

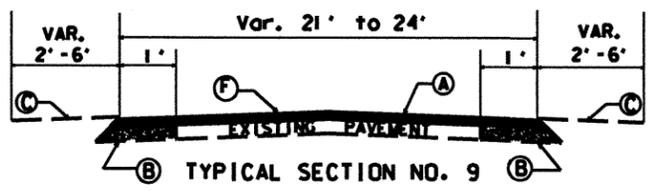
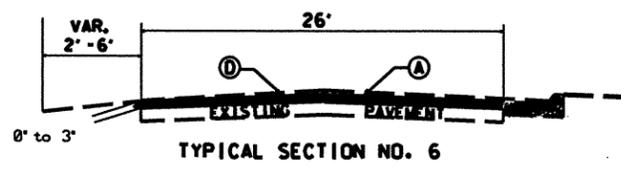
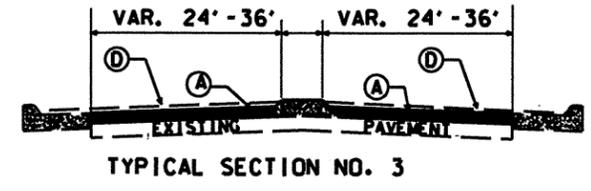
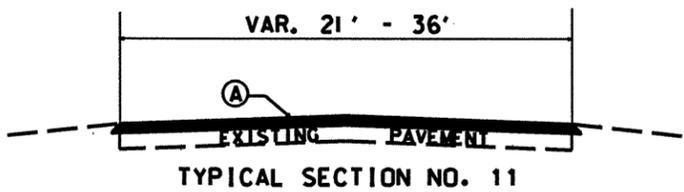
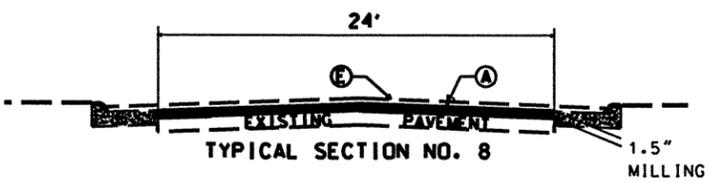
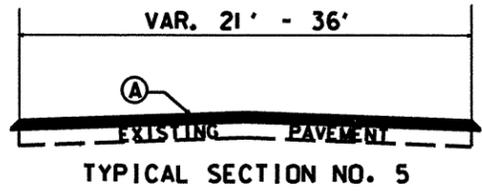
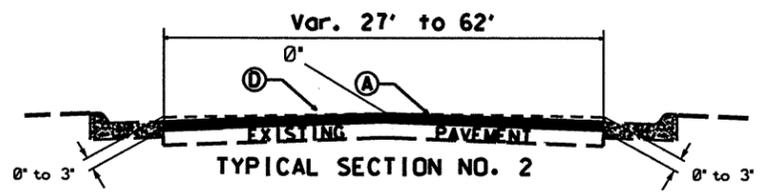
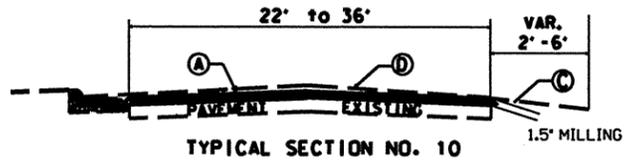
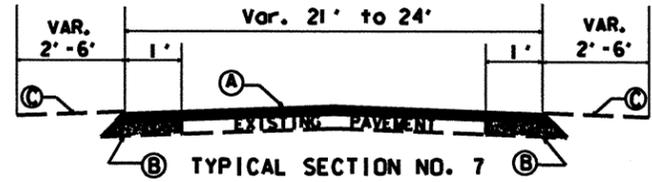
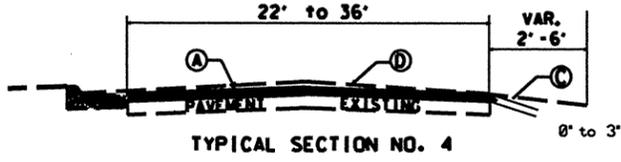
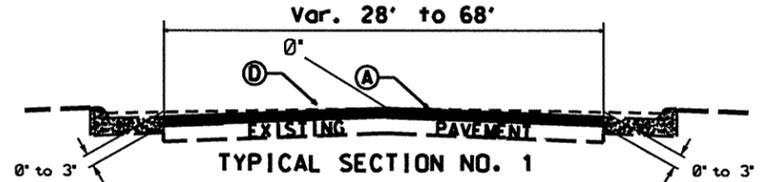
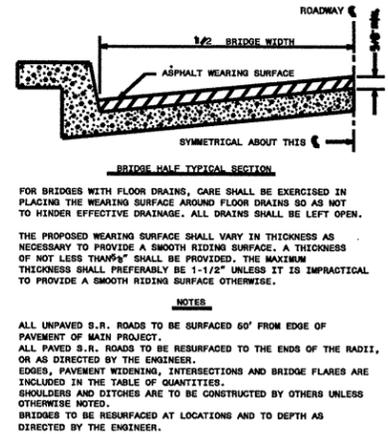
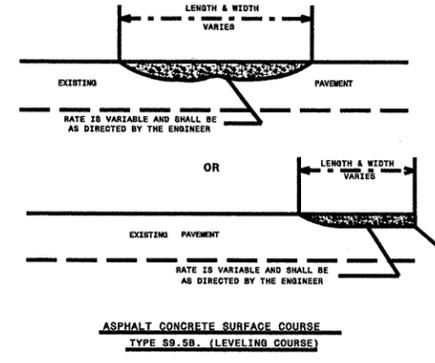
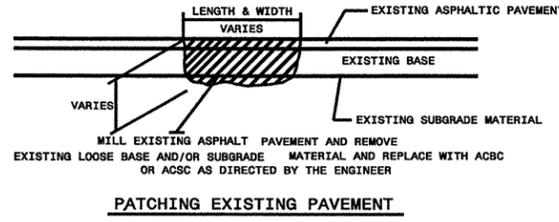
12CR.10491.9
12CR.20491.9
SHEET 1
IREDELL COUNTY



PROJ. REFERENCE NO.	SHEET NO.	TOTAL SHEETS
IREDELL COUNTY	3	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION
T2CR.10491.9		
T2CR.20491.9		

PAVEMENT SCHEDULE	
A	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
B	PROP. APPROX. 5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 570 LBS. PER SQ. YD.
C	SHOULDER RECONSTRUCTION
D	MILL ASPHALT PAVEMENT APPROX. 0" - 3" OR AS DIRECTED BY ENGINEER 0" TO 3" MILLING MAY REQUIRE 2 PASSES
E	MILL ASPHALT PAVEMENT 1.5" AS DIRECTED BY ENGINEER
F	PROP. APPROX 3" ASPHALT INTERMEDIATE COURSE, I 19.0B AT A AVERAGE RATE OF 342 LBS. PER SQ. YD.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.
MILL BRIDGE APPROACHES 100' TO PROVIDE A SMOOTH TRANSITION AS DIRECTED, THIS WILL BE PAID FOR AS INCIDENTAL MILLING.



PROJECT NO.	SHEET NO.	TOTAL NO.
12CR.10491.9, 12CR.20491.9	4	

SUMMARY OF QUANTITIES

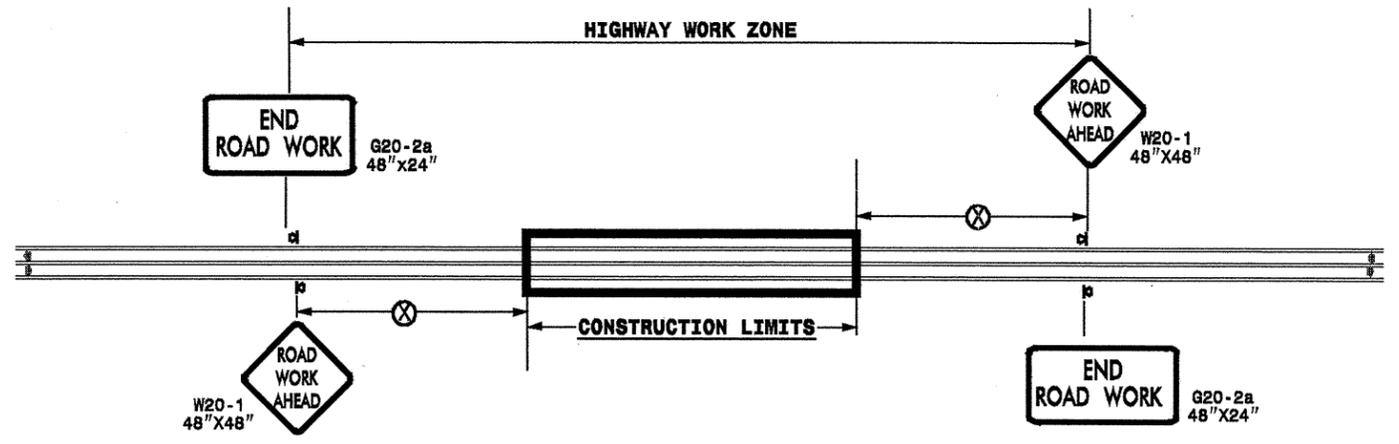
PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	LENGTH	WIDTH	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1 1/2" MILLING	0" TO 3" MILLING	INCIDENTAL MILLING	BASE COURSE, B25.0B	INTERMEDIATE COURSE, I19.0B	SURFACE COURSE, S9.5B	LEVELING COURSE, S9.5B	PG 64-22 PLANT MIX	PATCHING EXISTING PAVEMENT	1' 6" CONCRETE CURB AND GUTTER LF	2' 6" CONCRETE CURB AND GUTTER LF	DROP INLET	MANHOLES	METER OR VALVE BOX	PORTABLE LIGHTING	INDUCTIVE LOOP	
NO		NO			NO	MI	FT	TONS	SMI	SY	SY	SY	TONS	TONS	TONS	TONS	TONS	TONS	EA	EA	EA	EA	EA	EA	EA	
12CR.10491.9	Iredell	1	NC 90 (WEST END AVE)	FROM US 64 FRONT ST TO US 64 DAVIE AVE	1 2	1.15 0.09	28 to 68 27 to 62				42640				1,910	75	119	175			23	31	11	1.00	500	
TOTAL FOR MAP NO. 1						1.24					42640				1,910	75	119	175			23	31	11	1.00	500	
		2	US 64 (W. FRONT ST)	FROM US 70 GARNER BAGNAL BLVD TO NC 115 CENTER ST	1 3	1.81 0.07	28 to 68 24 to 36				97600				4,600	75	276	420	200	100	43	39	20	*	2,560	
TOTAL FOR MAP NO. 2						1.88					97600				4,600	75	276	420	200	100	43	39	20	*	2,560	
		3	US 21/64 (W. FRONT, TRADD ST)	FROM NC 115 TO DAVIE AVE, WATER ST INTERSECTION	1	0.44	28 to 68				19520				925	75	56	100			8	11	2	*	250	
TOTAL FOR MAP NO. 3						0.44					19520				925	75	56	100			8	11	2	*	250	
		4	NC 115 (DOWNTOWN MOORESVILLE)	FROM W. WILSON RD (MOORESVILLE) TO US 21/ NC 115 SPLIT	1 4 5	0.67 0.93 2.10	28 to 68 22 to 36 21 to 36	150	5.13		51916				8,271	100	503	500			15	29	15	*	2,000	
TOTAL FOR MAP NO. 4						3.7		150	5.13		51916				8,271	100	503	500			15	29	15	*	2,000	
		5	NC 3 (IREDELL AVE)	FROM SOUTH MOORESVILLE CITY LIMITS TO NC 150	1 4 5	1.09 0.19 0.63	28 to 68 22 to 36 21 to 36	100	1.45		43134				3,120	200	200	250			13	19	18		1,000	
TOTAL FOR MAP NO. 5						1.91		100	1.45		43134				3,120	200	200	250			13	19	18		1,000	
		6	NC 152 (N.MAIN ST)	FROM NC 3 TO SR-1150 LINWOOD RD	1	1.04	28 to 68	0			48668				2,289	100	144	200			9	12	12		300	
TOTAL FOR MAP NO. 6						1.04					48668				2,289	100	144	200			9	12	12		300	
TOTAL FOR PROJ NO. 12CR.10491.9						10.21		250	6.58		303478				21,115	625	1,298	1,645	200	100	111	141	78	1.00	6,610	
12CR.20491.9	Iredell	7	SR-1640 (RACE ST)	FROM SR-1420 WESTERN AVE TO NC 115 CENTER ST	1 2 6	0.74 0.82 0.15	28 to 68 27 to 62 26				66320				3,124	75	187	400				41	2		240	
TOTAL FOR MAP NO. 7						1.71		0			66320				3,124	75	187	400				41	2		240	
		8	SR-2173 (JAMES FARM RD)	FROM US 21 TO SR-2171 JANE SOWERS RD	5 7	0.61 0.58	21 to 36 21 to 24	100	1.16				295		1,585	100	114	140								
TOTAL FOR MAP NO. 8						1.19		100	1.16				295		1,585	100	114	140								
		9	SR-2320 (GREENBRIAR RD)	FROM SR-2333 EASTSIDE DR TO US 64 DAVIE AVE	10 11	0.04 3.70	27 to 62 21 to 36	100				500			4,615	250	293	420			1	12	5			
TOTAL FOR MAP NO. 9						3.74		100				500			4,615	250	293	420			1	12	5			
		10	SR-2319 (TWIN OAKS RD)	FROM SR-2320 GREENBRIAR RD TO SR-2318 FANJOY RD	11	1.47	21 to 36	100				500	0		1,700	100	108	170								
TOTAL FOR MAP NO. 10						1.47		100				500	0		1,700	100	108	170								
		11	SR-1314 (CLONTZ HILL RD)	FROM SR-1303 PERTH RD TO SR-1312 FLOWERHOUER LOOP	7	1.87	21 to 24	100	3.73				937		2,471	150	198	250								
TOTAL FOR MAP NO. 11						1.87		100	3.73				937		2,471	150	198	250								
		12	SR-1303 (PERTH RD)	FROM QUAIL HAVEN DR AT THE START OF CURB SECTION TO US 21	1 8	0.07 0.79	28 to 68 24				12602	300			1,186	75	76	120			22	18			130	
TOTAL FOR MAP NO. 12						0.86					12602	300	0		1,186	75	76	120			22	18			130	
		13	SR-2169 (CHESNUT GROVE RD)	FROM SR-2158 OLD MOCKSVILLE RD TO SR-2166 RIVER HILL RD	7	2	21 to 24	100	4				1000		2,305	230	195	230								
TOTAL FOR MAP NO. 13						2		100	4				1000		2,305	230	195	230								
		14	SR-1309 (BARFIELD RD)	FROM US 21 TO DEAD END	7 9	0.53 0.91	21 to 24 21 to 24	75	2.87				721	3025	1,664	75	278	300								
TOTAL FOR MAP NO. 14						1.44		75	2.87				721	3025	1,664	75	278	300								
		15	SR-2731 (WINSTON AVE)	FROM US 21 TO END OF STATE MAINTENANCE AT PAVEMENT CHANGE	1 5	0.33 0.23	28 to 68 21 to 36				8945				842	75	55	200			2	8	6		120	
TOTAL FOR MAP NO. 15						0.56					8945				842	75	55	200			2	8	6		120	
		16	SR-1254 (TIMBER RD)	FROM NC 115 TO SR-1125 SHEARERS RD	11	1.24	21 to 36	75							1,666	166	110	166				1	1			
TOTAL FOR MAP NO. 16						1.24		75							1,666	166	110	166				1	1			
TOTAL FOR PROJ NO. 12CR.20491.9						14.08		650	11.76		12602	75565	1000	2953	3025	21,158	1,296	1,614	2,396			25	80	14		490
GRAND TOTAL						24.29		900	18.34		12602	379043	1000	2953	3025	42,273	1,921	2,912	4,041	200	100	136	221	92	1.00	7,100

PROJECT NO.	SHEET NO.	TOTAL NO.
12CR.10491.9, 12CR.20491.9	5	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4685000000-E	4686000000-E			4702000000-E	4705000000-E	4710000000-E	4721000000-E					4725000000-E				4810000000-E	4825000000-E	4835000000-E	4905000000-N
					4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	8" X 120 M WHITE THERMO LF	12" X 120 M WHITE THERMO LF	16" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO RXR 120 M EA	THERMO MSG STOP 120 M EA	THERMO LT ARROW 90 M EA	THERMO STR & RT ARROW 90 M EA	THERMO STR & LT ARROW 90 M EA	THERMO RT ARROW 90 M EA	4" YELLOW PAINT LF	4" WHITE PAINT LF	12" WHITE PAINT LF	24" WHITE PAINT LF	SNOW PLOWABLE MARKERS EA			
12CR.10491.9	Iredell	1	NC 90 (WEST END AVE)	FROM US 64 FRONT ST TO US 64 DAVIE AVE	3,000	14,000		600	50	1,525	2	4	26										262	
TOTAL FOR MAP NO. 1					3,000	14,000		600	50	1,525	2	4	26										262	
		2	US 64 (W. FRONT ST)	FROM US 70 GARNER BAGNAL BLVD TO NC 115 CENTER ST	5,000	19,840		1,136	150	450	6		38	20	6	19							300	
TOTAL FOR MAP NO. 2					5,000	19,840		1,136	150	450	6		38	20	6	19							300	
		3	US 21/64 (W. FRONT, TRADD ST)	FROM NC 115 TO DAVIE AVE, WATER ST INTERSECTION	2,300	4,600		1,175		200			8	9	2	1							100	
TOTAL FOR MAP NO. 3					2,300	4,600		1,175		200			8	9	2	1							100	
		4	NC 115 (DOWNTOWN MOORESVILLE)	FROM W. WILSON RD (MOORESVILLE) TO US 21/ NC 115 SPLIT	48,365	29,310	360			276			29	9									400	
TOTAL FOR MAP NO. 4					48,365	29,310	360			276			29	9									400	
		5	NC 3 (IREDELL AVE)	FROM SOUTH MOORESVILLE CITY LIMITS TO NC 150	20,200	20,200		360		168			8	1									202	
TOTAL FOR MAP NO. 5					20,200	20,200		360		168			8	1									202	
		6	NC 152 (N.MAIN ST)	FROM NC 3 TO SR-1150 LINWOOD RD	5,000	11,000		80		132			43	6									110	
TOTAL FOR MAP NO. 6					5,000	11,000		80		132			43	6									110	
TOTAL FOR PROJ NO. 12CR.10491.9					83,865	98,950	360	3,351	200	2,751	8	4	152	45	8	20							1,374	
											12			225										
12CR.20491.9	Iredell	7	SR-1640 (RACE ST)	FROM SR-1420 WESTERN AVE TO NC 115 CENTER ST									10	10		2	18,080	1,890	1,220	80				
TOTAL FOR MAP NO. 7													10	10		2	18,080	1,890	1,220	80				
		8	SR-2173 (JAMES FARM RD)	FROM US 21 TO SR-2171 JANE SOWERS RD													25,180	25,180		24				
TOTAL FOR MAP NO. 8																	25,180	25,180		24				
		9	SR-2320 (GREENBRIAR RD)	FROM SR-2333 EASTSIDE DR TO US 64 DAVIE AVE											3	3	80,000	80,000		72				
TOTAL FOR MAP NO. 9															3	3	80,000	80,000		72				
		10	SR-2319 (TWIN OAKS RD)	FROM SR-2320 GREENBRIAR RD TO SR-2318 FANJOY RD													31,000	31,000						
TOTAL FOR MAP NO. 10																	31,000	31,000						
		11	SR-1314 (CLONTZ HILL RD)	FROM SR-1303 PERTH RD TO SR-1312 FLOWERHOUSR LOOP													39,405	39,405		24				
TOTAL FOR MAP NO. 11																	39,405	39,405		24				
		12	SR-1303 (PERTH RD)	FROM QUAIL HAVEN DR AT THE START OF CURB SECTION TO US 21									1			1	18,264			24		92		
TOTAL FOR MAP NO. 12													1			1	18,264			24		92		
		13	SR-2169 (CHESNUT GROVE RD)	FROM SR-2158 OLD MOCKSVILLE RD TO SR-2166 RIVER HILL RD													42,000	42,000		22				
TOTAL FOR MAP NO. 13																	42,000	42,000		22				
		14	SR-1309 (BARFIELD RD)	FROM US 21 TO DEAD END													30,320	30,320		12				
TOTAL FOR MAP NO. 14																	30,320	30,320		12				
		15	SR-2731 (WINSTON AVE)	FROM US 21 TO END OF STATE MAINTENANCE AT PAVEMENT CHANGE									1			1	11,820	11,820		48				
TOTAL FOR MAP NO. 15													1			1	11,820	11,820		48				
		16	SR-1254 (TIMBER RD)	FROM NC 115 TO SR-1125 SHEARERS RD													26,189	26,189		24				
TOTAL FOR MAP NO. 16																	26,189	26,189		24				
TOTAL FOR PROJ NO. 12CR.20491.9													12	10	3	7	322,258	287,804	1,220	330		92		

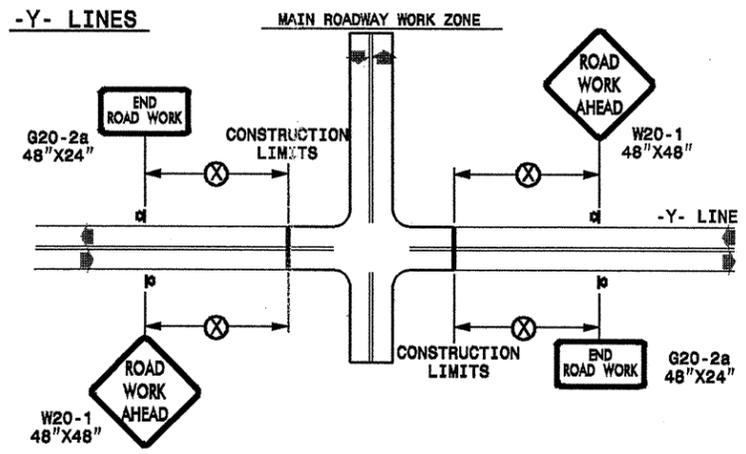
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

	PORTABLE SIGN
	DIRECTION OF TRAFFIC FLOW

**DETAIL DRAWING
FOR TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS**

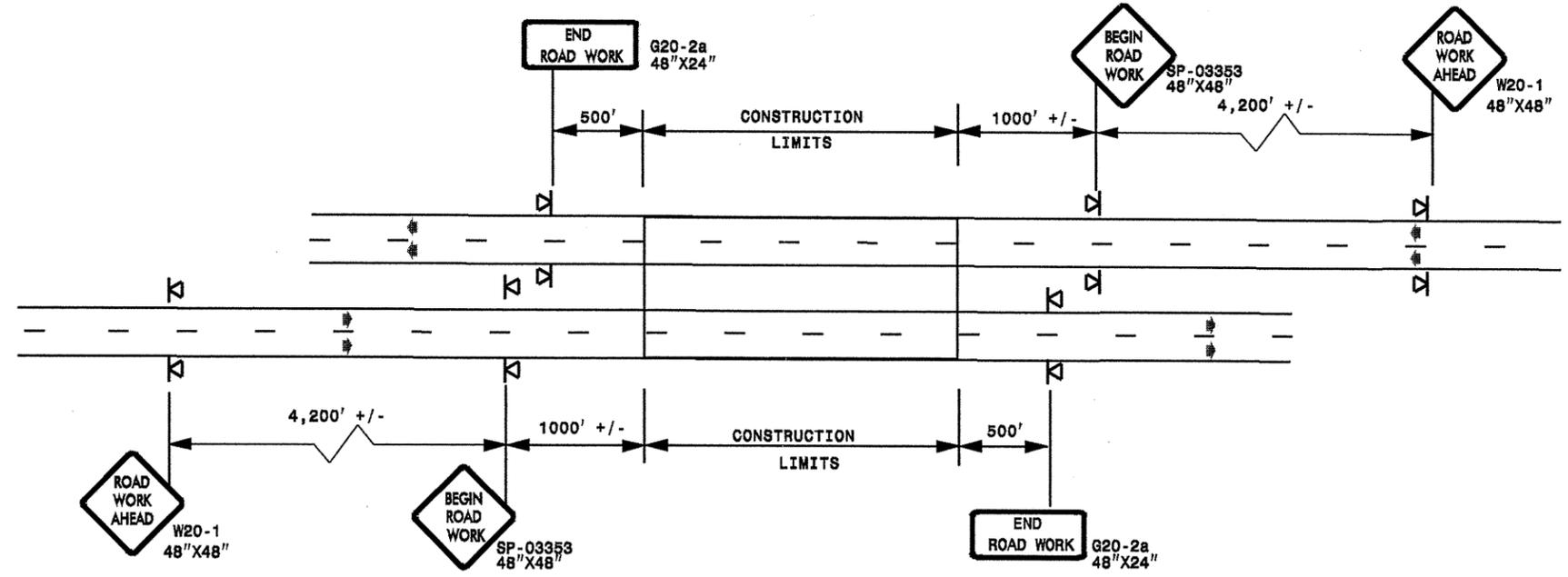
SHEET 1 OF 1

APPROVED: _____	DATE: _____	<p>DETAIL DRAWING FOR TWO-WAY UNDIVIDED ADVANCED WORK ZONE WARNING SIGNS</p>	
		DATE: _____	7-98 10/01
DESIGN BY: _____	10-98 03/04		
REVIEWED BY: _____	01/01 11/04		

21-DEC-2009 17:20
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 pseymore AT WZTC237502

ADVANCE WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

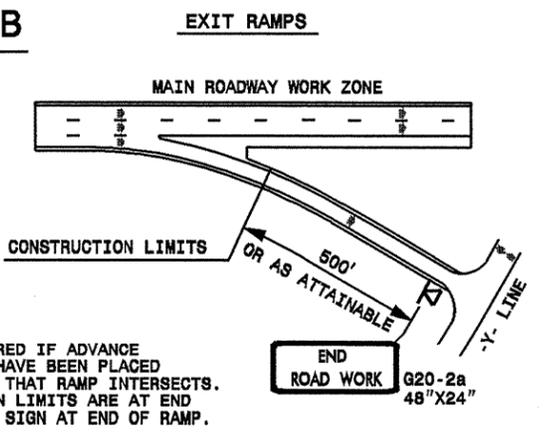
DETAIL A



STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

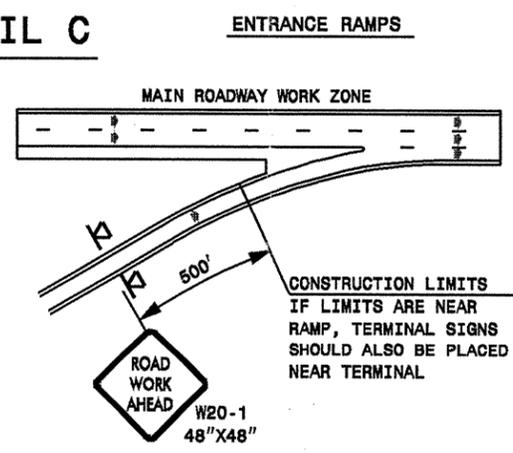
ROADWAYS INTERSECTING ALONG FREEWAY WORK ZONE (Y-LINES)

DETAIL B

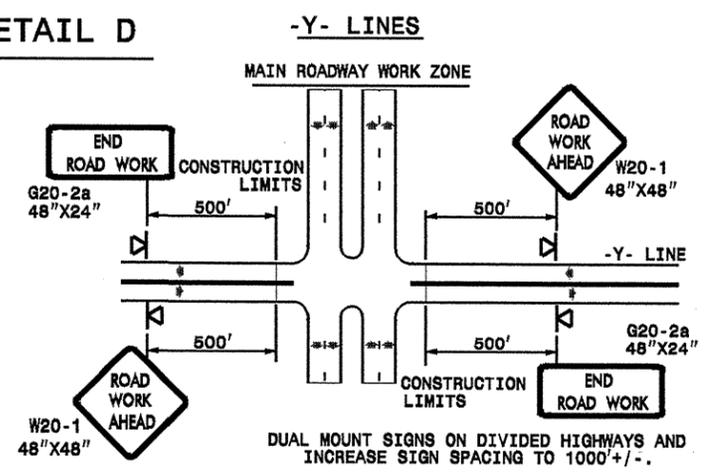


NOTE:
SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

DETAIL C



DETAIL D



**DETAIL DRAWING
FOR FREEWAYS
WORK ZONE WARNING SIGNS
(SHORT-DURATION LANE CLOSURES)**

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

◀ PORTABLE SIGN

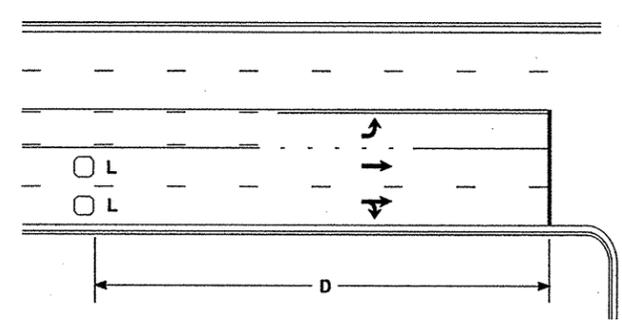
➡ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR FREEWAYS WORK ZONE WARNING SIGNS	
SEAL	SCALE: NONE		REVISIONS
	DATE: _____		7-98 10/01
	DWG. BY: _____		10-98 03/04
	DESIGN BY: _____		01/01 11/04
REVIEWED BY: _____	_____	_____	_____

2-DEC-2009 11:21 sa:\signing\resur\facimg\2010\div2\c202552a-b\l2cr10491x2-1r\deli_nc90ml6_c202552a-b_l2cr10491x2-1r\portable.dgn

High Speed Detection [≥40 mph (64 km/hr)]

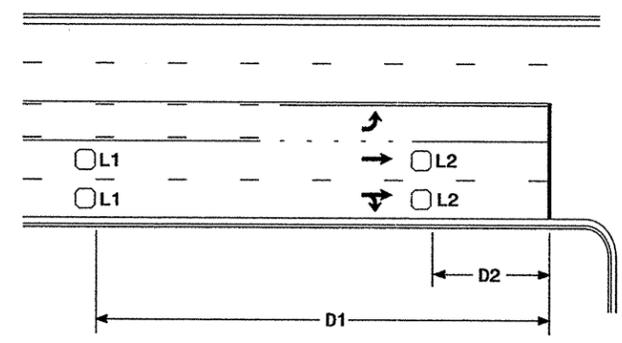


Speed Limit mph (km/hr)	D ft (m)
40 (64)	250 (75)
45 (72)	300 (90)
50 (80)	355 (110)
55 (88)	420 (130)

L = 6ft X 6ft (1.8m X 1.8m)
Wired in series for TS1
Controllers
Wired separately for TS2,
170, and 2070L Controllers

Volume Density Operation

OR



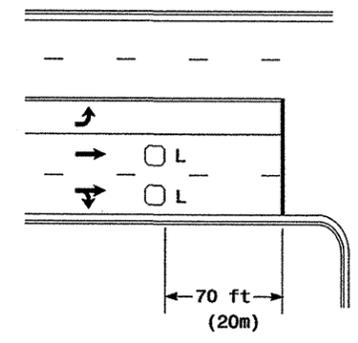
Speed Limit mph (km/hr)	D1 ft (m)	D2 ft (m)
40 (64)	250 (75)	80 (25)
45 (72)	300 (90)	90 (27)
50 (80)	355 (110)	100 (30)
55 (88)	420 (130)	110 (35)

L1 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series
L2 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series

"Stretch" Operation

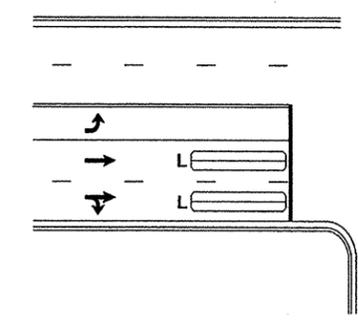
Low Speed Detection [≤35 mph (56 km/hr)]

12CR.10491.9 & 12CR.20491.9



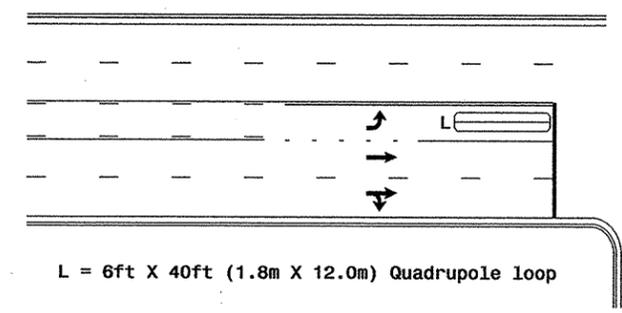
L = 6ft X 6ft (1.8m X 1.8m)
Wired in series

OR



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop, wired separately

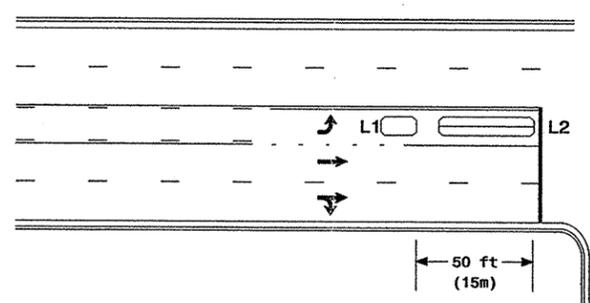
Left Turn Lane Detection



L = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

Presence Loop Detection

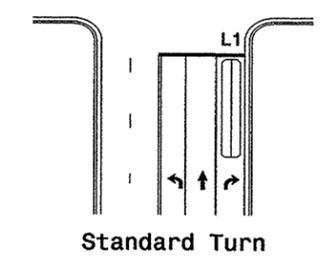
OR



L1 = 6ft X 15ft (1.8m X 4.6m) Queue detector
L2 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

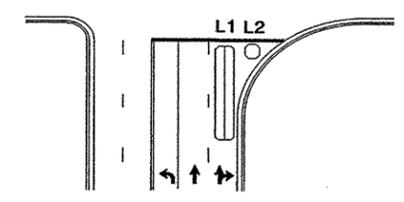
Queue Loop Detection

Right Turn Lane Detection

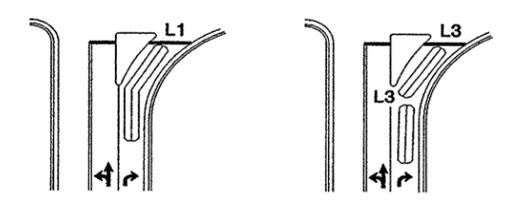


Standard Turn

L1 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop
L2 = 6ft X 6ft (1.8m X 1.8m) [Minimum] Presence loop
Wired separately
L3 = 6ft X 20ft (1.8m X 6.0m) Quadrupole loop
Wired in series

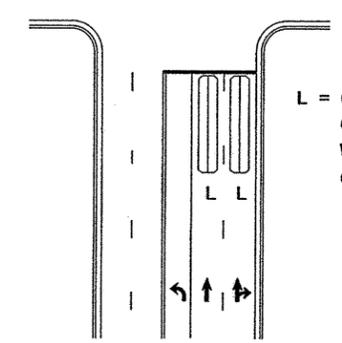


Wide Radius Turn



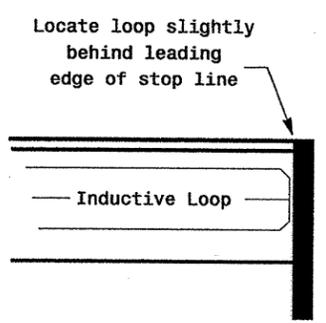
Channelized Turn

Side Street Detection



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines



Note:
Loop may be located in advance
of stop line when stop line is
greater than 15' (4.5m) from edge
of intersecting roadway; or, when
loop detects a permissive or
protected/permissive left turn.

Recommended Number of Turns

Single 6' X 6' (1.8m X 1.8m)
loop (wired separately):

Length of Lead-in ft (m)	Number of Turns
< 250 (75)	3
250-375 (75-115)	4
375-525 (115-160)	5
> 525 (160)	6

Quadrupole loops: Use 2-4-2 turns
6' X 15' (1.8m X 4.6m) Loops:
Lead-in < 150' (45 m), use 2 turns
Lead-in > 150' (45 m), use 3 turns

	Typical Loop Locations		
	PLAN DATE: June 2006 PREPARED BY: P. L. Alexander	REVIEWED BY: REVIEWED BY:	
REVISIONS Revise pavement markings		INIT. DATE ac 12/15/06	SIGNATURE DATE ac 12/15/06
SIG. INVENTORY NO.			

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ENGLISH DETAIL DRAWING FOR
INDUCTIVE DETECTION LOOPS

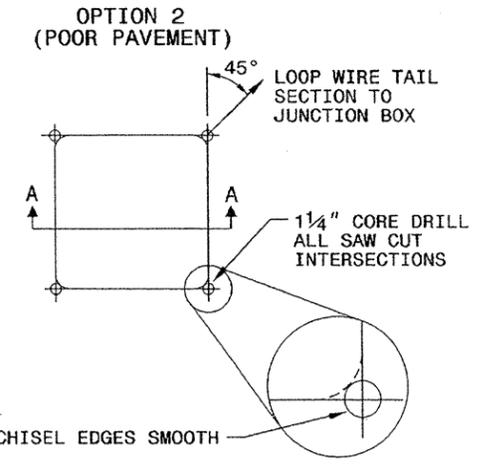
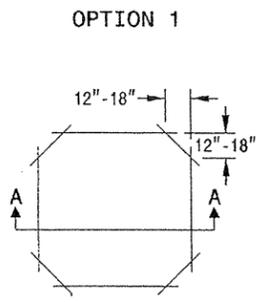
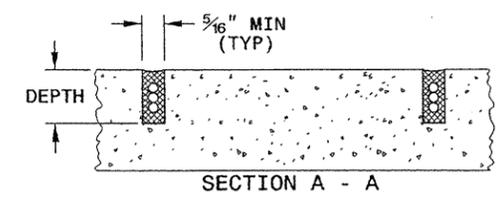
SHEET 1 OF 3
1725D01

CONVENTIONAL 4-SIDED LOOP

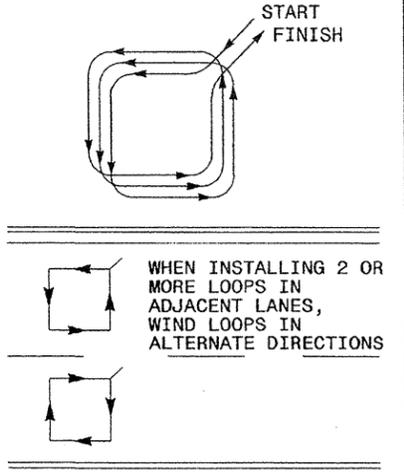
SAW CUT OPTIONS

SAW SLOT DEPTH CHART

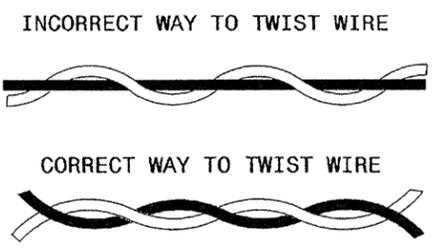
DEPTH (IN)	NO. OF WIRE TURNS				
	2	3	4	5	6
CONCRETE	2.0	2.0	2.5	2.5	3.0
ASPHALT	2.0	2.5	3.0	3.0	3.0



LOOP WINDING METHOD



LOOP WIRE TWISTING METHOD

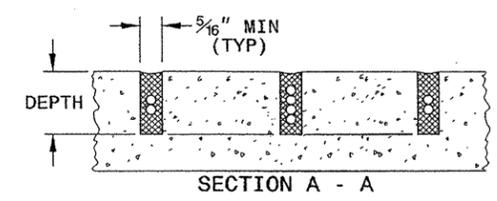
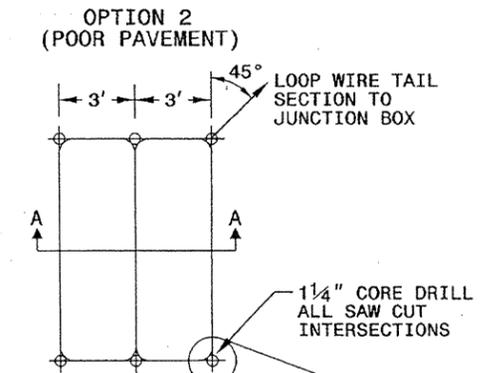
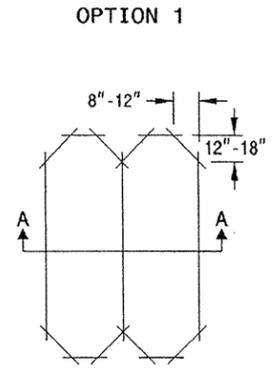


NOTES

- OVERLAP SAW CUTS AT CORNERS AND INTERSECTION POINTS TO ENSURE UNIFORM SAW SLOT DEPTH.
- MAINTAIN 12" SPACING BETWEEN LOOP WIRE TAIL SECTIONS.
- WIRE LOOPS CONNECTED TO THE SAME DETECTOR CHANNEL IN SERIES.
- LOCATE LOOPS IN CENTER OF LANES UNLESS OTHERWISE SHOWN ON PLANS OR APPROVED BY ENGINEER.

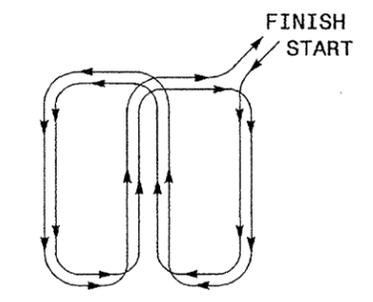
QUADRUPOLE LOOP

SAW CUT OPTIONS



DEPTH IS 2.5" FOR CONCRETE AND 3.0" FOR ASPHALT

LOOP WINDING METHOD



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ENGLISH DETAIL DRAWING FOR
INDUCTIVE DETECTION LOOPS

SHEET 1 OF 3
1725D01

See Plate for Title

Prepared in the Offices of:

750 N. Greenfield Parkway
Garner, NC 27529

SEAL

Milton Dean 11/24/08
SIGNATURE DATE

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DIVISION OF HIGHWAYS
RALEIGH, N.C.

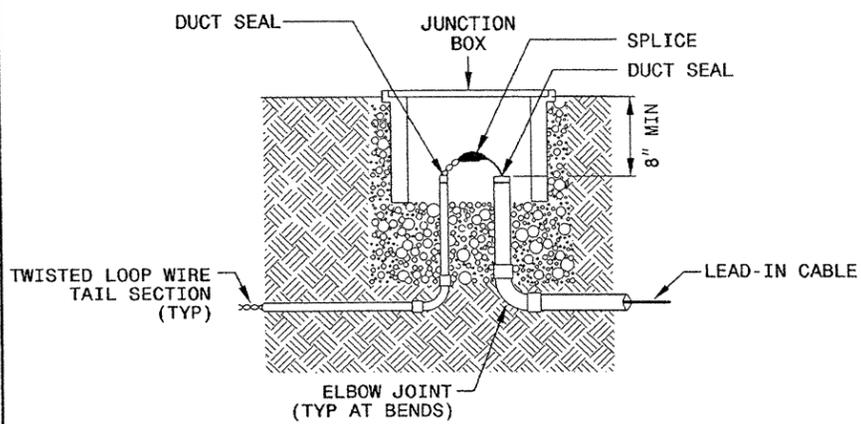
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ENGLISH DETAIL DRAWING FOR
INDUCTIVE DETECTION LOOPS
LOOP WIRE DETAILS

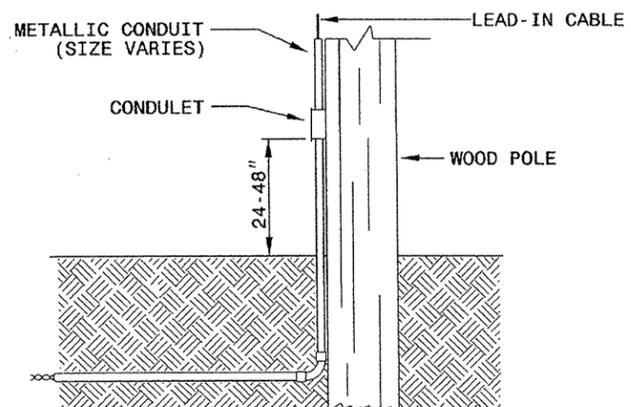
SHEET 2 OF 3
1725D01

LOOP WIRE SPLICE POINT DETAILS

LOOP WIRE AT JUNCTION BOX



LOOP WIRE AT POLE

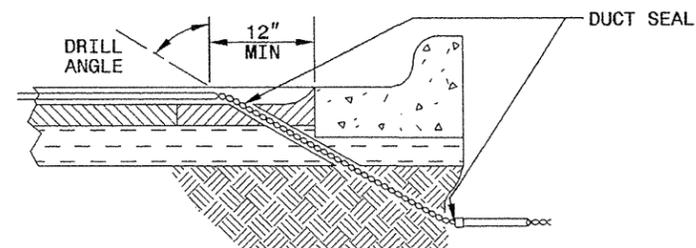


NOTE

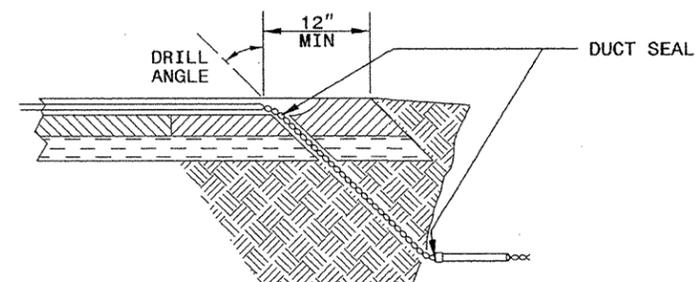
SPLICE ALL LOOP WIRE TAIL SECTIONS/LEAD-IN CABLE IN JUNCTION BOXES OR APPROVED CONDULETS.

LOOP WIRE PAVEMENT EDGE DETAILS

LOOP WIRE AT CURB & GUTTER SECTION



LOOP WIRE AT PAVEMENT SECTION



NOTES

1. DO NOT EXCAVATE UNDER CURB AND GUTTER SECTIONS FOR CONDUIT INSTALLATION.
2. TWIST LOOP WIRE TAIL SECTIONS FROM WHERE LOOP WIRE TAIL LEAVES SAW CUT TO JUNCTION BOX, INCLUDING THROUGH CONDUIT.
3. BEFORE SEALING LOOPS, INSTALL DUCT SEAL WHERE LOOP WIRE TAIL SECTION LEAVES SAW CUT IN PAVEMENT AND AT ENTRANCE OF CONDUIT TO JUNCTION BOX.

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ENGLISH DETAIL DRAWING FOR
INDUCTIVE DETECTION LOOPS
LOOP WIRE DETAILS

SHEET 2 OF 3
1725D01

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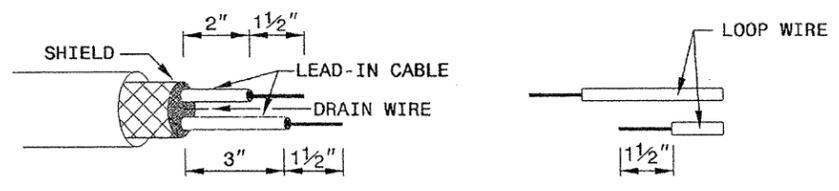
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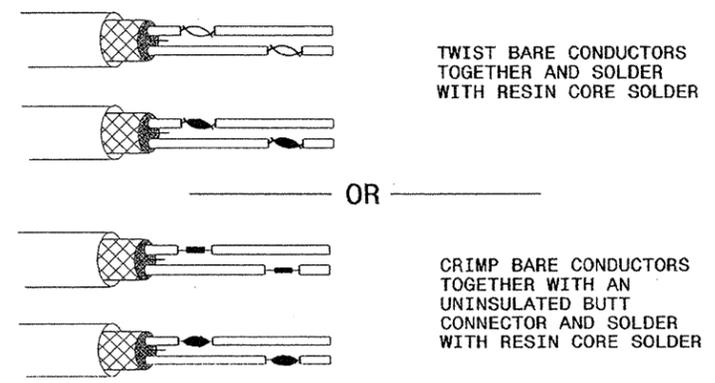
ENGLISH DETAIL DRAWING FOR
INDUCTIVE DETECTION LOOPS
 SPLICING FOR LEAD-IN CABLE AND LOOP WIRE

SHEET 3 OF 3
1725D01

STEP 1. STRIP LOOP WIRE AND LEAD-IN CABLE

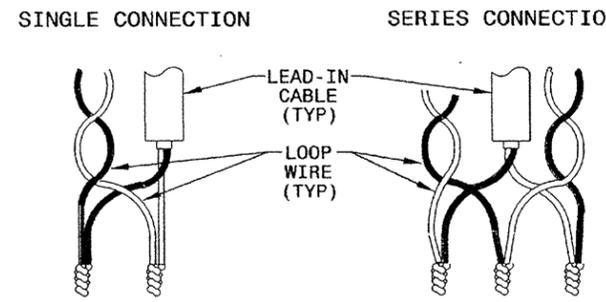


STEP 2. CONNECT AND SOLDER

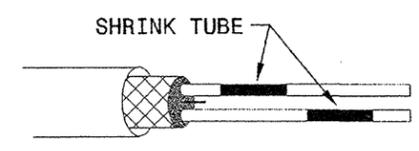


BOND SHIELD DRAIN WIRE AT SPLICE SECTIONS (DO NOT GROUND)

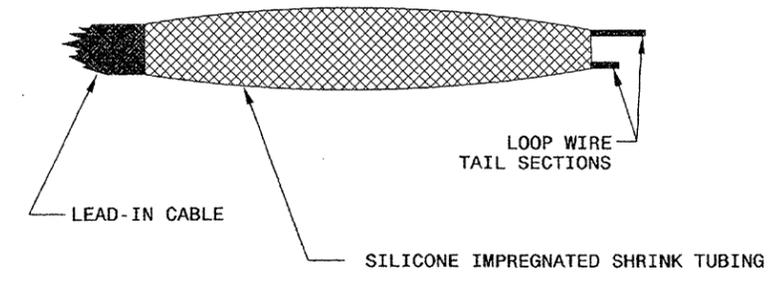
LOOP WIRE AND LEAD-IN CABLE CONNECTION DETAILS



STEP 3. INSULATE EACH SOLDER JOINT SEPARATELY



STEP 4. ENVIRONMENTALLY PROTECT SPLICE



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ENGLISH DETAIL DRAWING FOR
INDUCTIVE DETECTION LOOPS
 SPLICING FOR LEAD-IN CABLE AND LOOP WIRE

SHEET 3 OF 3
1725D01

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