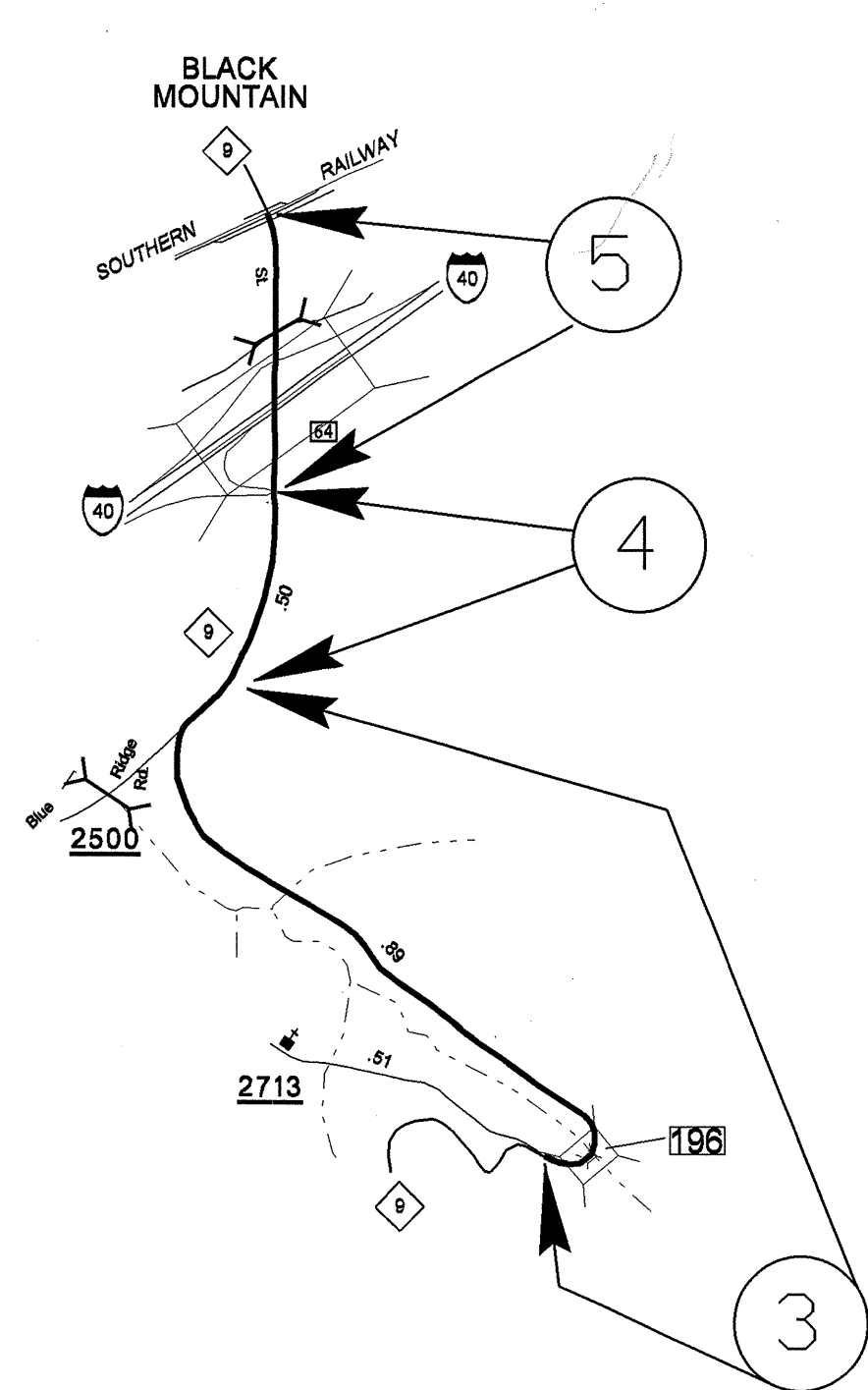
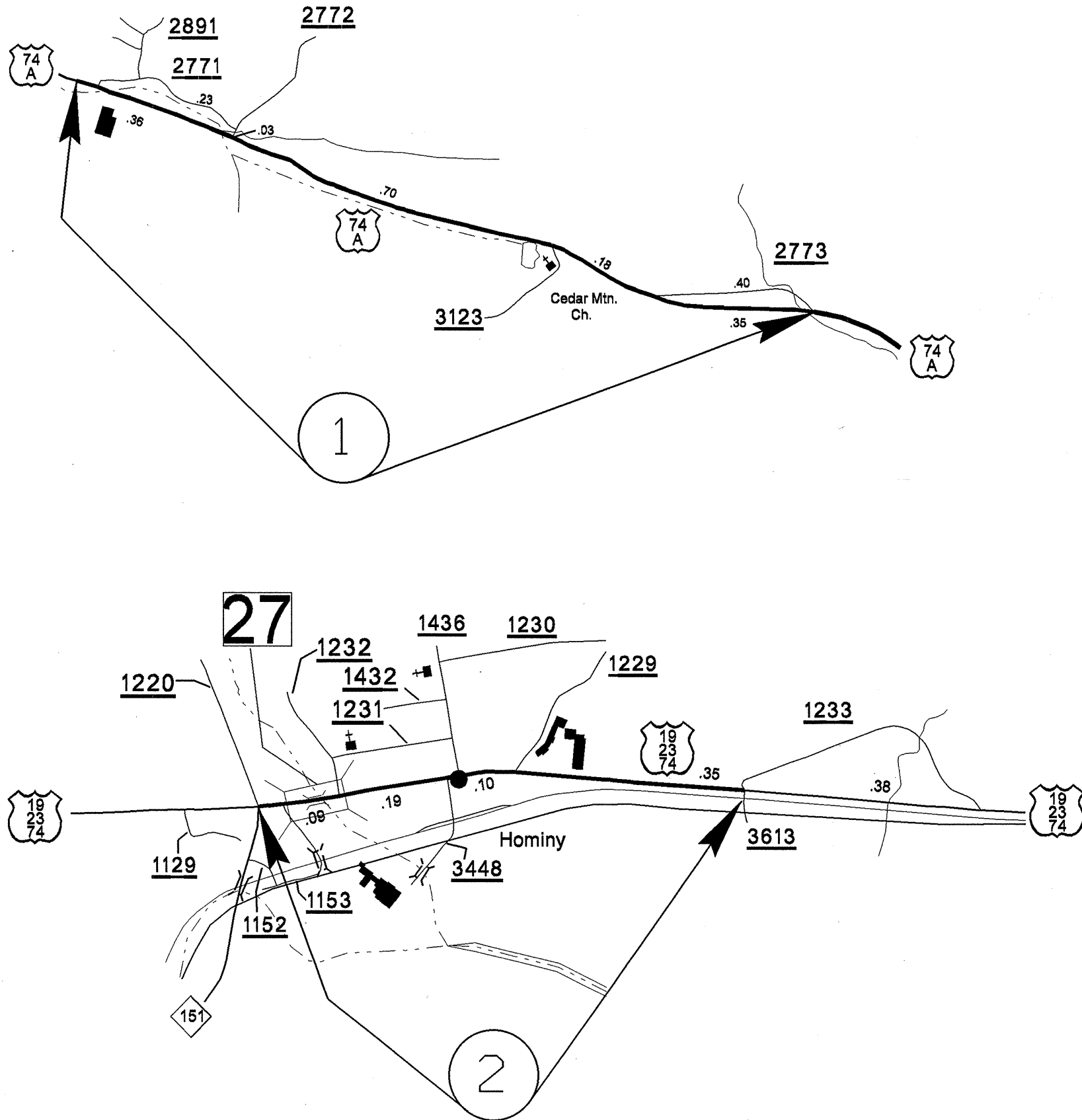
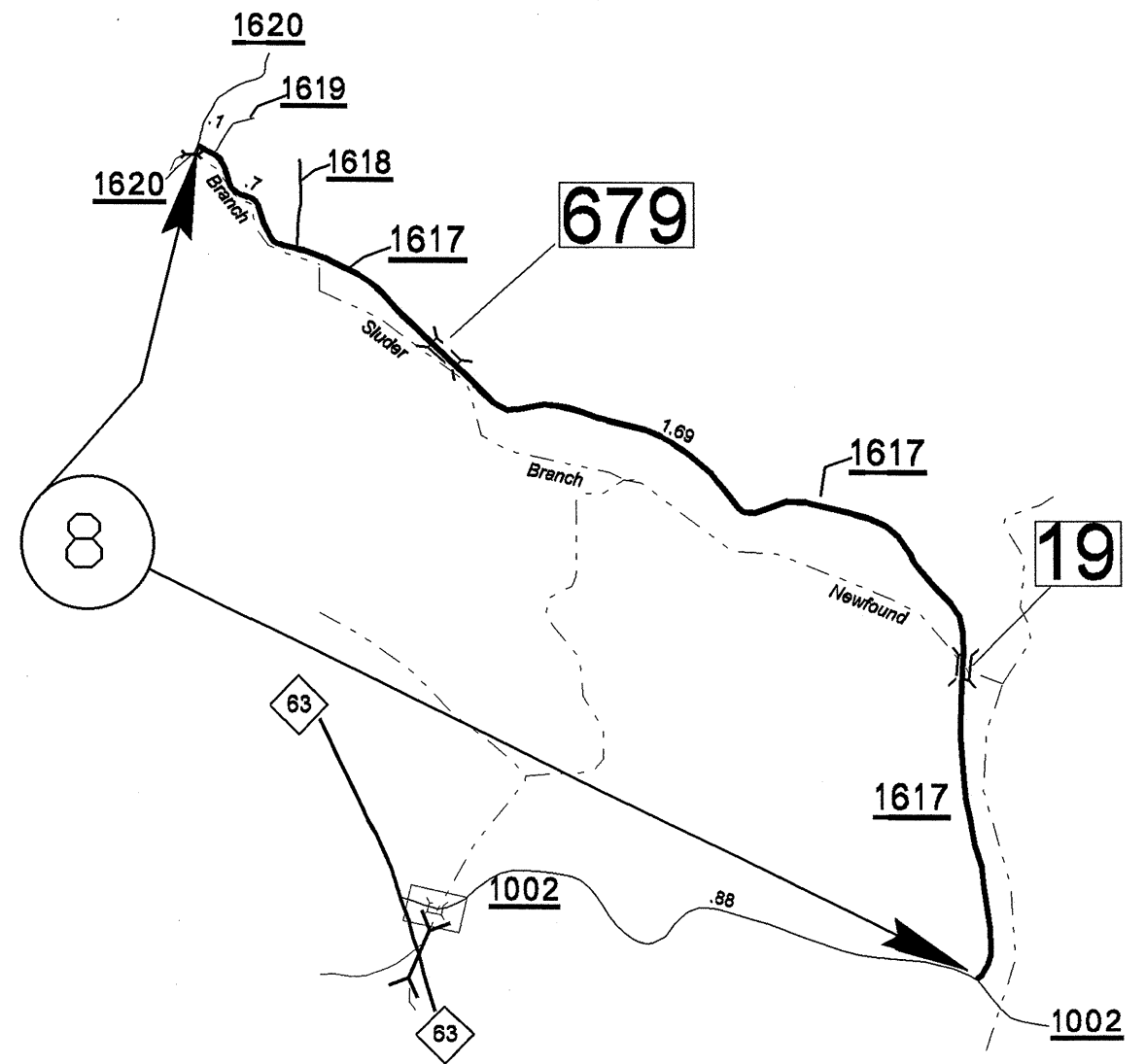
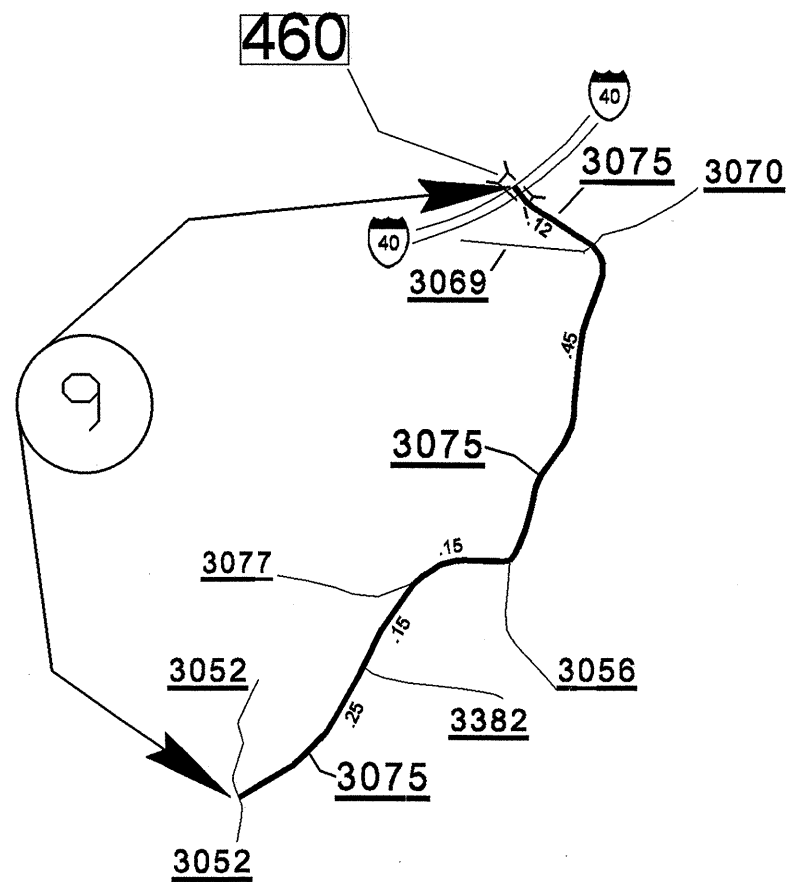
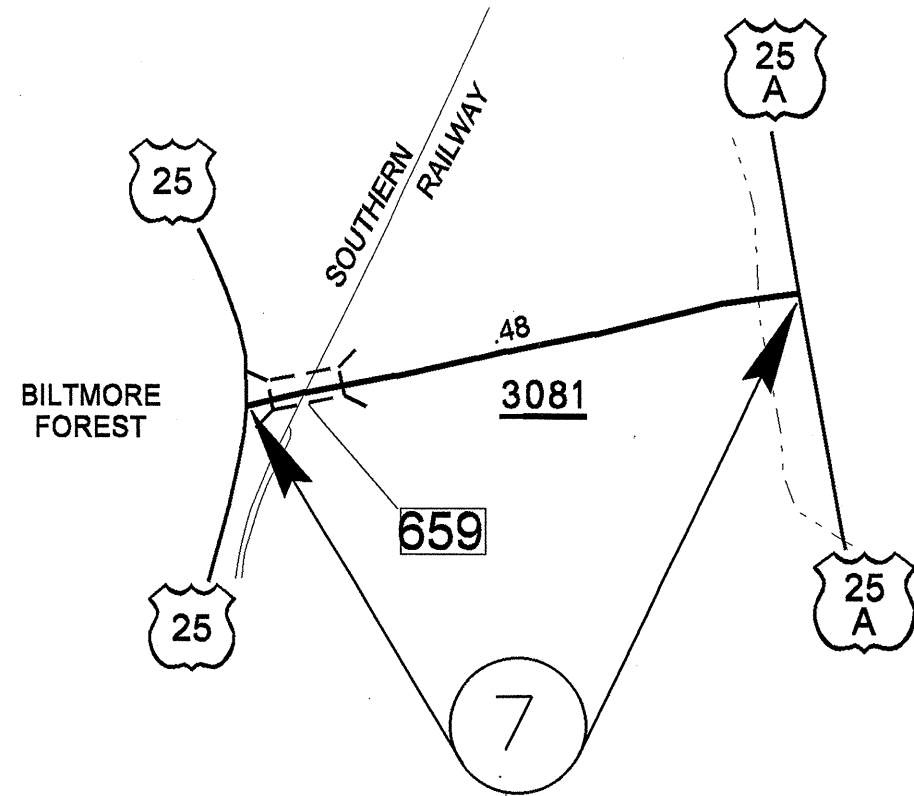
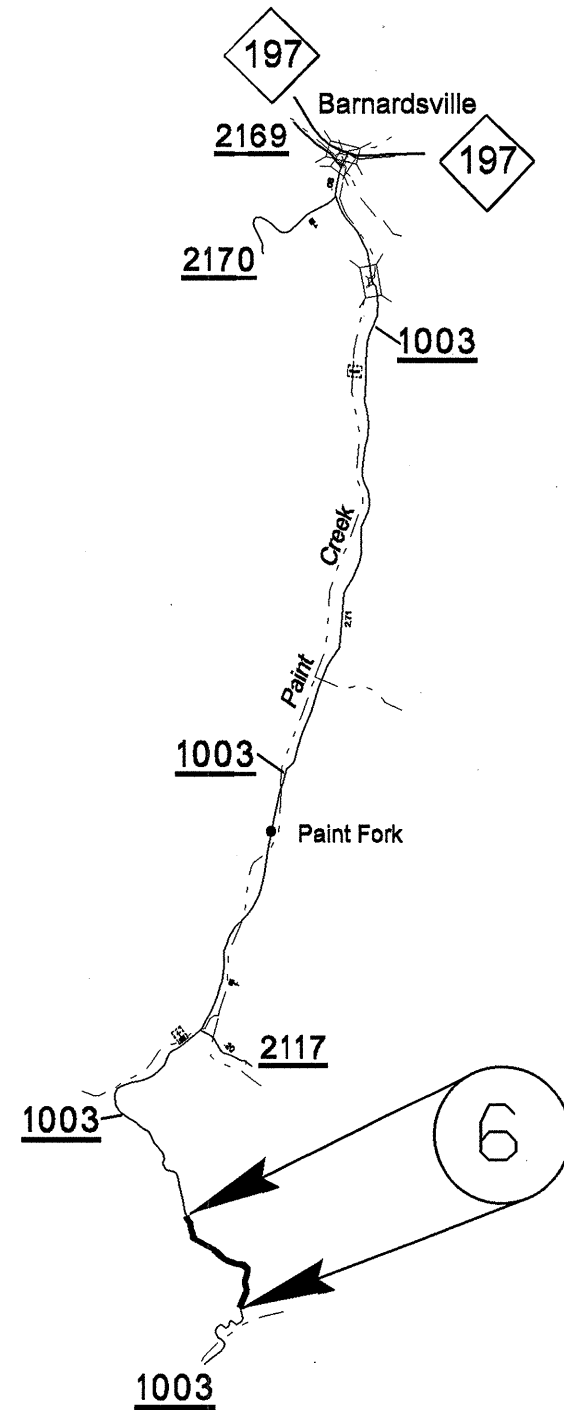


PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10 13CR.20111.8	1	18



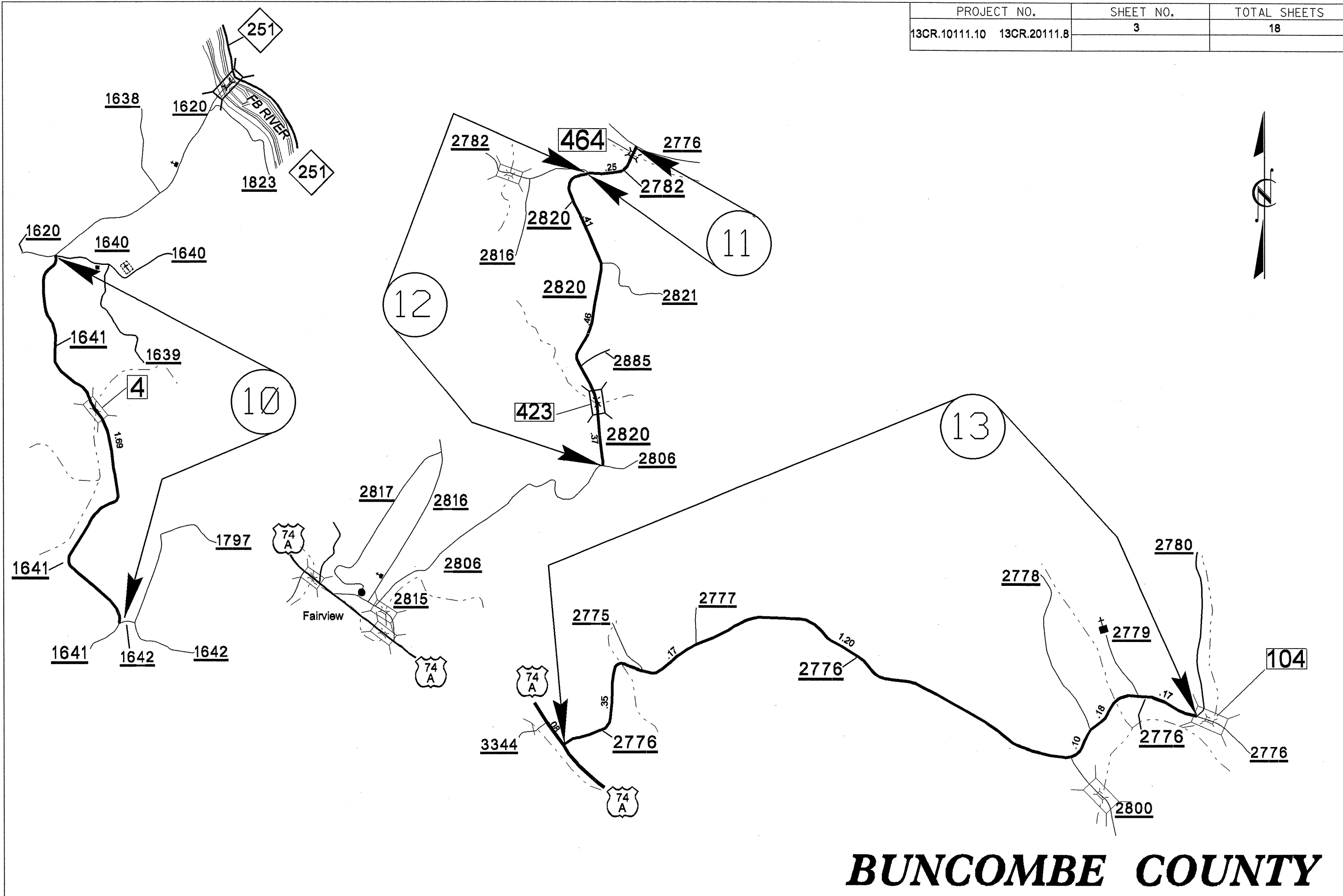
BUNCOMBE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10 13CR.20111.8	2	18



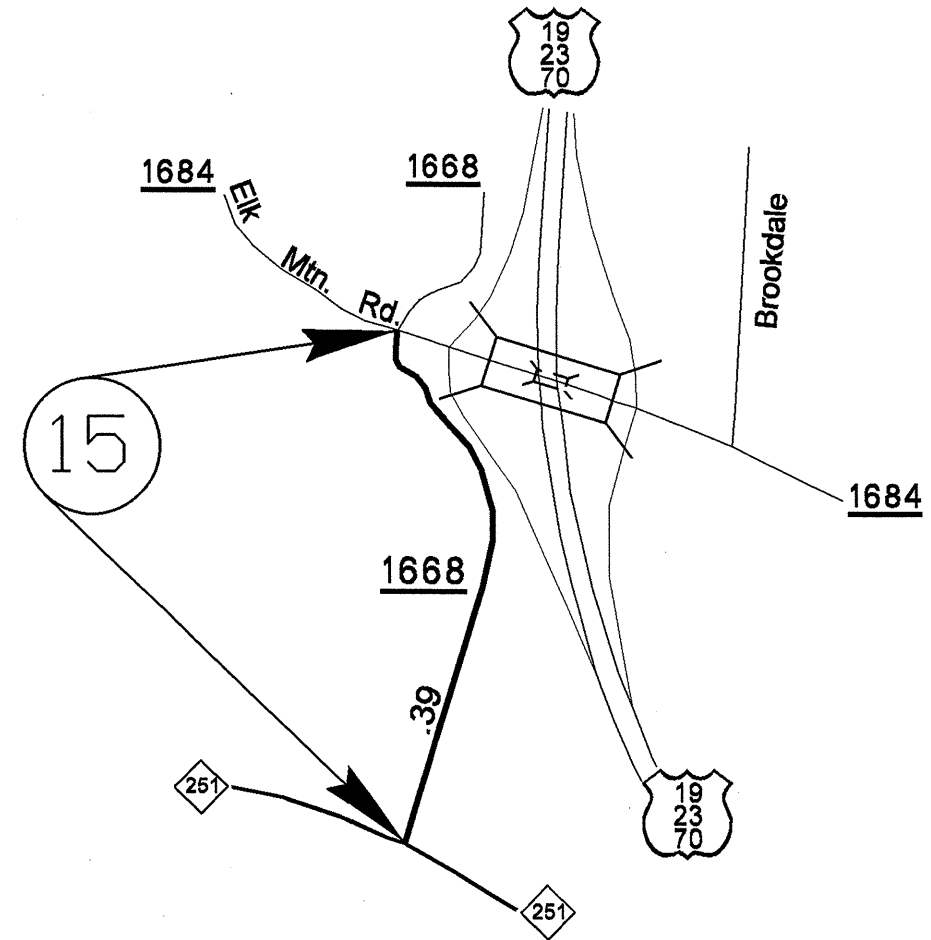
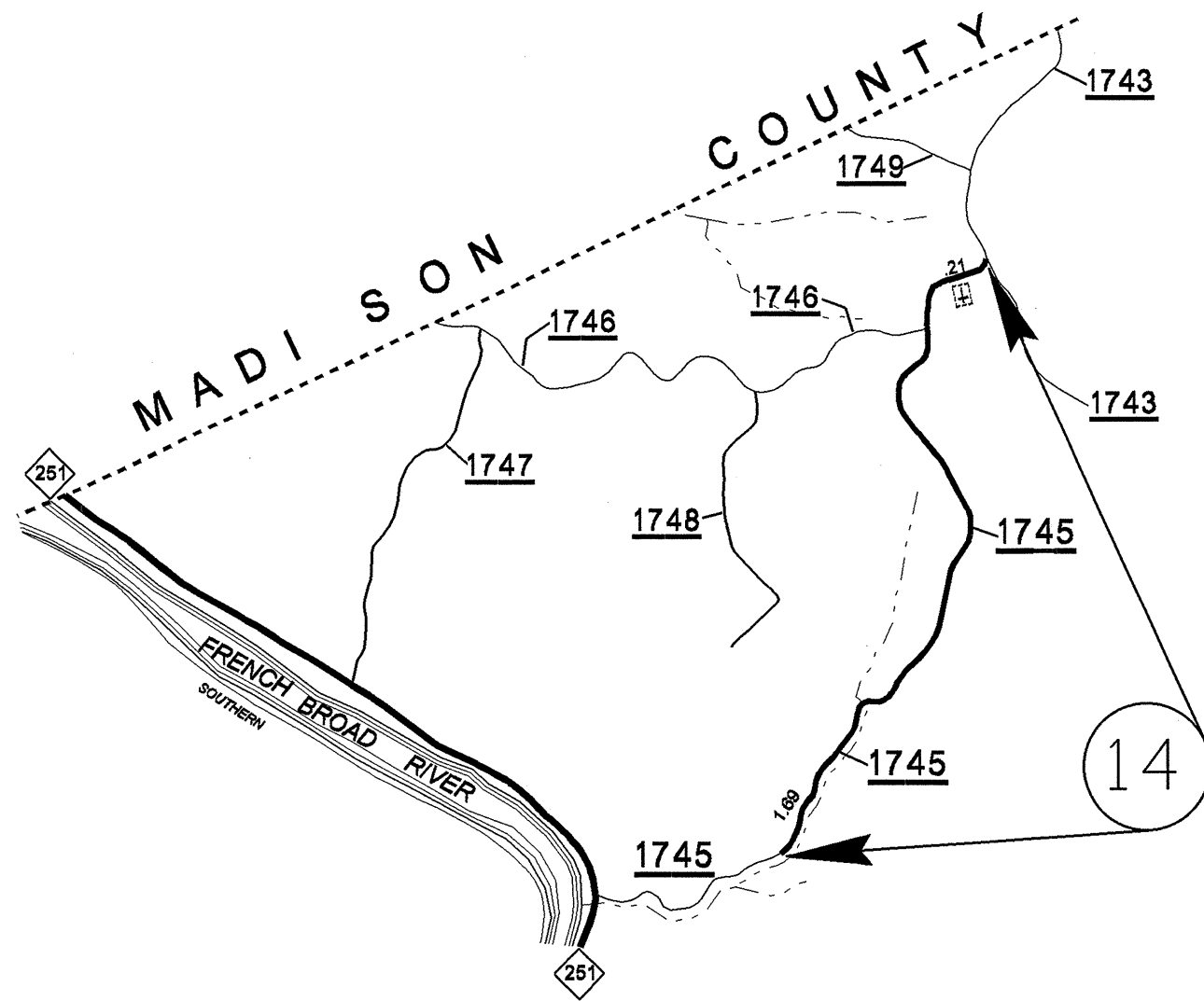
BUNCOMBE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10 13CR.20111.8	3	18



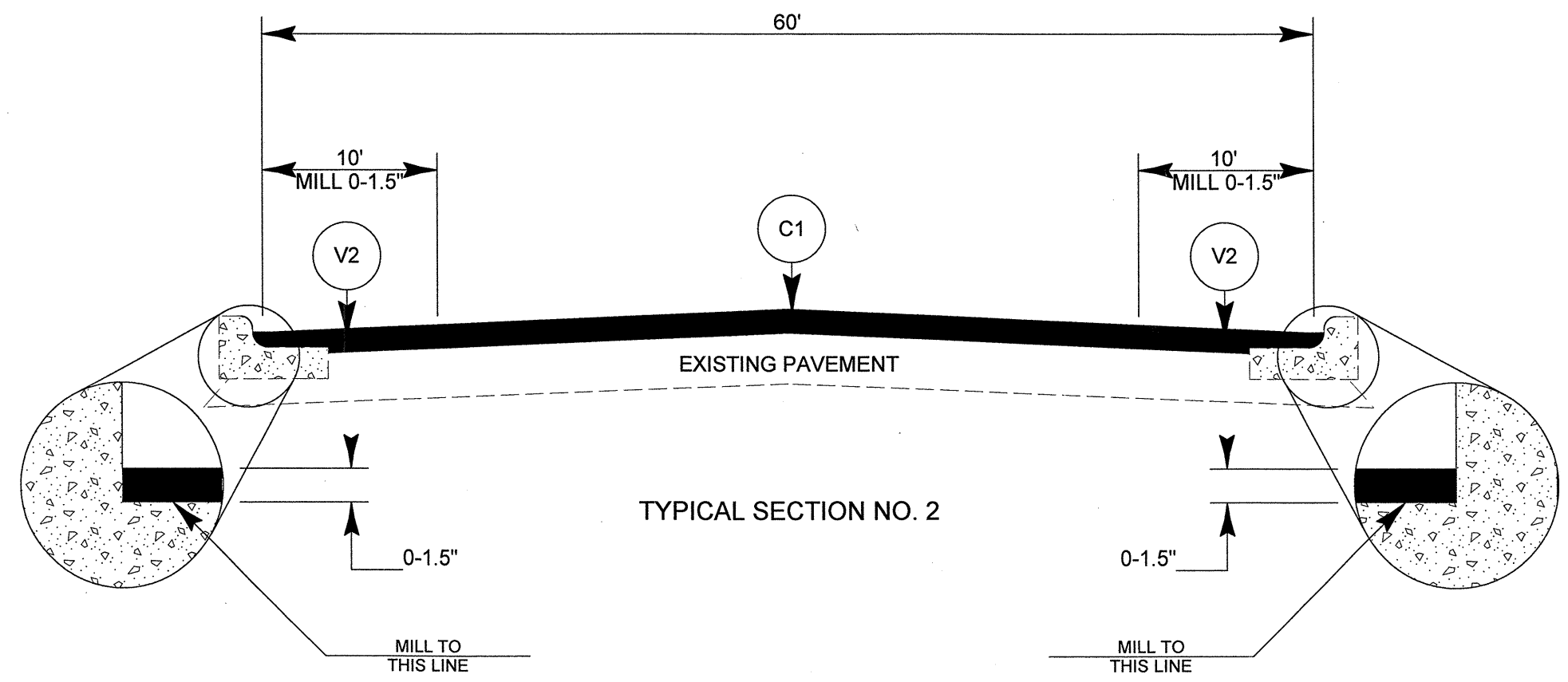
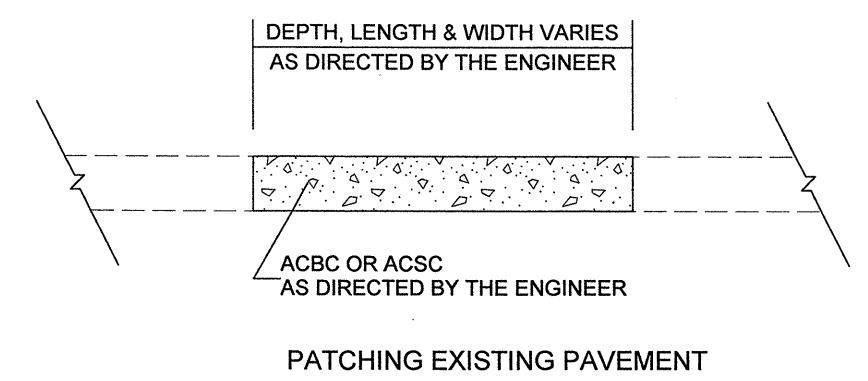
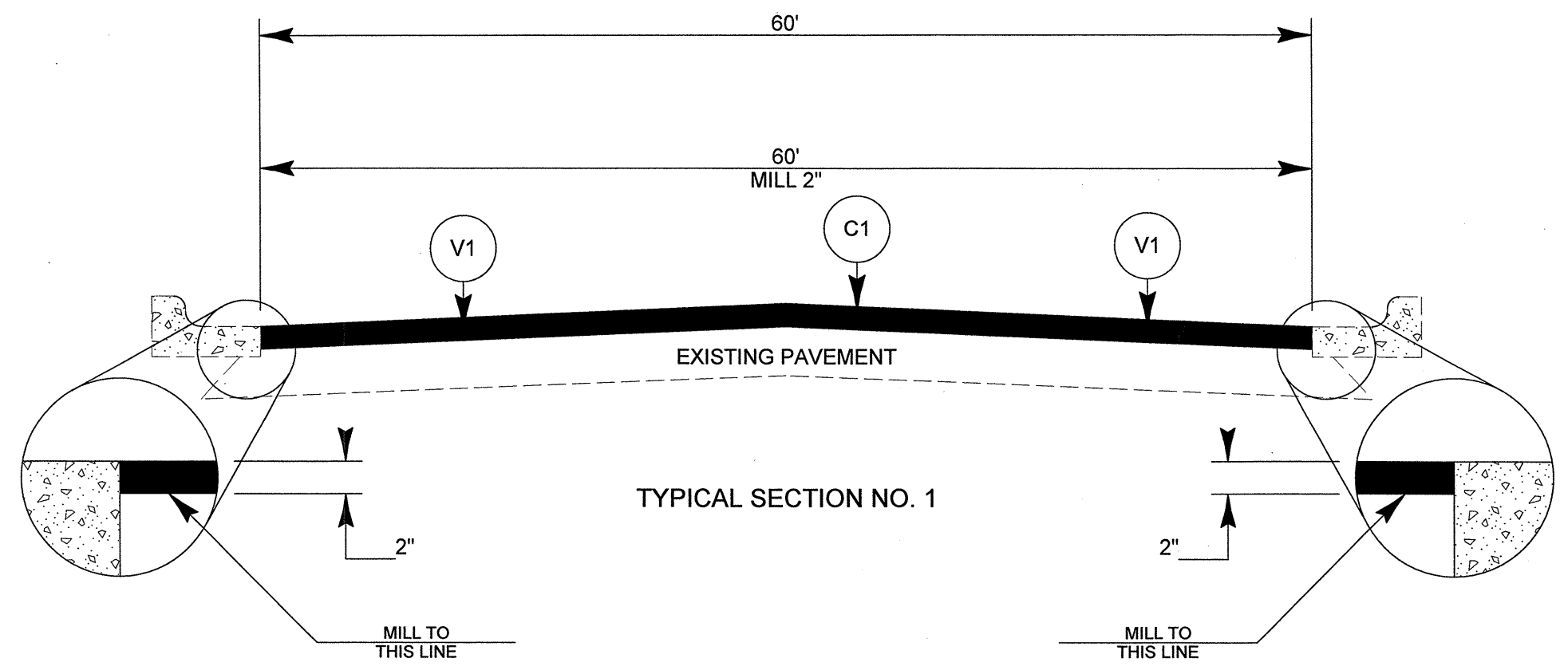
BUNCOMBE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10 13CR.20111.8	4	18



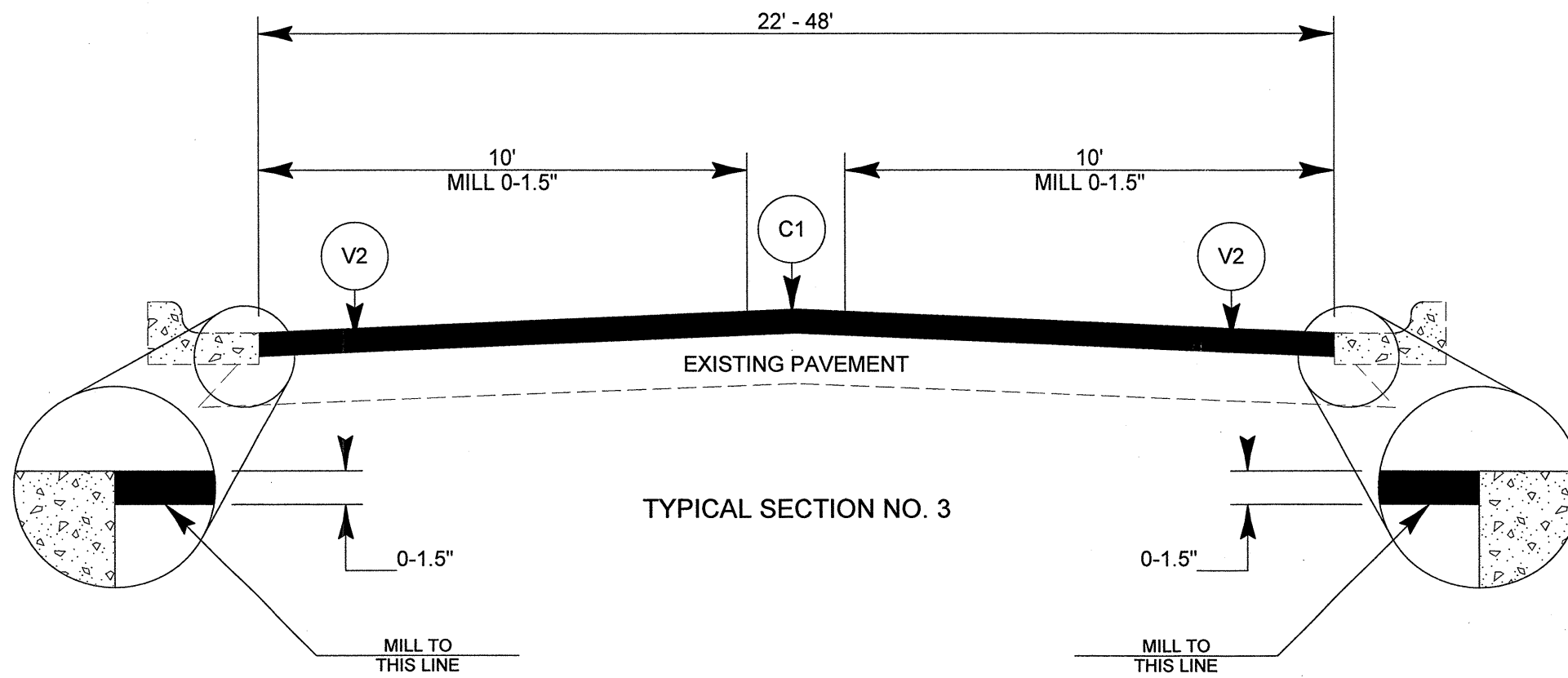
BUNCOMBE COUNTY

PROJECT NO. 13CR.10111.10, 13CR.20111.8	SHEET NO. 5	TOTAL SHEETS 18
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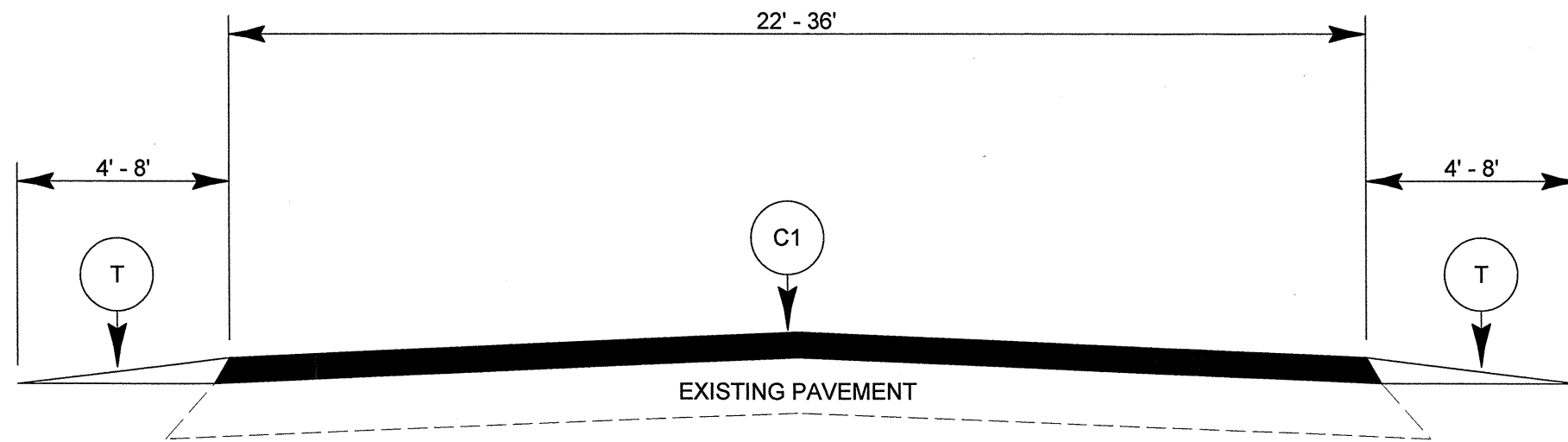


PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ACSC, TYPE S9.5B AT AN AVERAGE RATE OF 168 LBS PER SY
T	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT, 2" DEPTH
V2	MILLING ASPHALT PAVEMENT, 0 TO 1 1/2" DEPTH

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10, 13CR.20111.8	6	18

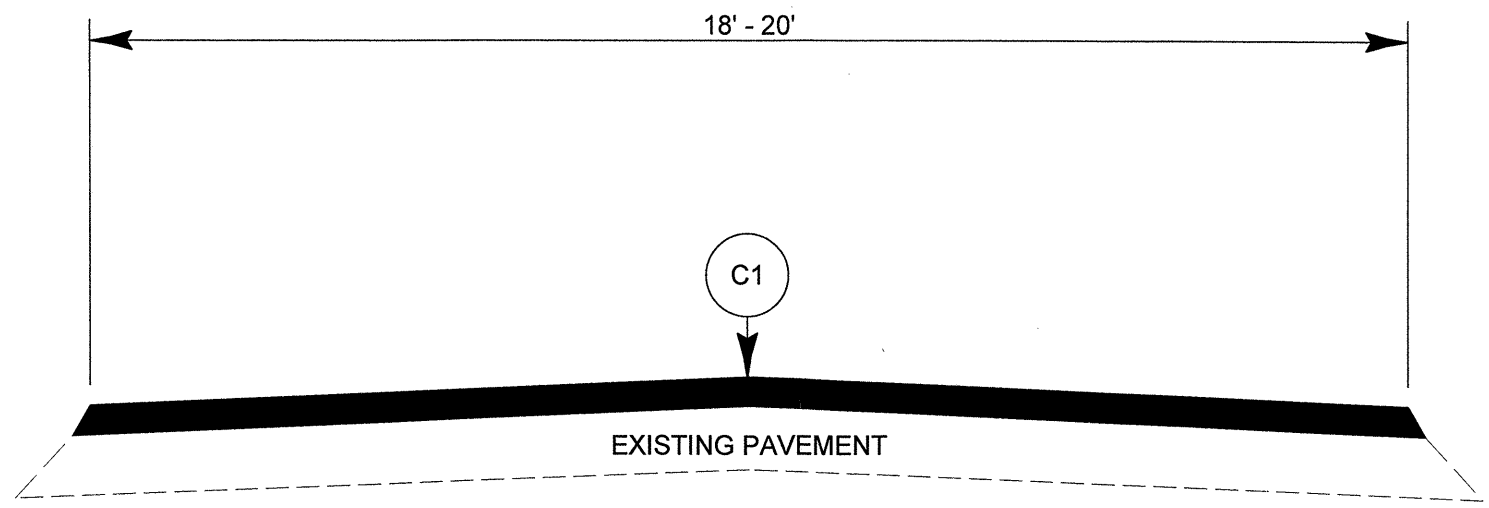


TYPICAL SECTION NO. 3



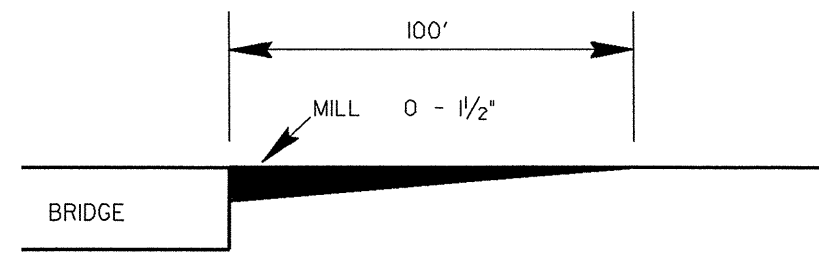
TYPICAL SECTION NO. 4

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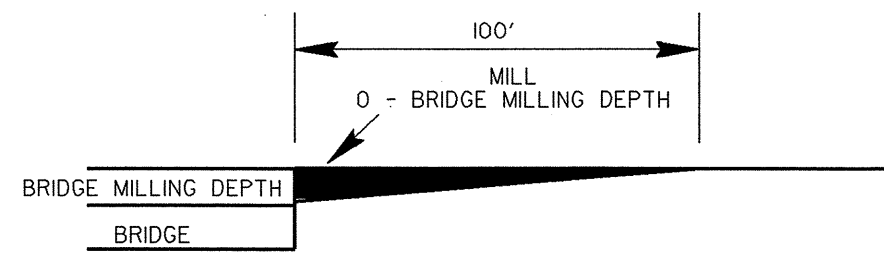


TYPICAL SECTION NO. 5

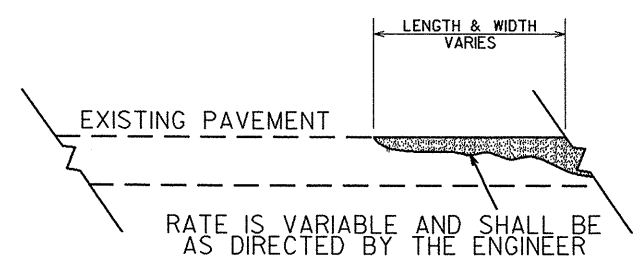
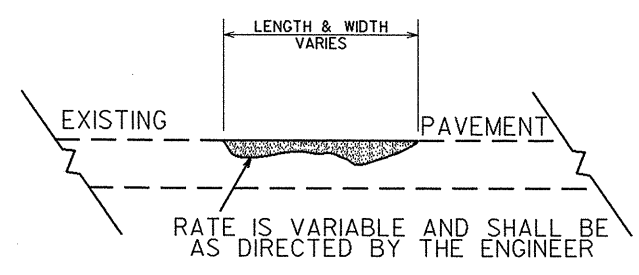
PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10III.10, 13CR.20III.8	8	18



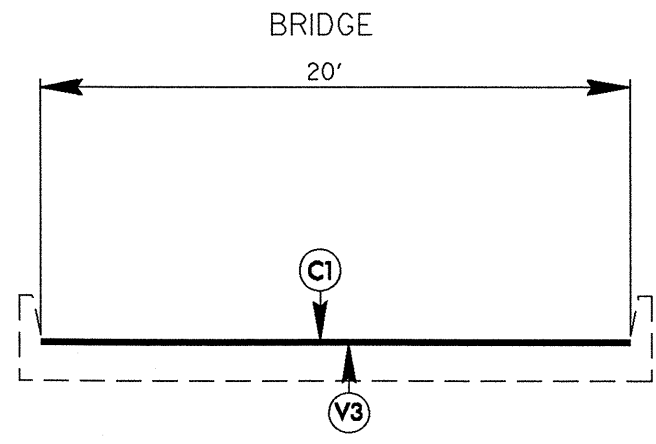
MILLING DETAIL AT BRIDGE APPROACHES
WHERE BRIDGE WILL NOT BE RESURFACED
COST OF MILLING IS INCIDENTAL TO OTHER ITEMS



MILLING DETAIL AT BRIDGE APPROACHES
WHERE BRIDGES WILL BE MILLED THEN RESURFACED
COST OF MILLING BRIDGE APPROACHES IS INCIDENTAL
TO OTHER ITEMS



DETAIL SHOWING METHOD OF WEDGING



BRIDGE DETAIL

BRIDGE NUMBER 27 MAP 2
MILL 1 1/2" OFF EXISTING PAVEMENT ON BRIDGE
THEN RESURFACE BRIDGE DECK WITH 1 1/2" OF S9.5B
SEE MAPS FOR BRIDGE LOCATION

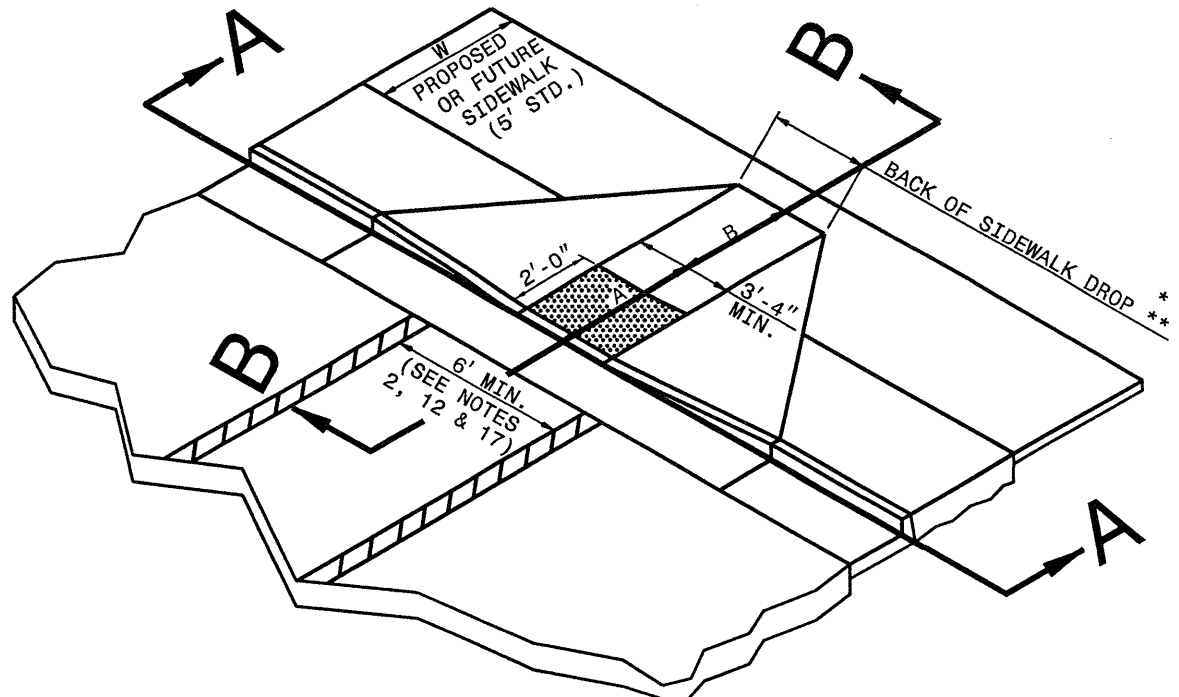
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
V3	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH

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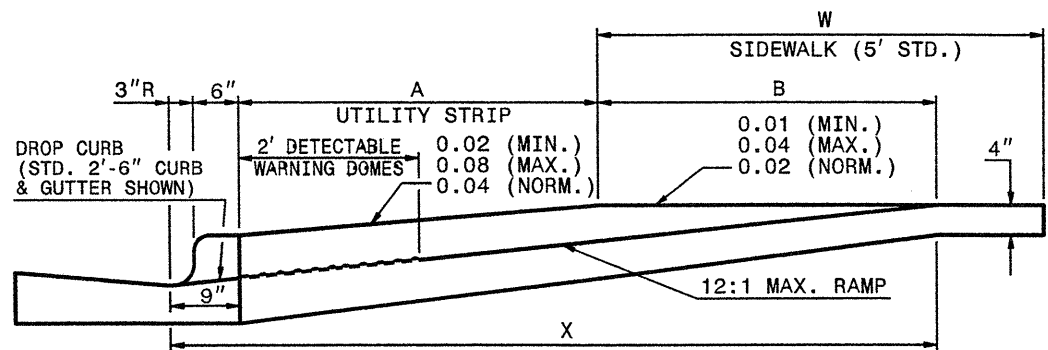
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
PROPOSED CURB AND GUTTER

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

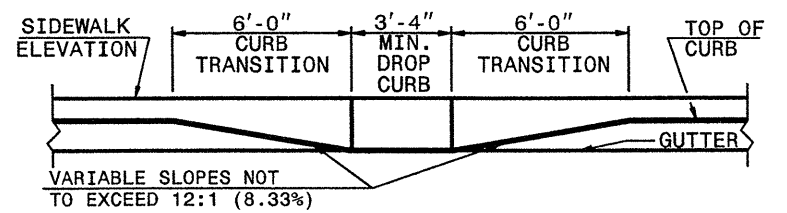
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
PROPOSED CURB AND GUTTER



ISOMETRIC VIEW

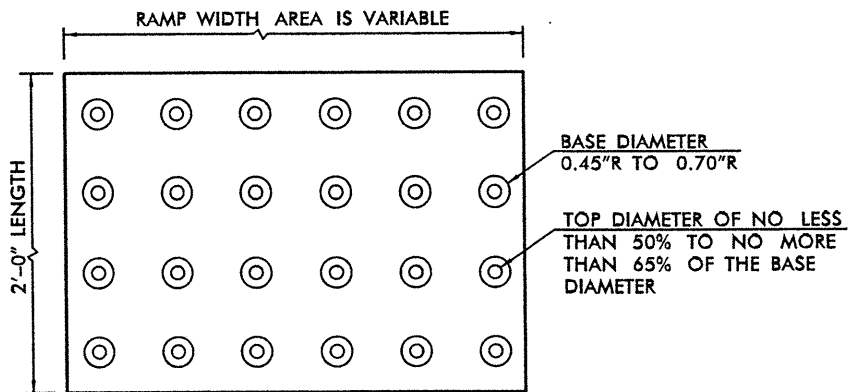


SECTION B-B



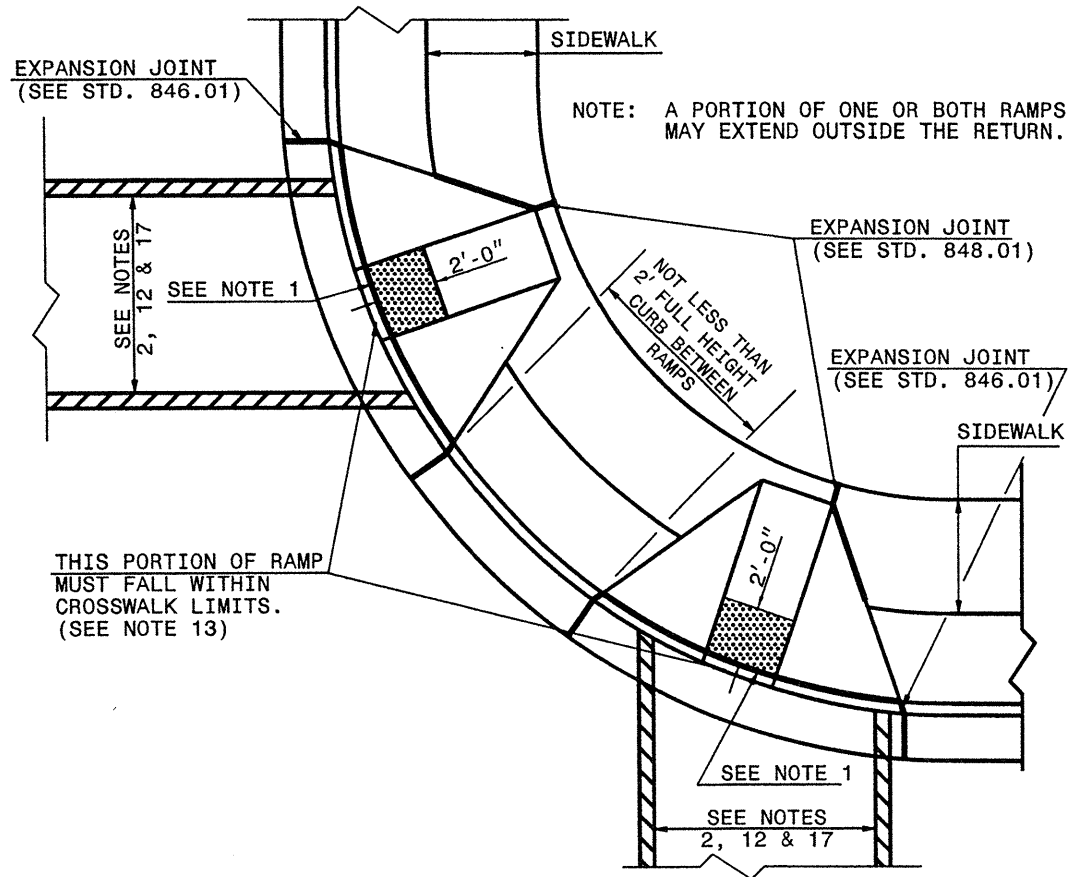
SECTION A-A

- NOTES:
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
 2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



W	A	W+A+9"	X	B
5'	0.0'	5.8'	5.8'	5.0'*
6'	0.0'	6.8'	6.8'	6.0'**
7'	0.0'	7.8'	7.3'	6.5'**
8'	0.0'	8.8'	7.3'	6.5'**
5'	2.0'	7.8'	7.8'	5.0'
5'	2.5'	8.3'	8.1'	4.8'
5'	3.0'	8.8'	8.3'	4.4'
5'	3.5'	9.3'	8.4'	4.1'
5'	4.0'	9.8'	8.6'	3.8'
5'	4.5'	10.3'	8.7'	3.4'
5'	5.0'	10.8'	8.9'	3.1'

$B = X - (A + 9")$
 B = DISTANCE FROM FRONT EDGE OF SIDEWALK TO BACK POINT OF 12:1 (8.33%) SLOPE.
 * BACK OF SIDEWALK DROP REQUIRED FOR ALL SIDEWALK SLOPES.
 ** BACK OF SIDEWALK DROP REQUIRED FOR SIDEWALK SLOPES 0.04.



PLAN VIEW

DUAL RAMPS
ANY RADII
(40" MIN. FLOOR WIDTH)

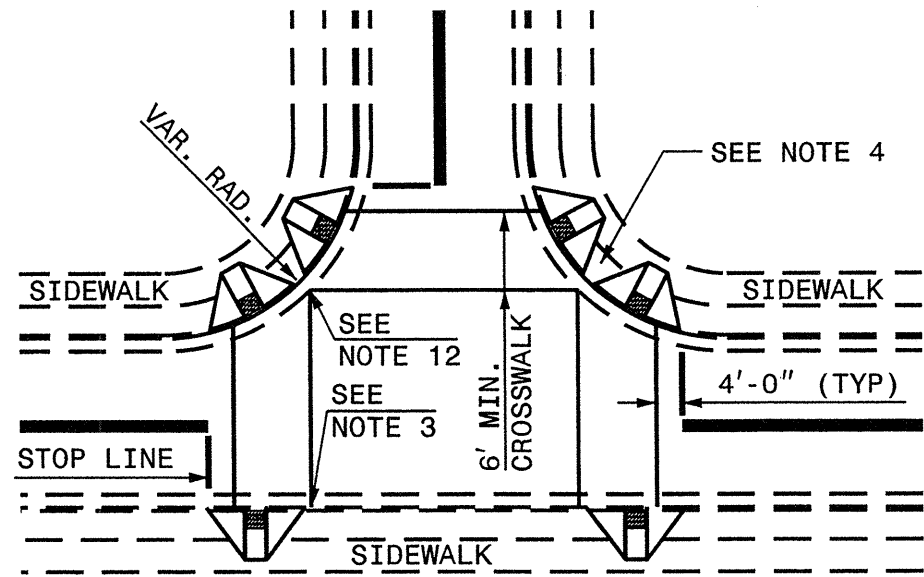
DETECTABLE WARNING DOMES

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

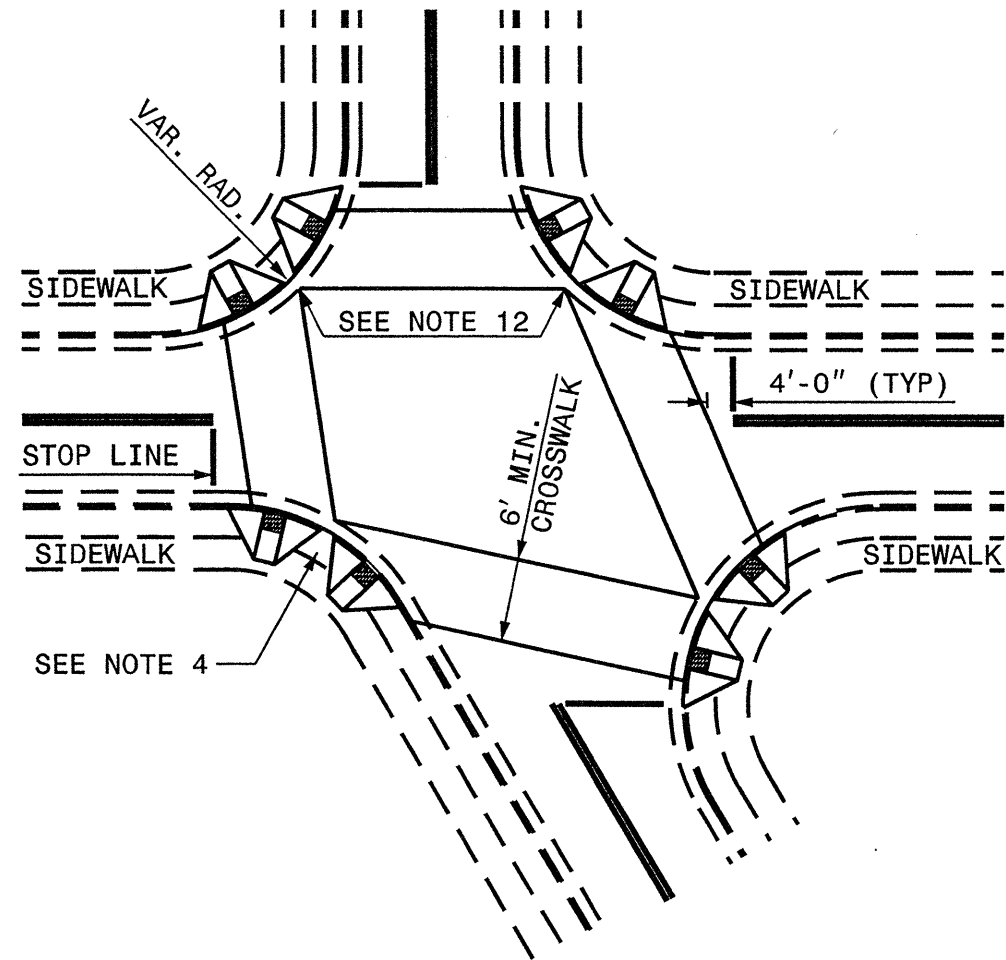
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
PROPOSED CURB AND GUTTER

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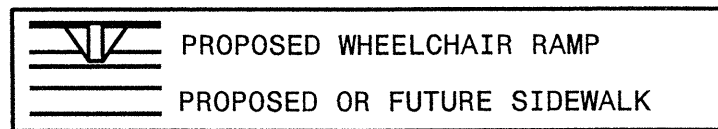
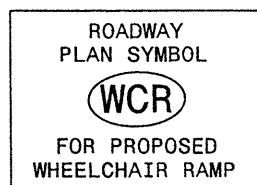
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
PROPOSED CURB AND GUTTER



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES



ALLOWABLE LOCATIONS

DUAL RAMP RADII.....ANY

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STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
PROPOSED CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
PROPOSED CURB AND GUTTER

NOTES:

1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
2. CROSSWALK WIDTHS AND CONFIGURATION VARY BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
3. NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.

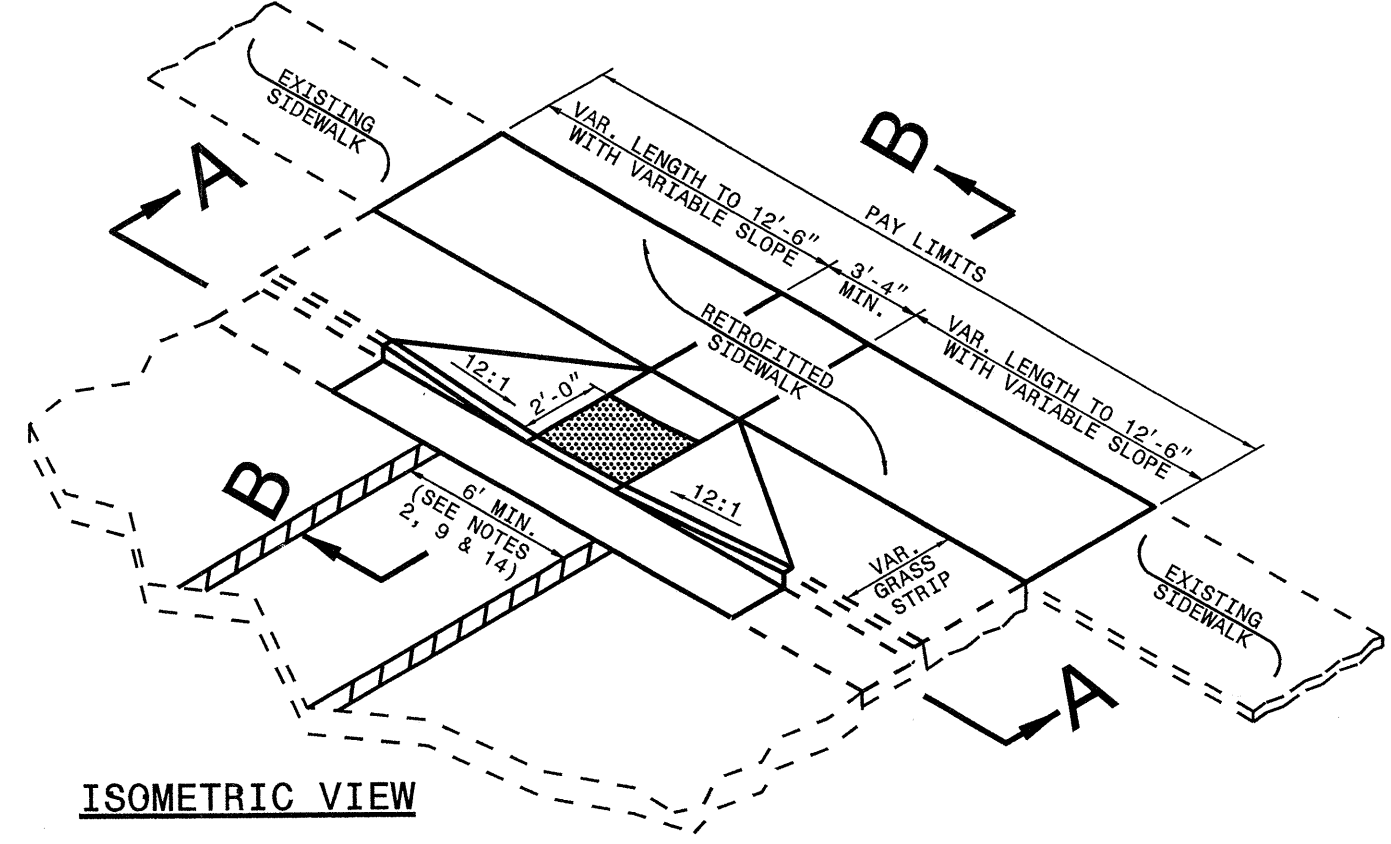
IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.

THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES. COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILIAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.
4. PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
5. DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
6. CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS.
7. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADII, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 17)
10. COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
11. CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
14. PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

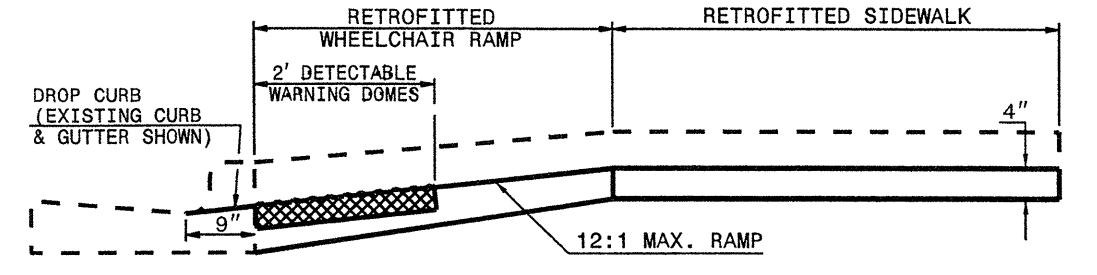
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DIVISION OF HIGHWAYS
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DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

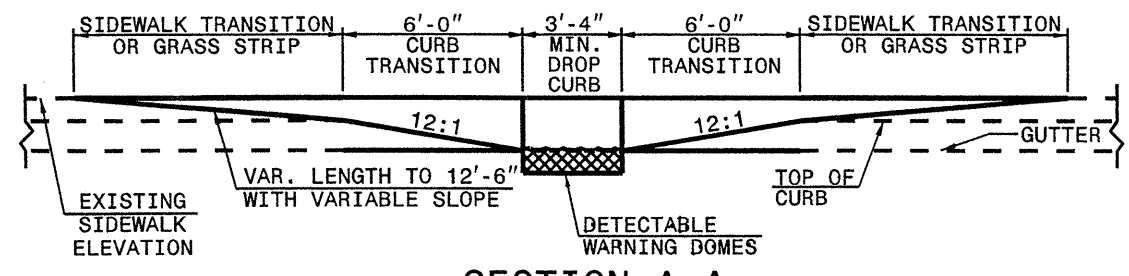
WHEELCHAIR RAMP AND EXISTING SIDEWALK WITH GRASS STRIP



ISOMETRIC VIEW

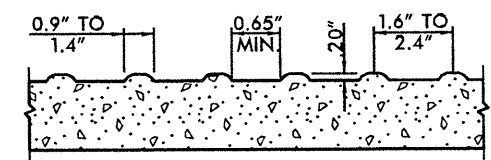
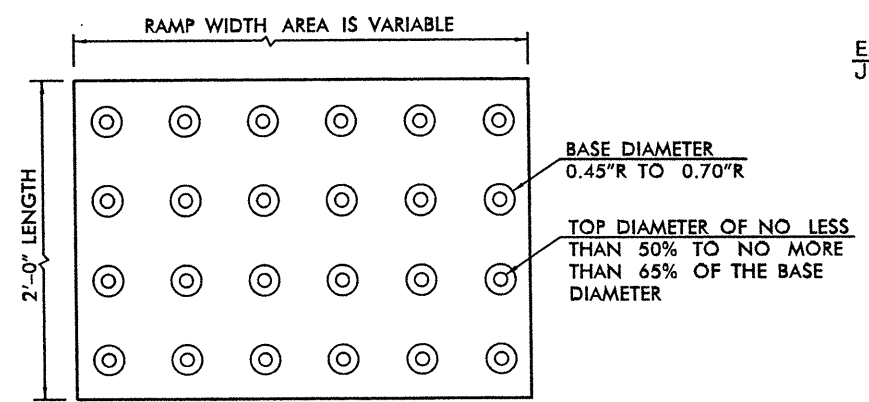


SECTION B-B

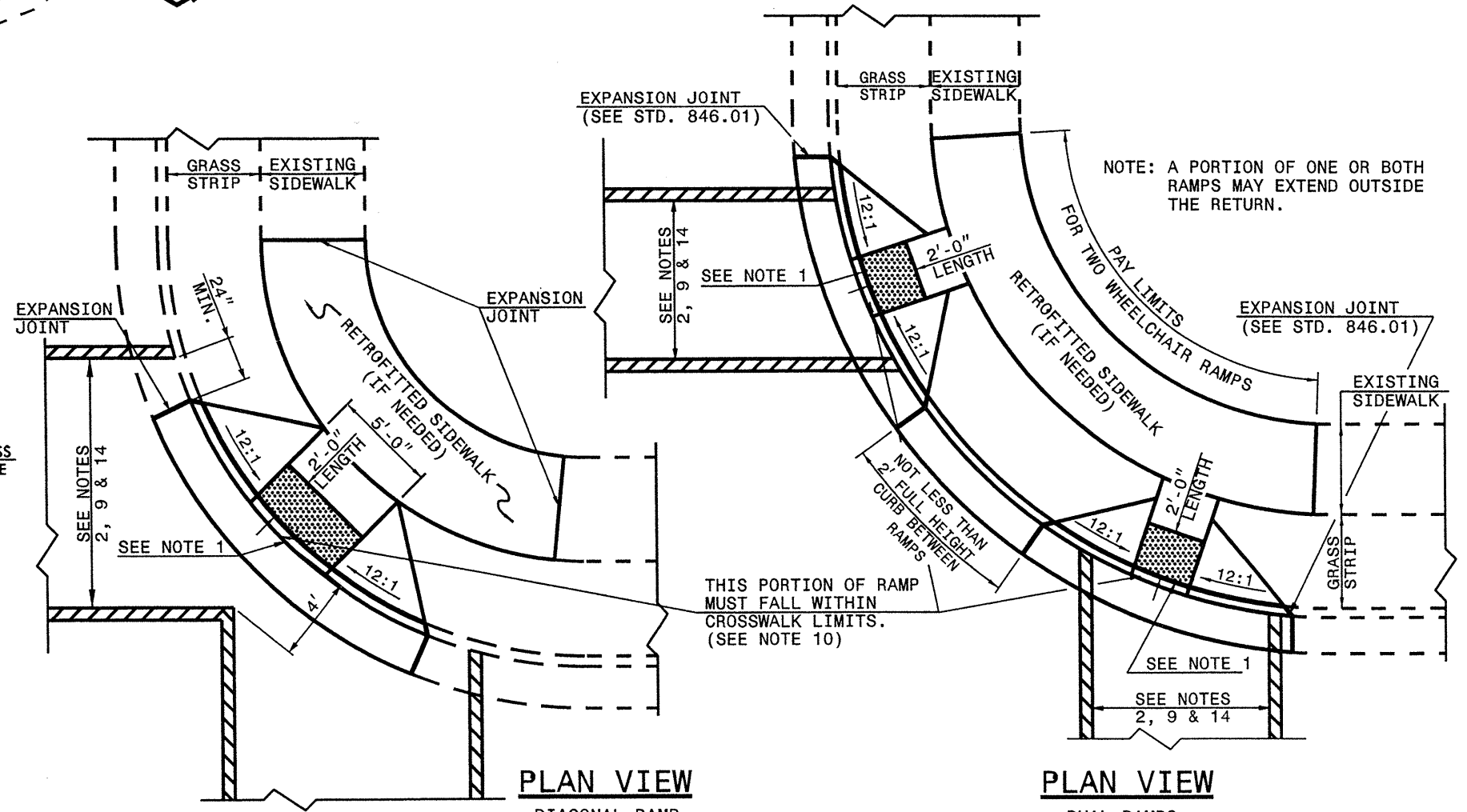


SECTION A-A

- NOTES:
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
 2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



DETECTABLE WARNING DOMES



PLAN VIEW

DIAGONAL RAMP
MAX. 25' RADII
(60" MIN. FLOOR WIDTH)

PLAN VIEW

DUAL RAMPS
ANY RADII
(40" MIN. FLOOR WIDTH)

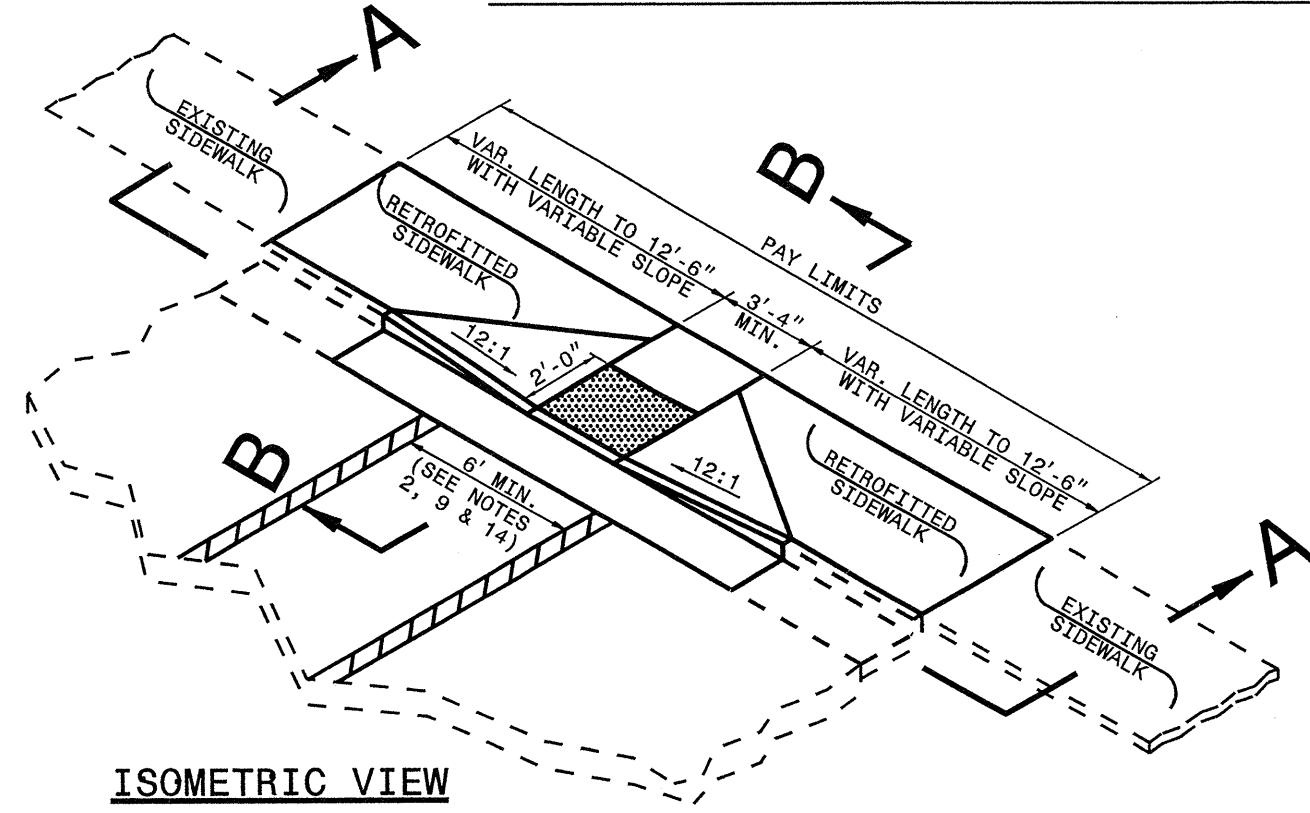
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

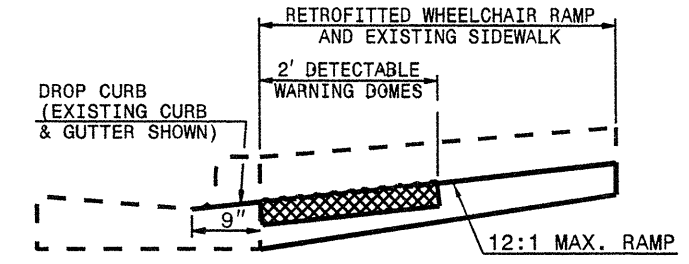
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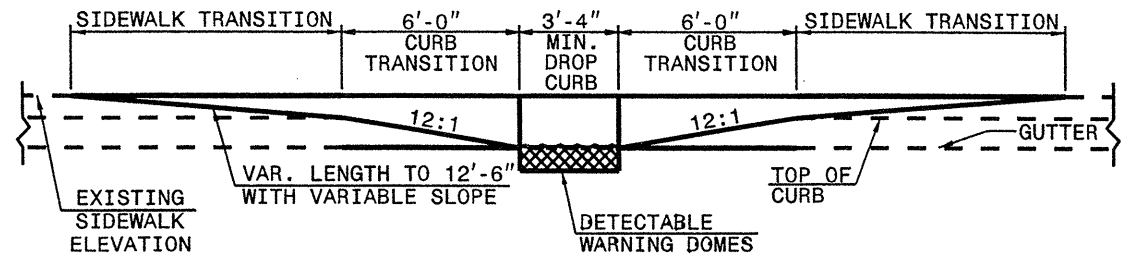
WHEELCHAIR RAMP AND EXISTING SIDEWALK ADJACENT TO CURB



ISOMETRIC VIEW

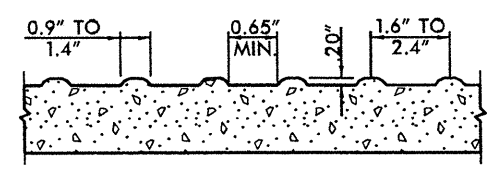
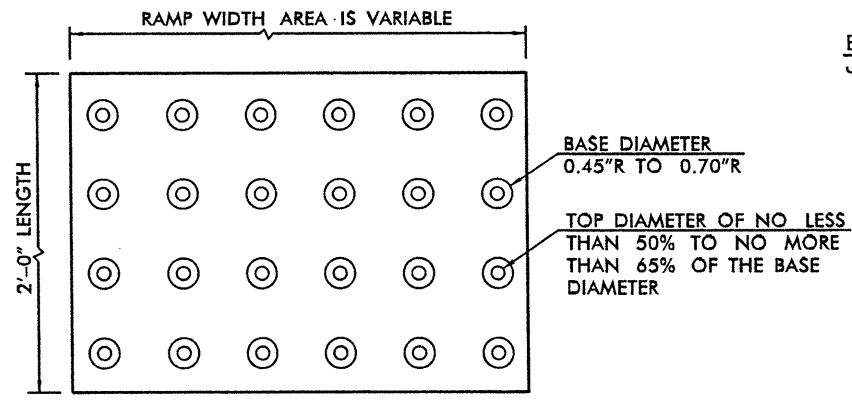


SECTION B-B

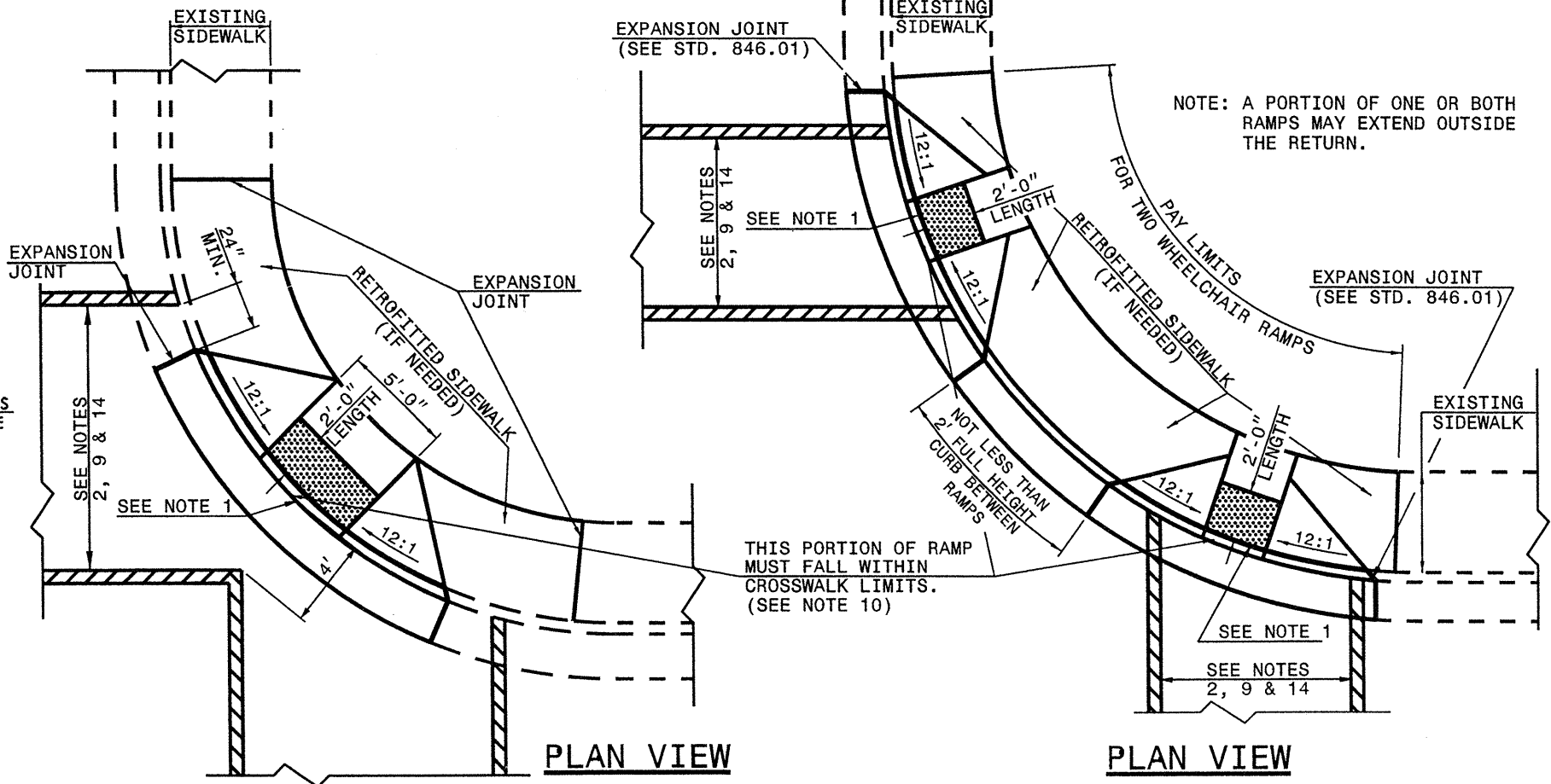


SECTION A-A

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DETECTABLE WARNING DOMES



PLAN VIEW

DIAGONAL RAMP
MAX. 25' RADII
(60" MIN. FLOOR WIDTH)

PLAN VIEW

DUAL RAMPS
ANY RADII
(40" MIN. FLOOR WIDTH)

NOTE: A PORTION OF ONE OR BOTH RAMPS MAY EXTEND OUTSIDE THE RETURN.

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

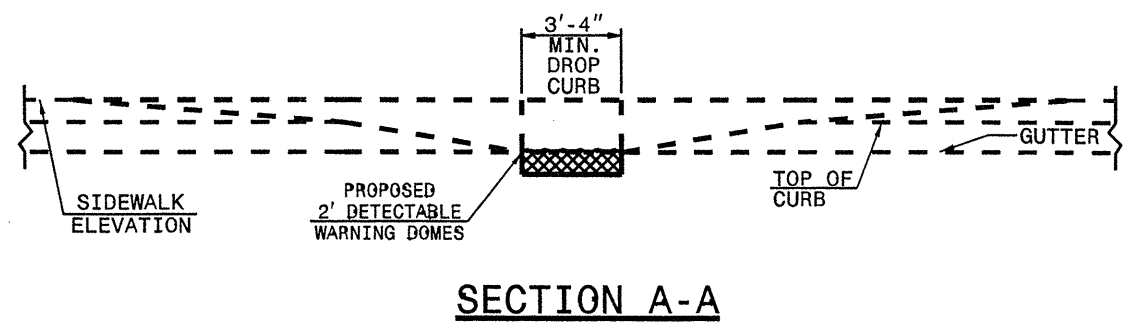
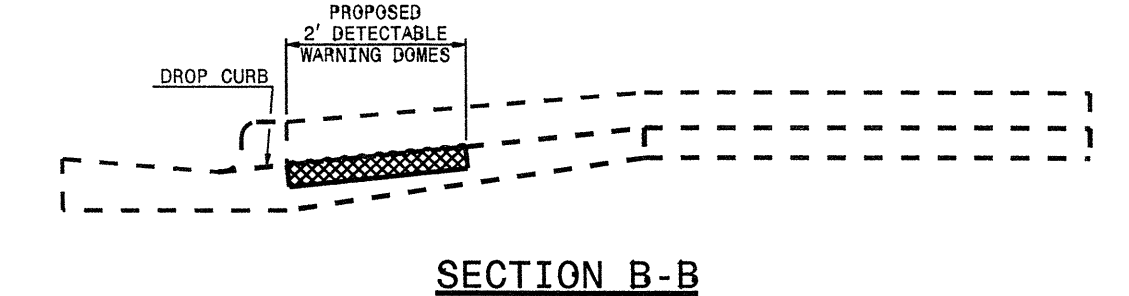
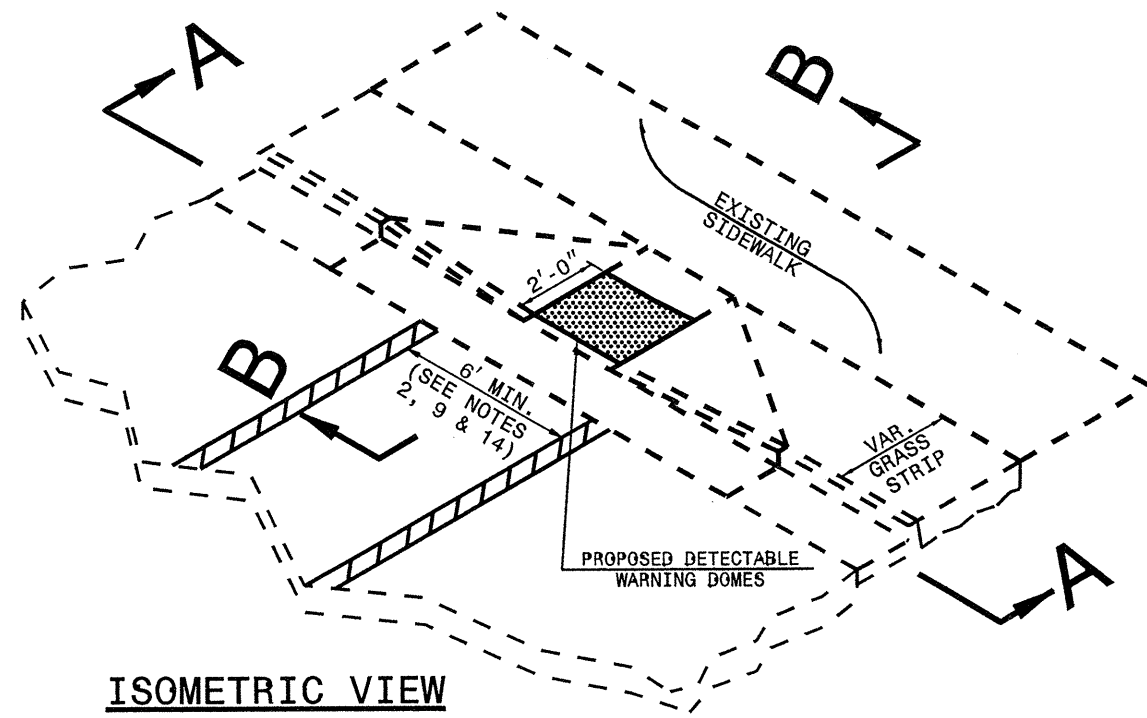
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RALEIGH, N.C.

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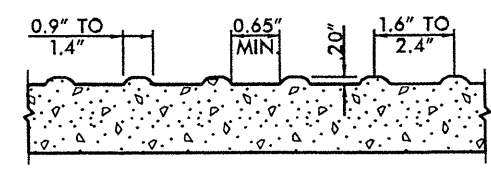
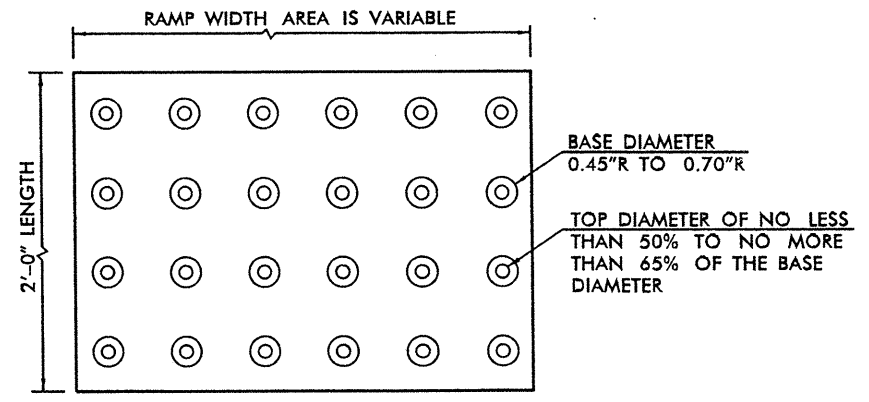
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

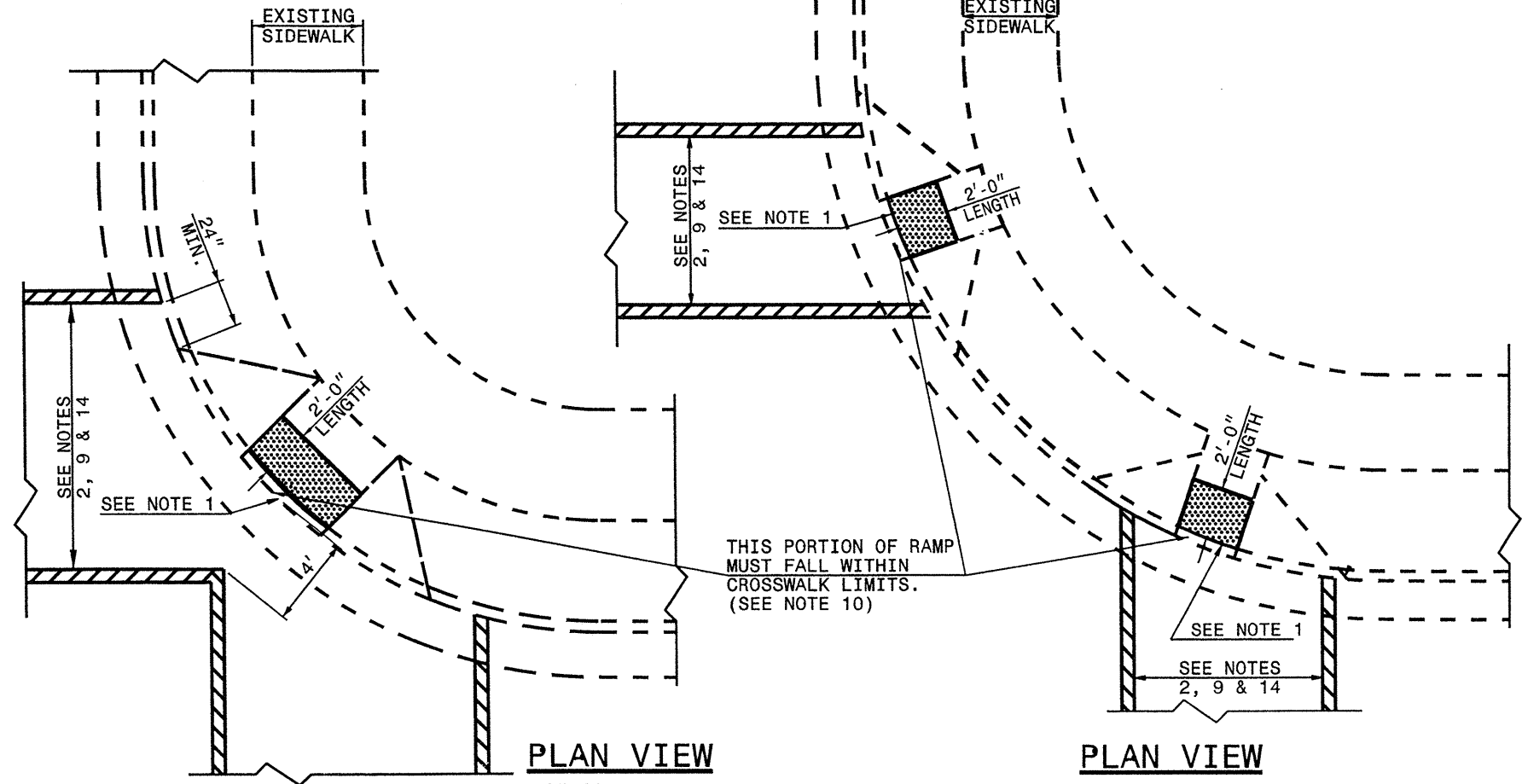
RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING WHEELCHAIR RAMP



- NOTES:
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DETECTABLE WARNING DOMES



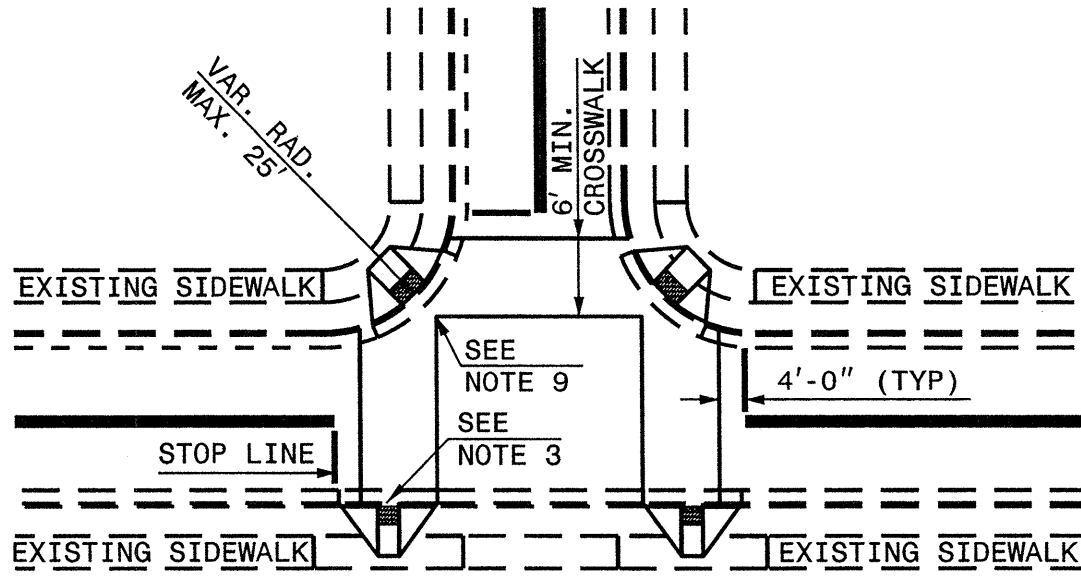
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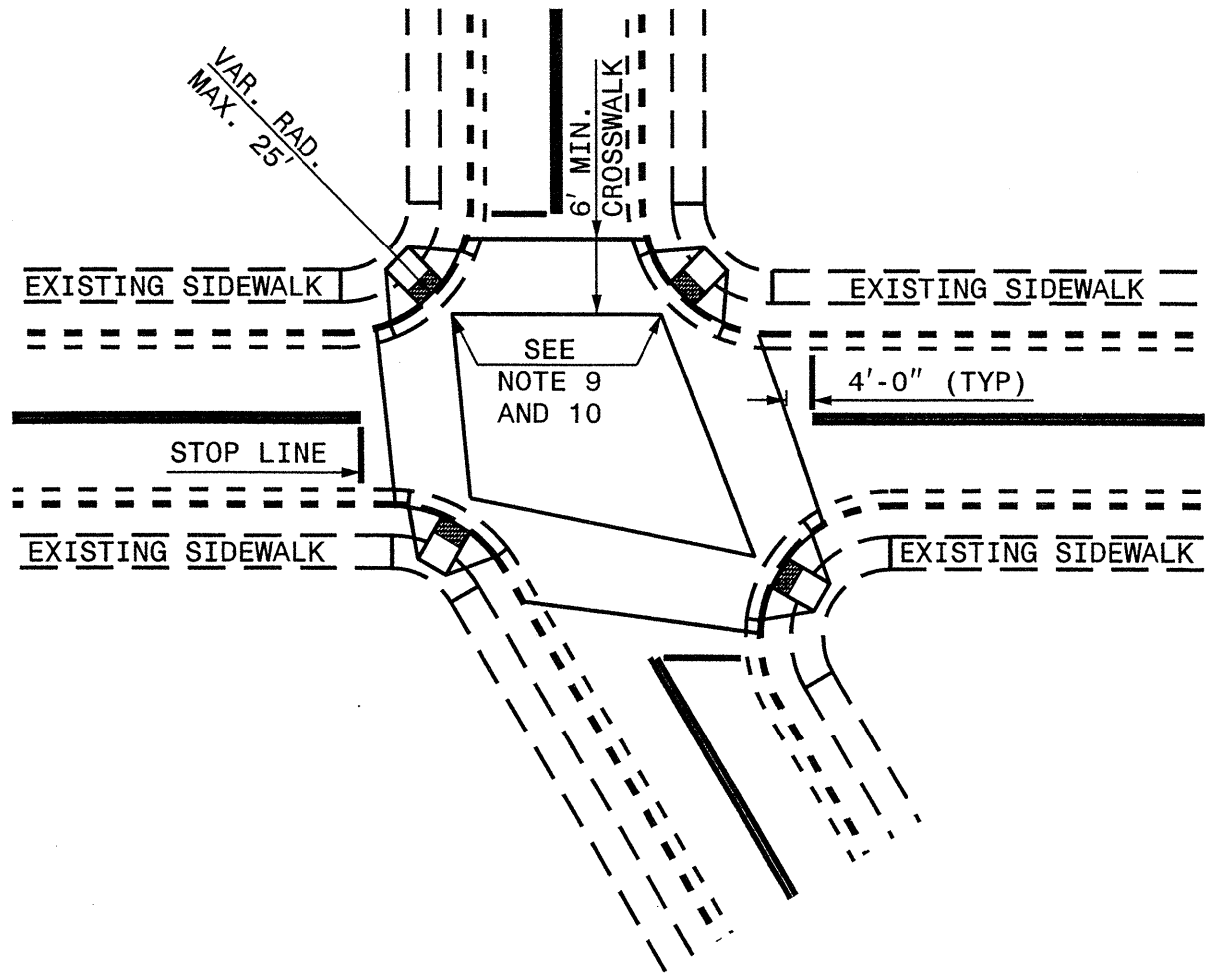
ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
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WHEELCHAIR RAMP AND EXISTING SIDEWALK

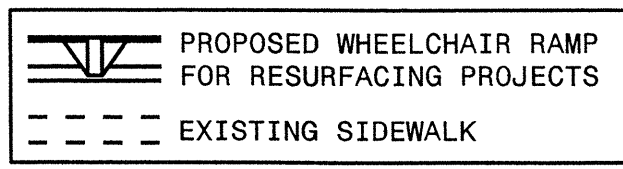


DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

RESURFACING PROJECTS



ALLOWABLE LOCATIONS
DIAGONAL RAMP RADII...MAX. 25'

WHEELCHAIR RAMP AND EXISTING SIDEWALK

NOTES:

- 1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
- 2. CROSSWALK WIDTHS AND CONFIGURATION VARY, BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
- 3. NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.

IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.

THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES, COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILIAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.

- 4. PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
- 5. DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
- 6. CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS AND 60" (5'-0") OR GREATER FOR DIAGONAL RAMPS.
- 7. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
- 8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
- 9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADII, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 14)
- 10. COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
- 11. CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
- 12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
- 13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
- 14. PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

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ENGLISH DETAIL DRAWING FOR
WHEELCHAIR RAMP
EXISTING CURB AND GUTTER

PROJECT NO.	SHEET NO.	TOTAL NO.
13CR.10111.10, 13CR.20111.8	17	18

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	MILLING ASPHALT PAVEMENT, 2" DEPTH SY	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH SY	MILLING ASPHALT PAVEMENT, 0" TO 1 1/2" DEPTH SY	SURFACE COURSE, TYPE S9.5B TONS	ASPHALT BINDER, GRADE PG 64-22 TONS	PATCHING EXISTING PAVEMENT TONS	RETROFIT EXIST. WHEEL CHAIR RAMPS EA	CONCRETE WHEELCHAIR RAMPS EA	ADJUSTMENT OF MANHOLES EA	ADJUSTMENT OF METER BOXES OR VALVE BOXES EA		
13CR.10111.10	Buncombe	1	US 74A	FROM 50' WEST OF SR 2771 (PAVEMENT CHANGE) TO SR 2773 (PAVEMENT CHANGE)	1	NO	1.57	60			55,365.00			5,117	307	100	6	9				
		2	US 19/23	FROM SR 1220 TO SR 3613	2	NO	0.78	60				300.00	9,260.00	2,542	153	50		8				
		3	NC 9	FROM LACKY GAP ROAD TO SUNSET DRIVE	3,4	NO	1.1	22	55.00	2.00			3,689.00	1,319	79	450		8	5	7		
		4	NC 9	FROM SUNSET DRIVE TO I-40 EAST OFF RAMP	3,4	NO	0.2	36	10.00	0.40			333.00	392	23	300			1	1		
		5	NC 9	FROM I-40 EAST OFF RAMP TO R/R TRACKS	3	NO	0.39	48					4,576.00	1,017	61	300		6	6	1		
TOTAL FOR PROJ NO. 13CR.10111.10							4.04		65.00	2.40	55,365.00	300.00	17,858.00	10,387	623	1,200	6	31	12	9		
13CR.20111.8	Buncombe	6	SR 1003	PAINT FORK FROM I2 TO I2	5	NO	0.32	20	16.00					349	21	420						
		7	SR 3081	ROCK HILL FROM US 25 TO US 25A	5	NO	0.45	20	22.50					491	29	250			8			
		8	SR 1617	SLUDER BR. FROM SR 1002 TO SR 1620	5	NO	2.5	18	125.00					2,456	147	1,800						
		9	SR 3075	ONTEORA FROM I 40 TO SR 3052	5	NO	1.09	18	54.50					1,071	64	262			5	2		
		10	SR 1641	JENKINS VALLEY FROM SR 1642 TO SR 1620	5	NO	1.7	18	85.00					1,670	100	500						
		11	SR 2782	WHITAKER RD. FROM SR 2820 TO SR 2776	5	NO	0.25	19	12.50					259	16	95						
		12	SR 2820	FROM SR 2806 TO SR 2782	5	NO	1.24	18	62.00					1,218	73	475						
		13	SR 2776	OLD FORT RD FROM US 74A TO SR 2780	5	NO	2.16	20	108.00					2,356	141	620						
		14	SR 1745	PANTHER BR. FROM LANDFILL TO SR 1743	5	NO	1.49	18	74.50					1,464	88	310						
		15	SR 1668	WOODFIN AVE FROM NC 251 TO SR 1684	5	NO	0.39	20	19.50					425	26	181				13	2	
		TOTAL FOR PROJ NO. 13CR.20111.8							11.59		579.50	0				11,759	705	4,913			13	2
		GRAND TOTAL							15.63		644.50	2.40	55,365.00	300.00	17,858.00	22,146	1,328	6,113	6	31	25	11

PROJECT NO.	SHEET NO.	TOTAL NO.
13CR.10111.10, 13CR.20111.8	18	18

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4685000000-E	4686000000-E		4705000000-E	4710000000-E	4721000000-E			4725000000-E		4810000000-E			
					4" X 90 M WHITE THERMO LF	4" X 120 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	16" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO MSG SCHOOL 120 M EA	THERMO RXR 120 M EA	THERMO MSG ONLY 120 M EA	THERMO LT ARROW 90 M EA	THERMO RT ARROW 90 M EA	4" WHITE PAINT LF	4" YELLOW PAINT LF		
13CR.10111.10	Buncombe	1	US 74A	FROM 50' WEST OF SR 2771 (PAVEMENT CHANGE) TO SR 2773 (PAVEMENT CHANGE)		4,300	20,724		159	6			51					
		2	US 19/23	FROM SR 1220 TO SR 3613	8,237	2,059	10,296		75				27					
		3	NC 9	FROM LACKY GAP ROAD TO SUNSET DRIVE	11,616		11,616		24									
		4	NC 9	FROM SUNSET DRIVE TO I-40 EAST OFF RAMP	2,112		2,640		72				7	1				
		5	NC 9	FROM I-40 EAST OFF RAMP TO R/R TRACKS		1,030	4,118	100	146		4	8		7	8			
TOTAL FOR PROJ NO. 13CR.10111.10					21,965	7,389	49,394	100	476	6	4	8	85	8				
						56,783					18		93					
13CR.20111.8	Buncombe	6	SR 1003	PAINT FORK FROM I2 TO I2											6,758	6,758		
		7	SR 3081	ROCK HILL FROM US 25 TO US 25A												9,504	9,504	
		8	SR 1617	SLUDER BR. FROM SR 1002 TO SR 1620												52,800	52,800	
		9	SR 3075	ONTEORA FROM I 40 TO SR 3052												23,021	23,021	
		10	SR 1641	JENKINS VALLEY FROM SR 1642 TO SR 1620												35,904	35,904	
		11	SR 2782	WHITAKER RD. FROM SR 2820 TO SR 2776												5,280	5,280	
		12	SR 2820	FROM SR 2806 TO SR 2782												26,189	26,189	
		13	SR 2776	OLD FORT RD FROM US 74A TO SR 2780												45,619	45,619	
		14	SR 1745	PANTHER BR. FROM LANDFILL TO SR 1743												31,469	31,469	
		15	SR 1668	WOODFIN AVE FROM NC 251 TO SR 1684												8,237	8,237	
		TOTAL FOR PROJ NO. 13CR.20111.8														244,781	244,781	
																	489,562	
		GRAND TOTAL					21,965	7,389	49,394	100	476	6	4	8	85	8	244,781	244,781
								56,783					18		93		489,562	

