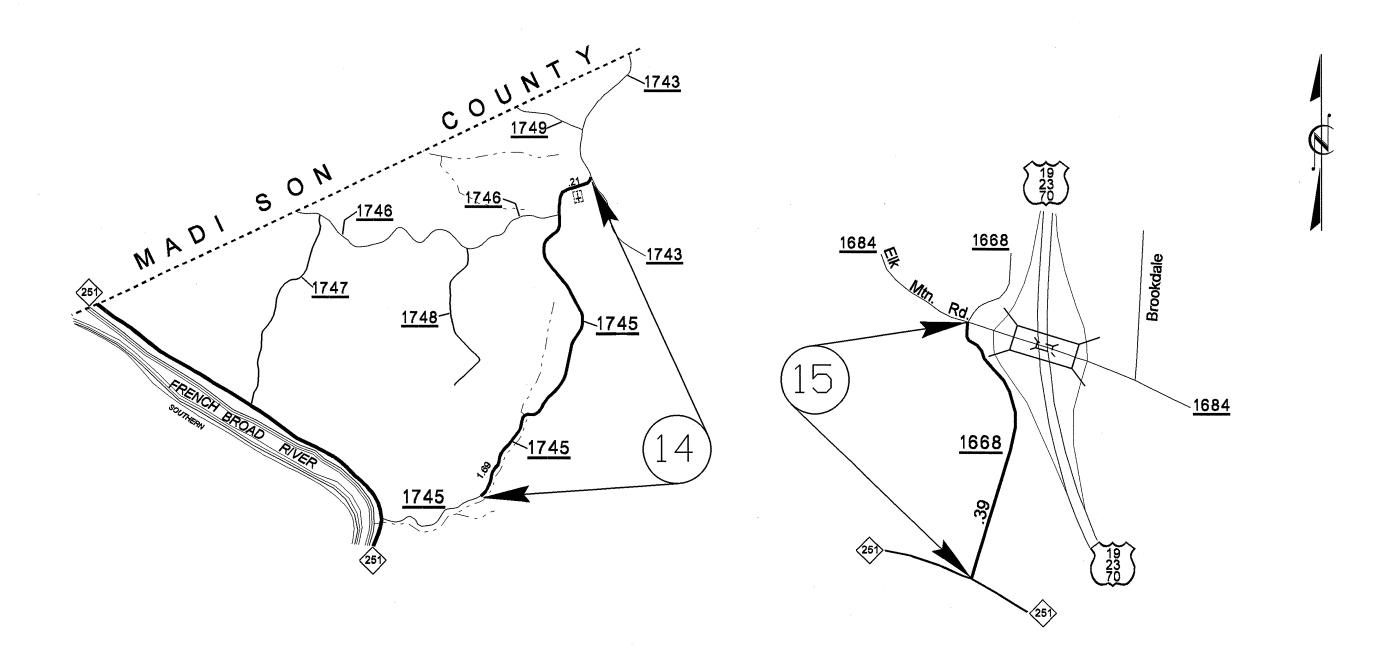
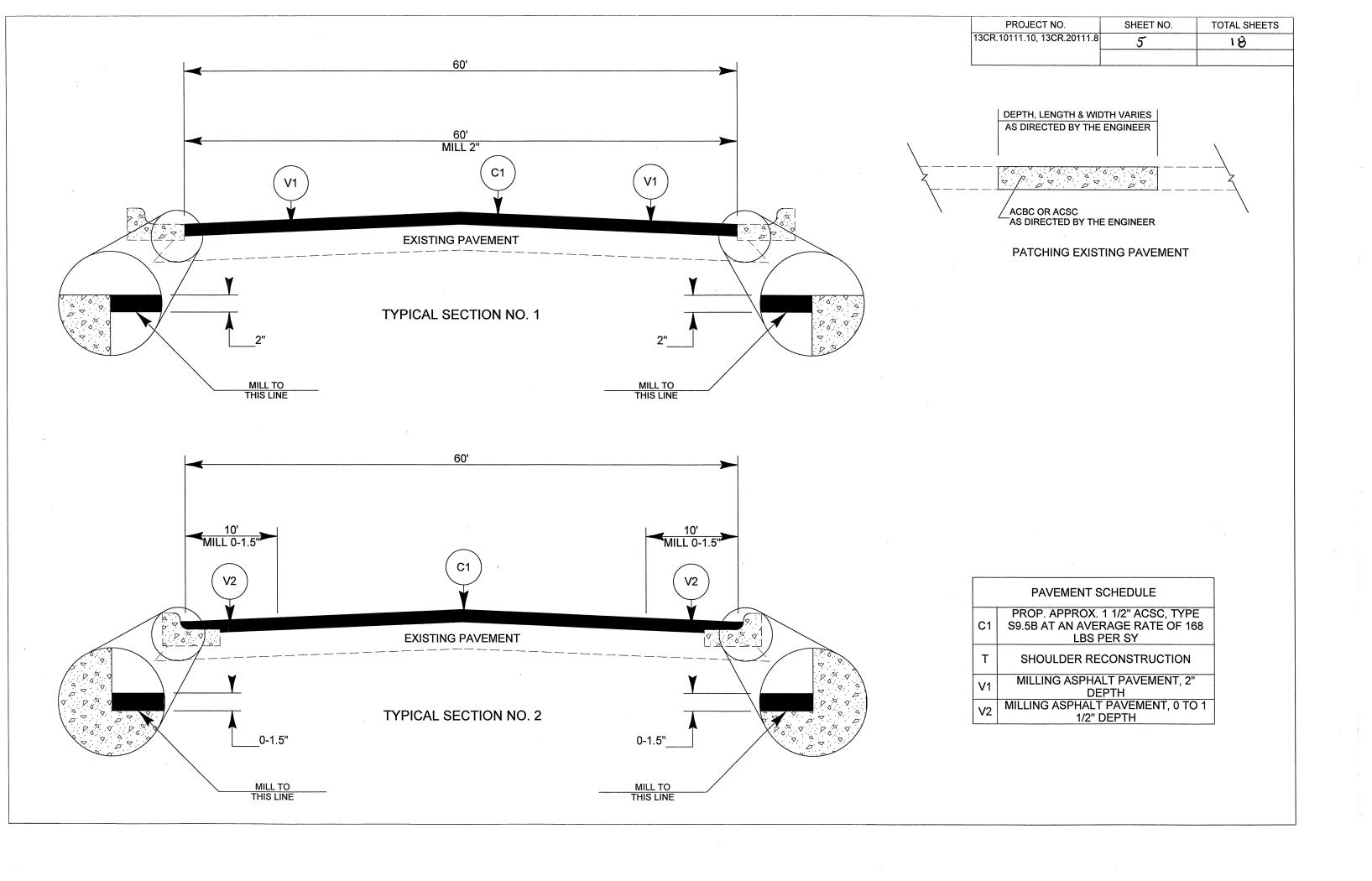
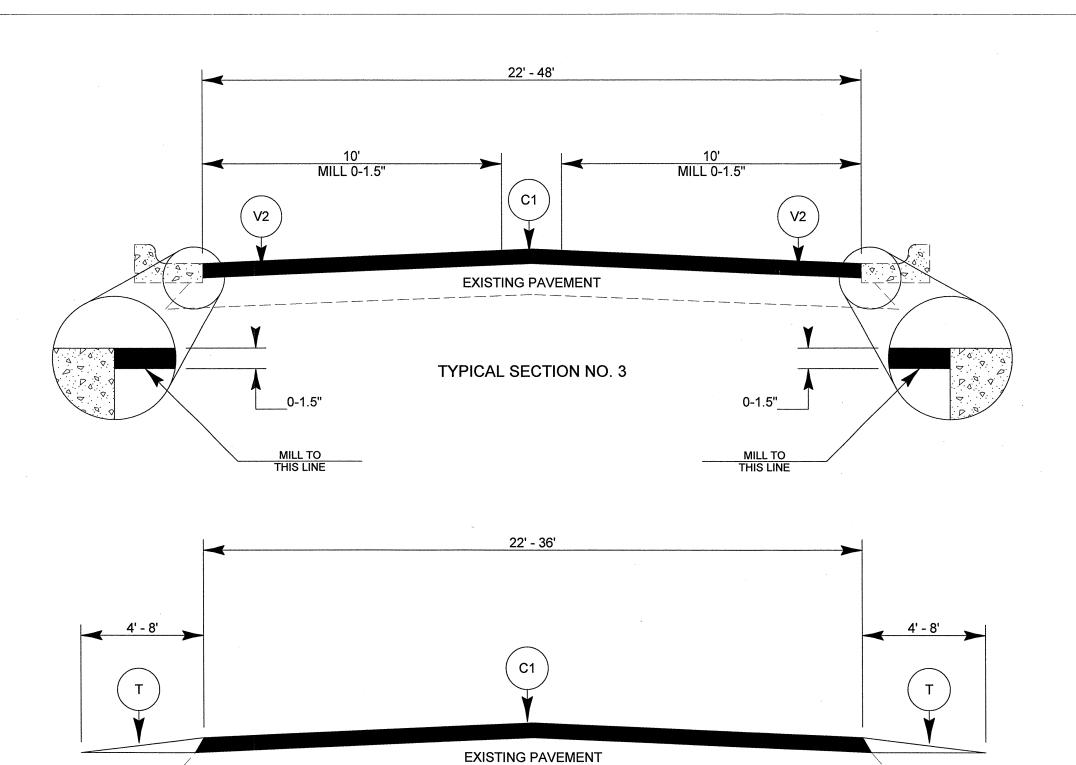


PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10 13CR.20111.8	4	18
13014.10111.10 13014.20111.0		



BUNCOMBE COUNTY





TYPICAL SECTION NO. 4

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.10, 13CR.20111.8	6	18

18' - 20'

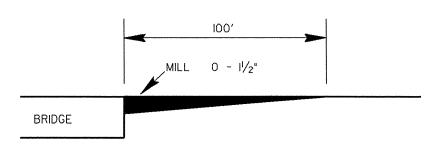
C1

EXISTING PAVEMENT

TYPICAL SECTION NO. 5

PROJECT NO. SHEET NO. TOTAL SHEETS

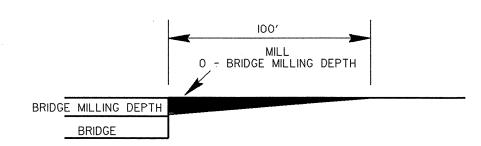
13CR.10111.10, 13CR.20111.8 7 38



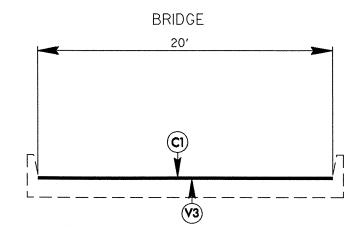
MILLING DETAIL AT BRIDGE APPROACHES

WHERE BRIDGE WILL NOT BE RESURFACED

COST OF MILLING IS INCIDENTAL TO OTHER ITEMS



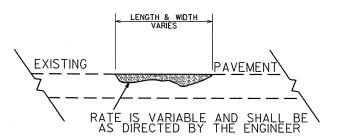
MILLING DETAIL AT BRIDGE APPROACHES
WHERE BRIDGES WILL BE MILLED THEN RESURFACED
COST OF MILLING BRIDGE APPROACHES IS INCIDENTAL
TO OTHER ITEMS

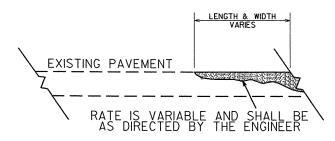


BRIDGE DETAIL

BRIDGE NUMBER 27 MAP 2
MILL 11/2" OFF EXISTING PAVEMENT ON BRIDGE
THEN RESURFACE BRIDGE DECK WITH 11/2" OF S9.5B
SEE MAPS FOR BRIDGE LOCATION

PROJECT NO.	SHEET NO.	TOTAL SHEETS			
13CR.10111.10. 13CR.20111.8	8	18			
1301/201130					





DETAIL SHOWING METHOD OF WEDGING

	PAVEMENT SCHEDULE									
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.									
٧3	MILLING ASPHALT PAVEMENT, 1½" DEPTH									

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

GUTTER RAMP AND Œ CURB

FOR DRAWING WHEELCHAI

PROPOSED

DETAIL ENGLISH

SHEET 1 OF 3

848D05

BACK OF SIDEMALK DROP **

ENGLISH

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TAIL

DRAWING

П SR R

PROPOSED

CURB

AND

GUTTER

WHEEL

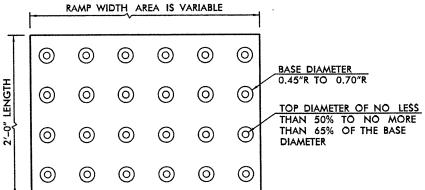
CHAIR

RAMP

1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.

ISOMETRIC VIEW

2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



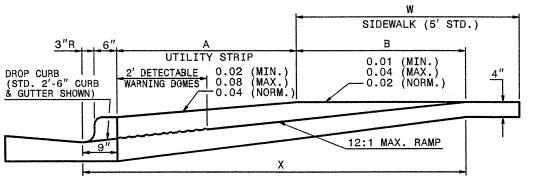
0.9" TO	0.65″	<u>1.6″ T</u> €	o,	
1.4" TO	MIN.	0 1.0° 10° 2.4″	1.	
. O O . O . O .	· · · · · · · · · · · ·	. p. 0. 0 . p	, ø. j	
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DETECTABLE WARNING DOMES

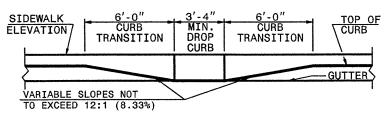
W	Α	W+A+9"	Х	В
5'	0.0'	5.8'	5.8'	5.0'*
6'	0.0'	6.8′	6.8'	6.0′**
7'	0.0'	7.8′	7.3'	6.5'**
8'	0.0'	8.8'	7.3'	6.5'**
5'	2.0'	7.8′	7.8'	5.0'
5'	2.5'	8.3'	8.1'	4.8'
5'	3.0'	8.8′	8.3'	4.4'
5'	3.5'	9.3'	8.4'	4.1'
5'	4.0'	9.8'	8.6'	3.8'
5′	4.5'	10.3'	8.7'	3.4'
5'	5.0'	10.8'	8.9'	3.1'

B = X - (A+9'')

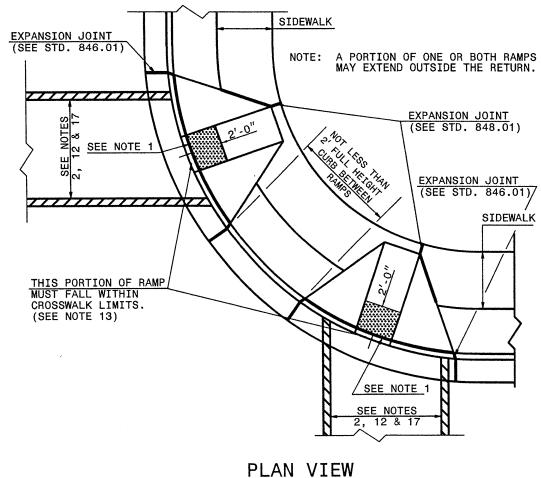
- SIDEWALK SLOPES.
- SIDEWALK SLOPES 0.04.



SECTION B-B



SECTION A-A



DUAL RAMPS

ANY RADII

(40" MIN. FLOOR WIDTH)

- B = DISTANCE FROM FRONT EDGE OF SIDEWALK TO BACK POINT OF 12:1 (8.33%) SLOPE.
- * BACK OF SIDEWALK DROP REQUIRED FOR ALL
- ** BACK OF SIDEWALK DROP REQUIRED FOR

SHEET 1 OF 3

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

848D05

RALEIGH

STATE OF
NORTH CAROLINA
EPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS

DEPT

GUTTER

FOR

DRAWING

WHEELCHAIR RAMP AND CURB PROPOSED

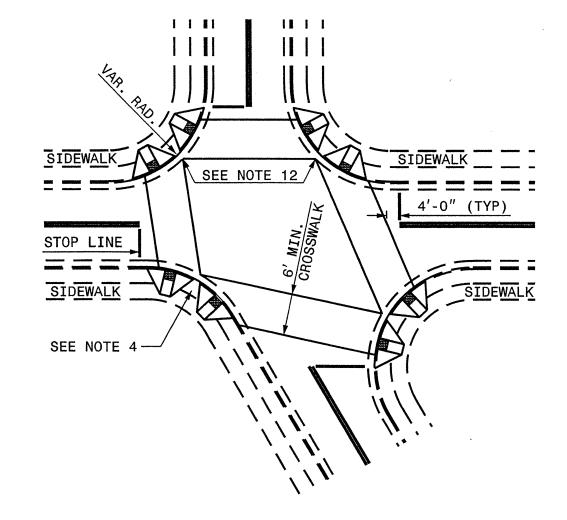
DETAIL ENGLISH

DUAL RAMP RADII.....ANY

SHEET 2 OF 3 848D05

1 1 11 11 1 1 SEE NOTE 4 SIDEWALK 6' MIN. CROSSWALK NOTE 12 SEE NOTE 3 STOP LINE

DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

ALLOWABLE LOCATIONS

ROADWAY PLAN SYMBOL (WCR) FOR PROPOSED WHEELCHAIR RAMP

PROPOSED WHEELCHAIR RAMP PROPOSED OR FUTURE SIDEWALK

SHEET 2 OF 3

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL

DRAWING

FOR

WHEELCHAIR

RAMP

848D05

PROPOSED

CURB

AND GUTTER

STATE OF
NORTH CAROLIN,
OF TRANSPORT
/ISION OF HIGHW
RALEIGH

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П 9 NOTES:

- 1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
- 2. CROSSWALK WIDTHS AND CONFIGURATION VARY BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
- NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILTIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.

IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1,1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.

THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES. COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILIAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.

- PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
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- CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS.
- USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
- 8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
- 9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADII. WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 17)
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- CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
- 12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
- 13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
- PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

SHEET 3 OF 3

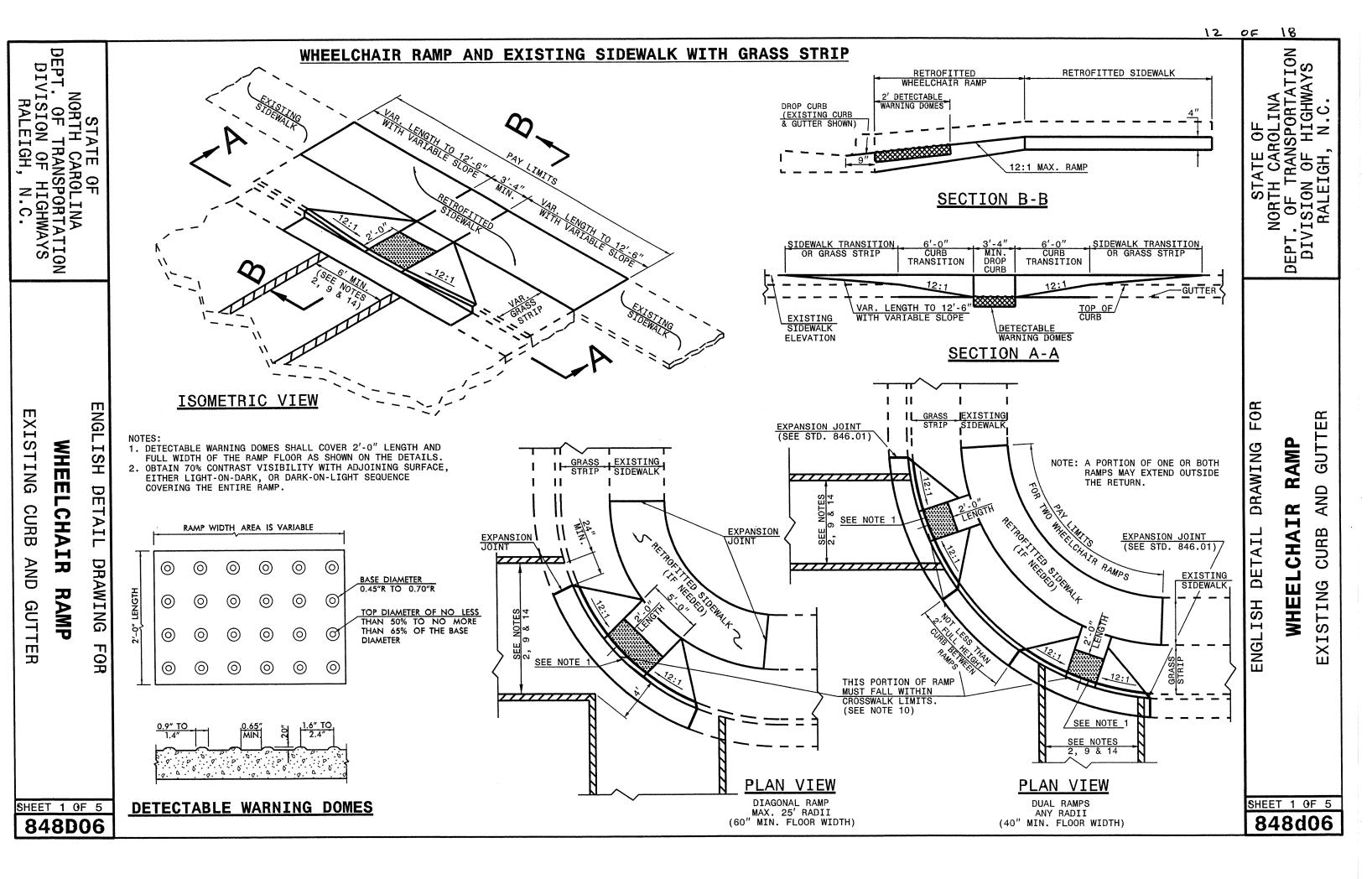
848D05

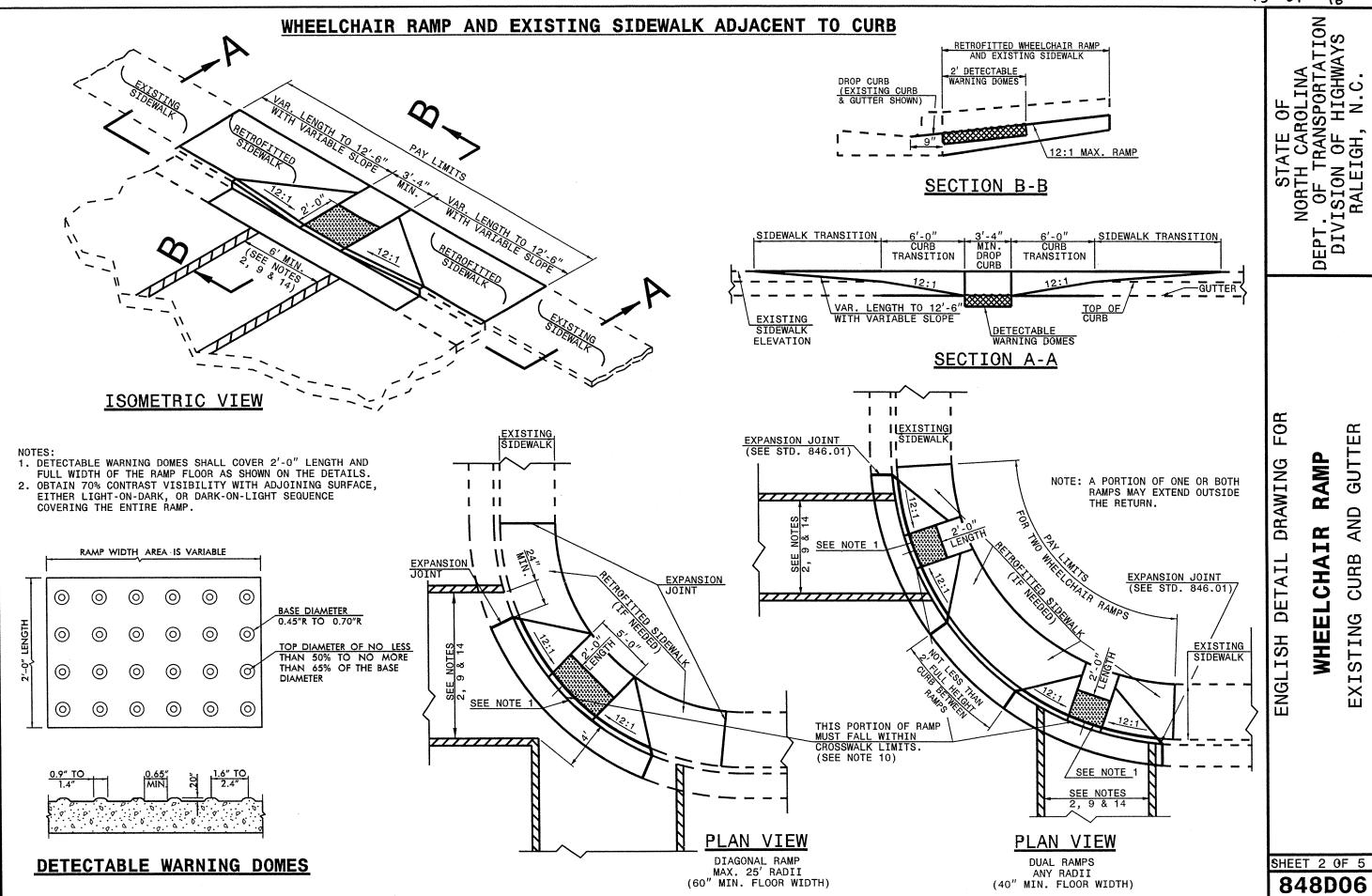
SHEET 3 OF 3

848D05

AMP GUT 配 α CHAI CURB 닙 **PROPOSED** Ш

DRAWING DETAIL ENGLISH





WHEELCHAIR **CURB** AND RAMP GUTTER

EXISTING

ENGLISH DE

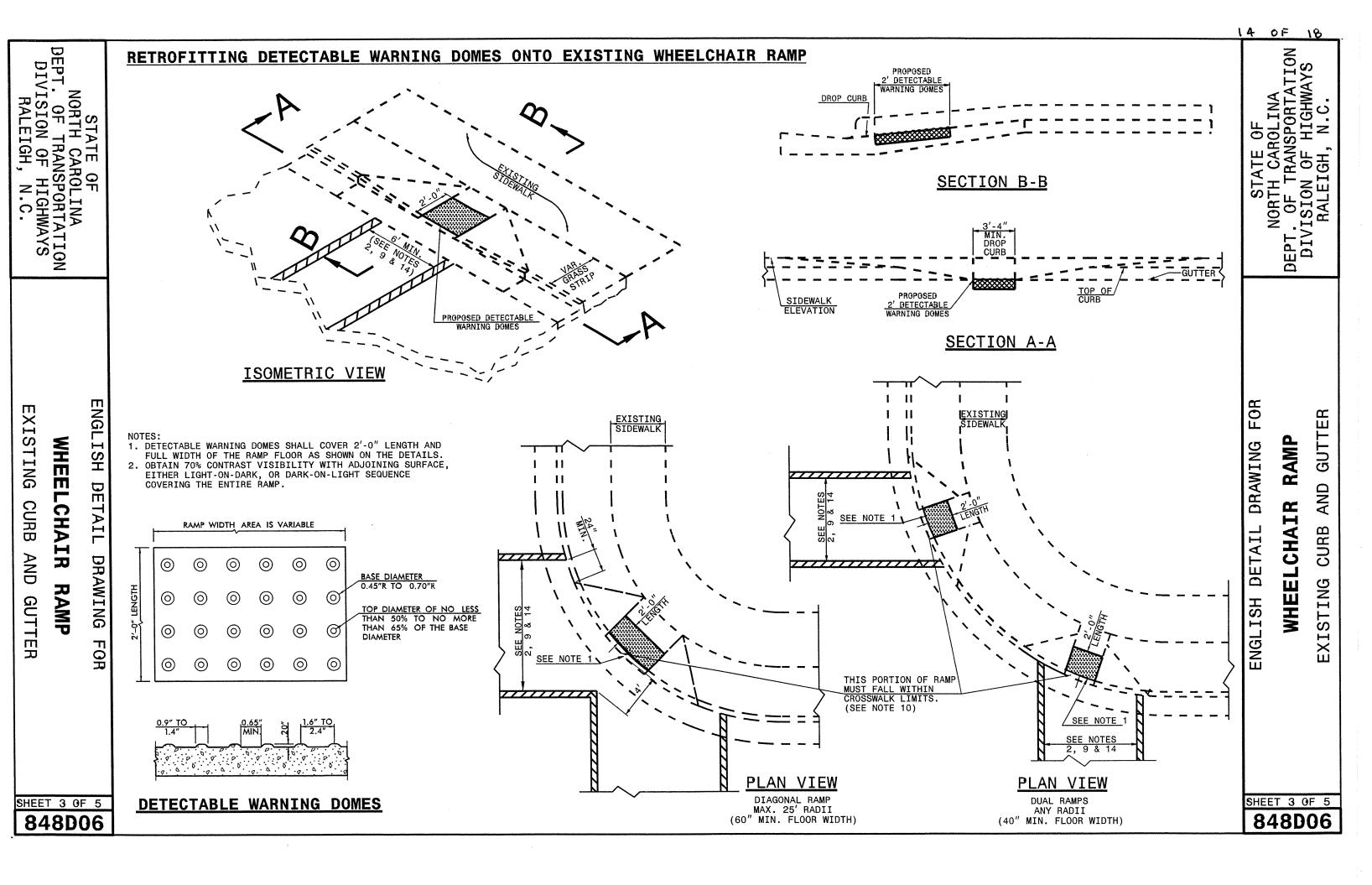
TAIL

DRAWING

FOR

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

SHEET 2 OF 5 848D06



EXISTING

ENGLISH

DETAIL

DRAWING

FOR

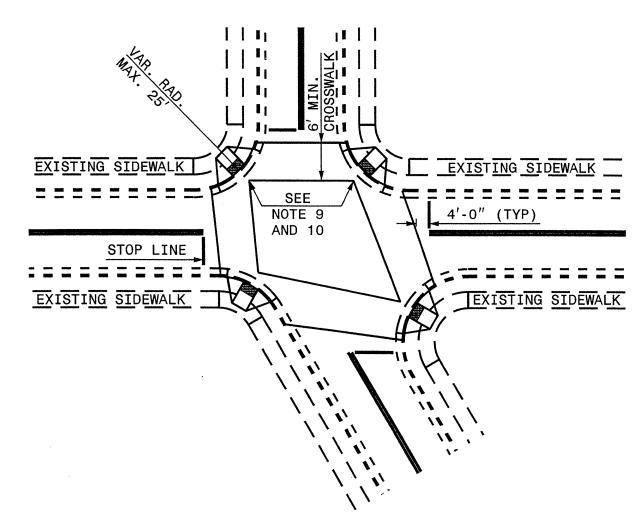
EXISTING SIDEWALK

EXISTING SIDEWALK EXISTING SIDEWALK NOTE 9 SEE STOP LINE NOTE 3

DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS

EXISTING

SIDEWALK



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

RESURFACING PROJECTS

PROPOSED WHEELCHAIR RAMP FOR RESURFACING PROJECTS EXISTING SIDEWALK

ALLOWABLE LOCATIONS DIAGONAL RAMP RADII...MAX. 25'

SHEET 4 OF 5 848D06

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STATE OF
NORTH CAROLINA
T. OF TRANSPORTATION

DEPT

FOR

DRAWING

DETAIL

ENGLISH

18

DIVISION OF RALEIGH,

GUTTER

AND

CURB

EXISTING

RAMP

WHEELCHAIR

STATE OF
NORTH CAROLINA
JEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

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STATE OF
NORTH CAROLINA
T. OF TRANSPORTATION
VISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH

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TAIL

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FOR

NOTES:

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848D06

PROJECT NO.	SHEET NO.	TOTAL NO.
13CR.10111.10, 13CR.20111.8	17	18

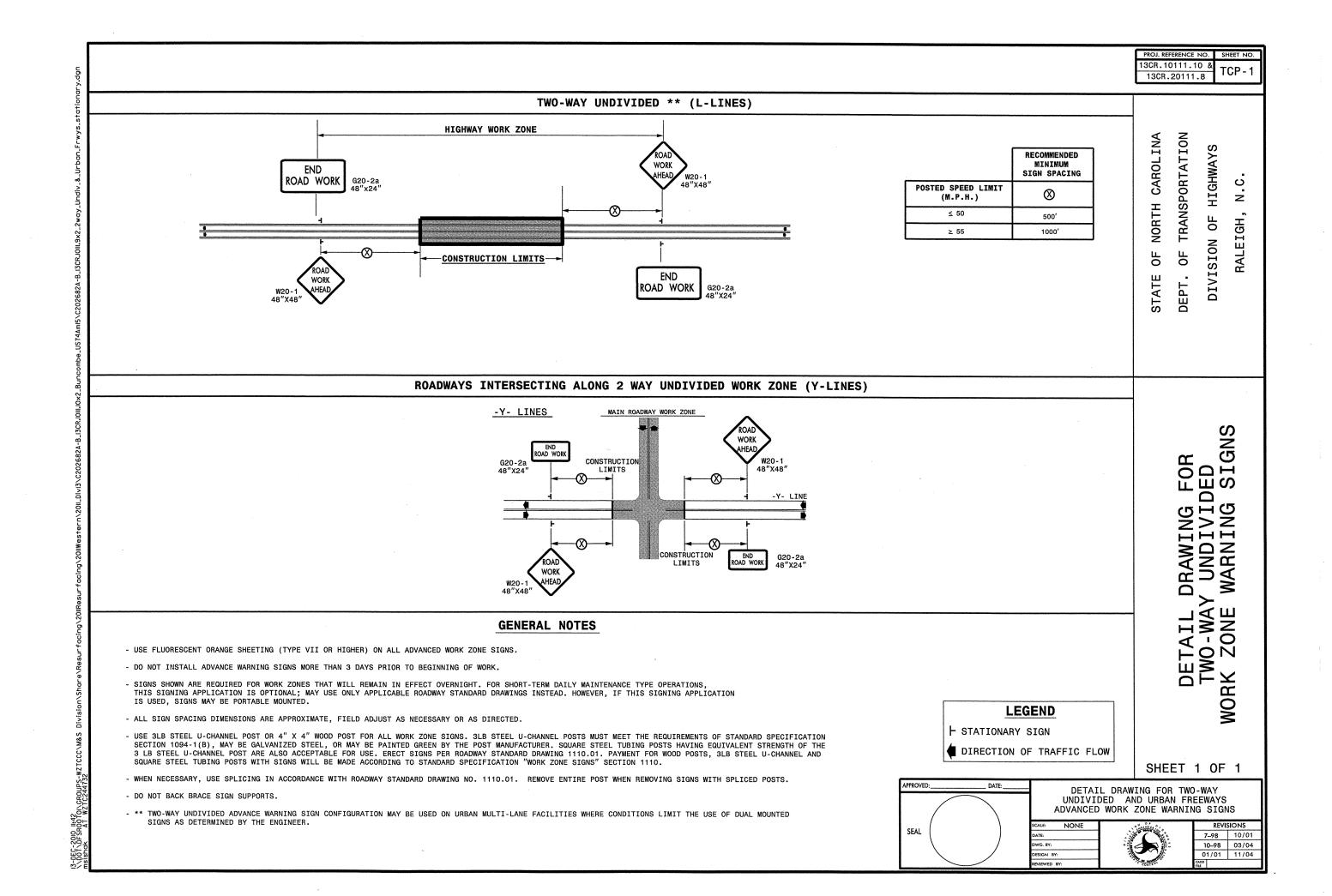
SUMMARY OF QUANTITIES

PROJECT	COUNTY	MAP	ROUTE	DESCRIPTION	TYP	FINAL SURFACE TESTING REQUIRED	LENGTH	WIDTH	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH	MILLING ASPHALT PAVEMENT, 0" TO 1 1/2" DEPTH	SURFACE COURSE, TYPE S9.5B	ASPHALT BINDER, GRADE PG 64-22	PATCHING EXISTING PAVEMENT	RETROFIT EXIST. WHEEL CHAIR	CONCRETE WHEELCHAIR RAMPS	ADJUSTMENT OF MANHOLES	ADJUSTMENT OF METER BOXES OR VALVE BOXES
NO		NO			NO		MI	FT	TONS	SMI	SY	SY	SY	TONS	TONS	TONS	RAMPS EA	EA	EA	EA
13CR.10111.10	Buncombe	1	US 74A	FROM 50' WEST OF SR 2771 (PAVEMENT CHANGE) TO SR 2773 (PAVEMENT CHANGE)	1	NO	1.57	60			55,365.00			5,117	307	100	6	9		
			US 19/23	FROM SR 1220 TO SR 3613	2	NO	0.78	60				300.00	9,260.00	2,542	153	50		8		
		3	NC 9	FROM LACKY GAP ROAD TO SUNSET DRIVE FROM SUNSET DRIVE TO I-40	3,4	NO	1.1	22	55.00	2.00			3,689.00	1,319	79	450		8	5	7
		4	NC 9	EAST OFF RAMP	3.4	NO	0.2	36	10.00	0.40			333.00	392	23	300			1	1
				FROM I-40 EAST OFF RAMP TO	1															
		5	NC 9	R/R TRACKS	3	NO	0.39	48	05.00	0.40	55 005 00	200.00	4,576.00	1,017	61	300		6	6	11
TOTAL FOR	PROJ NO.	13CR.1	0111.10			<u> </u>	4.04	L	65.00	2.40	55,365.00	300.00	17,858.00	10,387	623	1,200	ь	31	12	9
13CR.20111.8	Buncombe	6	SR 1003	PAINT FORK FROM 12 TO 12	5	l NO	0.32	20	16.00		T	I	•	349	21	420				
				ROCK HILL FROM US 25 TO US		1														
		7	SR 3081	25A	5	NO	0.45	20	22.50				ļ	491	29	250			8	
		8	SR 1617	SLUDER BR. FROM SR 1002 TO SR 1620	5	NO	2.5	18	125.00					2,456	147	1,800				
		9	SR 3075	ONTEORA FROM I 40 TO SR 3052 JENKINS VALLEY FROM SR 1642		NO	1.09	18	54.50					1,071	64	262			5	2
		10	SR 1641	TO SR 1620 WHITAKER RD. FROM SR 2820 TO	5	NO	1.7	. 18	85.00					1,670	100	500				
		11	SR 2782	SR 2776	5	NO	0.25	19	12.50					259	16	95				
		12	SR 2820	FROM SR 2806 TO SR 2782	5	NO	1.24	18	62.00				ļ	1,218	73	475			-	
		13	SR 2776	OLD FORT RD FROM US 74A TO SR 2780	5	NO	2.16	20	108.00					2,356	141	620				
		14	SR 1745	PANTHER BR. FROM LANDFILL TO SR 1743	5	NO	1.49	18	74.50					1,464	88	310				
		15	SR 1668	WOODFIN AVE FROM NC 251 TO SR 1684	5	NO	0.39	20	19.50					425	26	181				
TOTAL FOR	PROJ NO.						11.59		579.50	0				11,759	705	4,913	<u> </u>	1	13	2
	RAND TO	AL			T	1	15.63	1	644.50	2.40	55,365.00	300.00	17.858.00	22,146	1.328	6,113	6	31	25	11

PROJECT NO.	SHEET NO.	TOTAL NO.			
13CR.10111.10, 13CR.20111.8	18	18			

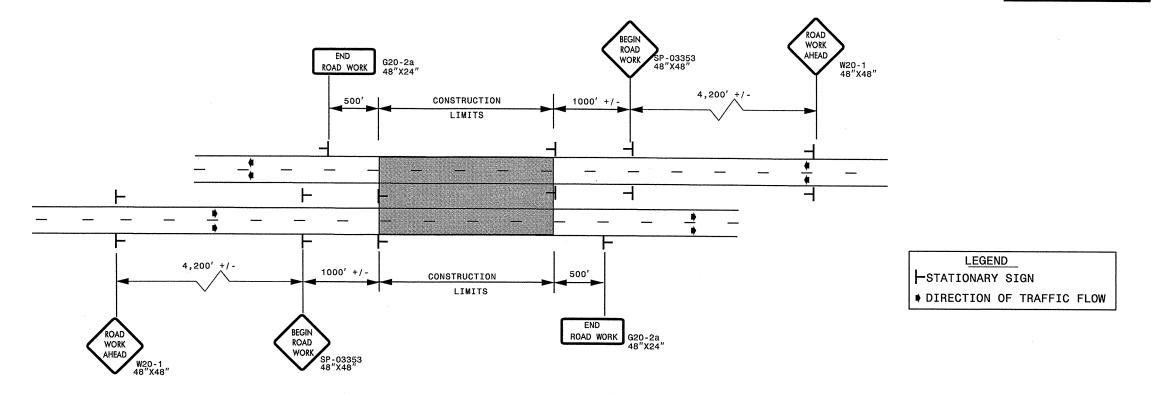
THERMOPLASTIC AND PAINT QUANTITIES

				1 11 1 1 1 1 1 1 1	** • •			AIID	1 / 11	• • • • •	JAN					
					4685000000-E		0000-E	4705000000-E	4710000000-E		721000000-E		47250	00000-E	481000	0000-E
PROJECT	COUNTY	MAP	ROUTE	DESCRIPTION	4" X 90 M	4" X 120 M	4" X 120 M	16" X 120 M	24" X 120 M	THERMO MSG	THERMO	THERMO	THERMO LT	THERMO RT	4" WHITE	4" YELLOW
					WHITE	WHITE	YELLOW	WHITE THERMO		SCHOOL 120 M	RXR 120 M	MSG ONLY	ARROW 90 M	ARROW 90 M	PAINT	PAINT
					THERMO	THERMO	THERMO		THERMO			120 M				
NO		NO			LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	LF	LF
				FROM 50' WEST OF SR 2771 (İ										
				PAVEMENT CHANGE) TO SR 2773												
13CR.10111.10	Buncombe		US 74A	(PAVEMENT CHANGE)		4,300	20,724		159	6			51	•		
		2	US 19/23	FROM SR 1220 TO SR 3613	8,237	2,059	10,296		75				27			
				FROM LACKY GAP ROAD TO				,								
		3	NC 9	SUNSET DRIVE	11,616		11,616		24							
				FROM SUNSET DRIVE TO I-40												
		4	NC 9	EAST OFF RAMP	2,112		2,640		72				7	1		
				FROM I-40 EAST OFF RAMP TO												
		5	NC 9	R/R TRACKS		1,030	4,118	100	146		-4	8		7		
TOTAL FOR I	PROJ NO 1	3CR 1	0111 10		21,965	7,389	49,394	100	476	6	4	8	85	8		
			•••••			56,	783				18			93		
	,				******************											
13CR.20111.8	Buncombe	6	SR 1003	PAINT FORK FROM 12 TO 12											6,758	6,758
		_		ROCK HILL FROM US 25 TO US												
		7	SR 3081	25A											9,504	9,504
	-			SLUDER BR. FROM SR 1002 TO												
		8	SR 1617	SR 1620											52,800	52,800
		9	SR 3075	ONTEORA FROM I 40 TO SR 3052											23,021	23,021
				JENKINS VALLEY FROM SR 1642						,						
		10	SR 1641	TO SR 1620								 			35,904	35,904
				WHITAKER RD. FROM SR 2820 TO							1					_
		11	SR 2782	SR 2776		ļ									5,280	5,280
		12	SR 2820	FROM SR 2806 TO SR 2782		ļ									26,189	26,189
				OLD FORT RD FROM US 74A TO				1								
		13	SR 2776	SR 2780		ļ	ļ								45,619	45,619
			SR 1745	PANTHER BR. FROM LANDFILL TO				1								
		14	SR 1745	SR 1743											31,469	31,469
		45	CD 4660	WOODFIN AVE FROM NC 251 TO			1									
			SR 1668	SR 1684					 						8,237	8,237
TOTAL FOR	PROJ NO. 1	13CR.2	20111.8			 	L	_			L	L			244,781	244,781
						1		L	L	<u> </u>			l		489	9,562
					21,965	7,389	49,394	100	476	6	1 4	T 8	85	0	244,781	244,781
G	RAND TOTA	AL			21,000		783	100	410	ļ	18	10		93).562

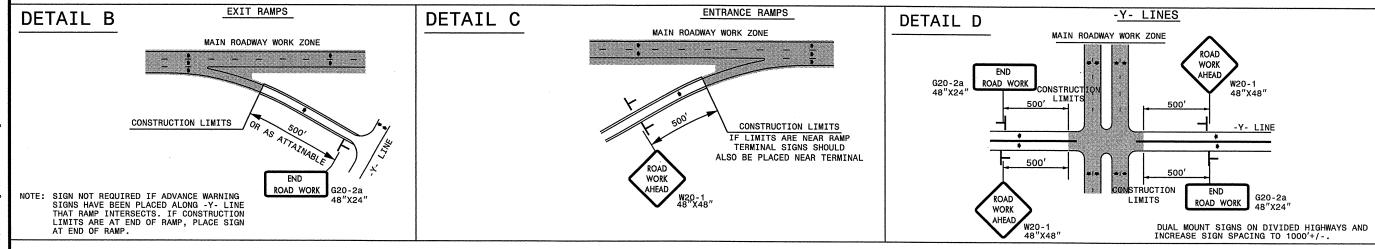


PROJ. REFERENCE NO. SHEET NO. 10CR.10111.10 & TCP-2

DETAIL A



* USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.



GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

