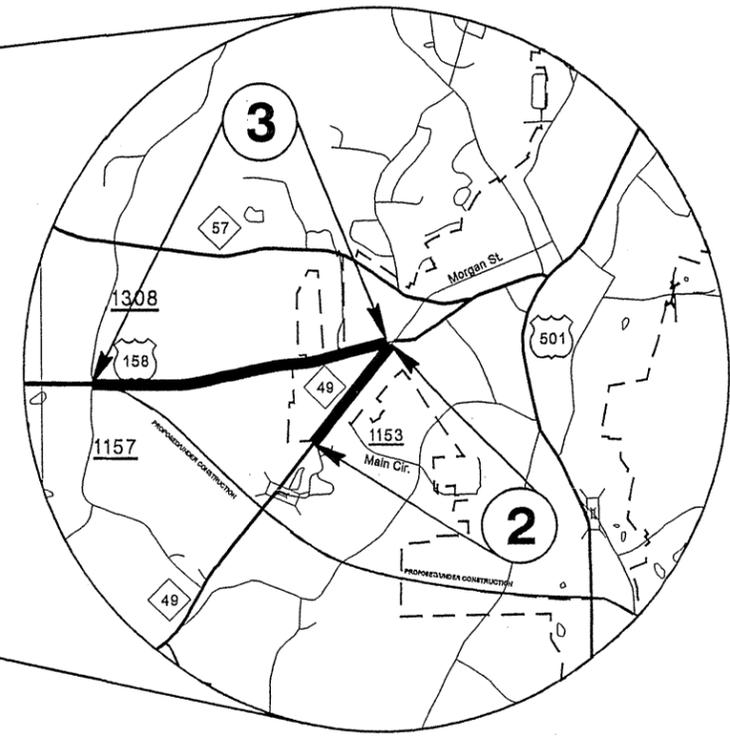
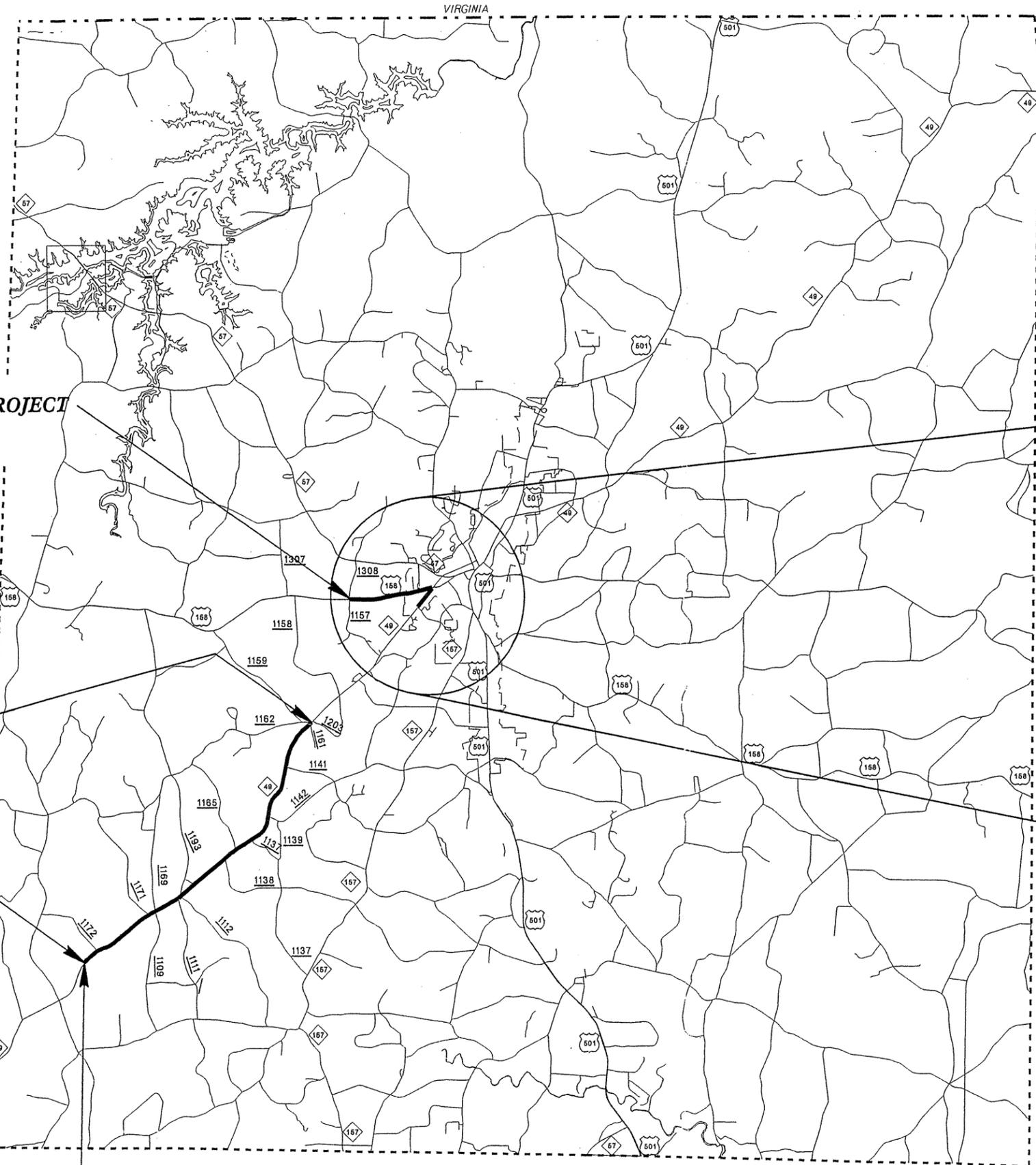


END PROJECT
R-5167A

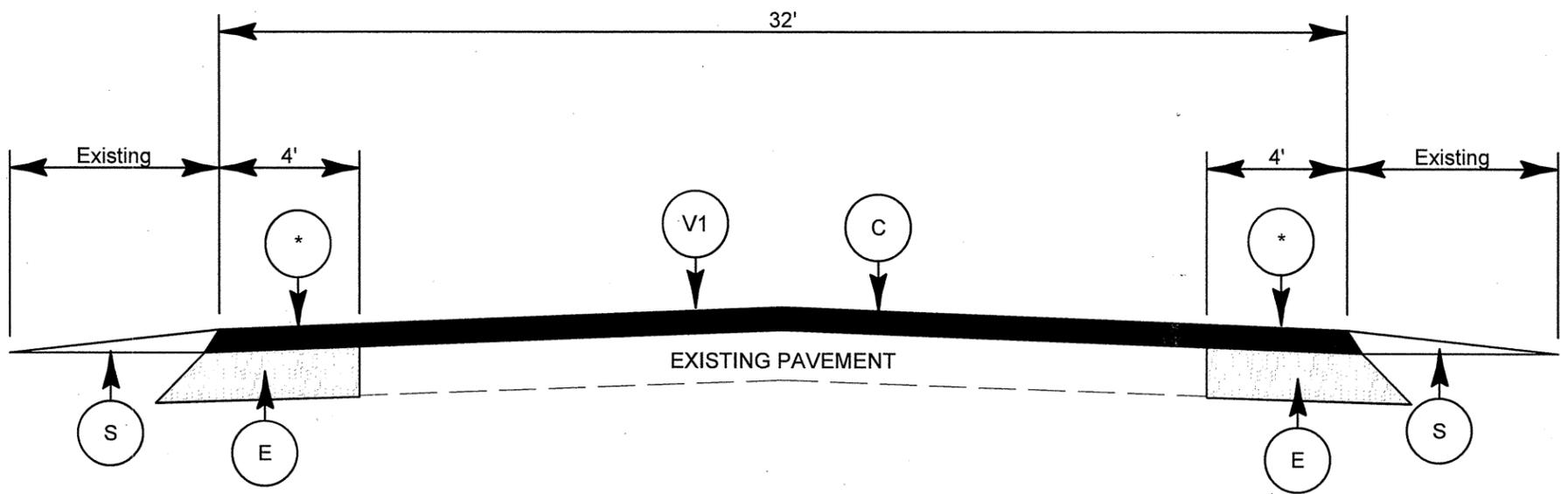
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BEGIN PROJECT
R-5167A

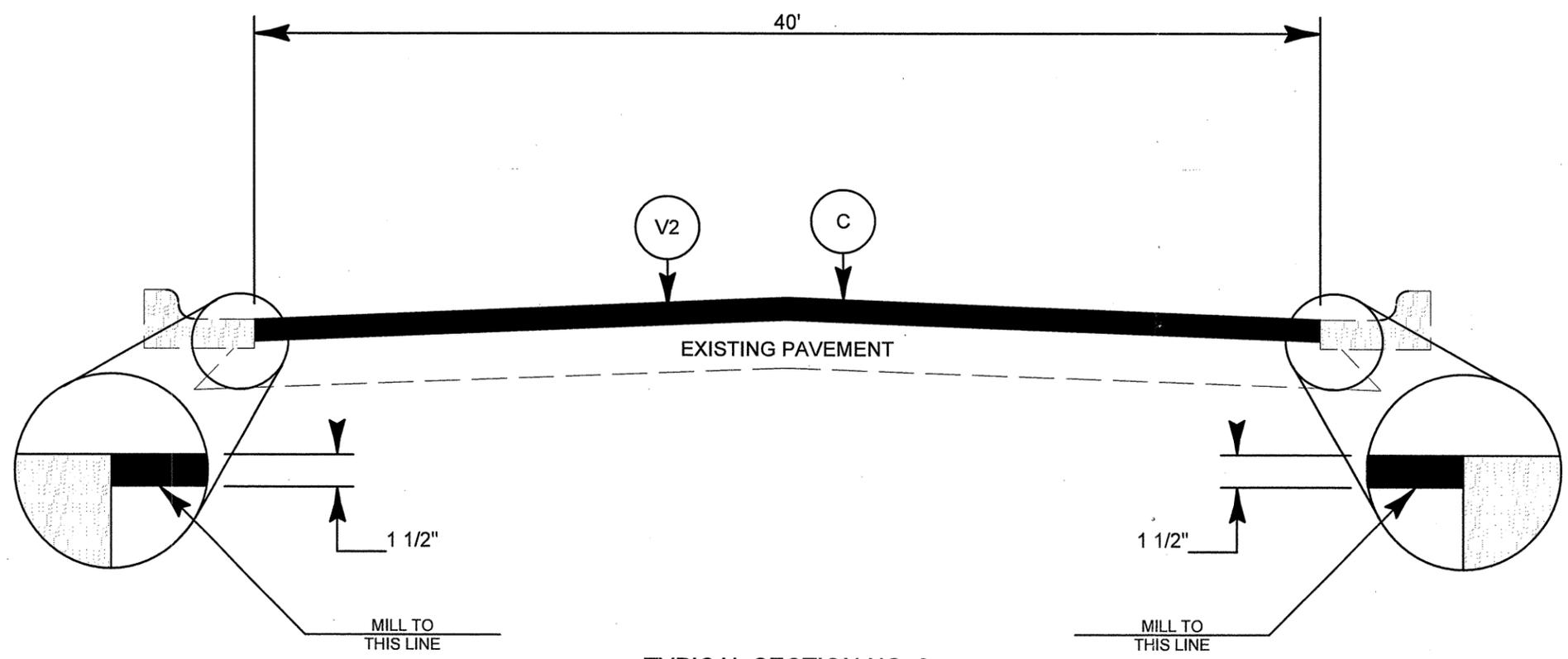
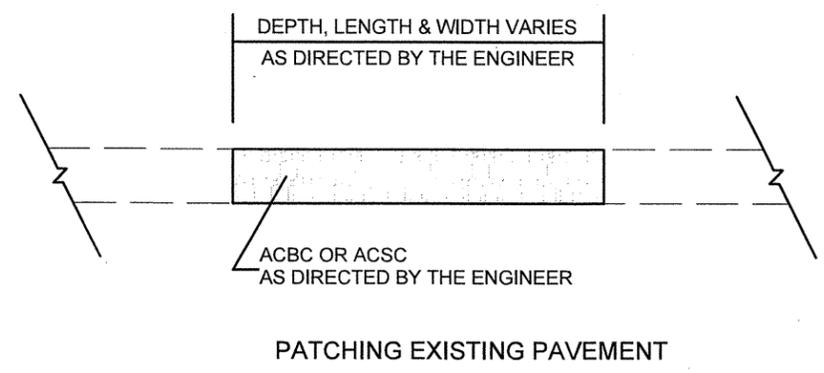


R-5167A
NC 49 and US 158
PERSON COUNTY

PROJECT NO. 45161.3.ST1 R-5167A	SHEET NO. 2	TOTAL SHEETS
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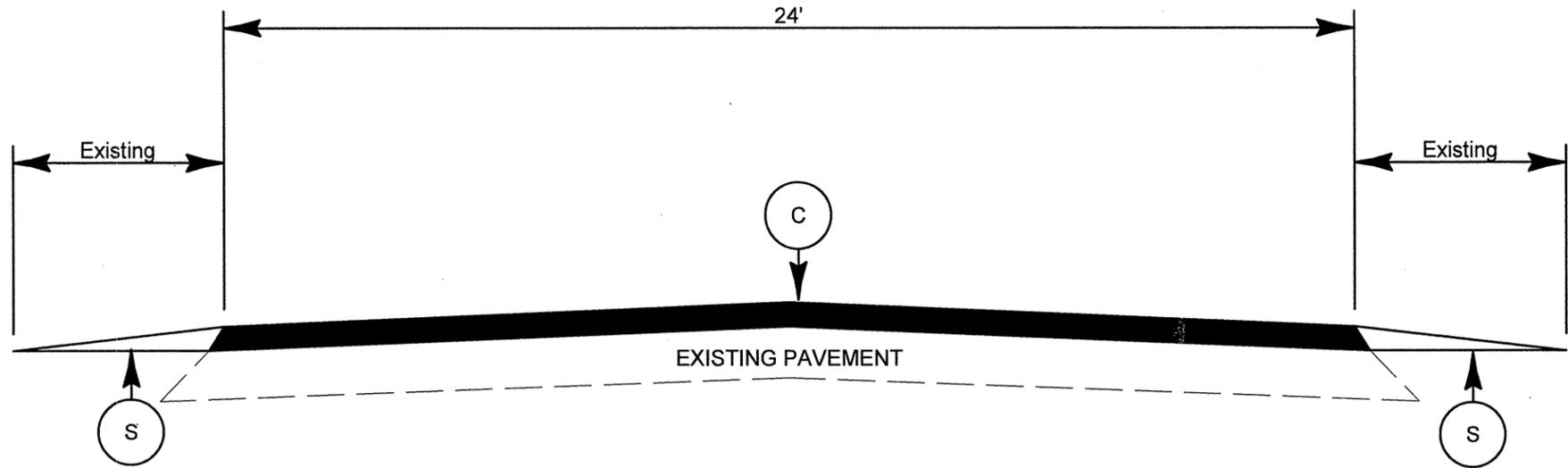
TYPICAL SECTION NO. 1



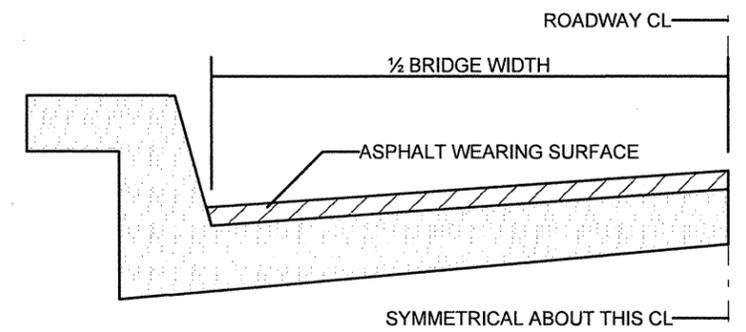
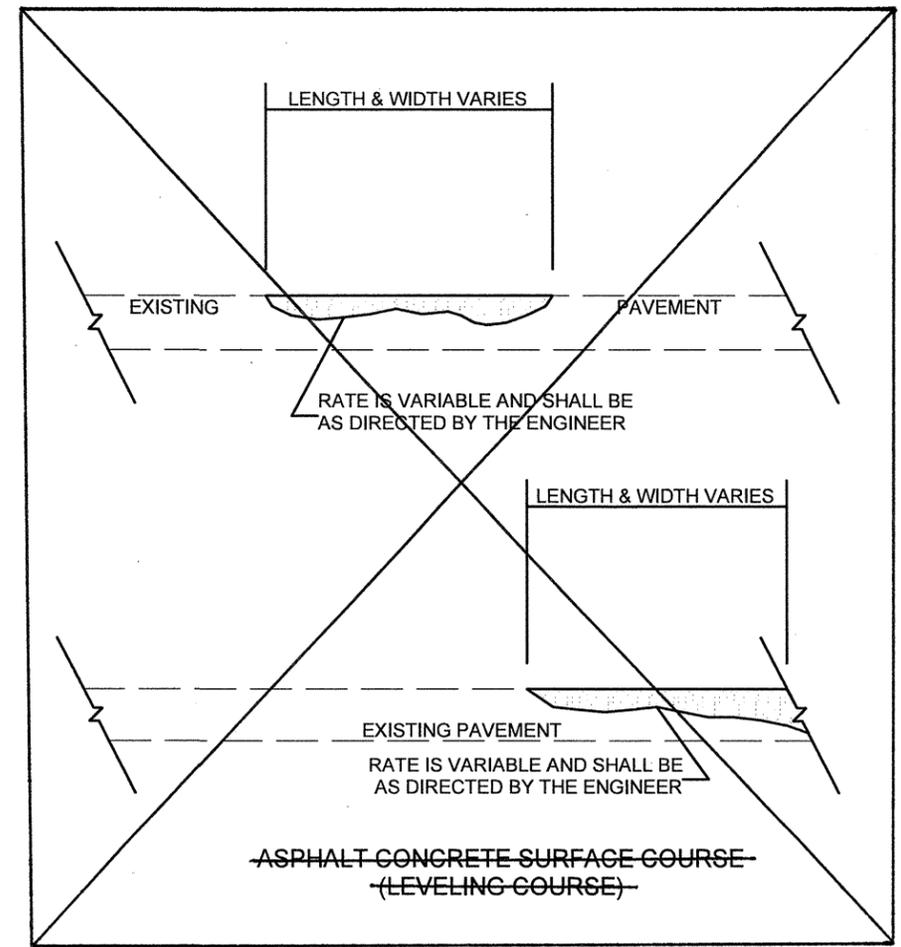
TYPICAL SECTION NO. 2

PAVEMENT SCHEDULE	
*	EXISTING PAVED SHOULDER REMOVAL INCIDENTAL TO B25.0B
C	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT A RATE OF 168 LBS/SY
E	PROP. APPROX 5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B AT AN AVERAGE RATE OF 570 LBS/SY
S	SHOULDER RECONSTRUCTION TO BE PERFORMED BY THE CONTRACTOR
V1	PROP. 1 1/2" DEPTH MILLING
V2	PROP. 2 1/2" DEPTH MILLING (CONTRACTOR RESPONSIBLE FOR REMOVING EXISTING PAVEMENT FROM GUTTER)

PROJECT NO. 45161.3.ST1 R-5167A	SHEET NO. 3	TOTAL SHEETS
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TYPICAL SECTION NO. 3



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1/2", SF9.5A 1.0", S9.5X 1.5", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2". THE MAXIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1.0", SF9.5A 1.5", S9.5X 2.0", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2".

NOTES

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.
 ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.
 EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.
 SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.
 BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

PROJECT NO.	SHEET NO.	TOTAL NO.
R-5167A 45161.3.ST1	4	

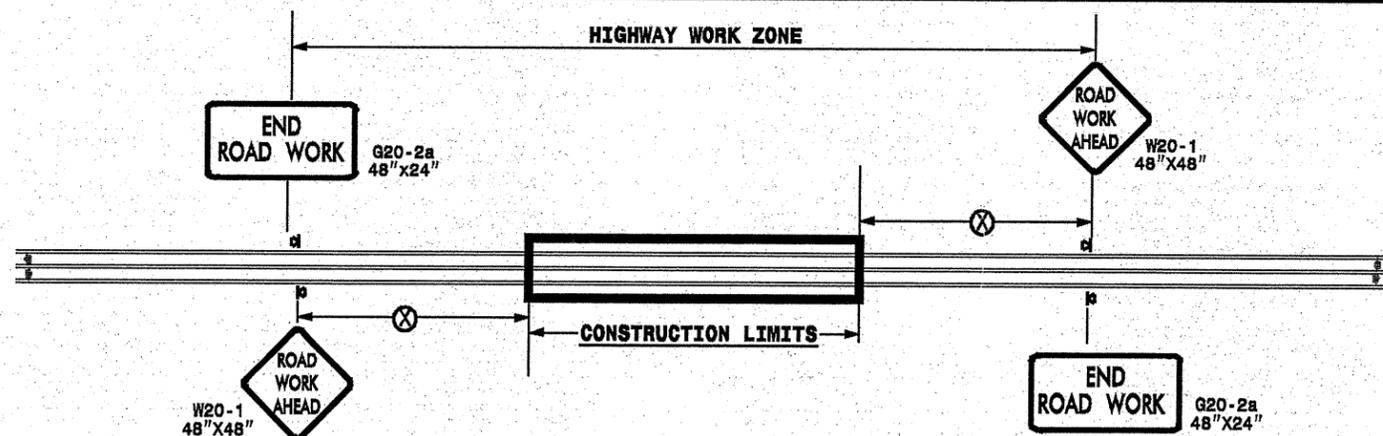
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	1 1/2" MILLING SY	2 1/2" MILLING SY	INCIDENTAL MILLING SY	BASE COURSE, B25.0B TONS	SURFACE COURSE, S9.5B TONS	PG 64-22 PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	SEED & MULCHING AC
R-5167A 45161.3.ST1	Person	1	NC 49	FROM 0.35 MI S OF SR 1172 (SALEM CHURCH) TO SR 1162 (HESTER'S STORE)	1	NO	6.54	32	40	13.08	92,083			9,296	10,561	1,033	20	9.51
TOTAL FOR MAP NO. 1							6.54		40		92,083			9,296	10,561	1,033	20	9.51
		2	NC 49	FROM SCL OF ROXBORO TO JOINT S OF US 158	2	NO	0.47	40				11,029			957	57		
TOTAL FOR MAP NO. 2							0.47					11,029			957	57		
		3	US 158	FROM JOINT AT NC 49 TO SR 1157 (DEE LONG RD)	3	NO	1.62	24	80	3.24			200		1,926	116	150	2.35
TOTAL FOR MAP NO. 3							1.62		80	3.24			200		1,926	116	150	2.35
TOTAL FOR PROJ. R-5167A							8.63		120	16.32	92,083	11,029	200	9,296	13,444	1,206	170	11.86
GRAND TOTAL							8.63		120	16.32	92,083	11,029	200	9,296	13,444	1,206	170	11.86

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4589000000-N	4685000000-E	4686000000-E		4725000000-E	4905000000-N
					TRAFFIC CONTROL LS	4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	4" X 120 M WHITE THERMO LF	THERMO STR & RT ARROW 90 M EA	SNOW PLOWABLE MARKERS EA
R-5167A 45161.3.ST1	Person	1	NC 49	FROM 0.35 MI S OF SR 1172 (SALEM CHURCH) TO SR 1162 (HESTER'S STORE)	1	70,370	43,164			432
TOTAL FOR MAP NO. 1					1	70,370	43,164			432
		2	NC 49	FROM SCL OF ROXBORO TO JOINT S OF US 158			4,963	400		93
TOTAL FOR MAP NO. 2							4,963	400		93
		3	US 158	FROM JOINT AT NC 49 TO SR 1157 (DEE LONG RD)		17,431	12,830		1	107
TOTAL FOR MAP NO. 3						17,431	12,830		1	107
TOTAL FOR PROJ NO. 45161.3.ST1					1	87,802	60,957	400	1	632
GRAND TOTAL					1	87,802	60,957	400	1	632

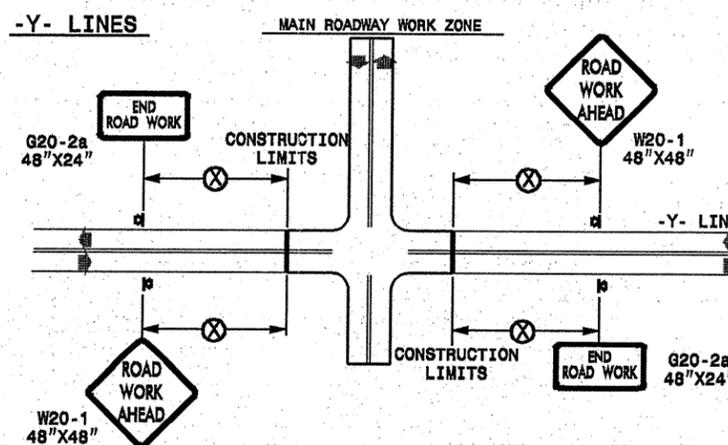
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING
 FOR TWO-WAY UNDIVIDED
 WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND	
⊗	PORTABLE SIGN
→	DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED ADVANCED WORK ZONE WARNING SIGNS	
SEAL	SCALE: NONE	REVISIONS	
	DATE: _____	7-98	10/01
	DWG. BY: _____	10-98	03/04
	DESIGN BY: _____	01/01	11/04
REVIEWED BY: _____			

17-SEP-2009 10:34
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