

**WBS ELEMENT: IC.066058, ETC.**

**CONTRACT NO:**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

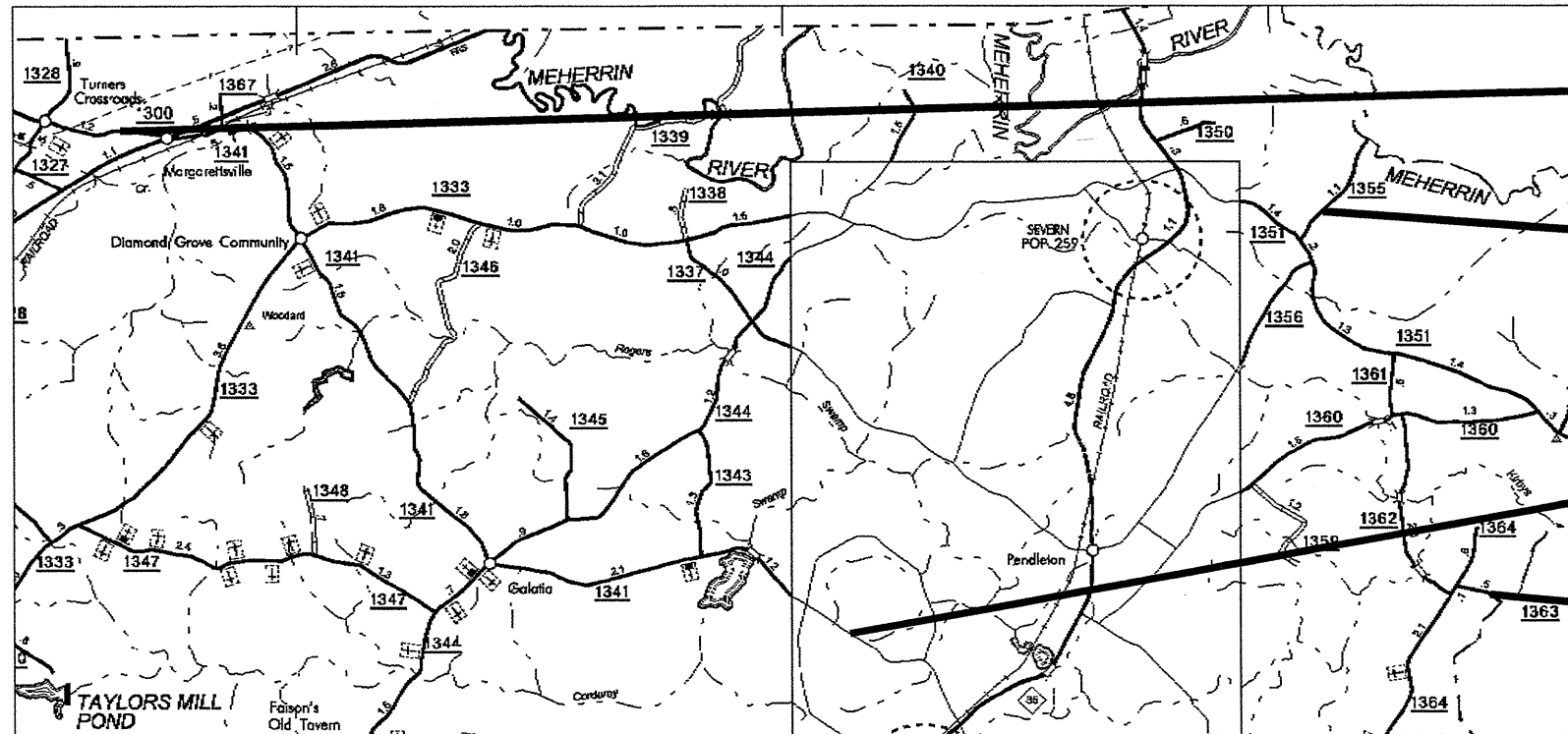
# NORTHAMPTON COUNTY

*LOCATION: Northampton County SR 1300 - From SR 1328 to NC 186  
Northampton County SR 1355 - From SR 1351 to Hertford County Line  
Northampton County SR 1341 - From NC 35 to SR 1343  
Northampton County SR 1363 - From SR 1364 to Hertford County Line*

*TYPE OF WORK: Widening, Resurfacing, and Paint Pavement Markings*



STATE	STATE PROJECT REFERENCE NO.	BID NO.	TOTAL BIDS
N.C.	IC.066058, ETC.	1	
STATE PROJ. NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
ICR.10661.17	11		
ICR.10661.18	14		
ICR.10661.19	12		
ICR.10661.20	13		
ICR.20661.56	1		
ICR.20661.57	2		
ICR.20661.58	3		
ICR.20661.59	4		
ICR.20661.60	5		
ICR.20661.61	6		
ICR.20661.62	10		
ICR.20661.63	8		
ICR.20661.64	9		
ICR.20661.65	7		



MAP 3

MAP 10

MAP 1

MAP 2

**NOT TO SCALE**

**PROJECT LENGTH**

- ICR.20661.58 - MAP 3 ..... 1.19 MILES
- ICR.20661.62 - MAP 10 ..... 1.20 MILES
- ICR.20661.56 - MAP 1 ..... 3.25 MILES
- ICR.20661.57 - MAP 2 ..... 0.47 MILES

Prepared in the Office of  
**DIVISION OF HIGHWAYS**  
113 Airport Dr., Edenton NC, 27932

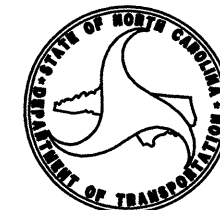
2006 STANDARD SPECIFICATIONS

LETTING DATE:

W. B. HOBBS, P.E.  
DIVISION PROJECT MANAGER

C. E. SLACHTA  
DIVISION PROPOSALS ENGINEER

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA



**CONTRACT NO: WBS ELEMENT: IC.066058, ETC.**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

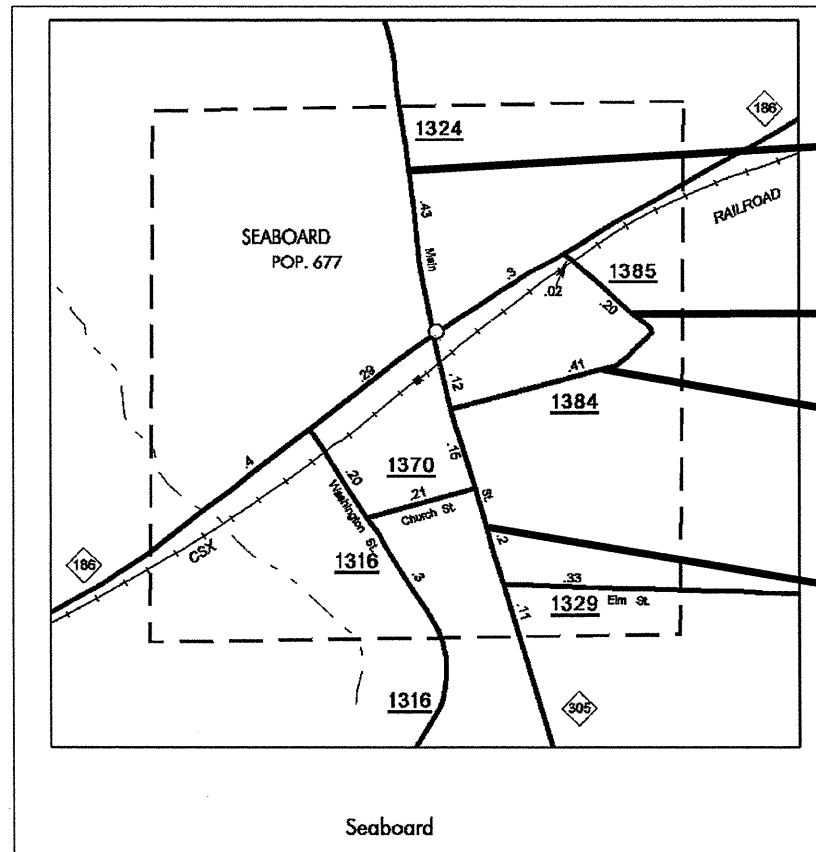
***NORTHAMPTON COUNTY***

*LOCATION: Northampton County SR 1324 From NC 186 to NCL Seaboard  
Northampton County SR 1385 From SR 1384 to R/R Crossing  
Northampton County SR 1384 From NC 305 to SR 1385  
Northampton County NC 305 From NC 186 to SCL Seaboard*

*TYPE OF WORK: Milling, Resurfacing, Utility Adjustments, and Paint Pavement Markings*



STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	IC.066058, ETC.	2	
STATE PROJ. NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
ICR.10661.17	11		
ICR.10661.18	14		
ICR.10661.19	12		
ICR.10661.20	13		
ICR.20661.56	1		
ICR.20661.57	2		
ICR.20661.58	3		
ICR.20661.59	4		
ICR.20661.60	5		
ICR.20661.61	6		
ICR.20661.62	10		
ICR.20661.63	8		
ICR.20661.64	9		
ICR.20661.65	7		



**MAP 6**

**MAP 5**

**MAP 4**

**MAP 12**

**NOT TO SCALE**

PROJECT LENGTH	
ICR.20661.61 - MAP 6 .....	0.43 MILES
ICR.20661.60 - MAP 5 .....	0.19 MILES
ICR.20661.59 - MAP 4 .....	0.41 MILES
ICR.10661.19 - MAP 12 .....	0.54 MILES

Prepared in the Office of:  
**DIVISION OF HIGHWAYS**  
115 Airport Dr., Edenton NC, 27932

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2006 STANDARD SPECIFICATIONS

LETTING DATE: \_\_\_\_\_

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**W. B. HOBBS, P.E.**  
DIVISION PROJECT MANAGER

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**C. E. SLACHTA**  
DIVISION PROPOSALS ENGINEER

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA

**CONTRACT NO: WBS ELEMENT: IC.066058, ETC.**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

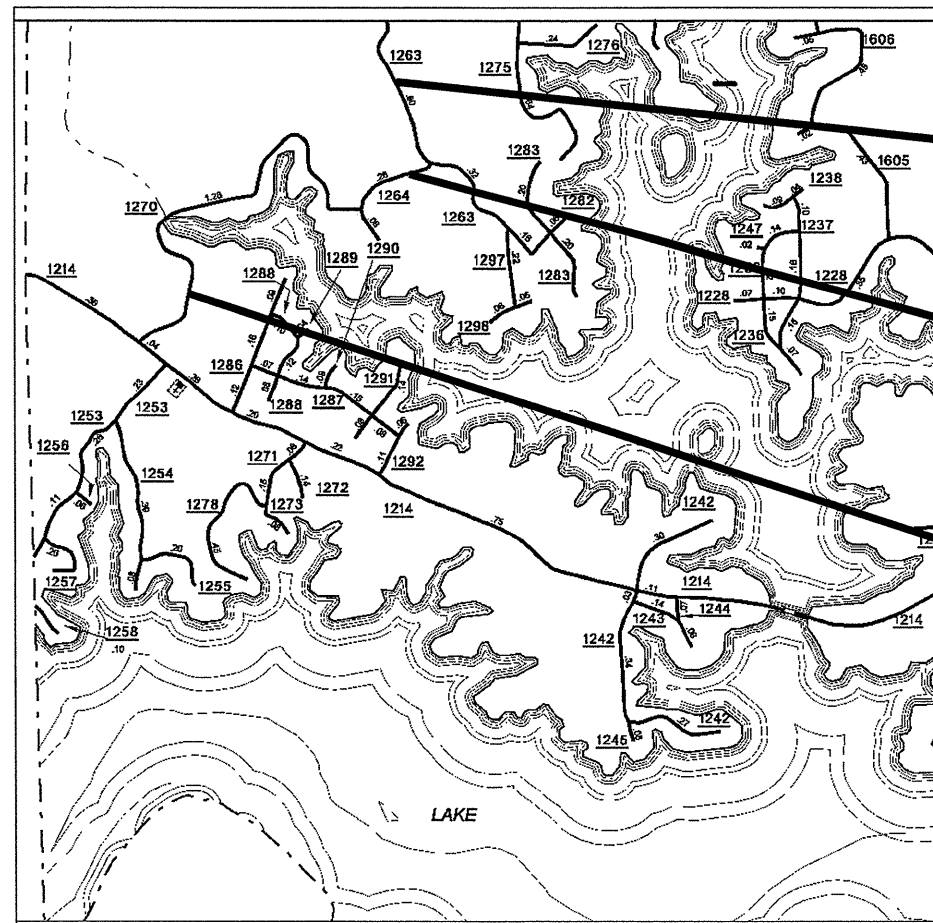
# NORTHAMPTON COUNTY



*LOCATION: Northampton County SR 1263 From VA Line to SR 1264  
Northampton County SR 1264 From SR 1263 to SR 1270  
Northampton County SR 1270 From SR 1214 to SR 1264*

*TYPE OF WORK: Resurfacing and Paint Pavement Markings*

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	IC.066058, ETC.	3	
STATE PROJ. NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
1CR.10661.17	11		
1CR.10661.18	14		
1CR.10661.19	12		
1CR.10661.20	13		
1CR.20661.56	1		
1CR.20661.57	2		
1CR.20661.58	3		
1CR.20661.59	4		
1CR.20661.60	5		
1CR.20661.61	6		
1CR.20661.62	10		
1CR.20661.63	8		
1CR.20661.64	9		
1CR.20661.65	7		



MAP 8

MAP 9

MAP 7

**NOT TO SCALE**

**PROJECT LENGTH**

1CR.20661.63 – MAP 8 ..... 0.58 MILES  
1CR.20661.64 – MAP 9 ..... 0.27 MILES  
1CR.20661.65 – MAP 7 ..... 1.27 MILES

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113 Airport Dr., Edenton NC, 27932

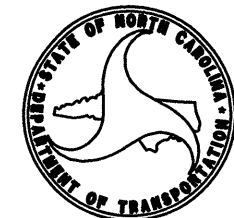
2006 STANDARD SPECIFICATIONS

LETTING DATE:

**W. B. HOBBS, P.E.**  
DIVISION PROJECT MANAGER

**C. E. SLAGHTA**  
DIVISION PROPOSALS ENGINEER

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA



**CONTRACT NO: WBS ELEMENT: IC.066058, ETC.**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

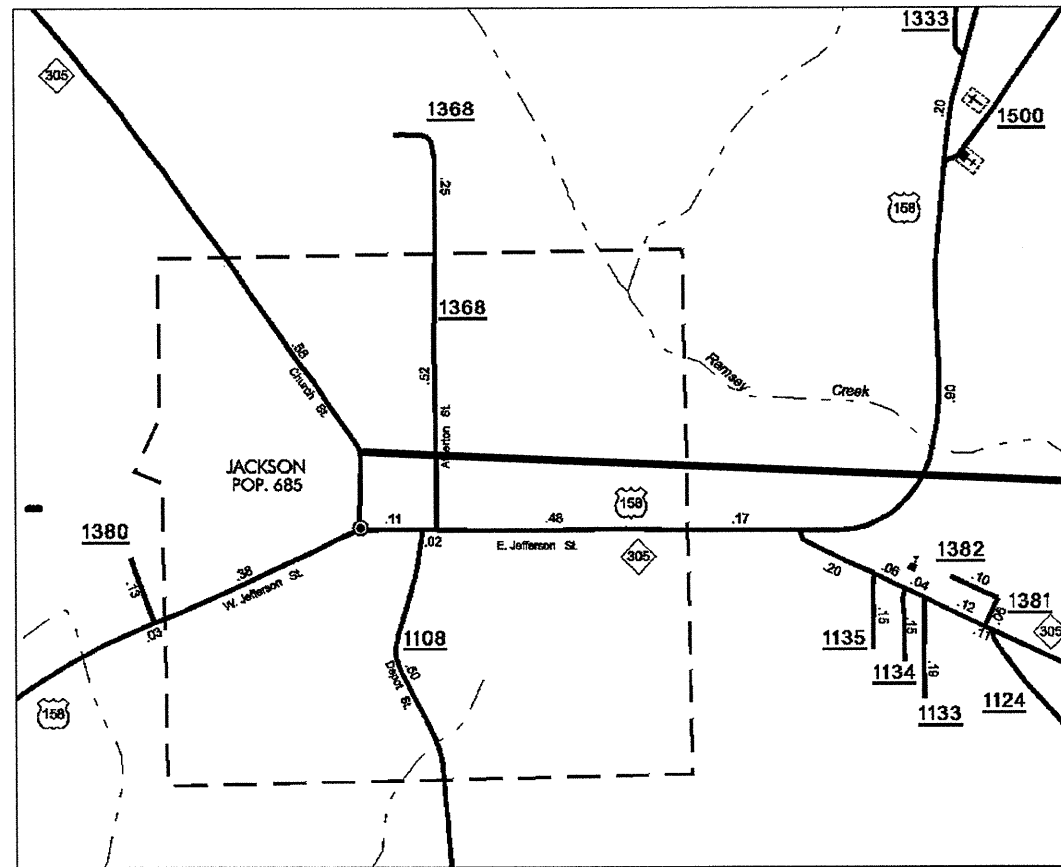
# NORTHAMPTON COUNTY

*LOCATION: Northampton County NC 305 From US 158 to NCL Jackson*

*TYPE OF WORK: Curb and Gutter, Wheelchair Ramps, Sidewalk, Milling,  
Resurfacing, Utility Adjustments, and Paint Pavement Markings*



STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	IC.066058, ETC.	4	
STATE PROJ. NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
1CR.10661.17	11		
1CR.10661.18	14		
1CR.10661.19	12		
1CR.10661.20	13		
1CR.20661.56	1		
1CR.20661.57	2		
1CR.20661.58	3		
1CR.20661.59	4		
1CR.20661.60	5		
1CR.20661.61	6		
1CR.20661.62	10		
1CR.20661.63	8		
1CR.20661.64	9		
1CR.20661.65	7		



MAP 13

**NOT TO SCALE**

**PROJECT LENGTH**

**1CR.10661.20 - MAP 13 ..... 0.68 MILES**

Prepared in the Office of:  
**DIVISION OF HIGHWAYS**  
113 Airport Dr., Edenton NC, 27932

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2006 STANDARD SPECIFICATIONS

LETTING DATE: \_\_\_\_\_

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**W. B. HOBBS, P.E.**  
DIVISION PROJECT MANAGER

---

**C. E. SLACHTA**  
DIVISION PROPOSALS ENGINEER

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA

**CONTRACT NO: WBS ELEMENT: IC.066058, ETC.**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

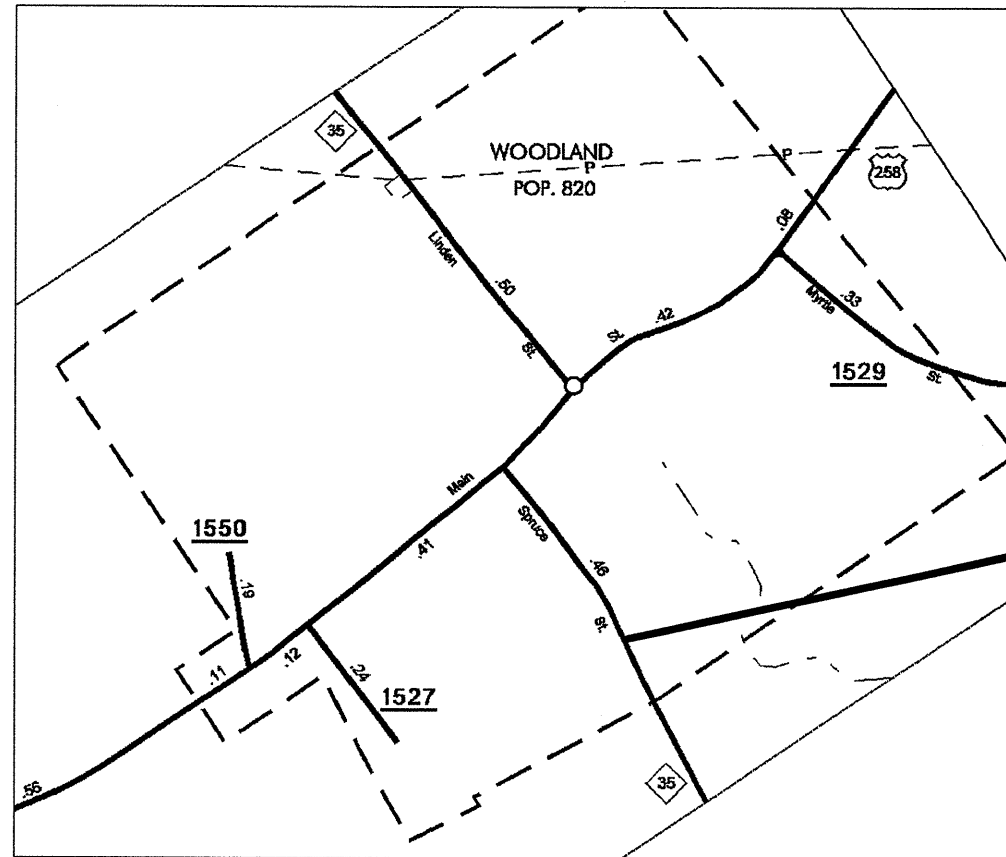
***NORTHAMPTON COUNTY***

*LOCATION: Northampton County NC 35 From US 258 to SCL Woodland*

*TYPE OF WORK: Milling, Leveling, Resurfacing and Paint Pavement Markings*



STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	IC.066058, ETC.	5	
STATE PROJ. NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
ICR.10661.17	11		
ICR.10661.18	14		
ICR.10661.19	12		
ICR.10661.20	13		
ICR.20661.56	1		
ICR.20661.57	2		
ICR.20661.58	3		
ICR.20661.59	4		
ICR.20661.60	5		
ICR.20661.61	6		
ICR.20661.62	10		
ICR.20661.63	8		
ICR.20661.64	9		
ICR.20661.65	7		



**MAP 11**

NOT TO SCALE

PROJECT LENGTH

ICR.10661.17 - MAP 11 ..... 0.45 MILES

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113 Airport Dr., Edenton NC, 27932

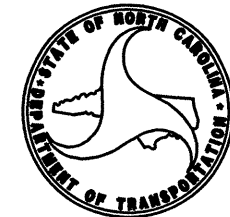
2006 STANDARD SPECIFICATIONS

LETTING DATE:

W. B. HOBBS, P.E.  
DIVISION PROJECT MANAGER

C. E. SLACHTA  
DIVISION PROPOSALS ENGINEER

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA



**WBS ELEMENT: IC.066058, ETC.**

**CONTRACT NO:**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

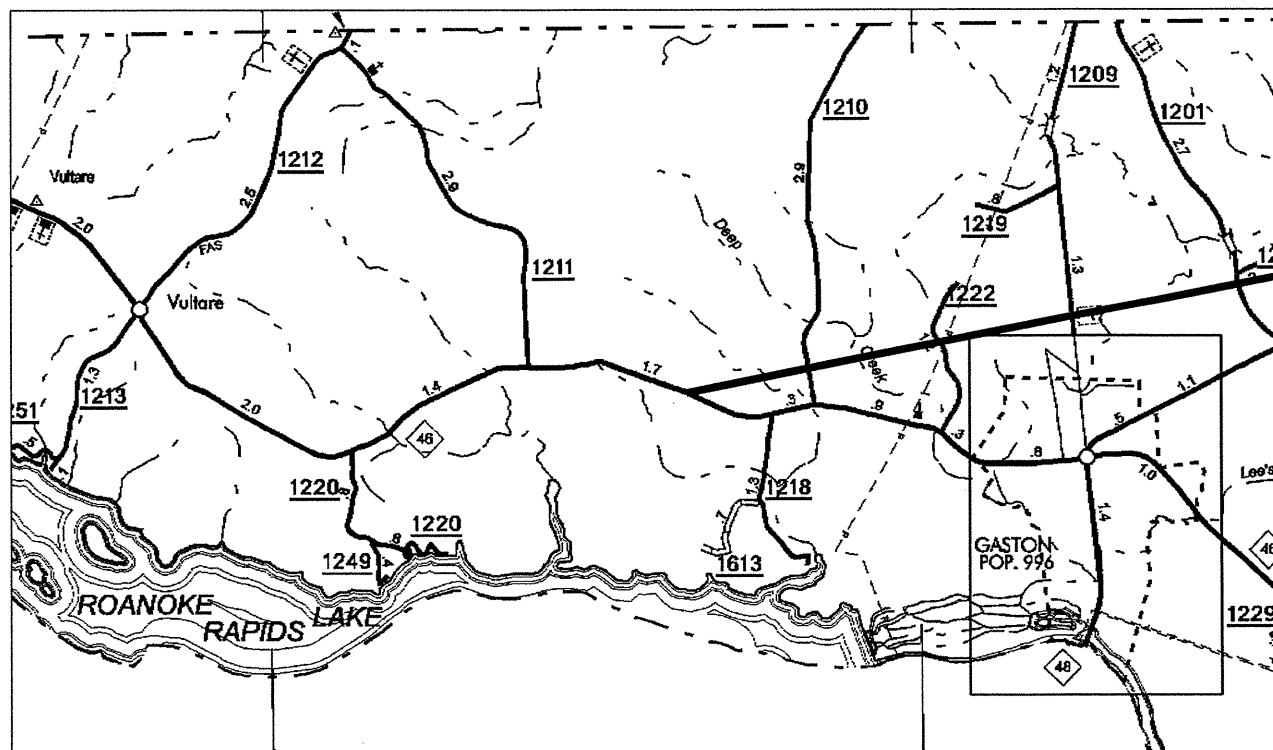
# NORTHAMPTON COUNTY

*LOCATION: Northampton County NC 46 From NC 48 to SR 1212*

*TYPE OF WORK: Resurfacing and Long-Life Pavement Markings*



STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	IC.066058, ETC.	6	
STATE PROJECT NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
1CR.10661.17	11		
1CR.10661.18	14		
1CR.10661.19	12		
1CR.10661.20	13		
1CR.20661.56	1		
1CR.20661.57	2		
1CR.20661.58	3		
1CR.20661.59	4		
1CR.20661.60	5		
1CR.20661.61	6		
1CR.20661.62	10		
1CR.20661.63	8		
1CR.20661.64	9		
1CR.20661.65	7		



**MAP 14**

**NOT TO SCALE**

*PROJECT LENGTH*

**1CR.10661.18 - MAP 14 ..... 7.44 MILES**

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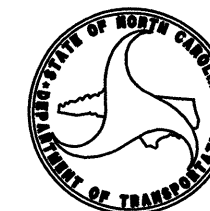
2006 STANDARD SPECIFICATIONS

LETTING DATE:

**W. B. HOBBS, P.E.**  
DIVISION PROJECT MANAGER

**C. E. SLACHTA**  
DIVISION PROPOSALS ENGINEER

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA



**CONTRACT NO: WBS ELEMENT: IC.066058, ETC.**

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

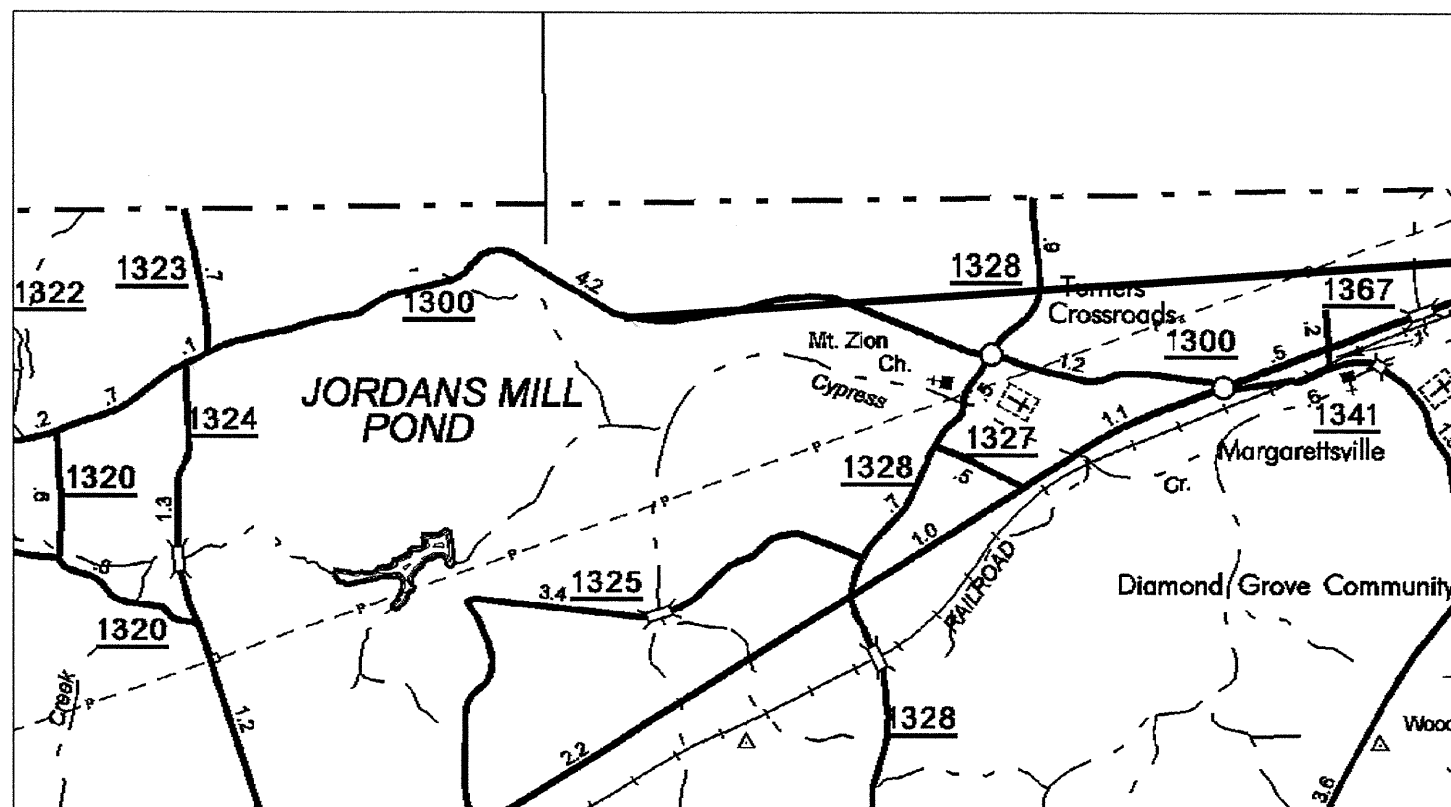
# NORTHAMPTON COUNTY

*LOCATION: Northampton County SR 1300 From SR 1324 to SR 1328*

*TYPE OF WORK: Widening, Resurfacing, and Paint Pavement Markings*



STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	IC.066058, ETC.	7	
STATE PROJ. NO.	MAP NO.	DESCRIPTION	
IC.066058	15		
1CR.10661.17	11		
1CR.10661.18	14		
1CR.10661.19	12		
1CR.10661.20	13		
1CR.20661.56	1		
1CR.20661.57	2		
1CR.20661.58	3		
1CR.20661.59	4		
1CR.20661.60	5		
1CR.20661.61	6		
1CR.20661.62	10		
1CR.20661.63	8		
1CR.20661.64	9		
1CR.20661.65	7		



MAP 15

NOT TO SCALE

PROJECT LENGTH  
  
IC.066058 - MAP 15 ..... 4.31 MILES

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113 Airport Dr., Edenton NC, 27932

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2006 STANDARD SPECIFICATIONS

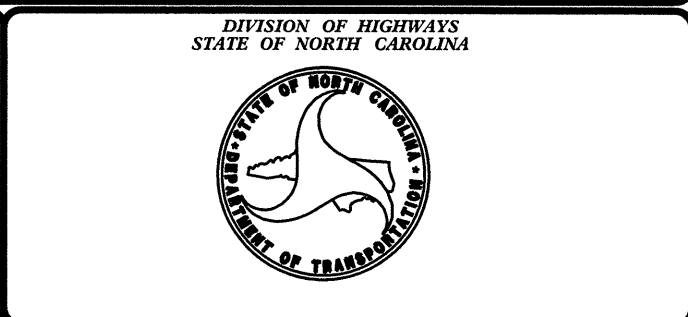
LETTING DATE: \_\_\_\_\_

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W. B. HOBBS, P.E.  
DIVISION PROJECT MANAGER

---

C. E. SLACHTA  
DIVISION PROPOSALS ENGINEER



PROJECT REFERENCE NO.	SHEET NO.
1C.066058, ETC.	8

Ⓒ	PROP. APPROX. 1.25" ACSC TYPE SF9.5A AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
⒰	EXISTING PAVEMENT

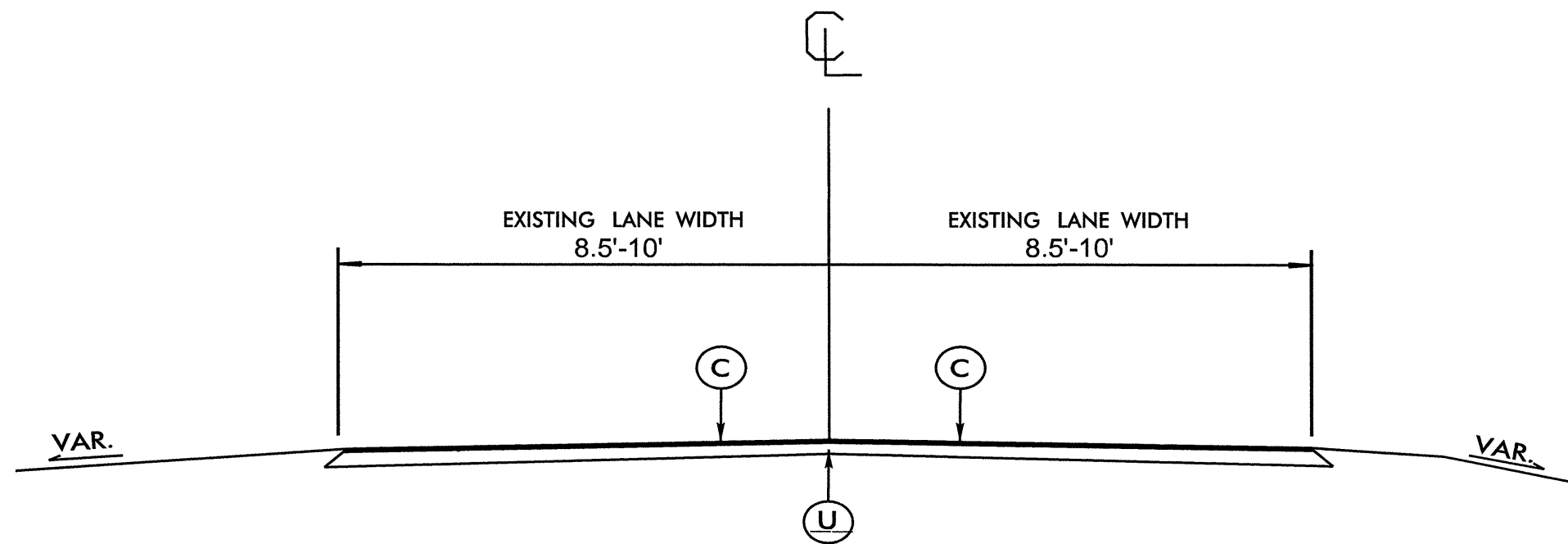
NOTES:

ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII,  
OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS, AND BRIDGE FLARES ARE  
INCLUDED IN THE TABLE OF QUANTITIES

PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE

SHOULDER RECONSTRUCTION TO BE PERFORMED BY OTHERS



**TYPICAL SECTION #1**

**USE WITH MAP 1, 2, 4-7, 9**



PROJECT REFERENCE NO.	SHEET NO.
1C.066058, ETC.	9

NOTES:

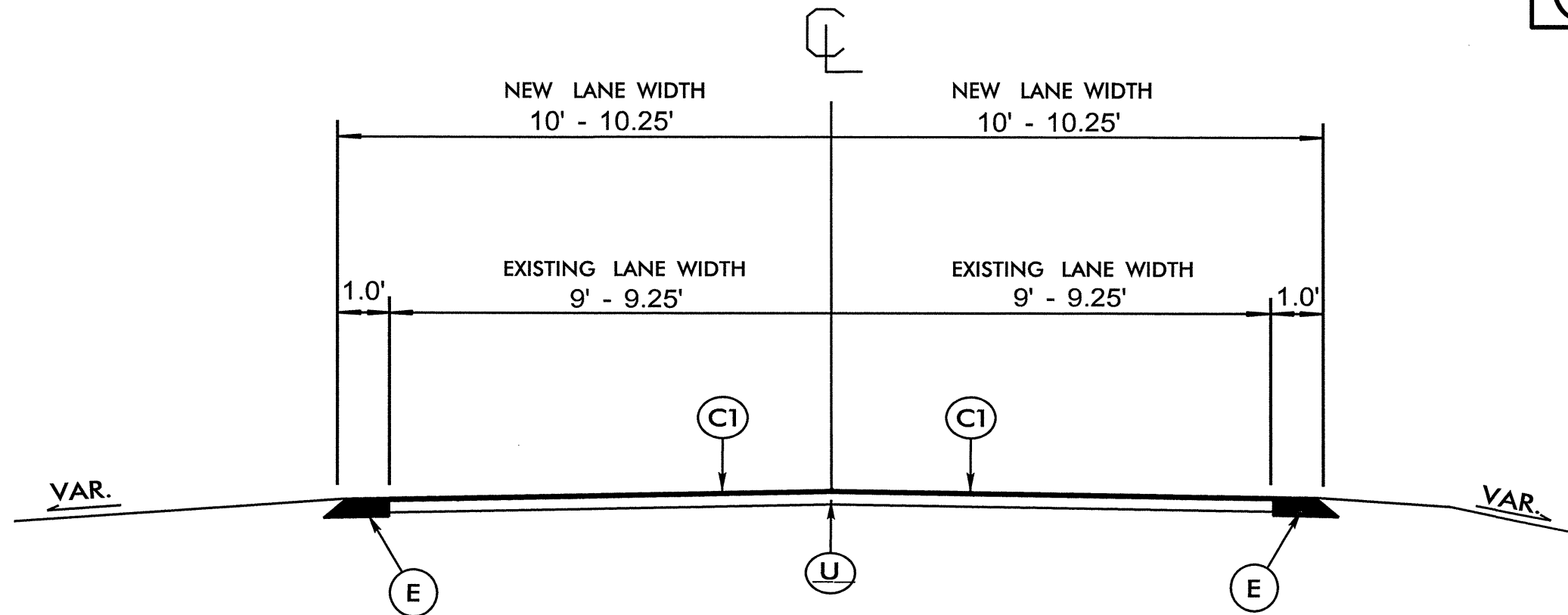
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS, AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES

PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE

SHOULDER RECONSTRUCTION TO BE PERFORMED BY OTHERS

<b>(C1)</b>	PROP. APPROX. 1.25" ACSC TYPE SF9.5A AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
<b>(E)</b>	PROP. APPROX. 5.5" ACBC, TYPE B25.0B AT AN AVERAGE RATE OF 627 LBS. PER SQ. YD.
<b>(U)</b>	EXISTING PAVEMENT



***TYPICAL SECTION #2***

**USE WITH MAPS 3, 8, 10, & 15**

NOTES:

ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII,  
OR AS DIRECTED BY THE ENGINEER

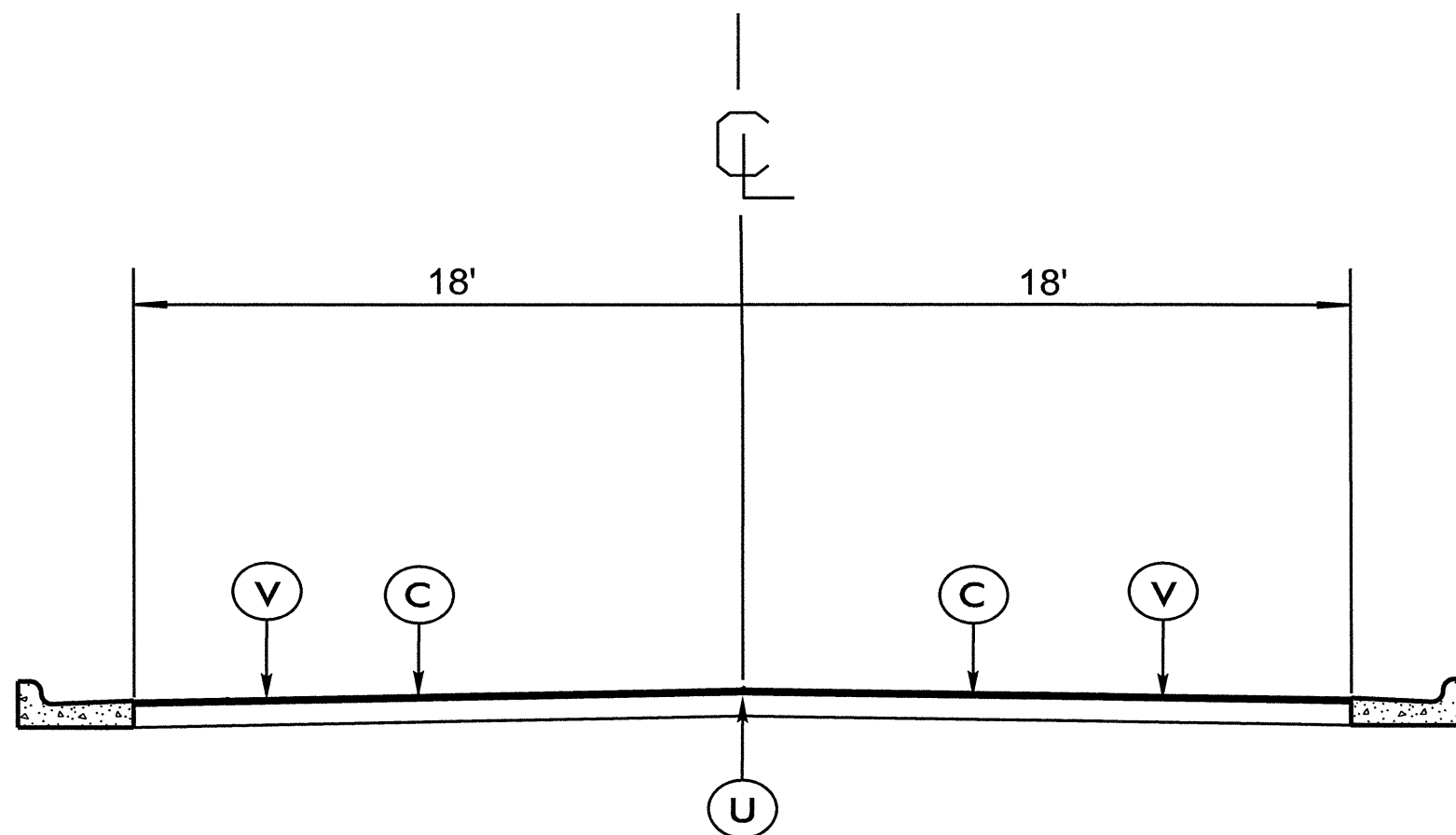
EDGES, PAVEMENT WIDENING, INTERSECTIONS, AND BRIDGE FLARES ARE  
INCLUDED IN THE TABLE OF QUANTITIES

LEVELING COURSE WILL BE APPLIED AT THE DISCRETION OF THE ENGINEER

LOCATIONS OF LEVELING COURSE TO BE DETERMINED AFTER MILLING

PROJECT REFERENCE NO.	SHEET NO.
1C.066058, ETC.	10

(C)	PROP. APPROX. 1.5" ACSC TYPE S9.5B AT AN AVERAGE RATE OF 168.0 LBS. PER SQ. YD.
(U)	EXISTING PAVEMENT
(V)	MILLING BITUMINOUS PAVEMENT 1.5" TO 3" DEPTH



***TYPICAL SECTION #3***

USE WITH MAP 11

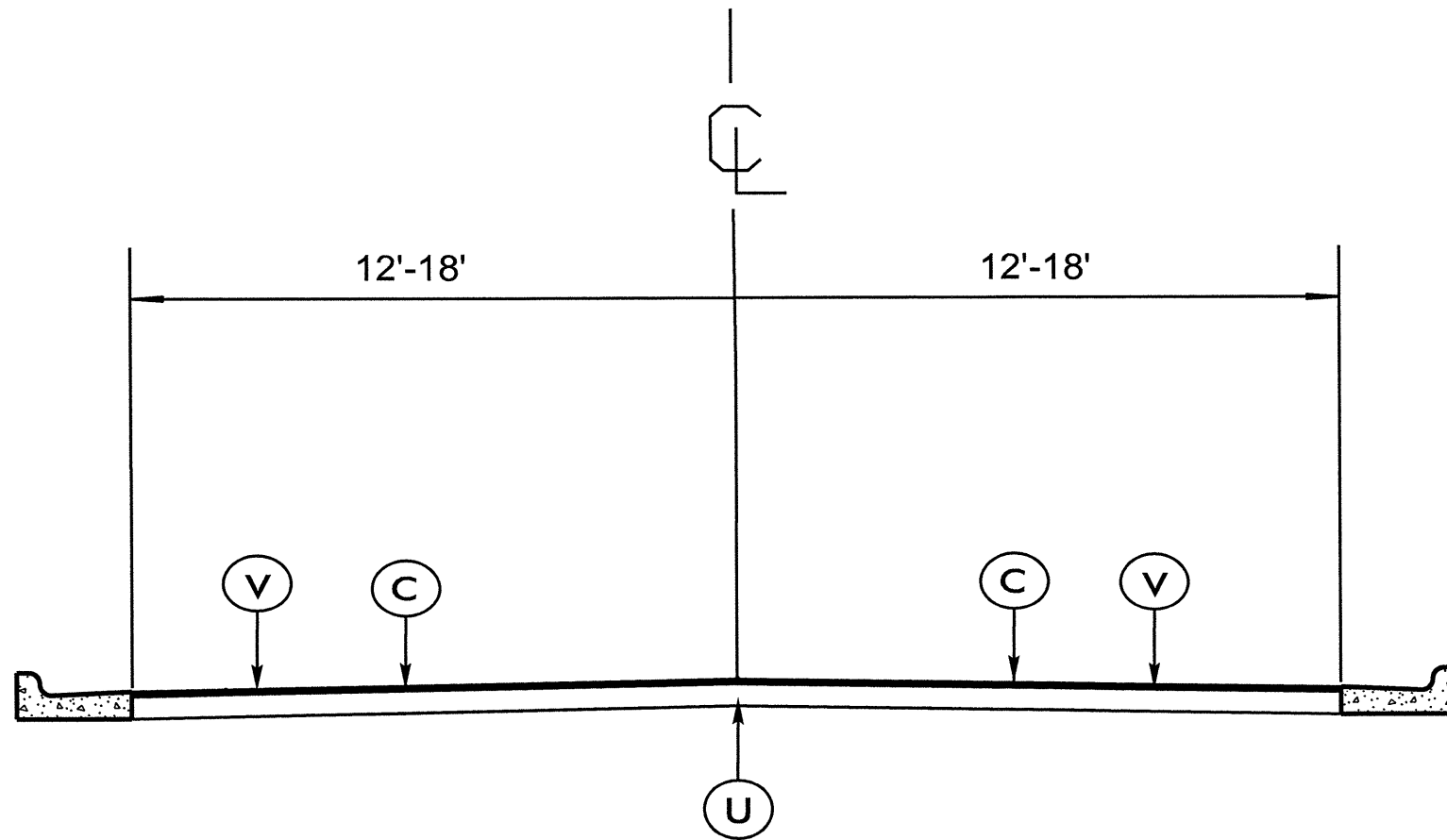
NOTES:

ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII,  
OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS, AND BRIDGE FLARES ARE  
INCLUDED IN THE TABLE OF QUANTITIES

PROJECT REFERENCE NO.	SHEET NO.
1C.066058, ETC.	11

(C)	PROP. APPROX. 1.5" ACSC TYPE S9.5B AT AN AVERAGE RATE OF 168.0 LBS. PER SQ. YD.
(U)	EXISTING PAVEMENT
(V)	MILLING BITUMINOUS PAVEMENT 1.5" TO 2.5" DEPTH



***TYPICAL SECTION #4***

USE WITH MAP 12

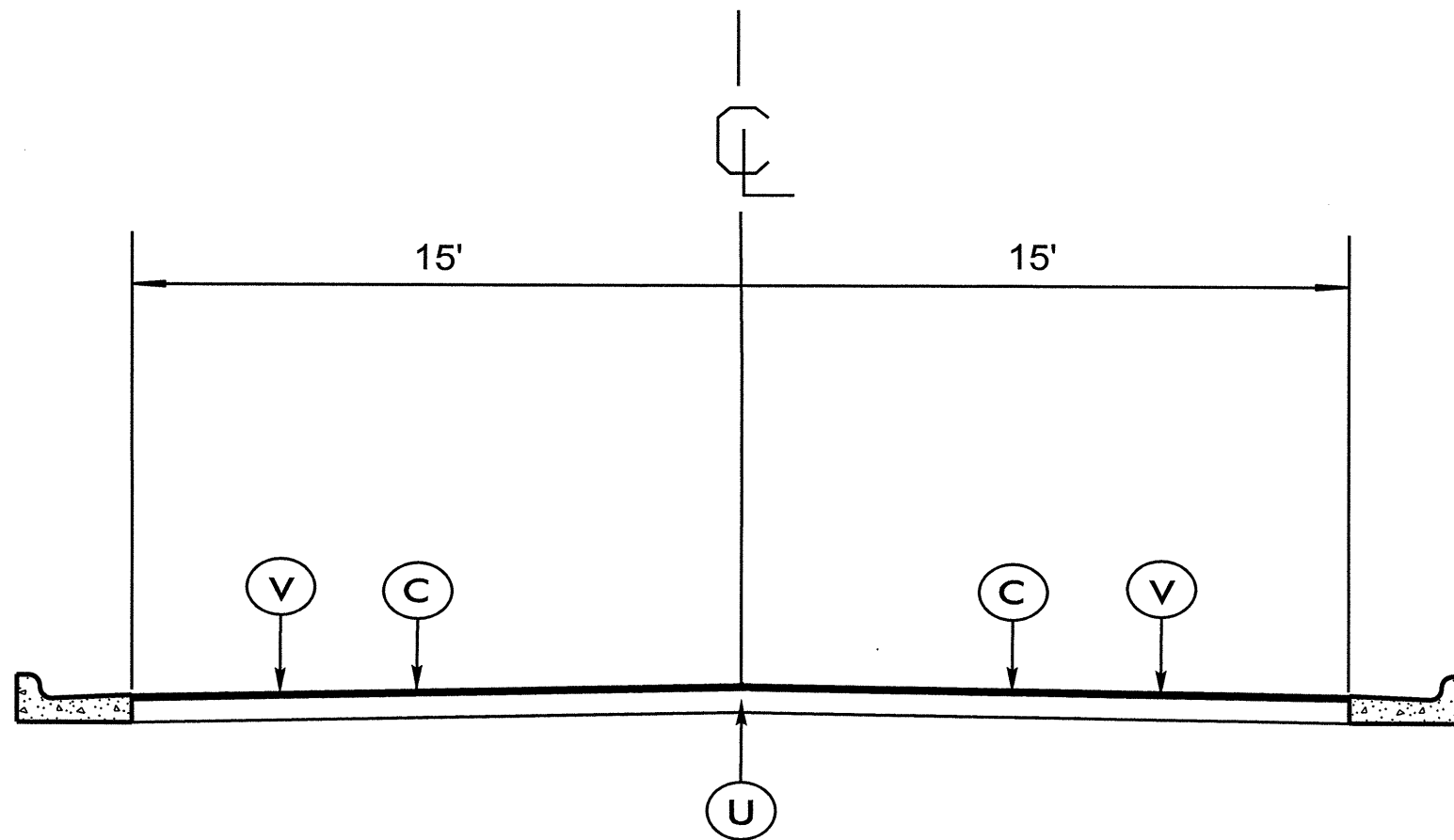
NOTES:

ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII,  
OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS, AND BRIDGE FLARES ARE  
INCLUDED IN THE TABLE OF QUANTITIES

PROJECT REFERENCE NO.	SHEET NO.
1C.066058, ETC.	12

(C)	PROP. APPROX. 1.5" ACSC TYPE S9.5B AT AN AVERAGE RATE OF 168.0 LBS. PER SQ. YD.
(U)	EXISTING PAVEMENT
(V)	MILLING BITUMINOUS PAVEMENT 2" DEPTH



***TYPICAL SECTION #5***

USE WITH MAP 13

PROJECT REFERENCE NO.	SHEET NO.
1C.066058, ETC.	13

NOTES:

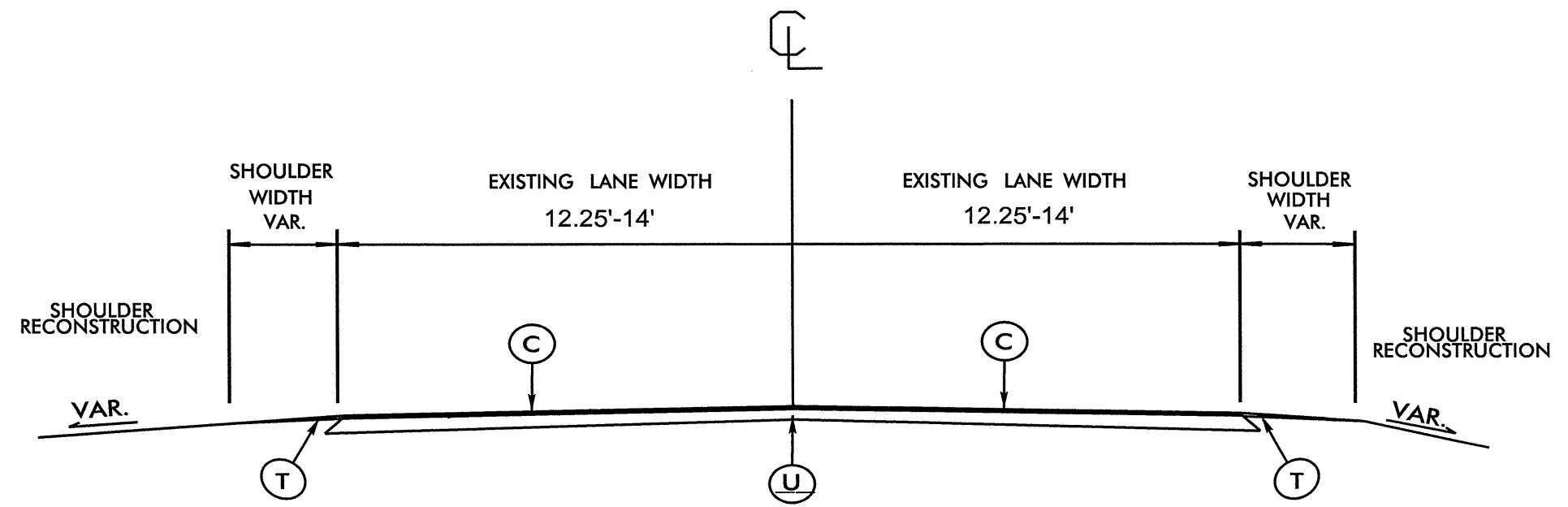
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII,  
OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS, AND BRIDGE FLARES ARE  
INCLUDED IN THE TABLE OF QUANTITIES

PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE

SHOULDER RECONSTRUCTION TO BE PERFORMED BY CONTRACTOR

(C)	PROP. APPROX. 1.5" ACSC TYPE S9.5B AT AN AVERAGE RATE OF 168.0 LBS. PER SQ. YD.
(U)	EXISTING PAVEMENT
(T)	EARTH MATERIAL



## TYPICAL SECTION #6

USE WITH MAP 14

PROJECT NO.	SHEET NO.	TOTAL NO.
1C.066058, ETC.	14	

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	ROAD NAME	DESCRIPTION	TYPICAL NO	LENGTH MI	WIDTH FT	MOBILIZATION LS	2" MILLING SY	1.5" TO 3" MILLING SY	1.5" TO 2.5" MILLING SY	INCIDENTAL MILLING SY	BASE COURSE, B25.0B TONS	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, SF9.5A TONS	PG 64-22 PLANT MIX TONS
1C.066058	Northampton	15	SR 1300	BIG JOHN'S STORE RD.	FROM SR 1324 TO SR 1328	2	4.31	20'	1					2,150		3,650	330
1CR.10661.17	Northampton	11	NC 35	SPRUCE ST.	FROM US 258 TO SCL WOODLAND	3	0.45	36'	*		9,500				900		80
1CR.10661.18	Northampton	14	NC 46	NC 46 HIGHWAY	FROM NC 48 TO SR 1212	6	7.44	24.5'-28'	*						9,850		591
1CR.10661.19	Northampton	12	NC 305	S. MAIN ST.	FROM NC 186 TO SCL SEABOARD	4	0.54	24'-36'	*			9,300			850		51
1CR.10661.20	Northampton	13	NC 305	CHURCH ST.	FROM US 158 TO NCL JACKSON	5	0.68	30'	*	11,960					1,050		63
1CR.20661.56	Northampton	1	SR 1341	DEBERRY'S MILL RD.	FROM NC 35 TO SR 1343	1	3.25	20'	*				150			2,875	187
1CR.20661.57	Northampton	2	SR 1363	TURKEY BRANCH RD.	FROM SR 1364 TO HERTFORD CO. LINE	1	0.47	19'	*							400	26
1CR.20661.58	Northampton	3	SR 1300	BIG JOHN'S STORE RD.	SR 1328 TO NC 186	2	1.19	20'	*					600		1,100	98
1CR.20661.59	Northampton	4	SR 1384	PARK ST.	FROM NC 305 TO SR 1385	1	0.41	19'	*							350	23
1CR.20661.60	Northampton	5	SR 1385	CALVERT ST.	FROM SR 1384 TO R/R CROSSING	1	0.19	17'	*							160	11
1CR.20661.61	Northampton	6	SR 1324	N. MAIN ST.	FROM NC 186 TO NCL SEABOARD	1	0.43	20'	*							420	28
1CR.20661.62	Northampton	10	SR 1355	BOONE'S BRIDGE RD.	FROM SR 1351 TO HERTFORD CO. LINE	2	1.20	20.5'	*				1,050	580		1,100	94
1CR.20661.63	Northampton	8	SR 1263	HONEYSUCKLE DR.	FROM NC/VA ST LINE TO SR 1264	2	0.58	20.5'	*					290		550	49
1CR.20661.64	Northampton	9	SR 1264	PEARSON DR.	FROM SR 1263 TO SR 1270	1	0.27	18.5'	*							285	19
1CR.20661.65	Northampton	7	SR 1270	MONCURE RD.	FROM SR 1214 TO SR 1264	1	1.27	20'	*							1,200	78
<b>GRAND TOTAL</b>							<b>22.68</b>		<b>1</b>	<b>11,960</b>	<b>9,500</b>	<b>9,300</b>	<b>1,200</b>	<b>3,620</b>	<b>12,650</b>	<b>12,090</b>	<b>1,728</b>

PROJECT NO	COUNTY	MAP NO	ROUTE	ROAD NAME	DESCRIPTION	TYPICAL NO	LENGTH MI	WIDTH FT	MOBILIZATION LS	AC PLANT MIX (REPAIR) TONS	LEVELING COURSE, S4.75A TONS	2'-6" CURB & GUTTER LF	4" CONCRETE SIDEWALK SY	RETROFIT EXISTING WHEELCHAIR RAMPS EA	WHEELCHAIR RAMPS EA	ADJ. OF MANHOLES EA	ADJ. OF METER OR VALVE BOX EA
1C.066058	Northampton	15	SR 1300	BIG JOHN'S STORE RD.	FROM SR 1324 TO SR 1328	2	4.31	18'	0.19								
1CR.10661.17	Northampton	11	NC 35	SPRUCE ST.	FROM US 258 TO SCL WOODLAND	3	0.45	36'	0.02	150	360	100				18	2
1CR.10661.18	Northampton	14	NC 46	NC 46 HIGHWAY	FROM NC 48 TO SR 1212	6	7.44	24.5'-28'	0.33							2	8
1CR.10661.19	Northampton	12	NC 305	S. MAIN ST.	FROM NC 186 TO SCL SEABOARD	4	0.54	24'-36'	0.02	150			30	3		8	5
1CR.10661.20	Northampton	13	NC 305	CHURCH ST.	FROM US 158 TO NCL JACKSON	5	0.68	30'	0.03	100		200	24	1	10	9	14
1CR.20661.56	Northampton	1	SR 1341	DEBERRY'S MILL RD.	FROM NC 35 TO SR 1343	1	3.25	20'	0.14								
1CR.20661.57	Northampton	2	SR 1363	TURKEY BRANCH RD.	FROM SR 1364 TO HERTFORD CO. LINE	1	0.47	19'	0.02								
1CR.20661.58	Northampton	3	SR 1300	BIG JOHN'S STORE RD.	SR 1328 TO NC 186	2	1.19	18'	0.05								
1CR.20661.59	Northampton	4	SR 1384	PARK ST.	FROM NC 305 TO SR 1385	1	0.41	19'	0.02							4	6
1CR.20661.60	Northampton	5	SR 1385	CALVERT ST.	FROM SR 1384 TO R/R CROSSING	1	0.19	17'	0.01								
1CR.20661.61	Northampton	6	SR 1324	N. MAIN ST.	FROM NC 186 TO NCL SEABOARD	1	0.43	20'	0.02							3	
1CR.20661.62	Northampton	10	SR 1355	BOONE'S BRIDGE RD.	FROM SR 1351 TO HERTFORD CO. LINE	2	1.20	18.5'	0.05								
1CR.20661.63	Northampton	8	SR 1263	HONEYSUCKLE DR.	FROM NC/VA ST LINE TO SR 1264	2	0.58	18.5'	0.03								
1CR.20661.64	Northampton	9	SR 1264	PEARSON DR.	FROM SR 1263 TO SR 1270	1	0.27	18.5'	0.01								
1CR.20661.65	Northampton	7	SR 1270	MONCURE RD.	FROM SR 1214 TO SR 1264	1	1.27	20'	0.06								
<b>GRAND TOTAL</b>							<b>22.68</b>		<b>1</b>	<b>400</b>	<b>360</b>	<b>300</b>	<b>54</b>	<b>4</b>	<b>10</b>	<b>44</b>	<b>35</b>

## SHOULDER RECONSTRUCTION AND EROSION CONTROL QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	ROAD NAME	DESCRIPTION	BORROW CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	TEMPORARY SILT FENCE LF	MATTING FOR EROSION CONTROL SY	COIR FIBER WATTLE LF	POLYACRYLAMIDE (PAM) LB	SEEDING & MULCHING AC
1CR.10661.18	Northampton	14	NC 46	NC 46 HIGHWAY	FROM NC 48 TO SR 1212	300	480	14.9	500	64	160	14	10.9
<b>GRAND TOTAL</b>						<b>300</b>	<b>480</b>	<b>14.9</b>	<b>500</b>	<b>64</b>	<b>160</b>	<b>14</b>	<b>10.9</b>

PROJECT NO.	SHEET NO.	TOTAL NO.
1C.066058, ETC.	15	

## THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	ROAD NAME	DESCRIPTION	FINAL SURFACE TESTING REQUIRED	4589000000-N	4685000000-E	4686000000-E			4697000000-E	4710000000-E	4721000000-E	4725000000-E	4810000000-E		4900000000-N	
							GENERIC TRAFFIC CONTROL ITEM (LUMP SUM) LS	4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	4" X 120 M WHITE THERMO LF	8" X 120 M YELLOW THERMO LF	24" X 120 M WHITE THERMO LF	THERMO CHARACTER S - 120 M EA	THERMO LT ARROW 90 M EA	4" WHITE PAINT LF	4" YELLOW PAINT LF	CRYSTAL & RED MARKERS EA	YELLOW & YELLOW MARKERS EA	
1C.066058	Northampton	15	SR 1300	BIG JOHN'S STORE RD.	FROM SR 1324 TO SR 1328	NO	1.00									45,800	32,100		
1CR.10661.17	Northampton	11	NC 35	SPRUCE ST.	FROM US 258 TO SCL WOODLAND	NO	*										5,000		
1CR.10661.18	Northampton	14	NC 46	NC 46 HIGHWAY	FROM NC 48 TO SR 1212	NO	*	81,900	51,050	300	400	100	12	2			50,200	12	536
1CR.10661.19	Northampton	12	NC 305	S. MAIN ST.	FROM NC 186 TO SCL SEABOARD	NO	*										5,800		
1CR.10661.20	Northampton	13	NC 305	CHURCH ST.	FROM US 158 TO NCL JACKSON	NO	*										7,300		
1CR.20661.56	Northampton	1	SR 1341	DEBERRY'S MILL RD.	FROM NC 35 TO SR 1343	NO	*									34,800	27,100		
1CR.20661.57	Northampton	2	SR 1363	TURKEY BRANCH RD.	FROM SR 1364 TO HERTFORD CO. LINE	NO	*									5,000	4,000		
1CR.20661.58	Northampton	3	SR 1300	BIG JOHN'S STORE RD.	SR 1328 TO NC 186	NO	*									12,700	10,200		
1CR.20661.59	Northampton	4	SR 1384	PARK ST.	FROM NC 305 TO SR 1385	NO	*									4,400	4,400		
1CR.20661.60	Northampton	5	SR 1385	CALVERT ST.	FROM SR 1384 TO R/R CROSSING	NO	*									2,050	2,050		
1CR.20661.61	Northampton	6	SR 1324	N. MAIN ST.	FROM NC 186 TO NCL SEABOARD	NO	*									4,600	4,600		
1CR.20661.62	Northampton	10	SR 1355	BOONE'S BRIDGE RD.	FROM SR 1351 TO HERTFORD CO. LINE	NO	*									12,912	7,920		
1CR.20661.63	Northampton	8	SR 1263	HONEYSUCKLE DR.	FROM NC/VA ST LINE TO SR 1264	NO	*									6,200	6,200		
1CR.20661.64	Northampton	9	SR 1264	PEARSON DR.	FROM SR 1263 TO SR 1270	NO	*									3,300	3,300		
1CR.20661.65	Northampton	7	SR 1270	MONCURE RD.	FROM SR 1214 TO SR 1264	NO	*									13,665	13,665		
<b>GRAND TOTAL</b>																<b>145,427</b>	<b>183,835</b>	<b>12</b>	<b>536</b>
							<b>1</b>	<b>81,900</b>	<b>51,050</b>	<b>300</b>	<b>400</b>	<b>100</b>	<b>12</b>	<b>2</b>	<b>329,262</b>	<b>548</b>			

PROJECT REFERENCE NO. <b>K.066058, ETC.</b>	SHEET NO. <b>16</b>
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

# WATTLE WITH POLYACRYLAMIDE (PAM) DETAIL

**NOTES:**

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

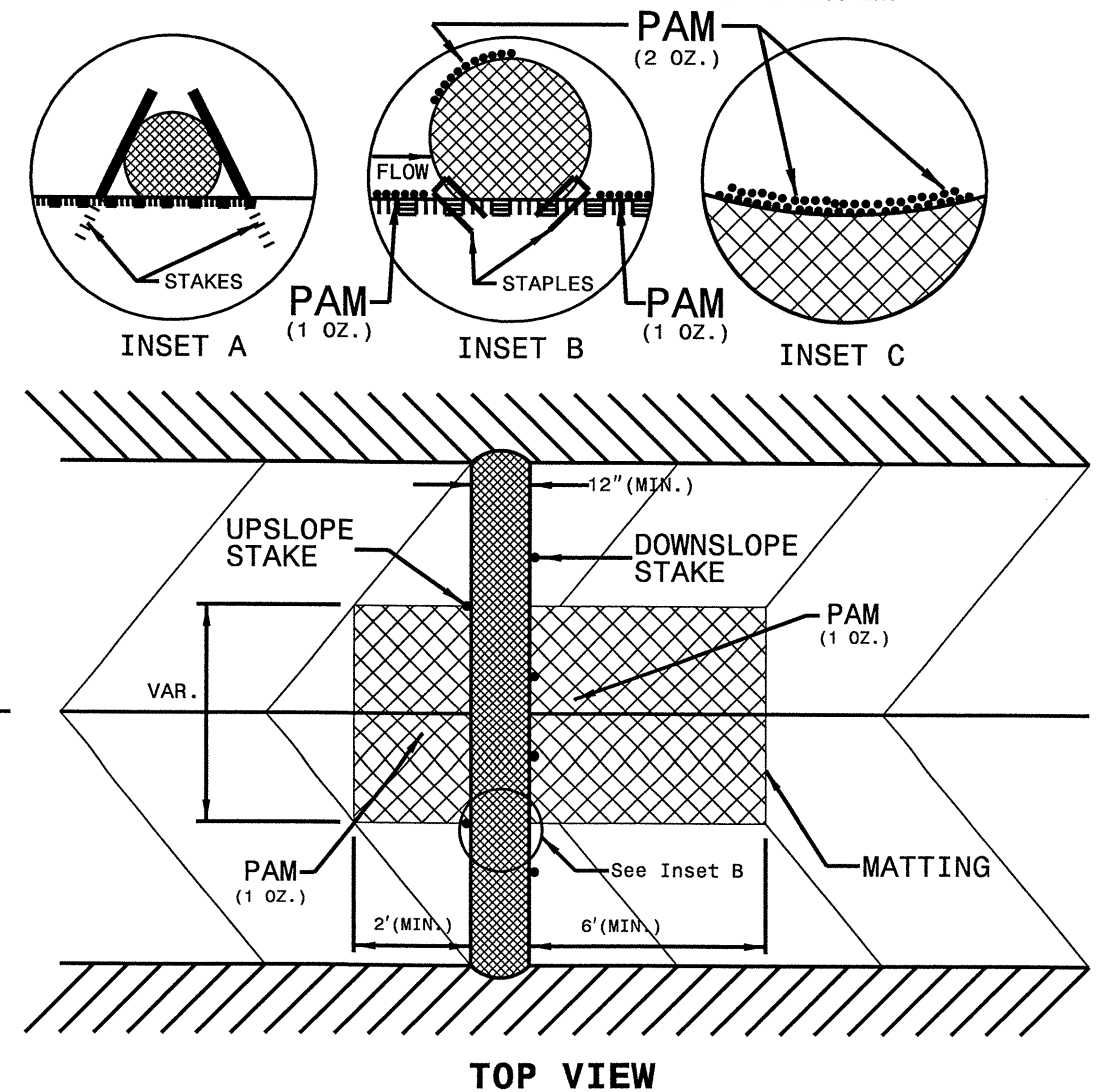
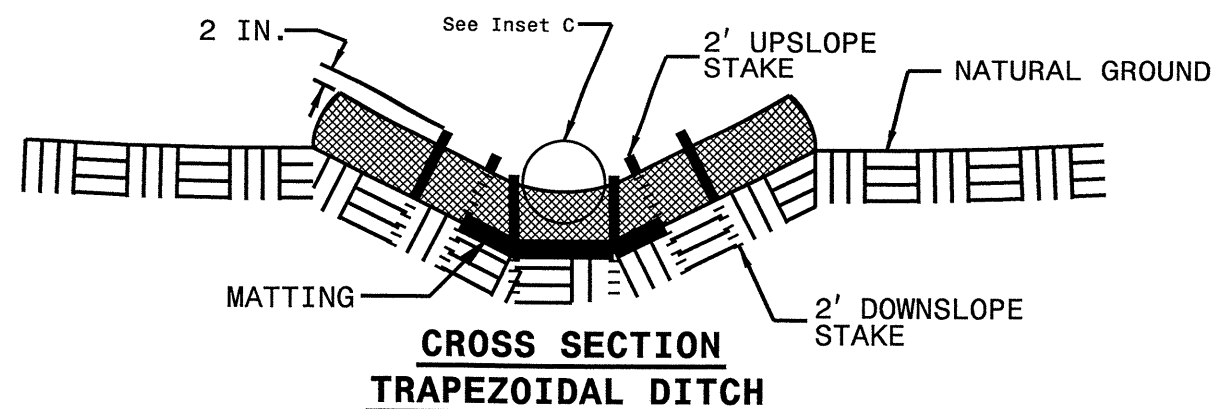
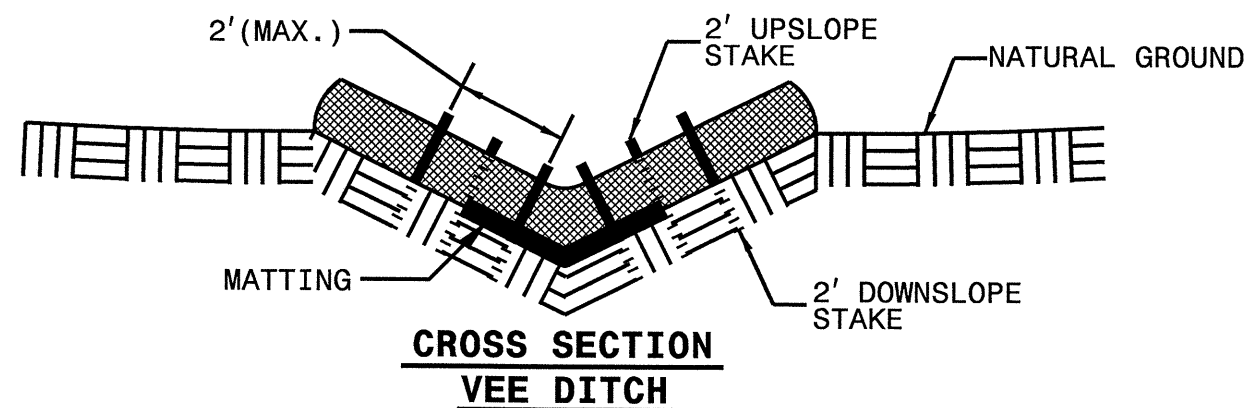
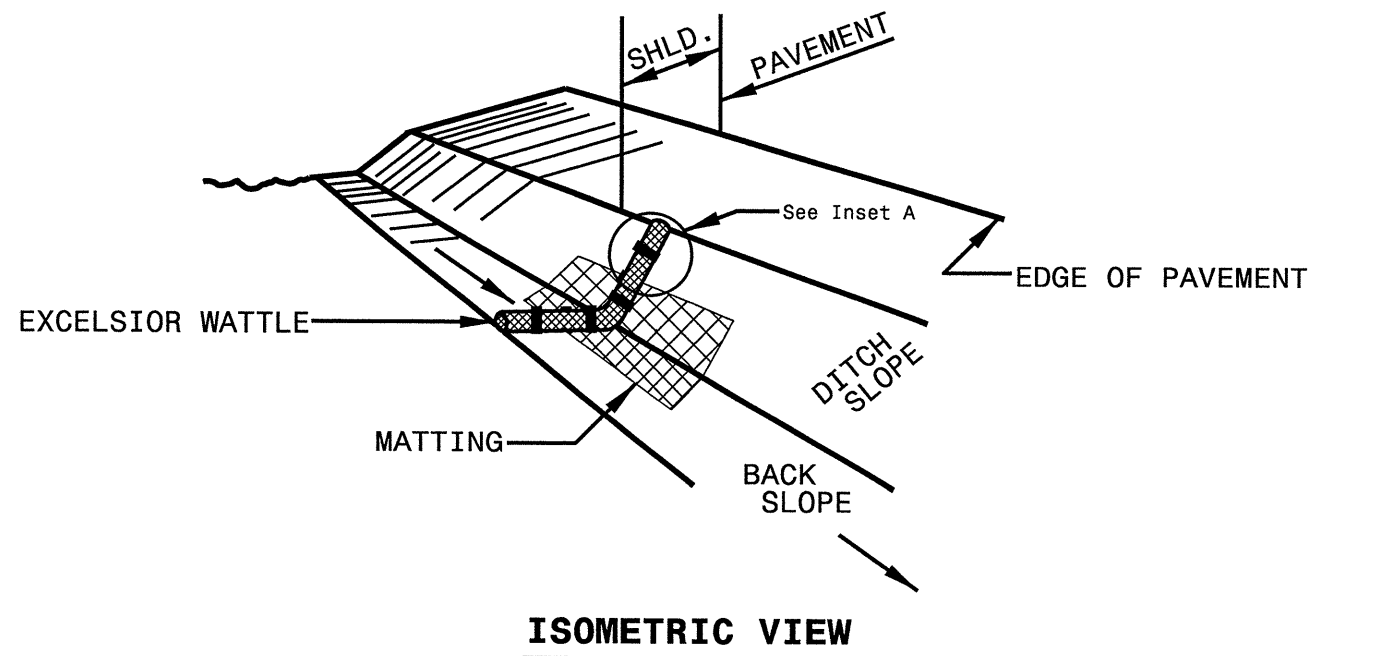
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH WATTLE.

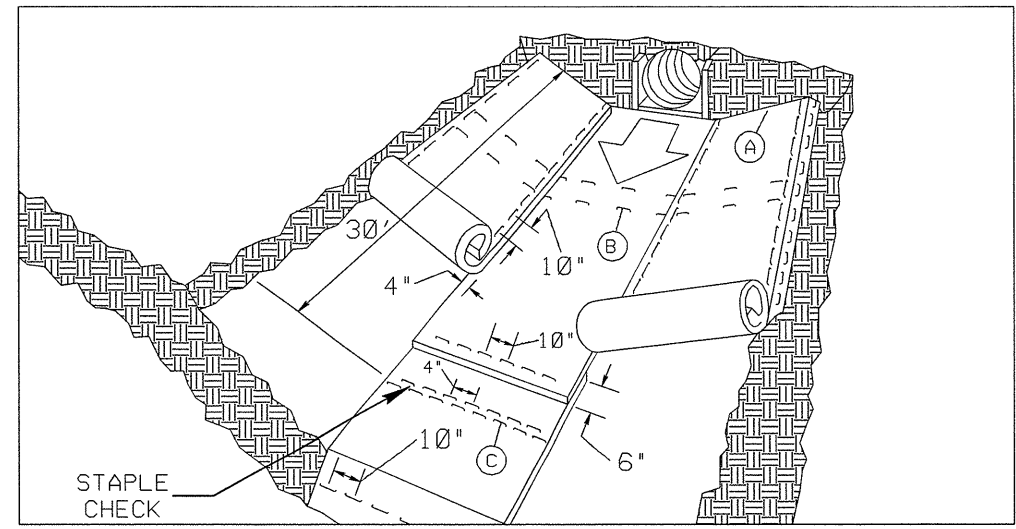
INITIALLY APPLY 2 OUNCES OF ANIONIC OR NEUTRALLY CHARGED PAM OVER WATTLE WHERE WATER WILL FLOW AND 1 OUNCE OF PAM ON MATTING ON EACH SIDE OF WATTLE. REAPPLY PAM AFTER EVERY RAINFALL EVENT THAT IS EQUAL TO OR EXCEEDS 0.50 IN.





PROJECT REFERENCE NO. <i>1.06058, etc</i>	SHEET NO. <i>17</i>
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

# MATTING INSTALLATION DETAIL



**MATTING IN DITCHES**

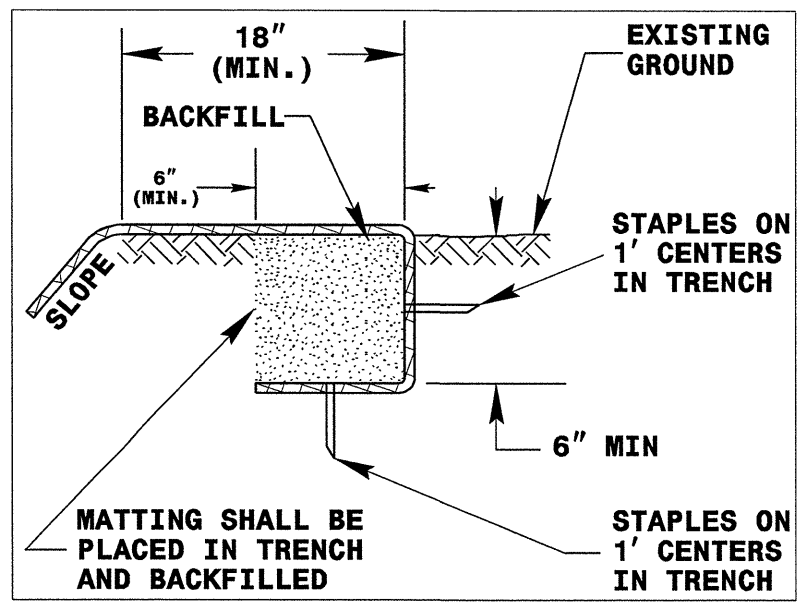
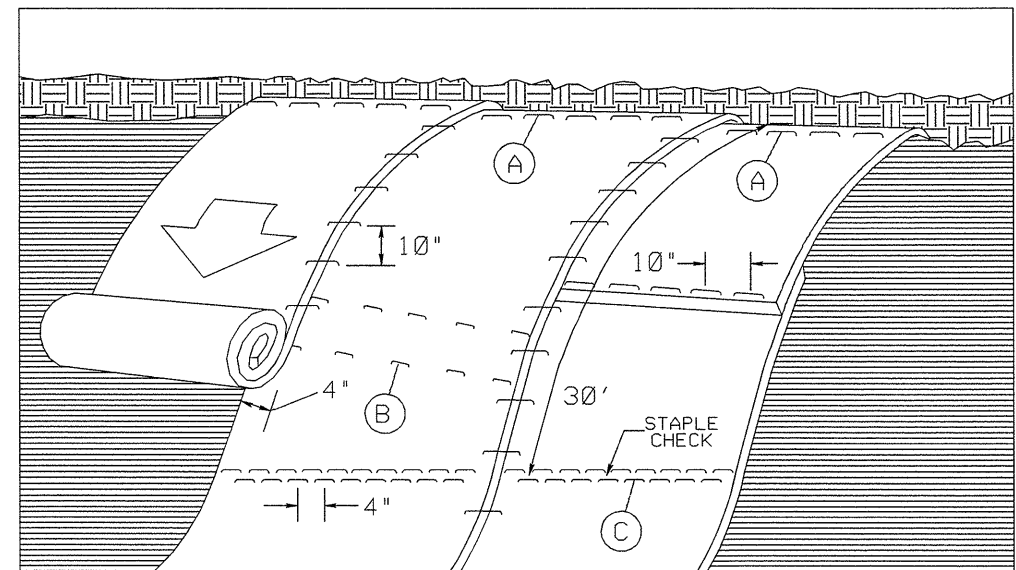


DIAGRAM (A)



**MATTING ON SLOPES**

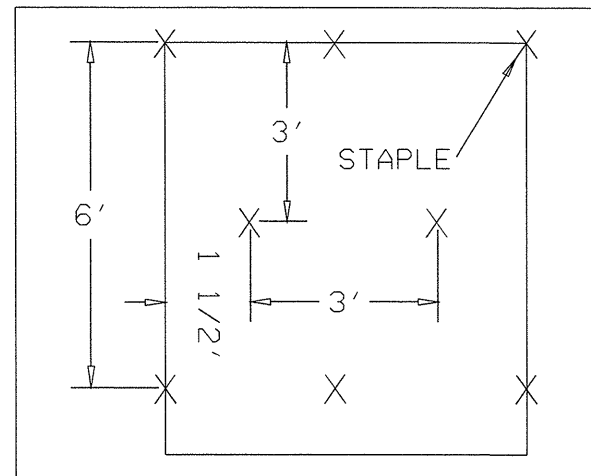


DIAGRAM (B)

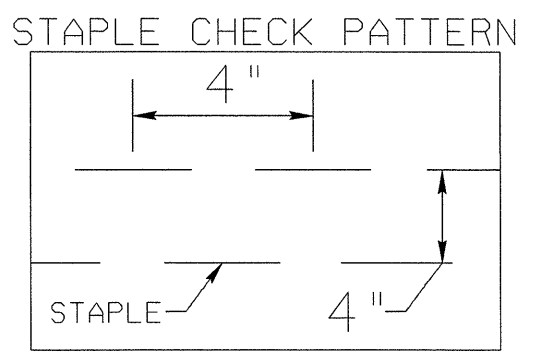


DIAGRAM (C)

**NOTES:**  
 THIS DETAIL APPLIES TO STRAW, EXCELSIOR, AND PERMANENT SOIL REINFORCEMENT MAT (PSRM) INSTALLATION.  
 STAPLES SHALL BE NO. 11 GAUGE STEEL WIRE FORMED INTO A "U" SHAPE WITH A MINIMUM THROAT WIDTH OF 1 INCH AND NOT LESS THAN 6 INCHES IN LENGTH.

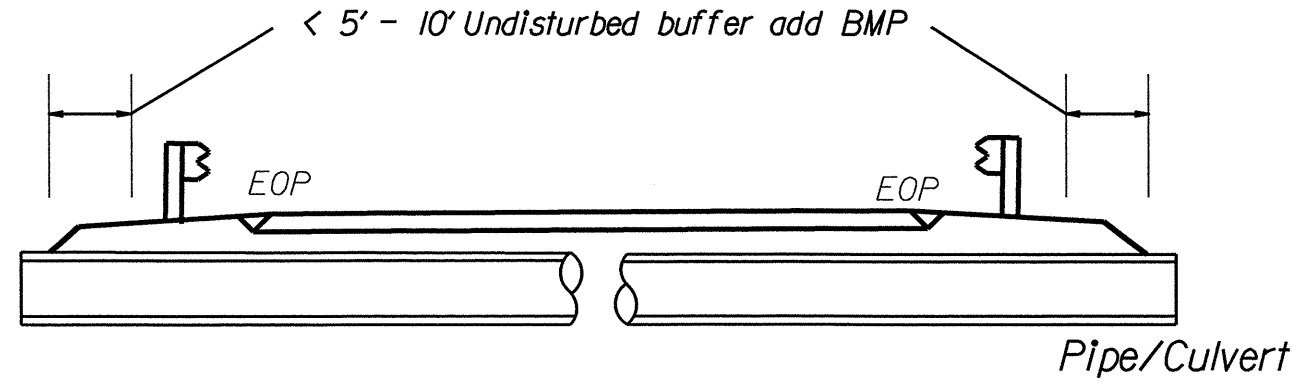
NOT TO SCALE

PROJECT REFERENCE NO. <b>KD66058, etc.</b>	SHEET NO. <b>17</b>
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

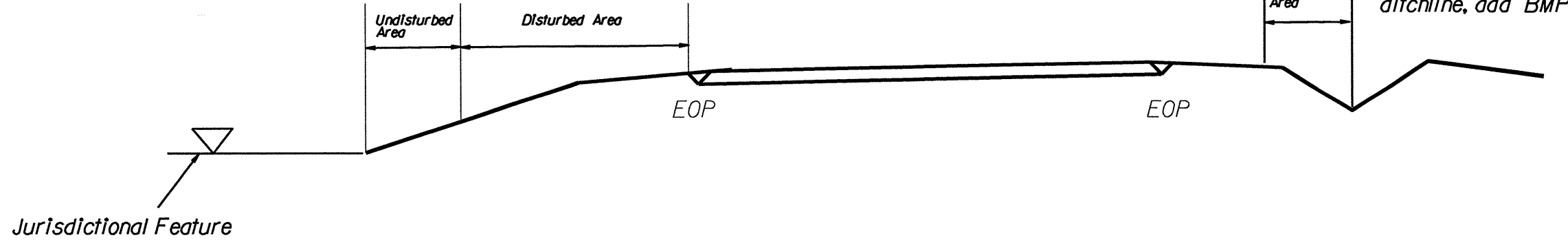
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

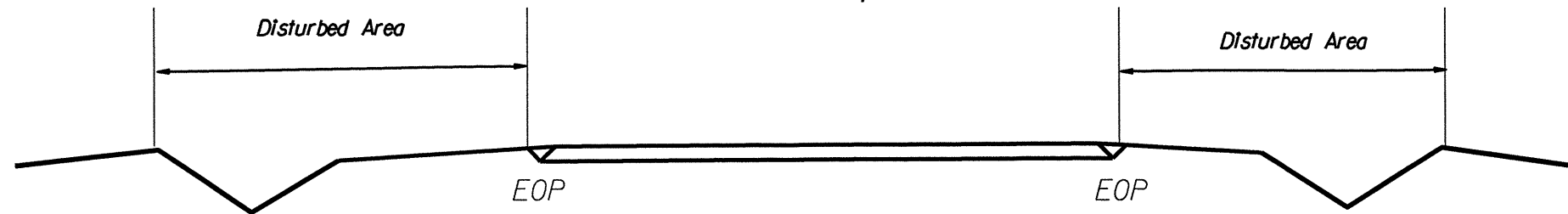
# EROSION CONTROL DETAIL



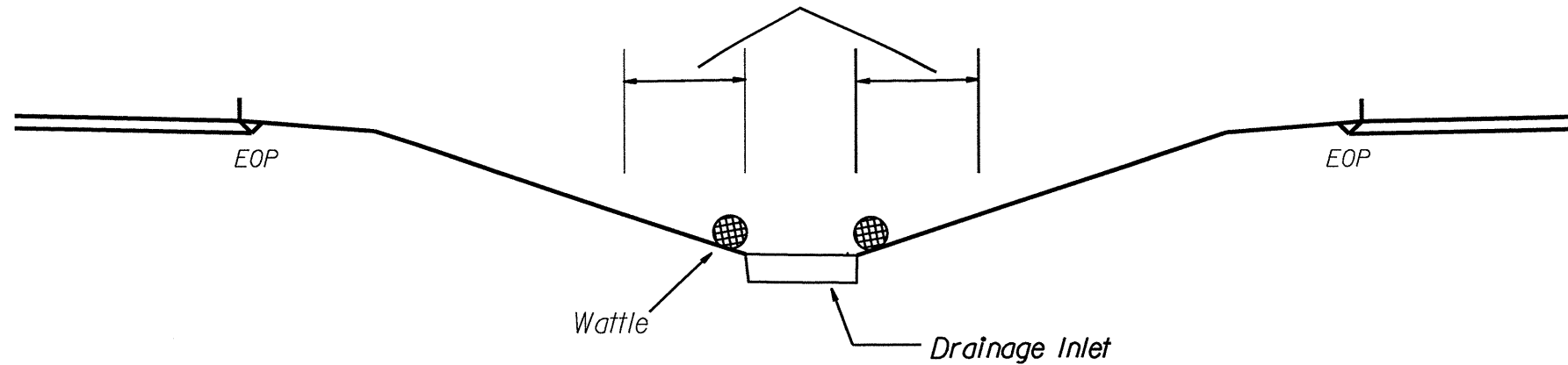
< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP



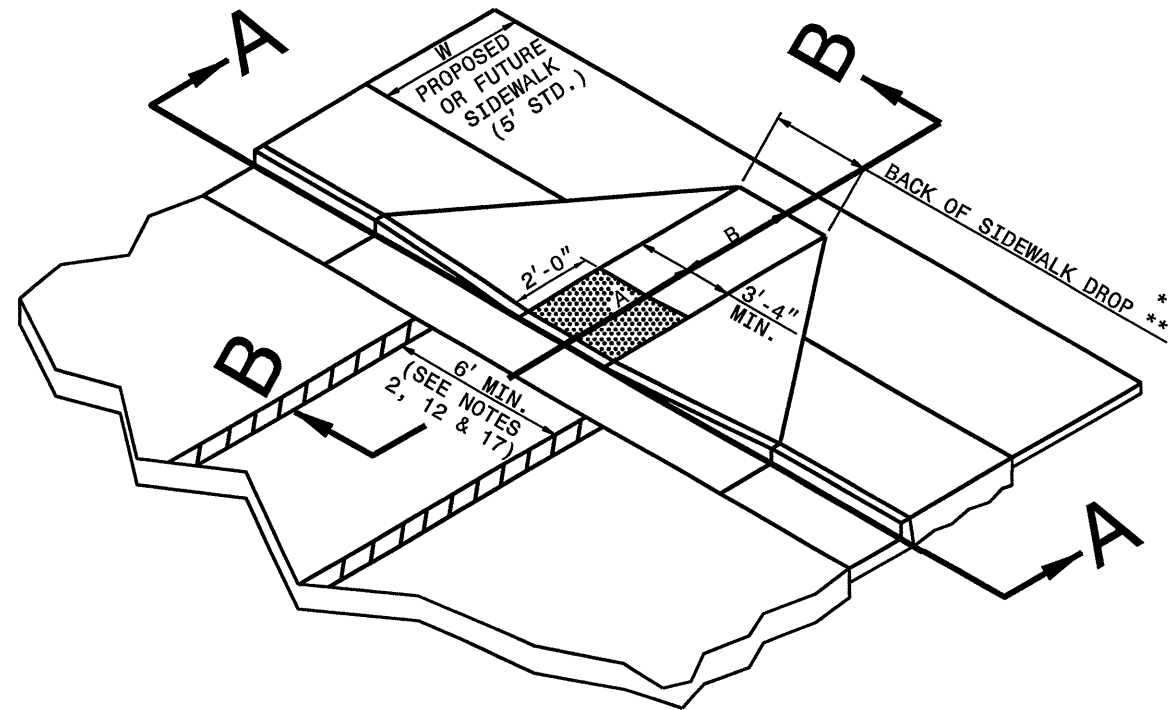
Use BMP's if shoulders and/or front slopes and/or ditchline and/or backslopes are disturbed



< 5' - 10' Undisturbed buffer from inlet, add wattle



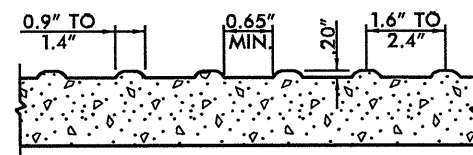
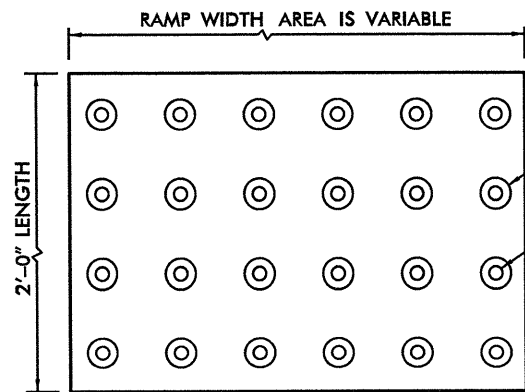
NOT TO SCALE



ISOMETRIC VIEW

NOTES:

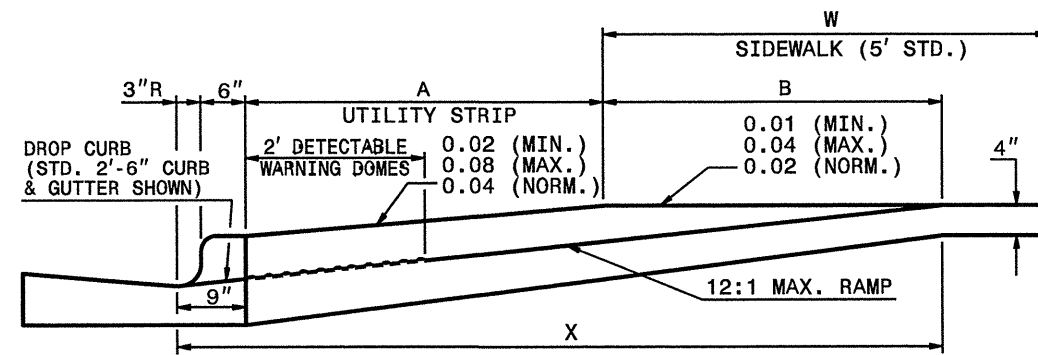
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



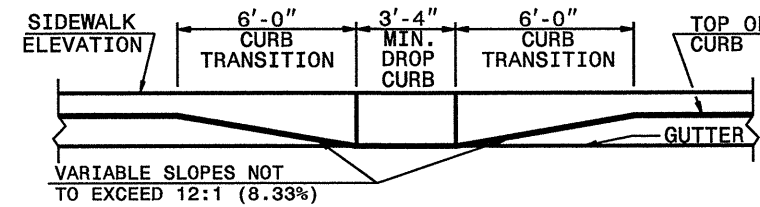
DETECTABLE WARNING DOMES

W	A	W+A+9"	X	B
5'	0.0'	5.8'	5.8'	5.0'*
6'	0.0'	6.8'	6.8'	6.0'**
7'	0.0'	7.8'	7.3'	6.5'**
8'	0.0'	8.8'	7.3'	6.5'**
5'	2.0'	7.8'	7.8'	5.0'
5'	2.5'	8.3'	8.1'	4.8'
5'	3.0'	8.8'	8.3'	4.4'
5'	3.5'	9.3'	8.4'	4.1'
5'	4.0'	9.8'	8.6'	3.8'
5'	4.5'	10.3'	8.7'	3.4'
5'	5.0'	10.8'	8.9'	3.1'

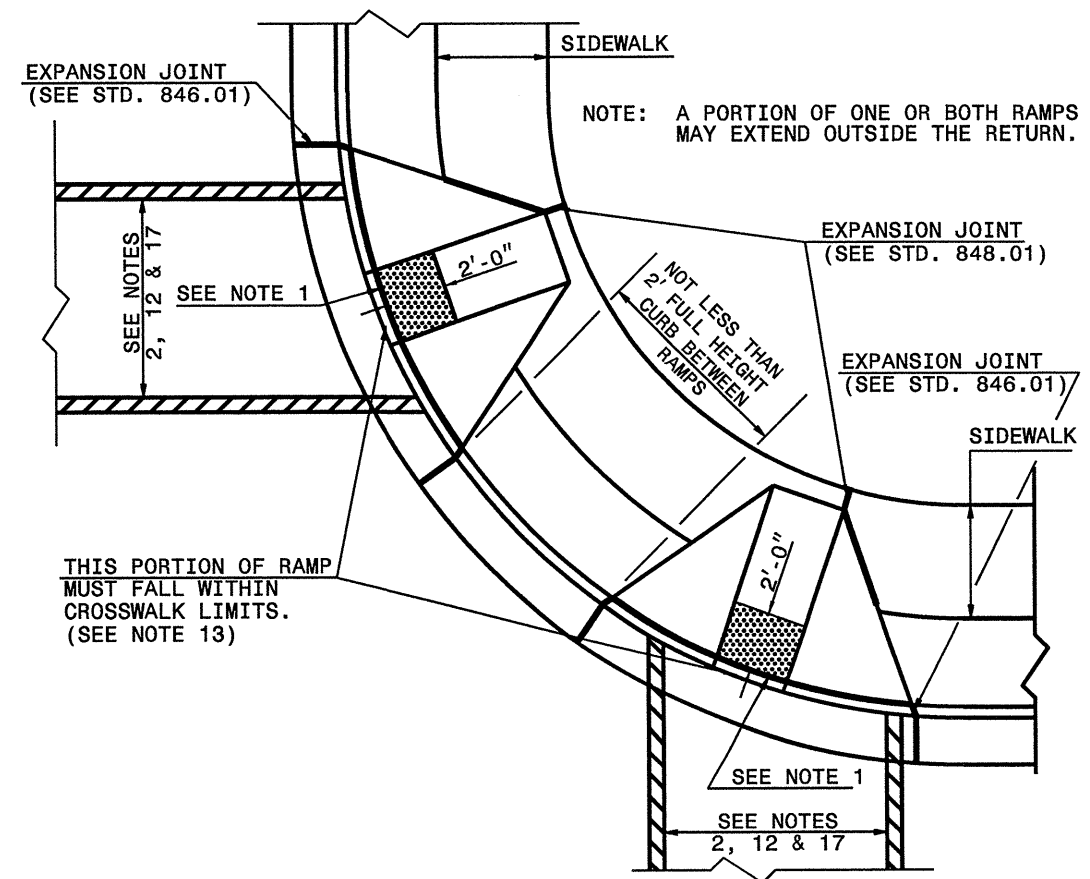
$B = X - (A + 9")$   
 B = DISTANCE FROM FRONT EDGE OF SIDEWALK TO BACK POINT OF 12:1 (8.33%) SLOPE.  
 \* BACK OF SIDEWALK DROP REQUIRED FOR ALL SIDEWALK SLOPES.  
 \*\* BACK OF SIDEWALK DROP REQUIRED FOR SIDEWALK SLOPES 0.04.



SECTION B-B

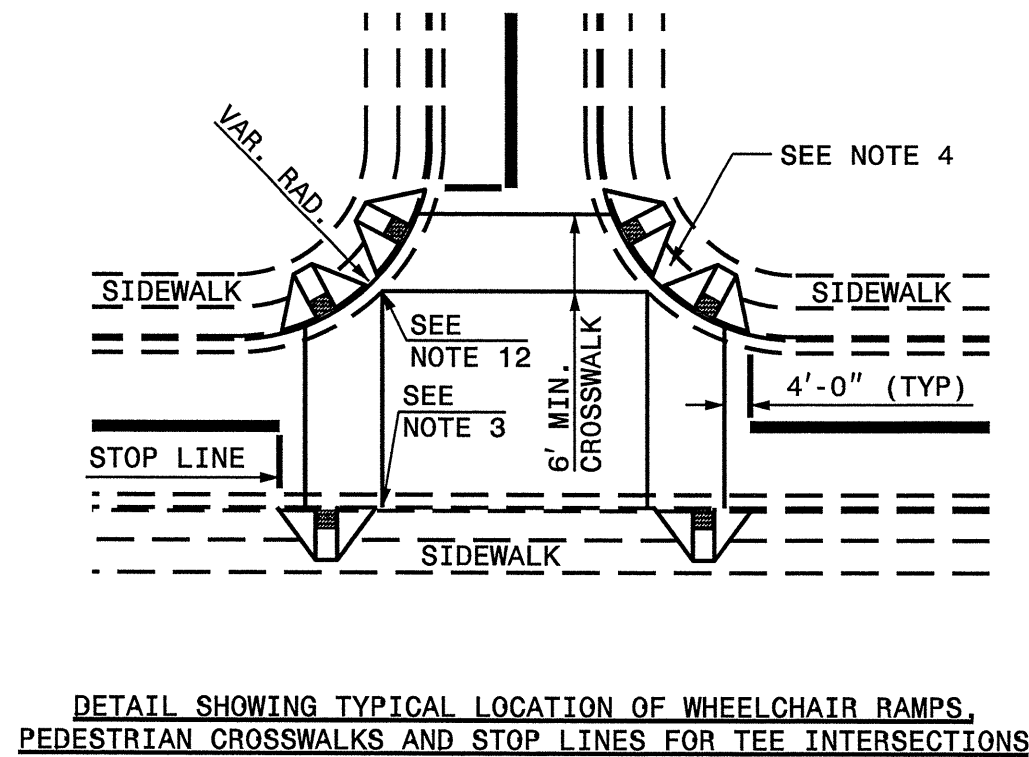


SECTION A-A

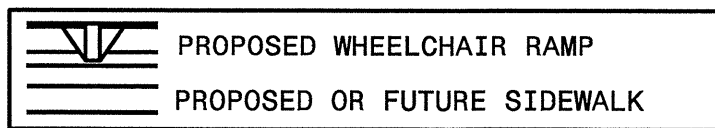
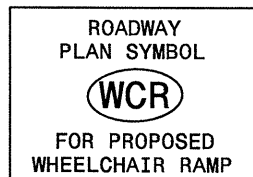


PLAN VIEW

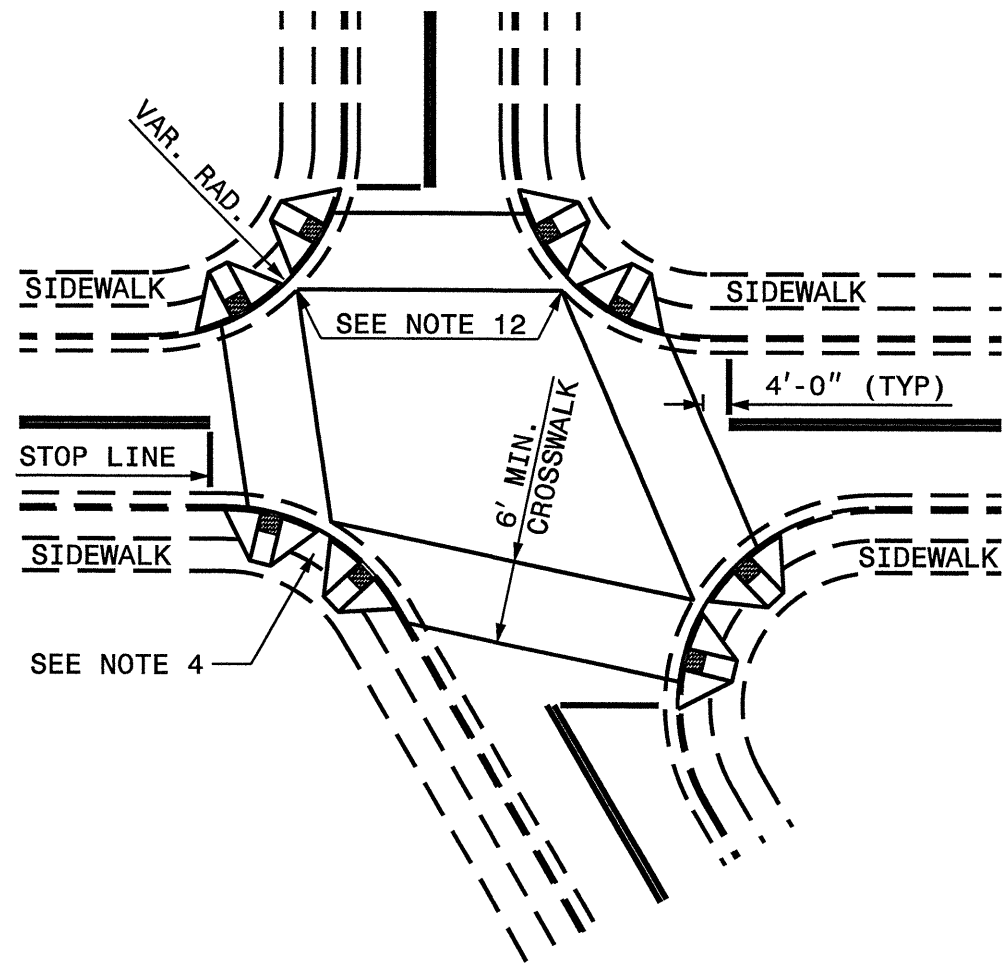
DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



ALLOWABLE LOCATIONS  
.....  
DUAL RAMP RADII.....ANY



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

NOTES:

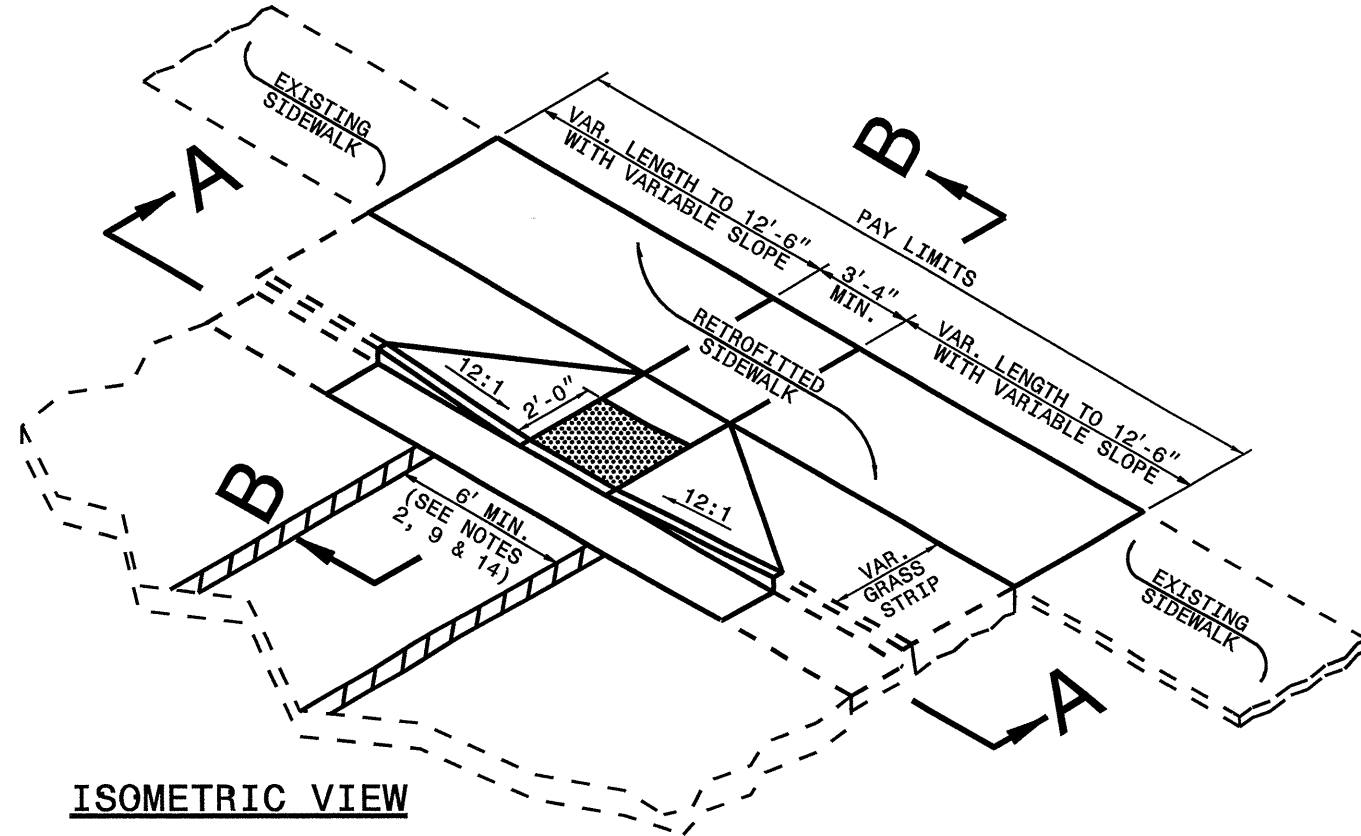
1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
2. CROSSWALK WIDTHS AND CONFIGURATION VARY BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
3. NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.

IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.

THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES. COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.

4. PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
5. DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
6. CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS.
7. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADII, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 17)
10. COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
11. CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
14. PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

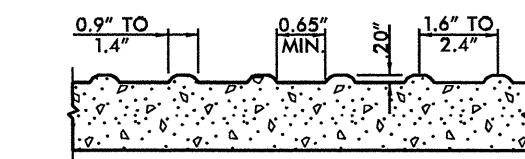
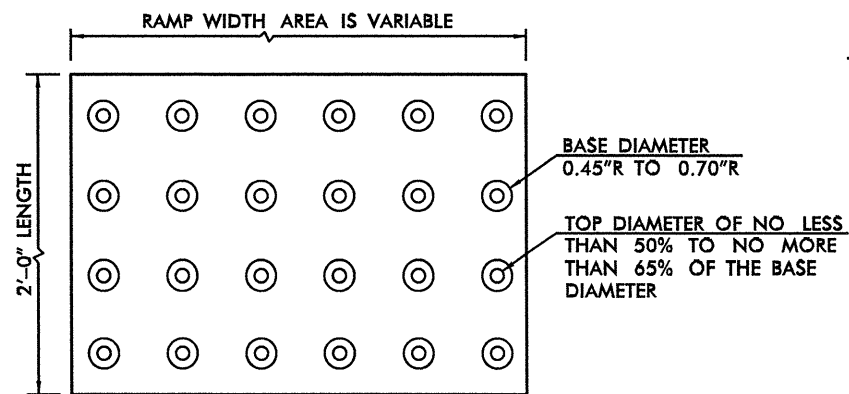
**WHEELCHAIR RAMP AND EXISTING SIDEWALK WITH GRASS STRIP**



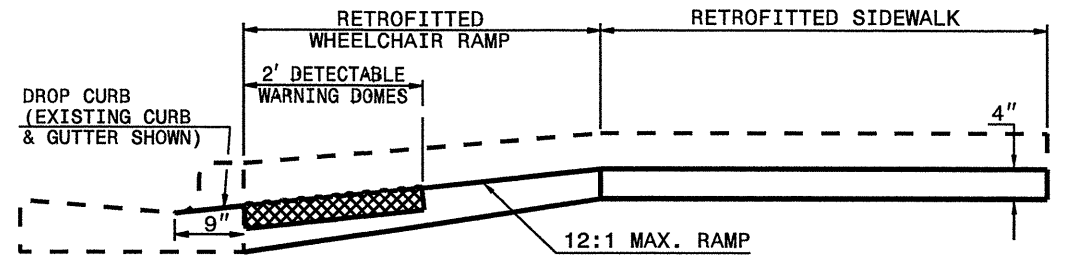
**ISOMETRIC VIEW**

**NOTES:**

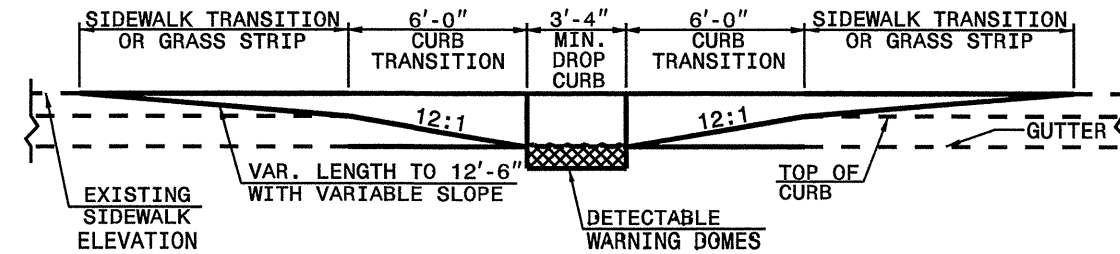
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



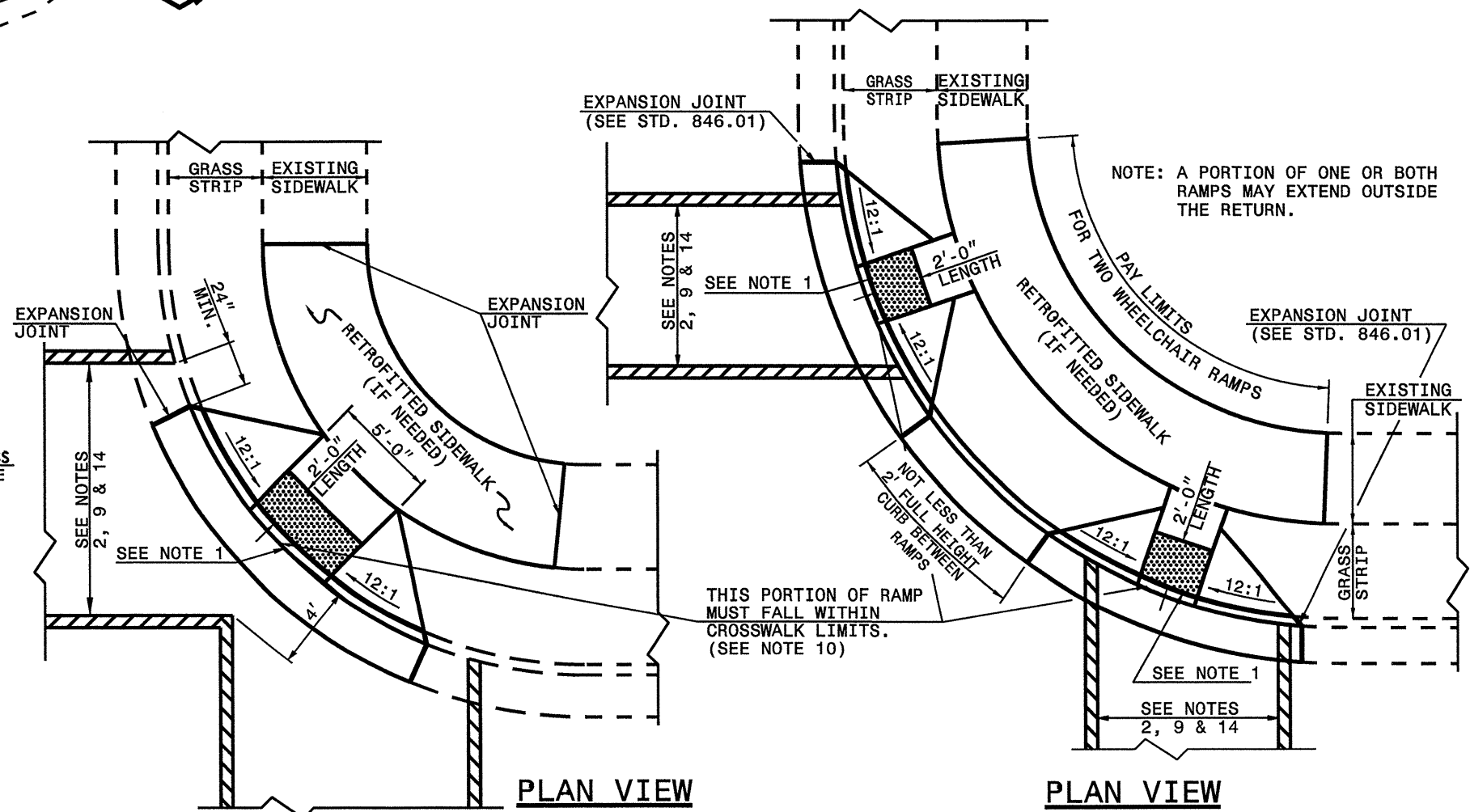
**DETECTABLE WARNING DOMES**



**SECTION B-B**



**SECTION A-A**



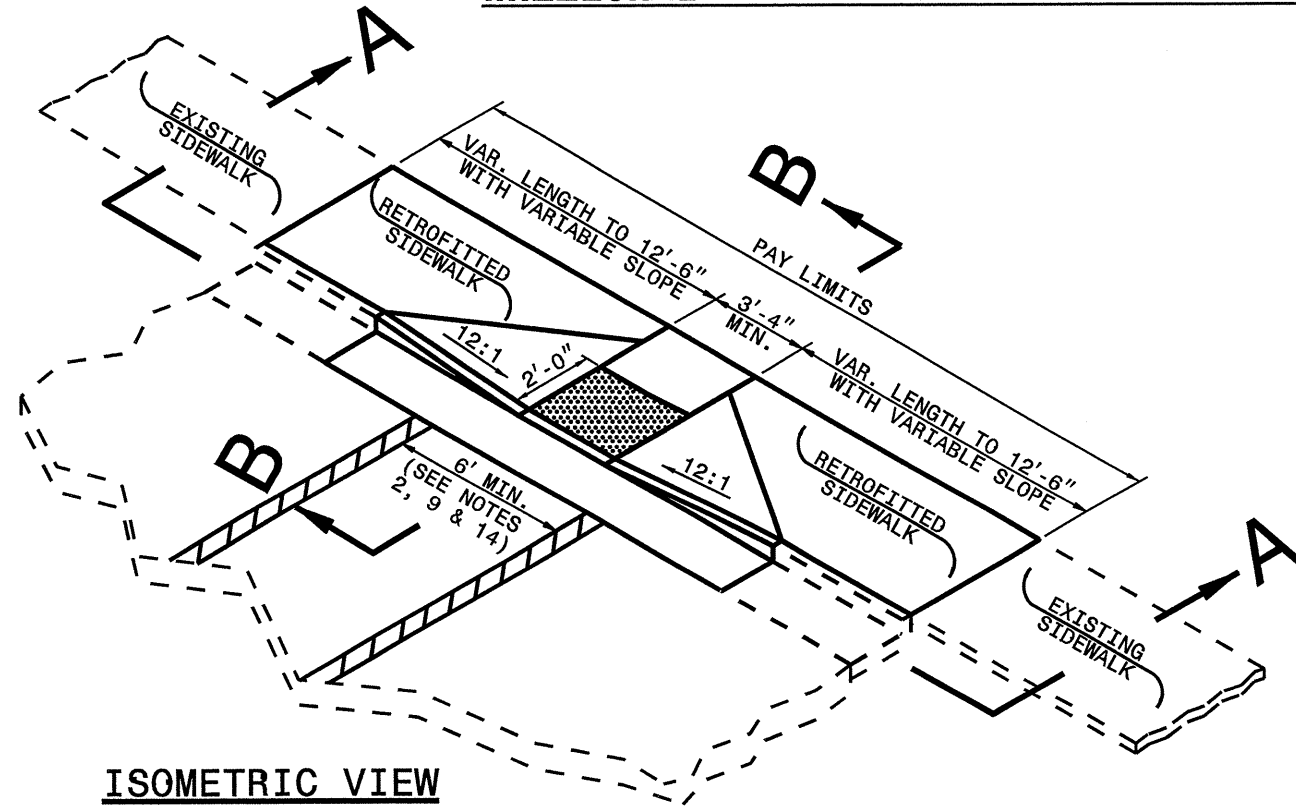
**PLAN VIEW**

DIAGONAL RAMP  
MAX. 25' RADII  
(60" MIN. FLOOR WIDTH)

**PLAN VIEW**

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

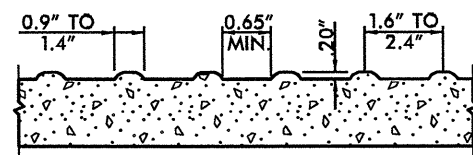
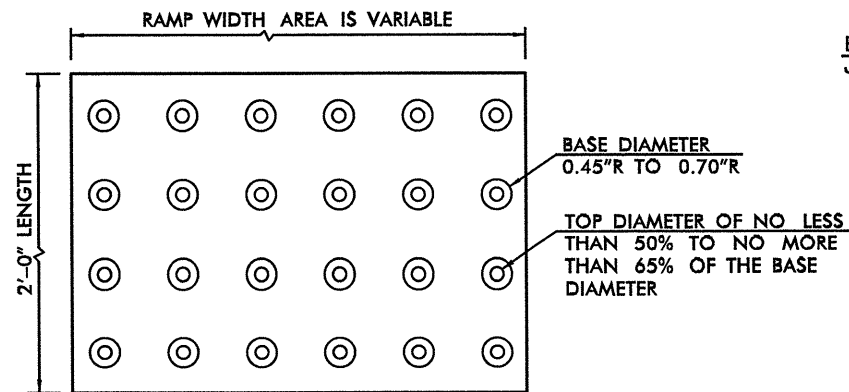
**WHEELCHAIR RAMP AND EXISTING SIDEWALK ADJACENT TO CURB**



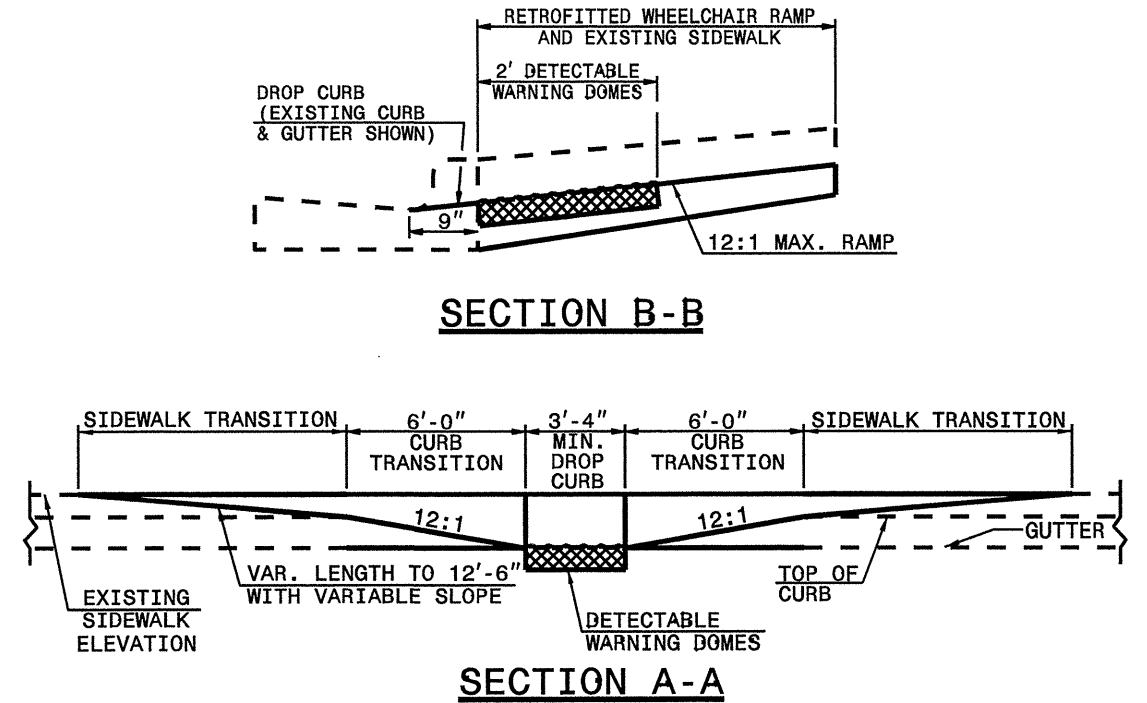
**ISOMETRIC VIEW**

**NOTES:**

1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.

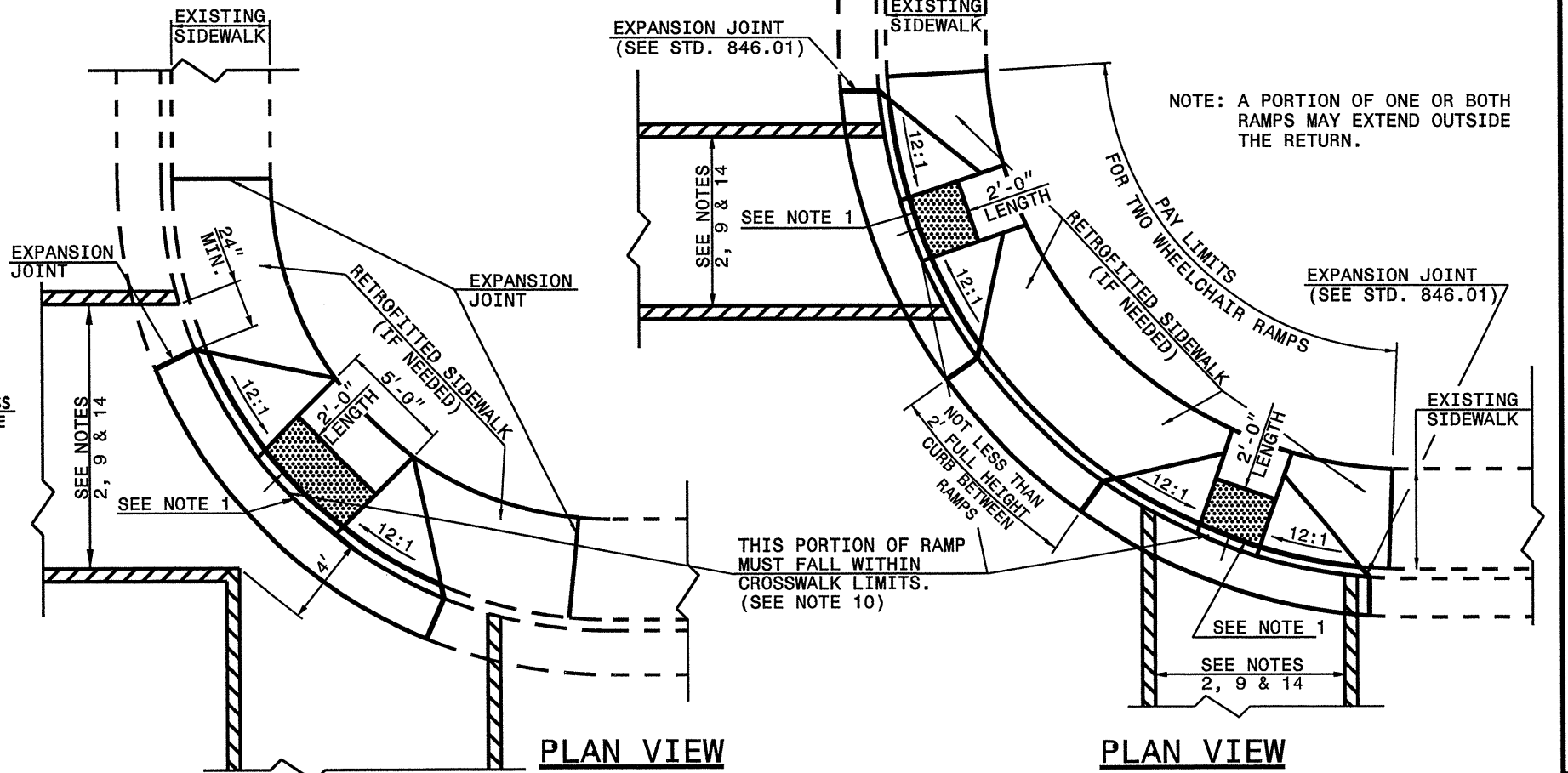


**DETECTABLE WARNING DOMES**



**SECTION B-B**

**SECTION A-A**



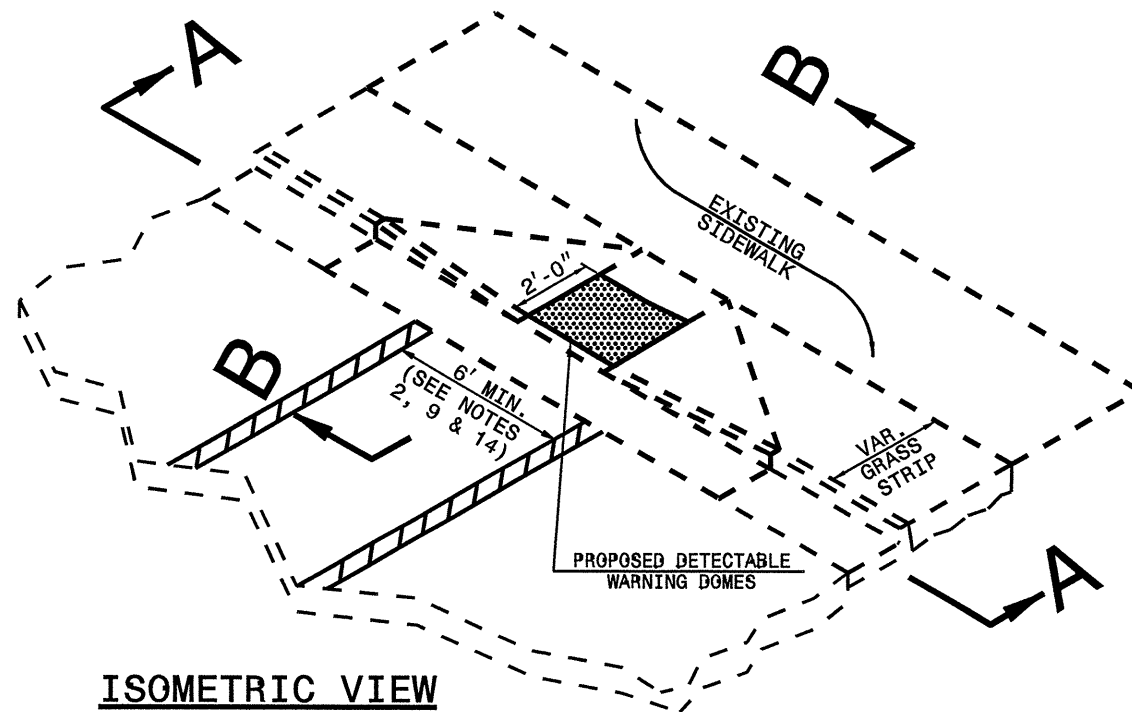
**PLAN VIEW**

**PLAN VIEW**

DIAGONAL RAMP  
MAX. 25' RADII  
(60" MIN. FLOOR WIDTH)

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

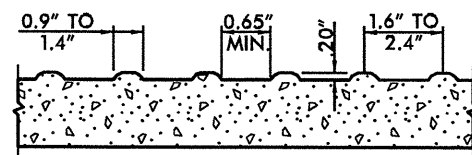
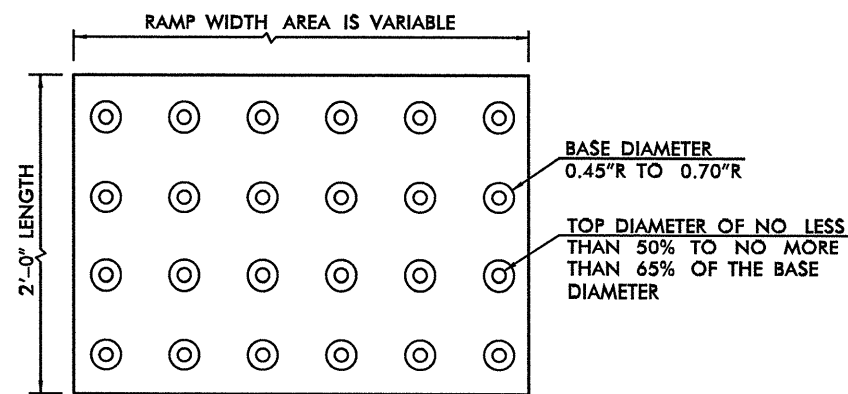
**RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING WHEELCHAIR RAMP**



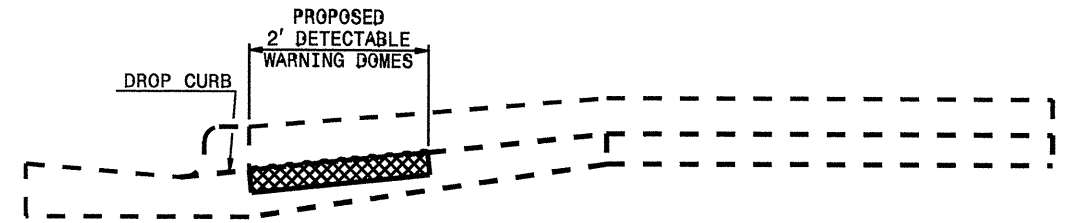
**ISOMETRIC VIEW**

**NOTES:**

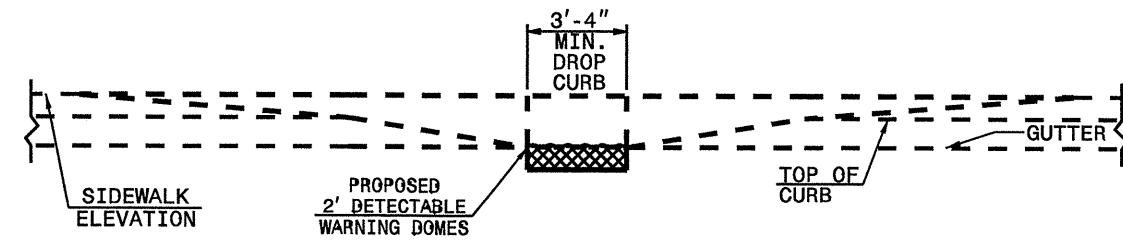
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



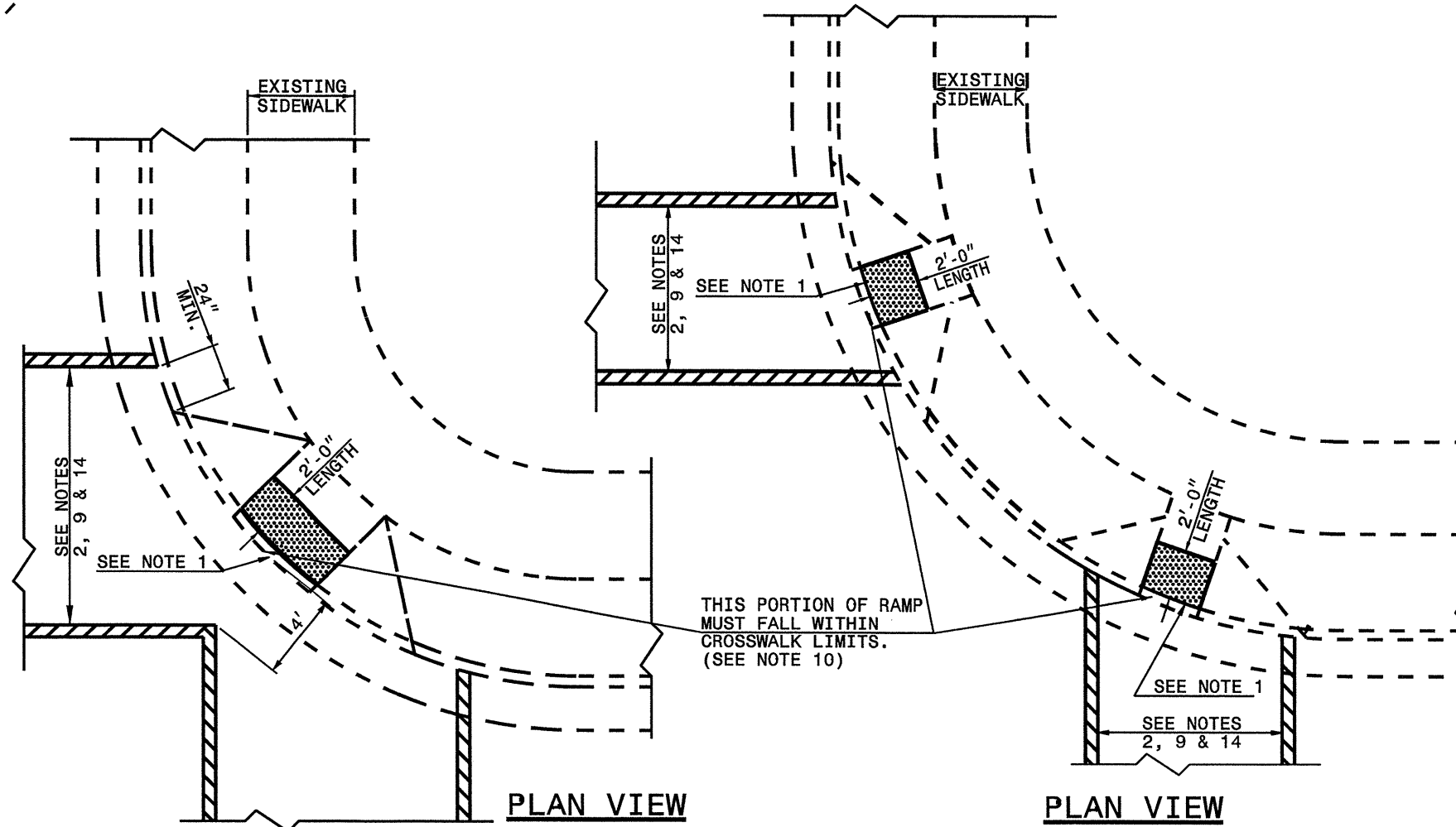
**DETECTABLE WARNING DOMES**



**SECTION B-B**



**SECTION A-A**



**PLAN VIEW**

DIAGONAL RAMP  
MAX. 25' RADII  
(60" MIN. FLOOR WIDTH)

**PLAN VIEW**

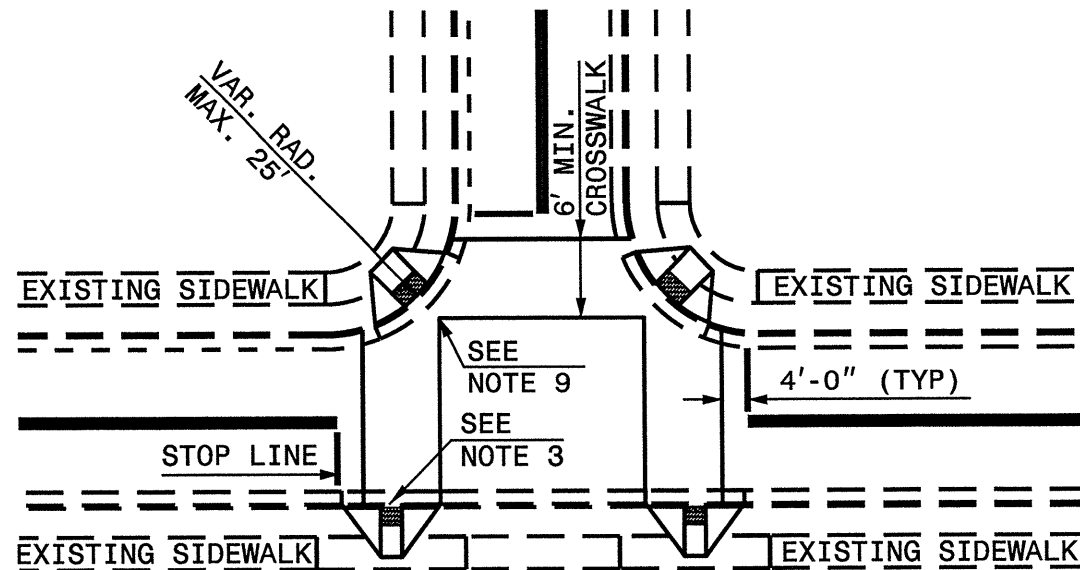
DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)



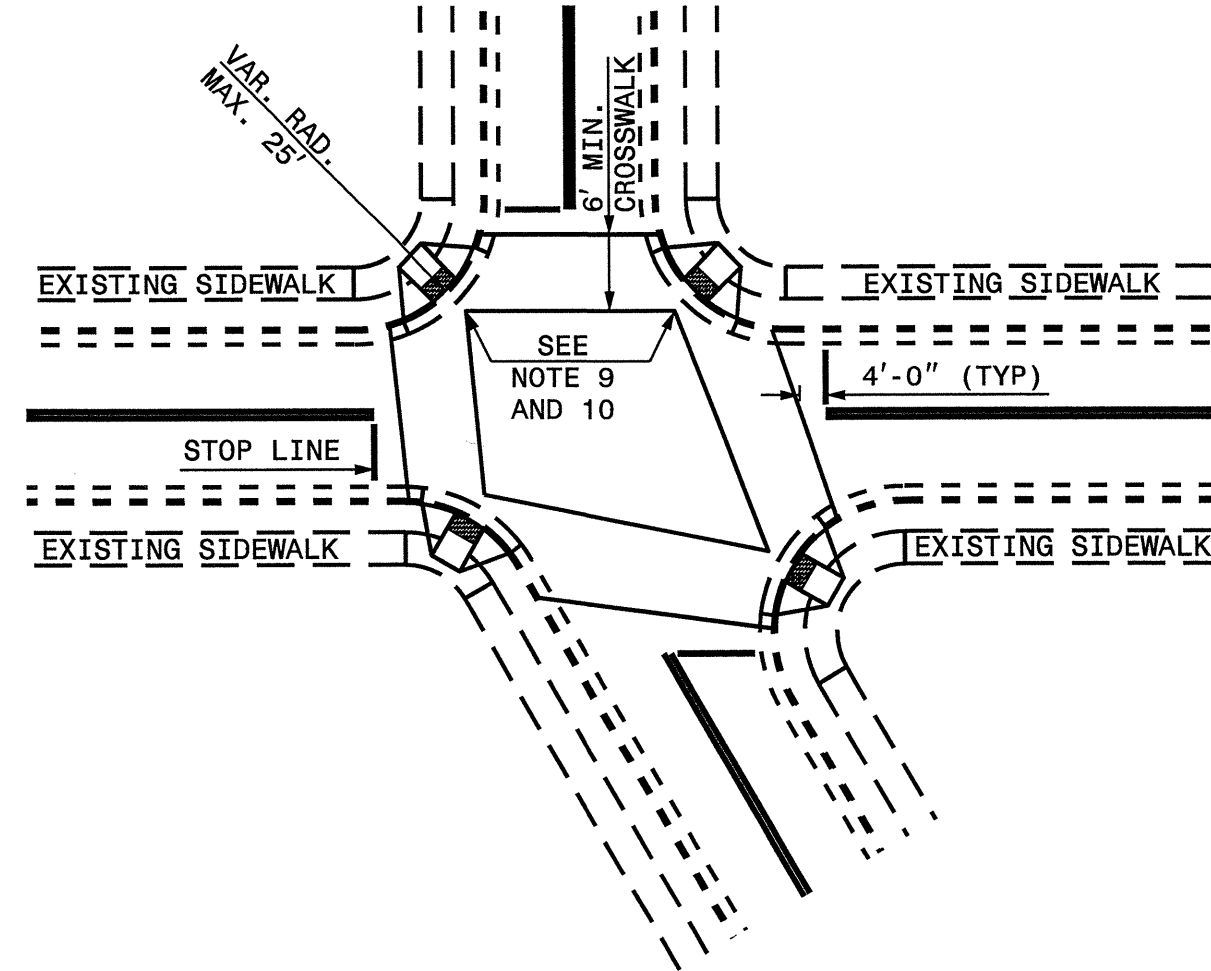
STATE OF  
NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

**WHEELCHAIR RAMP AND EXISTING SIDEWALK**

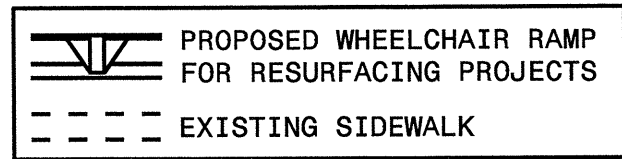


DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

**RESURFACING PROJECTS**



ALLOWABLE LOCATIONS  
DIAGONAL RAMP RADII...MAX. 25'

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ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

STATE OF  
NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

SHEET 5 OF 5

**848D06**

### WHEELCHAIR RAMP AND EXISTING SIDEWALK

NOTES:

1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
2. CROSSWALK WIDTHS AND CONFIGURATION VARY, BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
3. NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.  
  
IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.  
  
THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES, COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.
4. PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
5. DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
6. CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS AND 60" (5'-0") OR GREATER FOR DIAGONAL RAMPS.
7. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADII, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 14)
10. COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
11. CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
14. PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

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ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

SHEET 5 OF 5

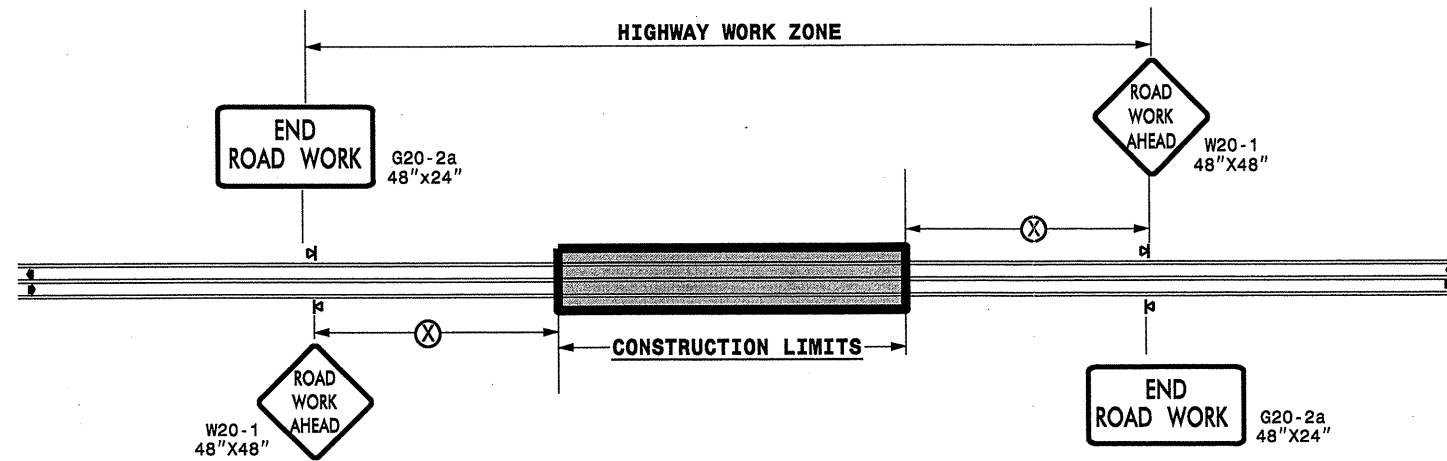
**848D06**

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 1CR.20661.62, 1CR.20661.63, 1CR.20661.64, 1CR.20661.65

PROJ. REFERENCE NO.	SHEET NO.
SEE TO THE LEFT NORTHAMPTON	TCP-1

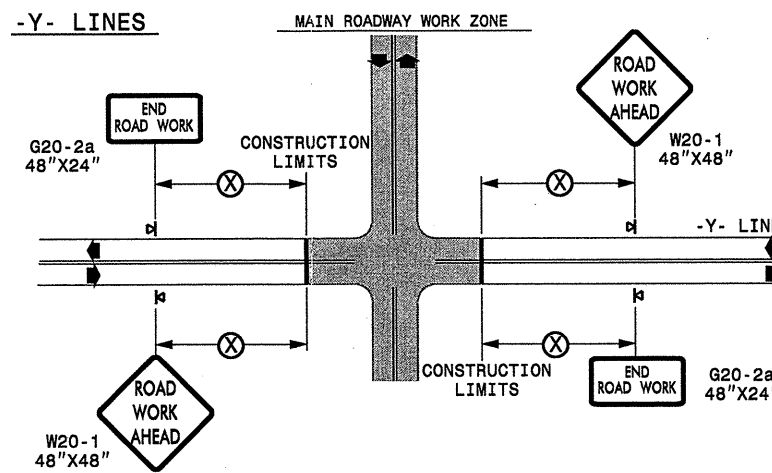
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA  
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 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.

**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

PORTABLE SIGN  
 DIRECTION OF TRAFFIC FLOW

**DETAIL DRAWING  
 FOR TWO-WAY UNDIVIDED  
 WORK ZONE WARNING SIGNS**

SHEET 1 OF 1

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED ADVANCED WORK ZONE WARNING SIGNS					
SEAL 	SCALE: NONE						
	DATE: 10/10						
	DWG. BY: _____						
	DESIGN BY: _____						
REVIEWED BY: _____		REVISIONS <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%; height: 20px;"> </td> <td style="width: 50%;"> </td> </tr> <tr> <td style="height: 20px;"> </td> <td> </td> </tr> </table>					