Western\2011.Div12\C202719A-B_12CR.10491.10x2_1r.edell_US21\C202719A-B_12CR.10491.10x2_1r.edell_US21\Freeways_4lanes_or_greater_stationary.dgn
 AT TE247733
 sngreen



ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9



APPROVED: _____ DATE: _____

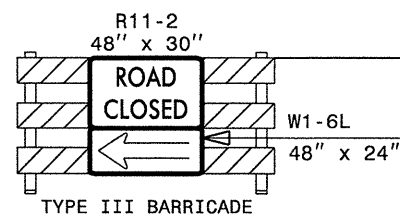
SEAL

OFF-SITE DETOUR FOR
I-40 CLOSED BETWEEN NC 10 & SHARON
SCHOOL ROAD

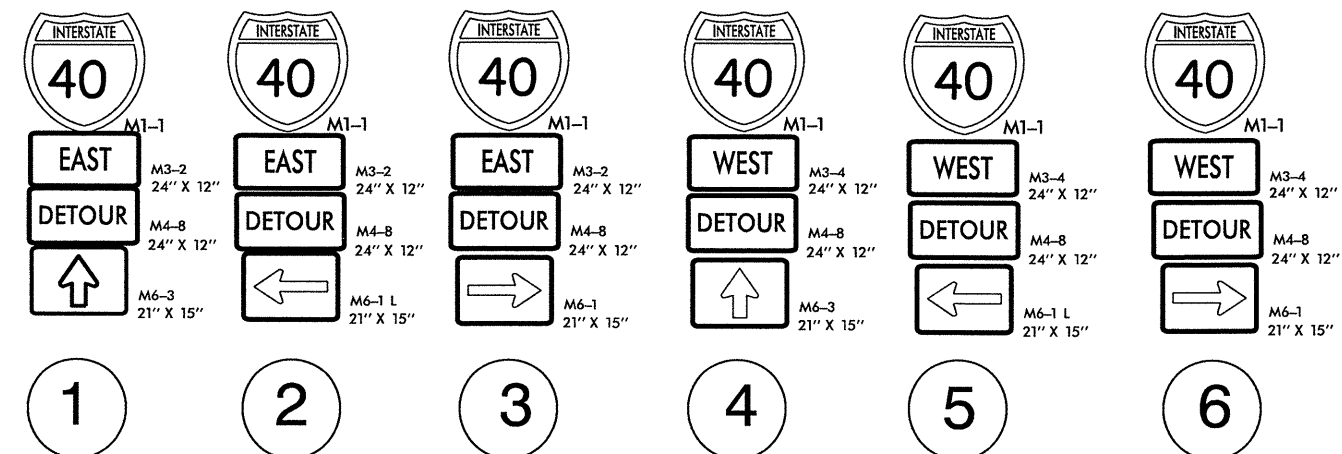
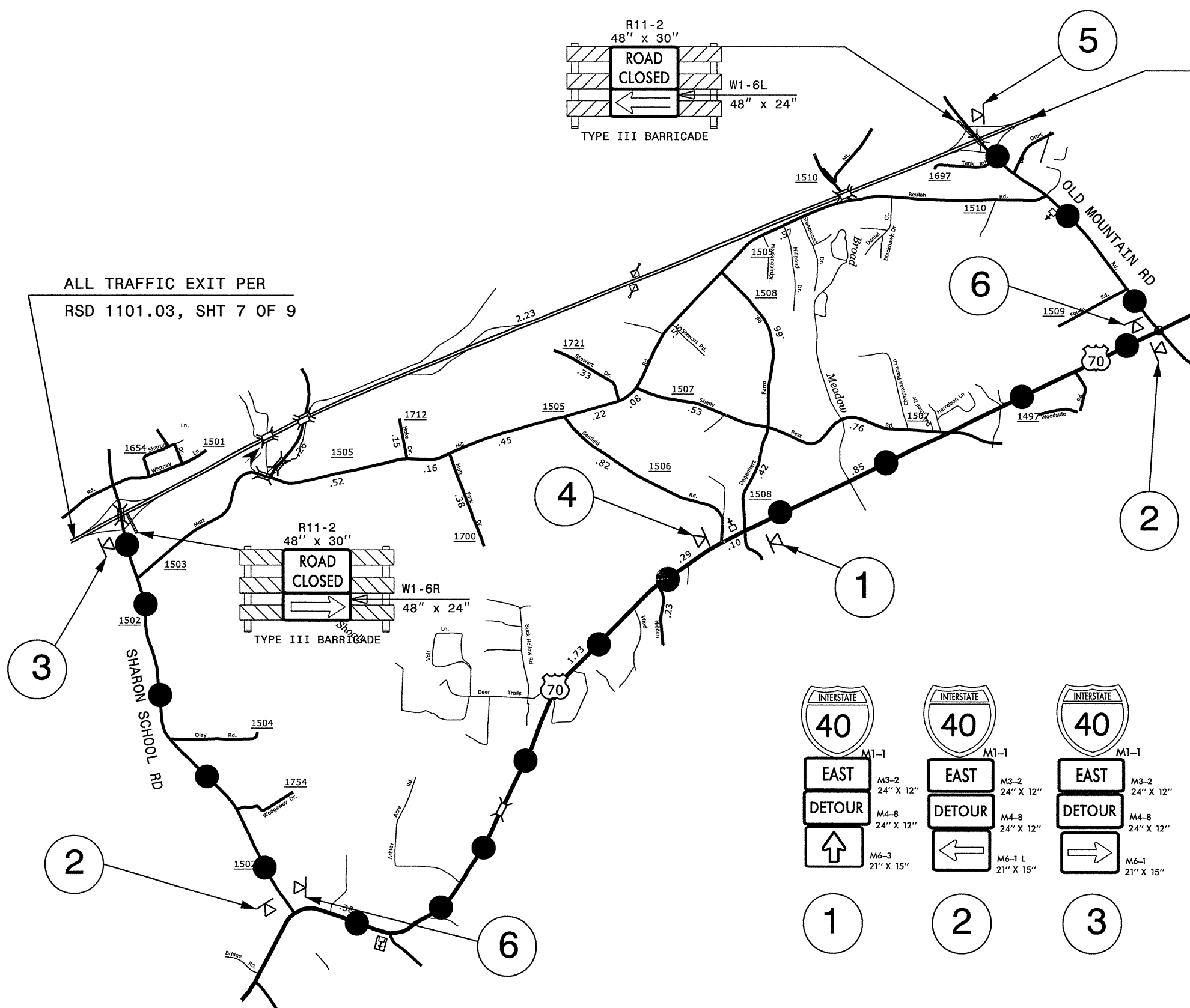
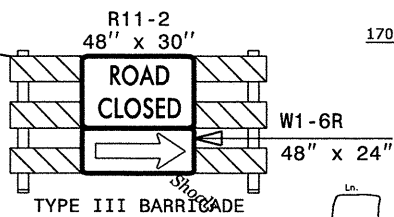
SCALE: NONE		REVISIONS
DATE: 5/11		
DESIGN BY:		
REVIEWED BY:		

27-MAY-2010 08:41
 C:\Data\cd drive\woolard_data\projects\I-40_Interstate_Maintenance_Catowbo-Iredell\Iredell.ms.ENG.dgn
 Woolard AT TE24740

ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9

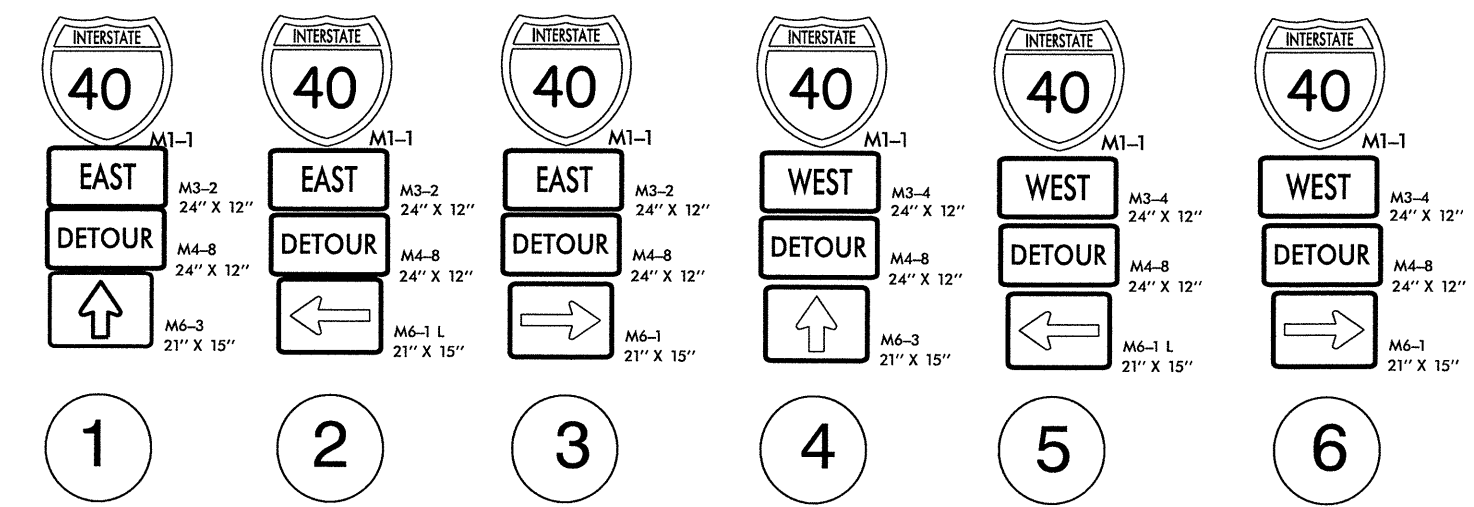
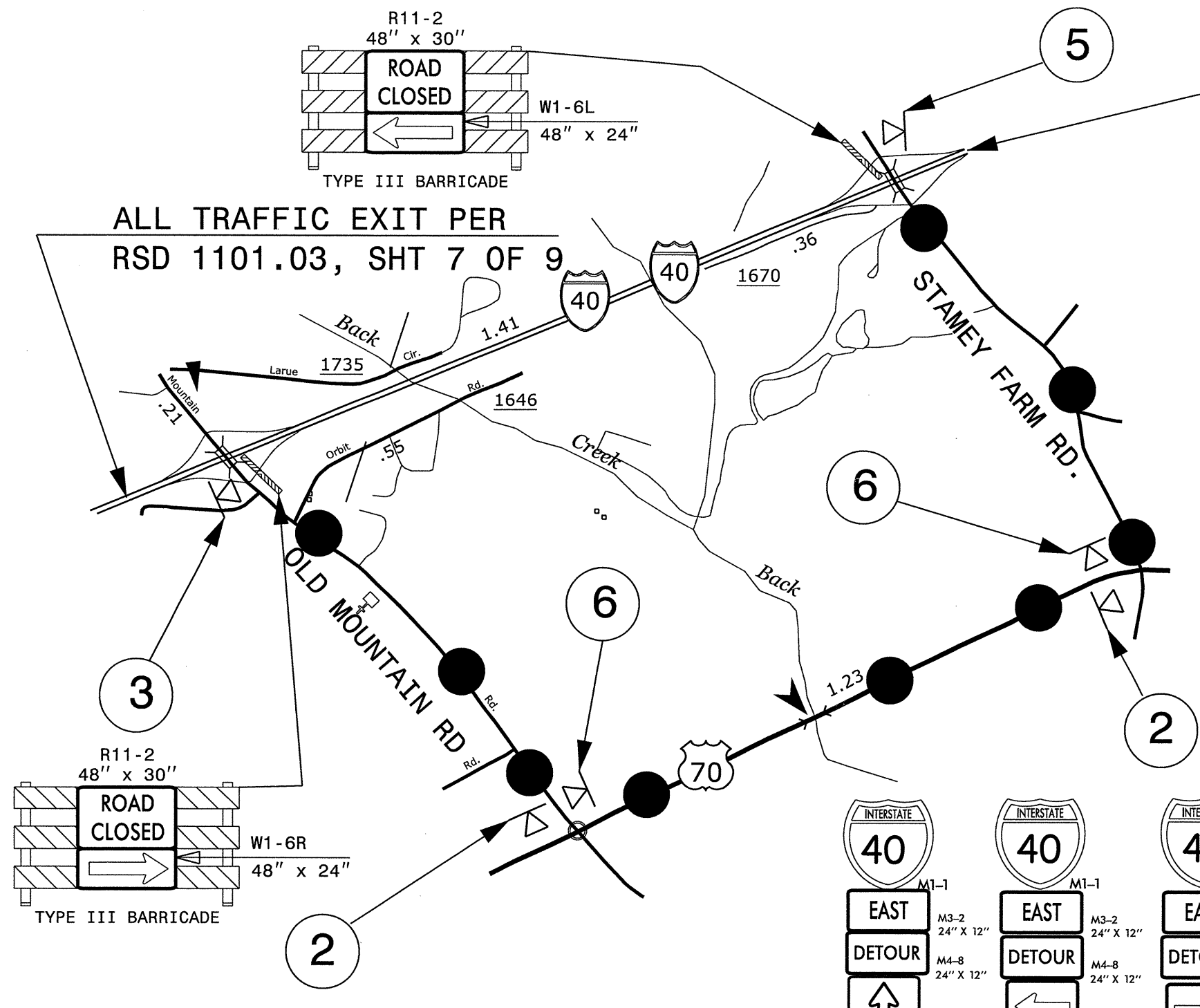


ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9



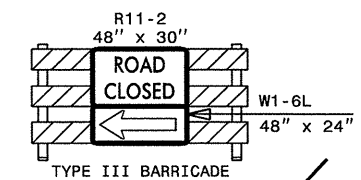
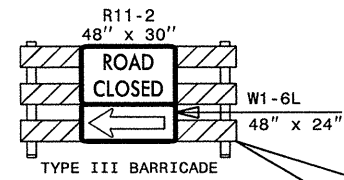
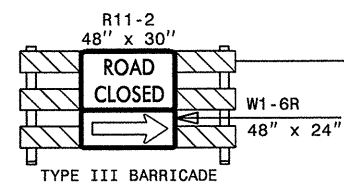
27-MAY-2011 08:42
 C:\Data\drive\woolard\data\projects\i-40\interstate-40\projects\i-40\interstate-40\woolard.ms_eng.dgn
 woolard AT 12244740

APPROVED: _____ DATE: _____	OFF-SITE DETOUR FOR I-40 CLOSED BETWEEN SHARON SCHOOL ROAD AND OLD MOUNTAIN ROAD	
SEAL		
SCALE: NONE		REVISIONS
DATE: 5/11		
DWG. BY: _____		
DESIGN BY: _____		
REVIEWED BY: _____		CARD FILE



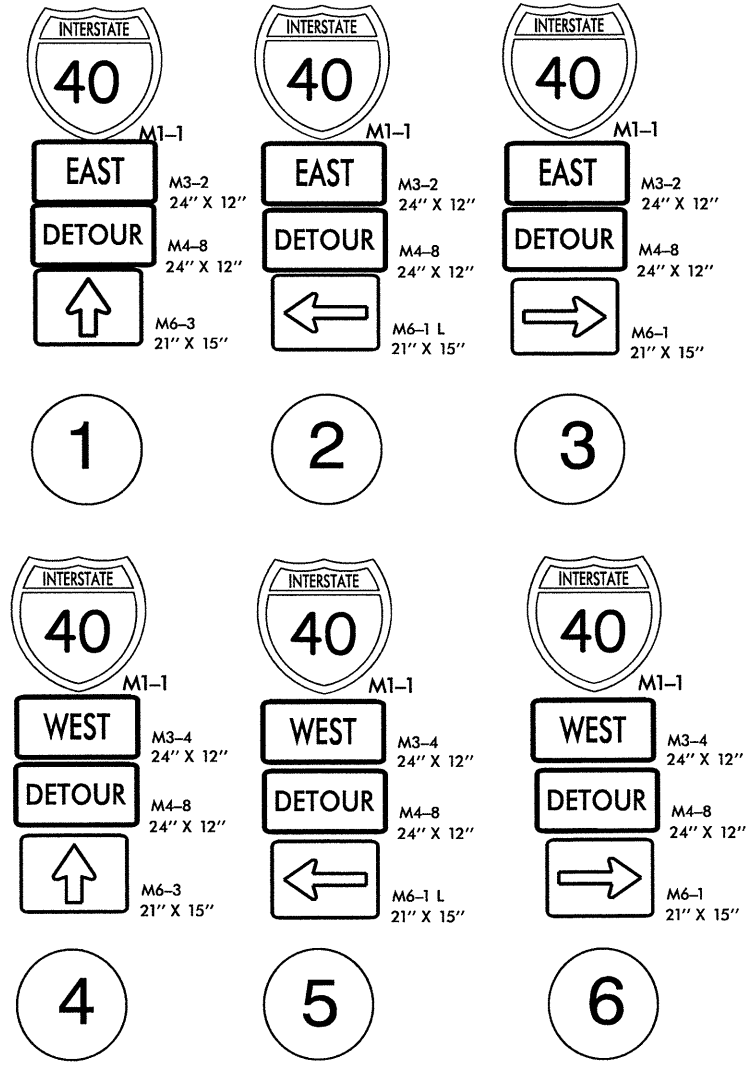
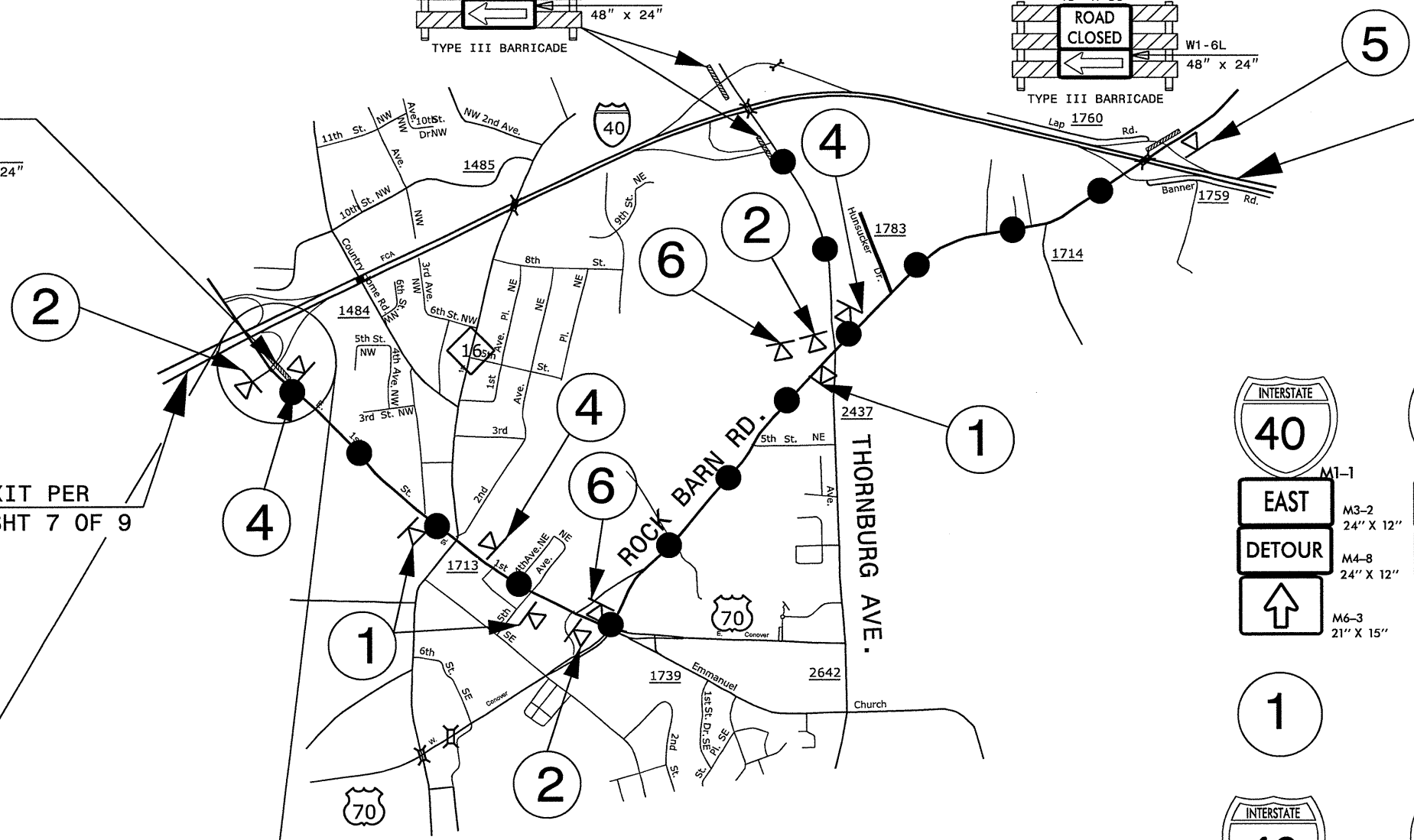
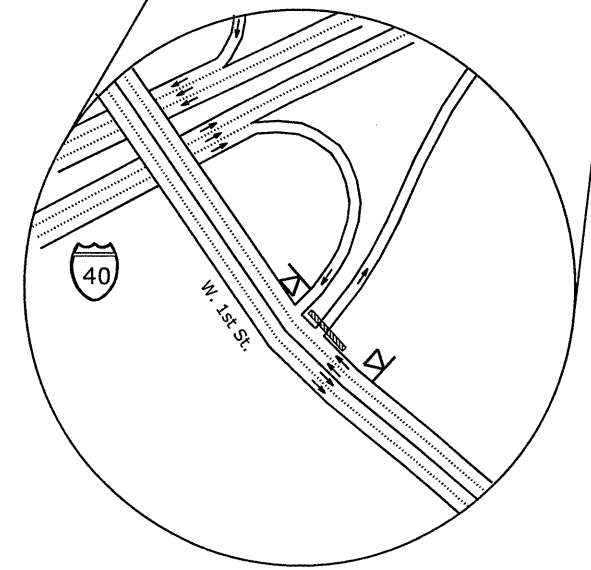
APPROVED: _____	DATE: _____	OFF-SITE DETOUR FOR I-40 CLOSED BETWEEN OLD MOUNTAIN ROAD AND STAMEY FARM ROAD	
SEAL	SCALE: NONE		REVISIONS
	DATE: 5/11		
	DWG. BY: _____		
	DESIGN BY: _____		
REVIEWED BY: _____			

27-MAY-2011 14:26
 C:\Data\drive\woolard_data\projects\I-40_interstate_maintenance\Catawba-Fedell\IREDELL_MS.ENG.dgn
 woolard AT TE244740

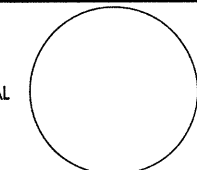


ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9

ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9

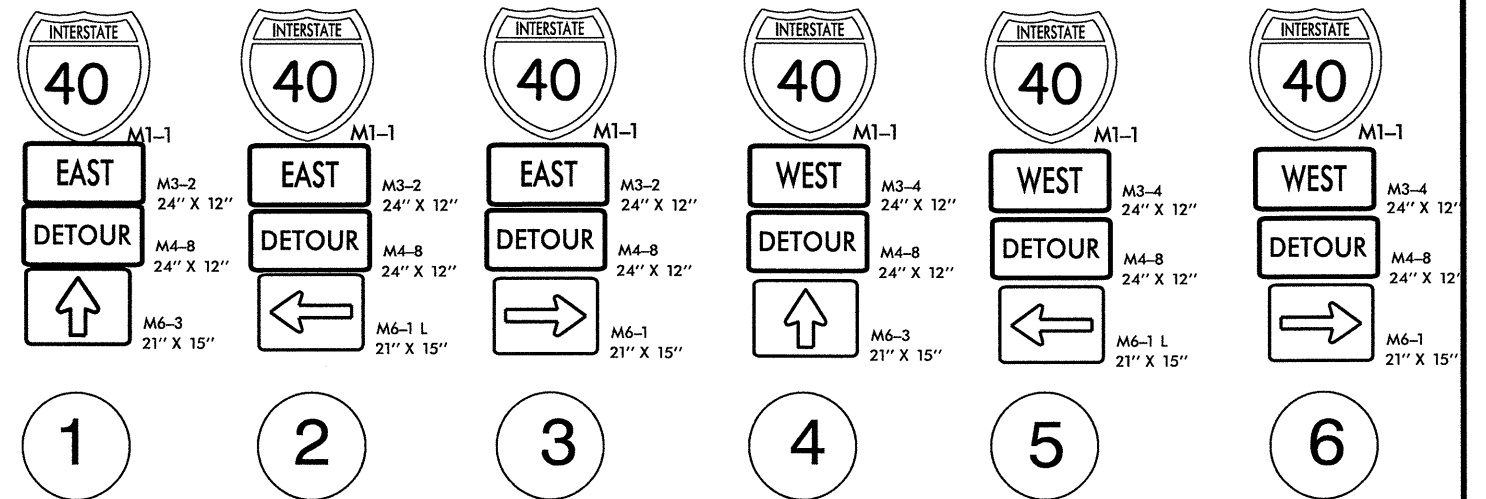
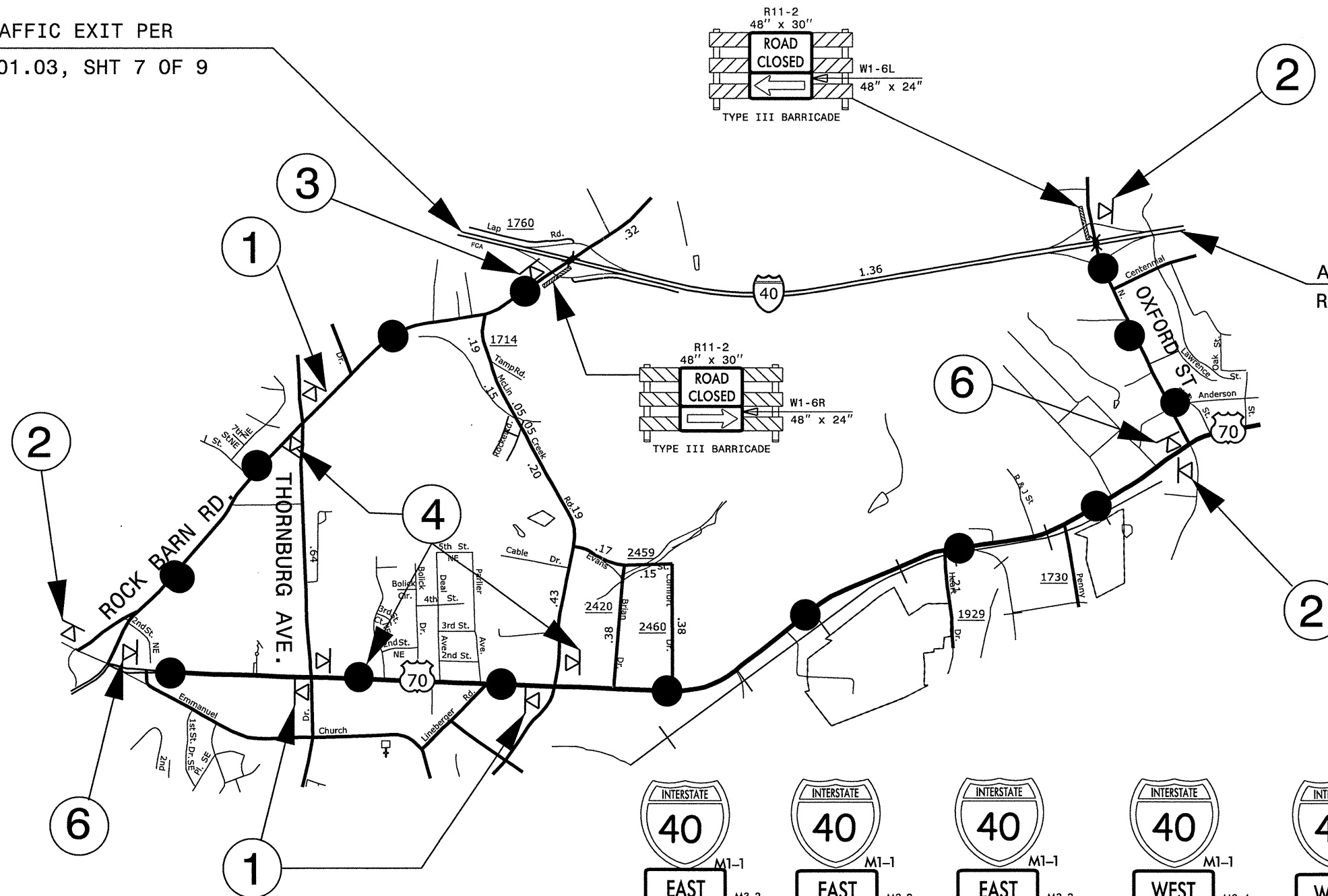


R:\JUL-2011\5-31 DOT OFF-SITE DETOUR GROUPS-WZTCCC\2011Western\2011Div2\C202802-I-5502-Corowbo_r\redell_i-40\sg\1-5502_Offsite_Detours.dgn
 sngr.een AT TE244733

APPROVED: _____	DATE: _____	OFF-SITE DETOUR FOR I-40 CLOSED BETWEEN NC 16 AND ROCK BARN ROAD		
				SCALE: NONE
		DESIGN BY: _____	REVIEWED BY: _____	DATE: _____

ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9

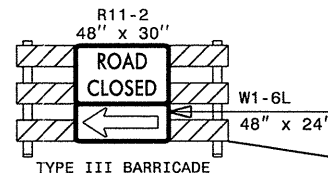
ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9



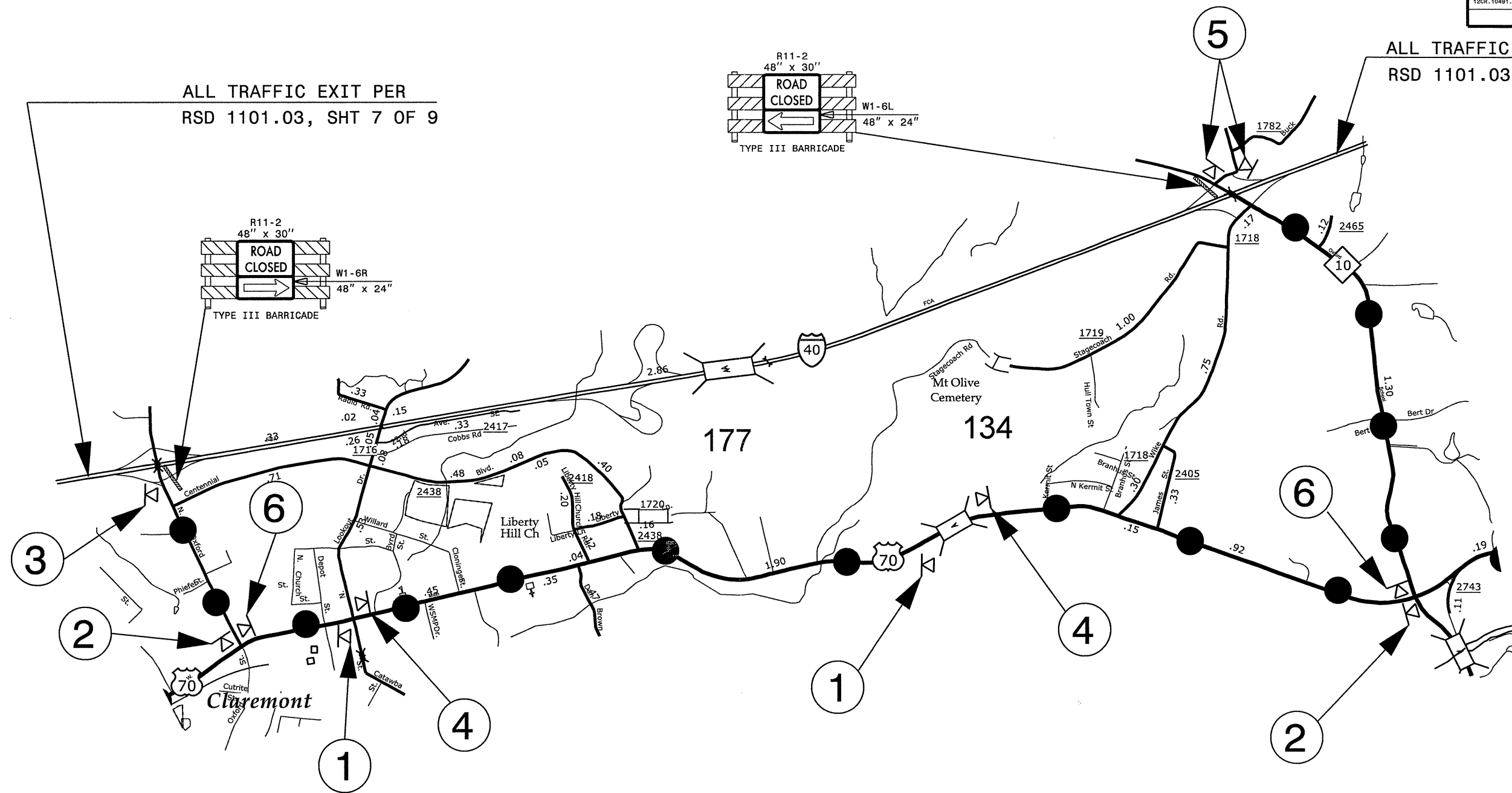
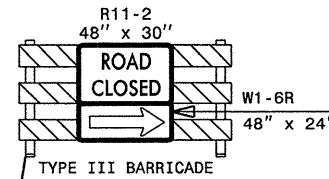
31-MAY-2011 14:17
 C:\Data\drive\woolard_data\projects\I-40_interstate_maintenance\catowba-iredeil\IREDEL.L_MS.ENG.dgn
 Woolard AT TE24740

APPROVED: _____	DATE: _____	OFF-SITE DETOUR FOR I-40 CLOSED BETWEEN ROCK BARN RD. AND OXFORD ST.	
SEAL			
SCALE: NONE		REVISIONS	
DATE: _____			
DWG. BY: _____			
DESIGN BY: _____			
REVIEWED BY: _____			

ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9



ALL TRAFFIC EXIT PER
RSD 1101.03, SHT 7 OF 9



M1-1
EAST
M3-2
24" X 12"
DETOUR
M4-8
24" X 12"
M6-3
21" X 15"

1



M1-1
EAST
M3-2
24" X 12"
DETOUR
M4-8
24" X 12"
M6-1 L
21" X 15"

2



M1-1
EAST
M3-2
24" X 12"
DETOUR
M4-8
24" X 12"
M6-1
21" X 15"

3



M1-1
WEST
M3-4
24" X 12"
DETOUR
M4-8
24" X 12"
M6-3
21" X 15"

4



M1-1
WEST
M3-4
24" X 12"
DETOUR
M4-8
24" X 12"
M6-1 L
21" X 15"

5



M1-1
WEST
M3-4
24" X 12"
DETOUR
M4-8
24" X 12"
M6-1
21" X 15"

6

APPROVED: _____ DATE: _____

SEAL

**OFF-SITE DETOUR FOR I-40
CLOSED BETWEEN OXFORD ST.
AND NC 10**

SCALE: NONE
DATE: 5/11
DWG. BY:
DESIGN BY:
REVIEWED BY:

REVISIONS	

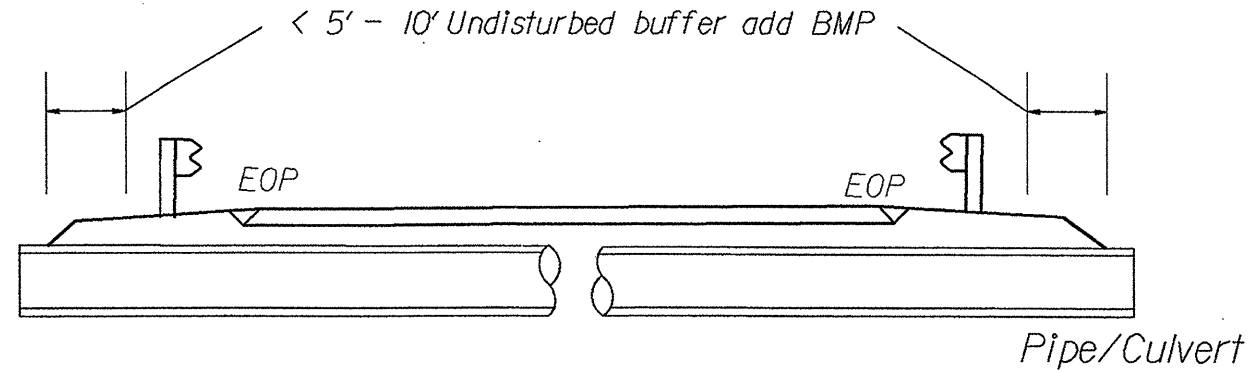
O:\JUN-2011\1359
 C:\Data\drive\woodard_data\projects\I-40_interstate_maintenance\Catawba-fredell\IREDELL_MS.ENG.dgn
 WOODARD AT 1E247470

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

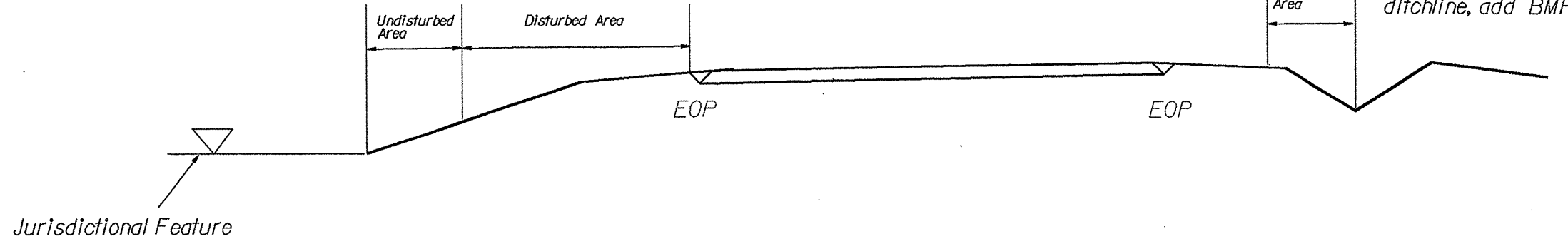
BMP Options: Wattle or Silt Fence

EROSION CONTROL DETAIL

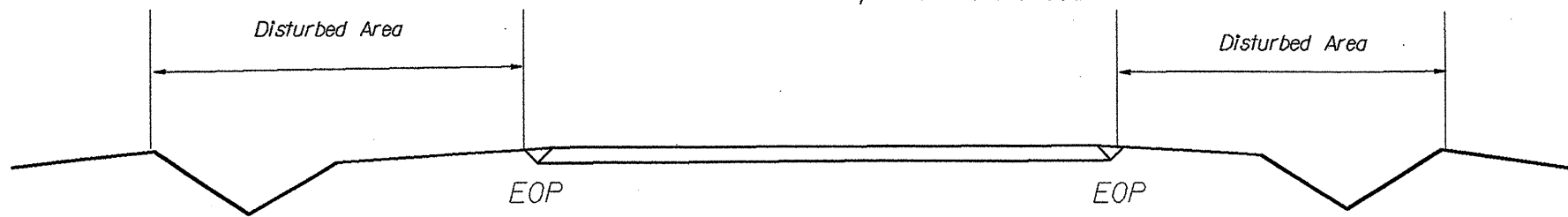
PROJECT REFERENCE NO. 1-5502	SHEET NO. EC-1
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER



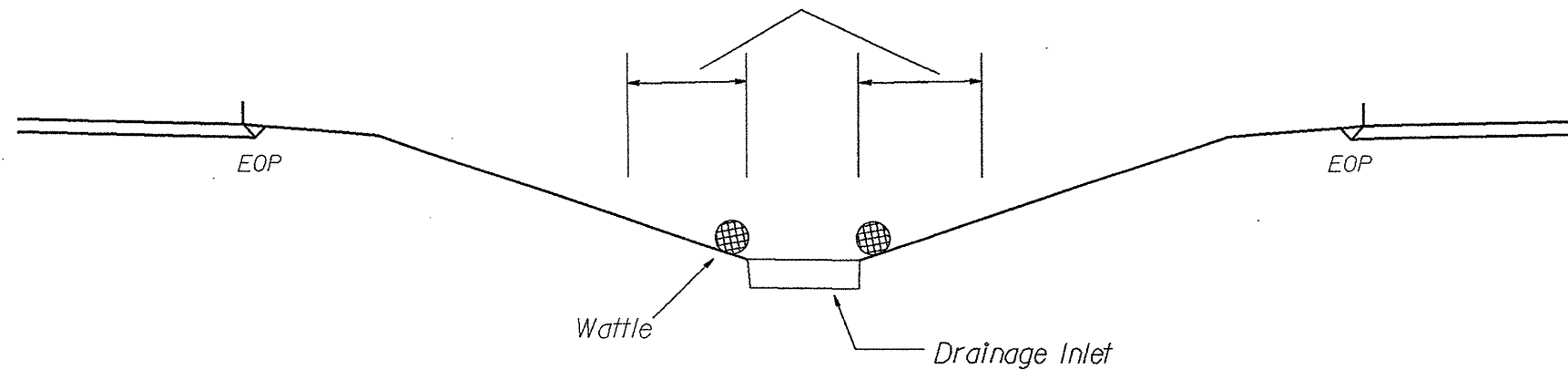
< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed



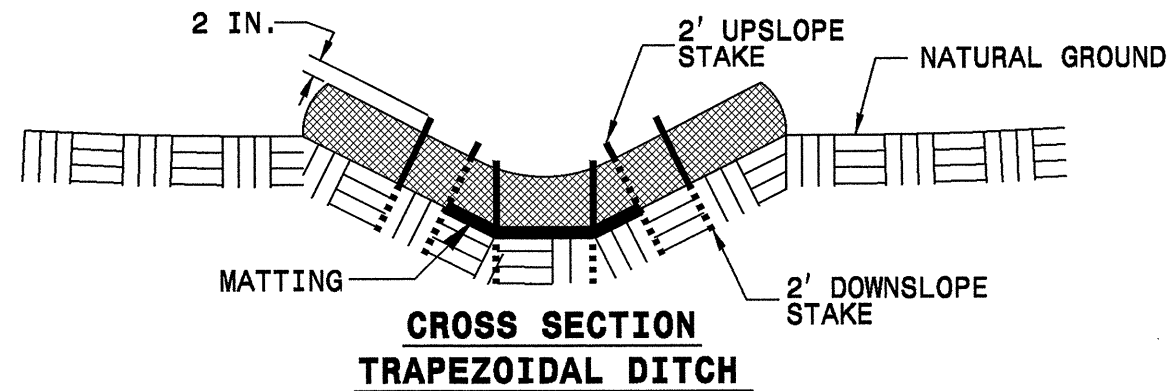
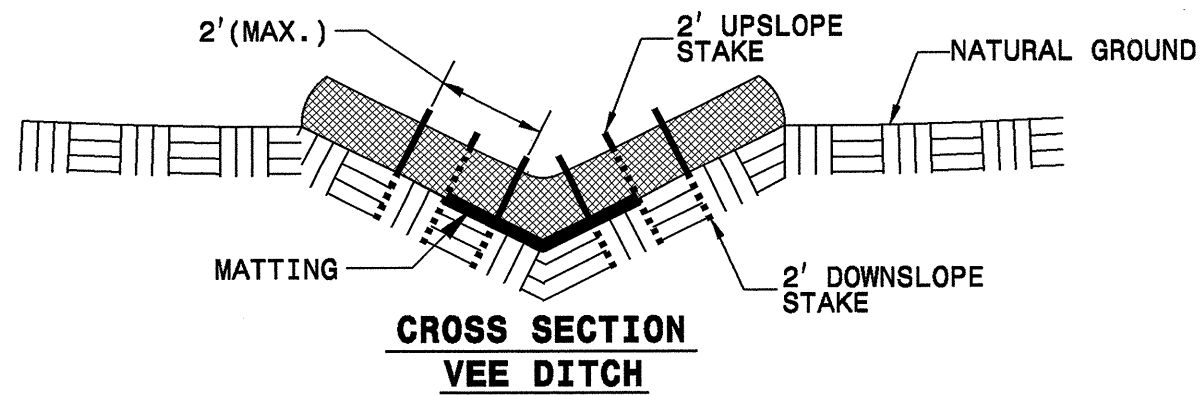
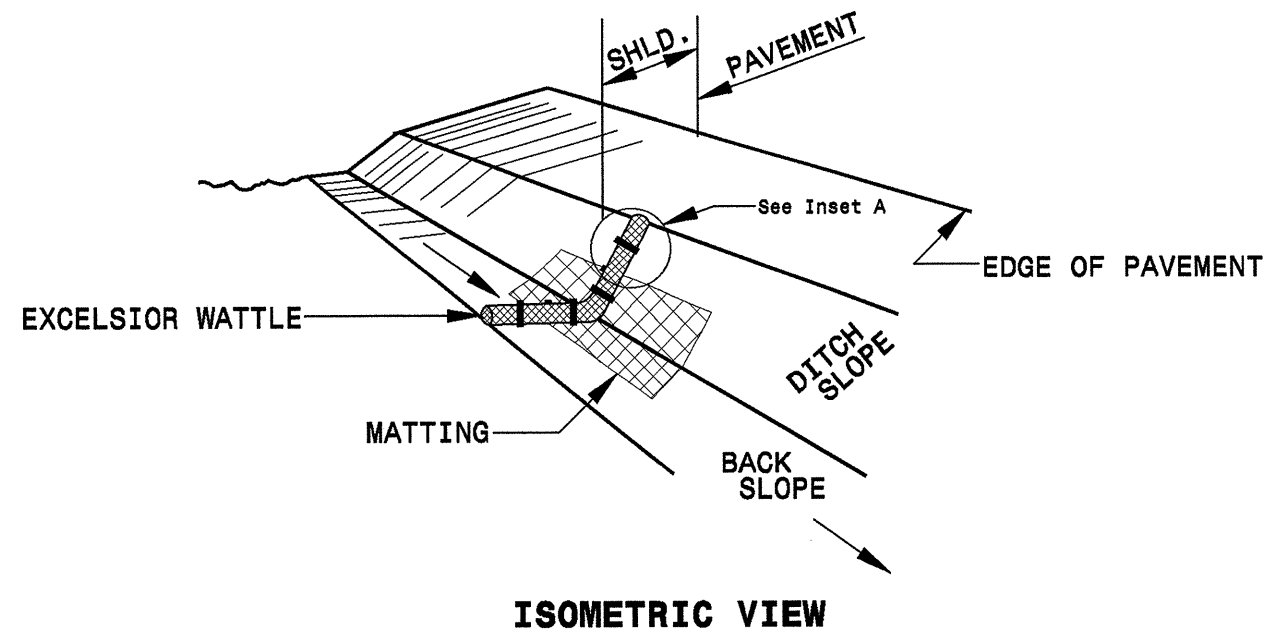
< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

PROJECT REFERENCE NO. 1-5502	SHEET NO. EC-2
R/W SHEET NO.	HYDRAULICS ENGINEER
ROADWAY DESIGN ENGINEER	

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

