

VICINITY MAP

6

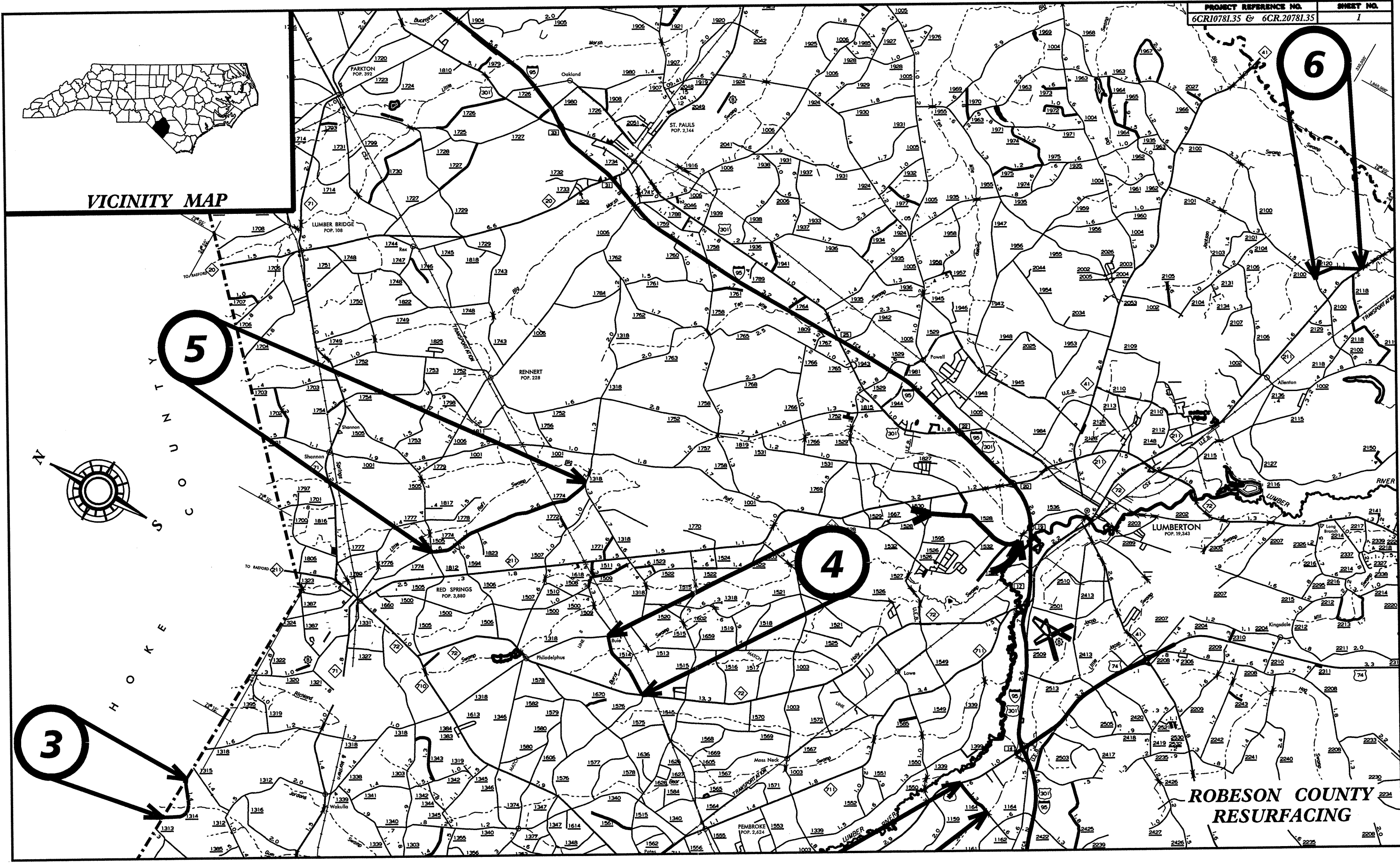
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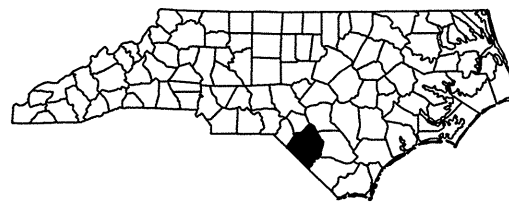
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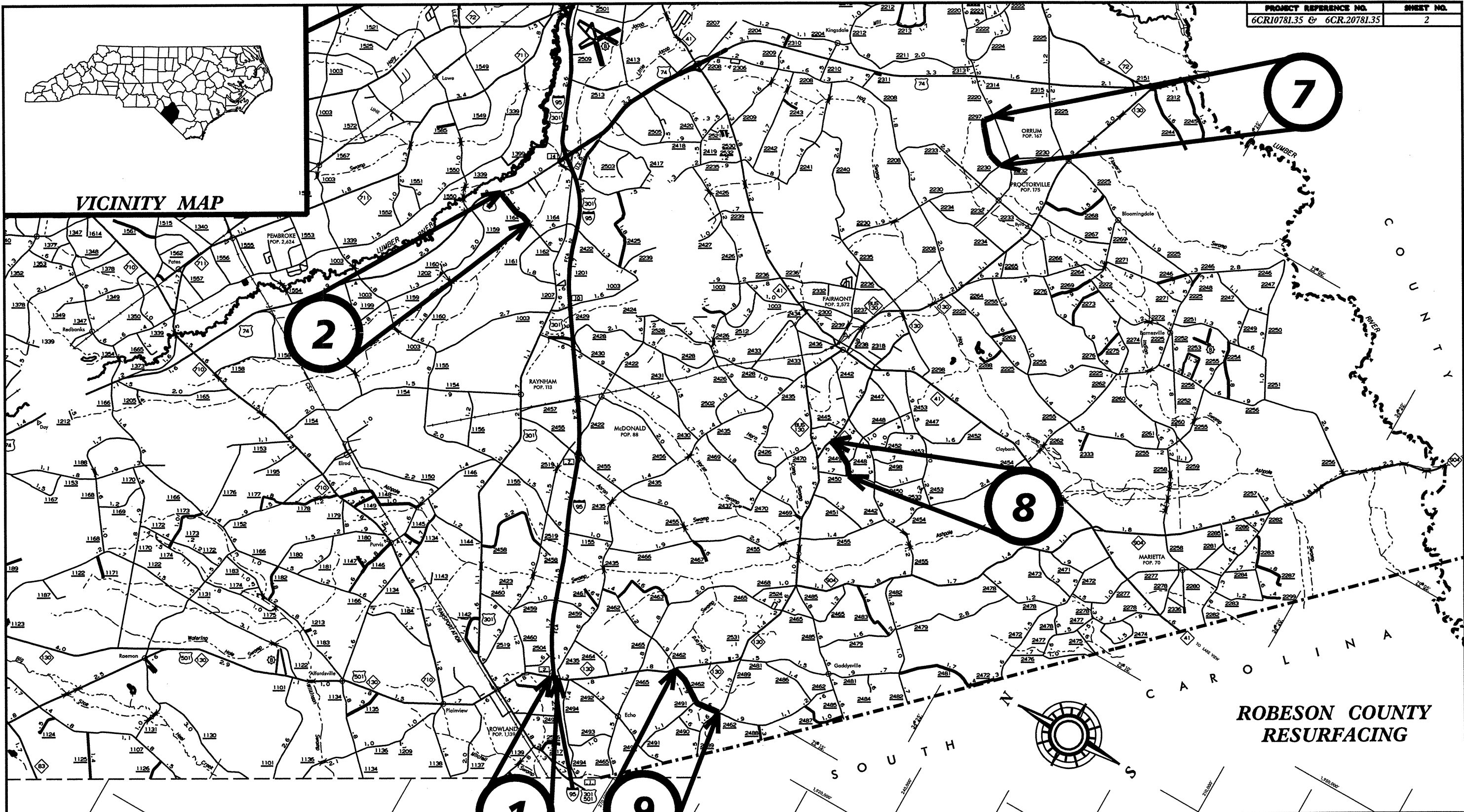
H O K E
S O C
I E T Y

ROBESON COUNTY
RESURFACING



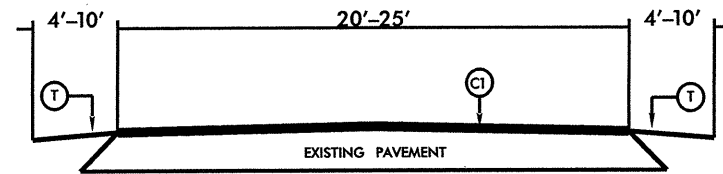


VICINITY MAP

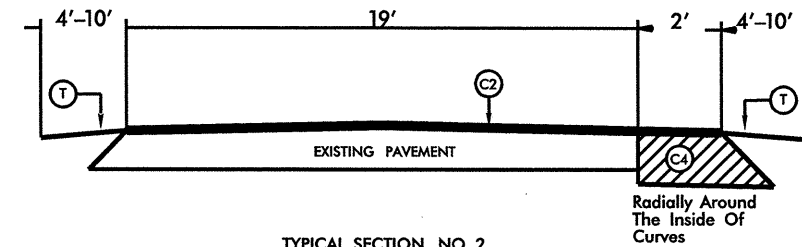


ROBESON COUNTY
RESURFACING

1 9

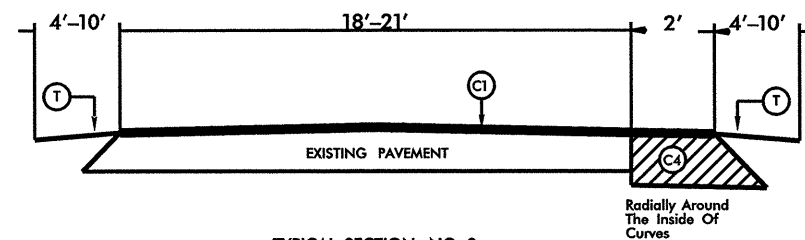


TYPICAL SECTION NO. 1



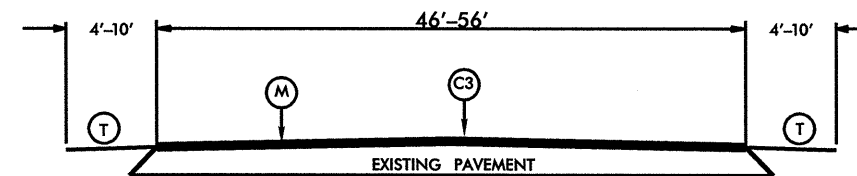
TYPICAL SECTION NO. 2

NOTE: Includes 2 Ft. widening of the inside radius of all curves, or as directed by the Engineer. See DETAIL 1

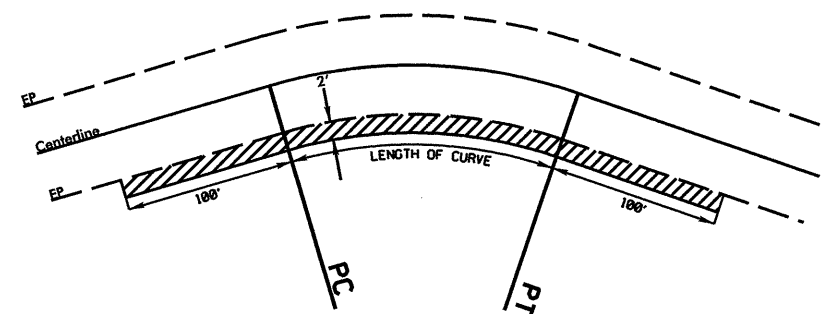


TYPICAL SECTION NO. 3

NOTE: Includes 2 Ft. widening of the inside radius of all curves, or as directed by the Engineer. See INSIDE CURVE WIDENING DETAIL 1



TYPICAL SECTION NO. 4



DETAIL 1
INSIDE CURVE WIDENING

NOTE: 2 Ft. widening of inside radius of curves, as directed by the Engineer



DETAIL 2
MILLING APPROACHES

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 138 LBS. PER SQ. YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C3	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C4	PROP. APPROX. 5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 570 LBS. PER SQ. YD.
M	MILLING BITUMINOUS PAVEMENT 1 1/2" IN DEPTH ACROSS ENTIRE SECTION.
M1	MILLING BITUMINOUS PAVEMENT 0-1 1/4" VARIABLE DEPTH ACROSS ENTIRE SECTION
T	SHOULDER RECONSTRUCTION WILL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.

MILLING SHALL BE PERFORMED AT BRIDGES AND RAILROAD APPROACHES AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH DETAIL 2.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



PROJECT NO.	SHEET NO.	TOTAL NO.
6CR.10781.35, 6CR.20781.35	4	

SUMMARY OF QUANTITIES

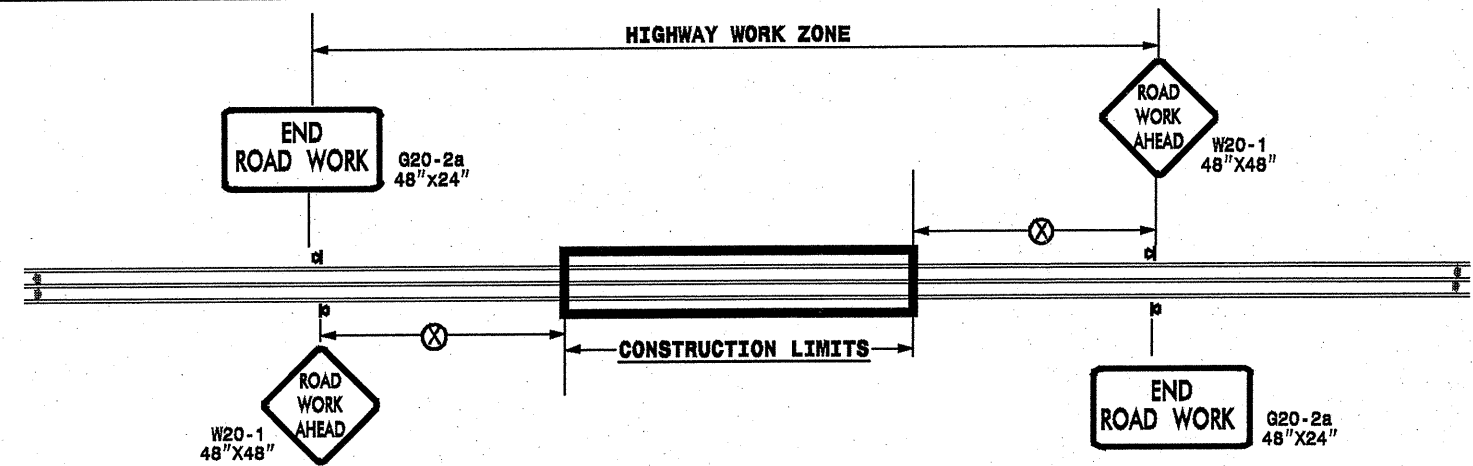
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	SHOULDER CONSTRUCTION SMI	SHOULDER RECONSTRUCTION SMI	1½" MILLING SY	0" TO 1.25" MILLING SY	INTERMEDIATE COURSE, 119.0B TONS	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, SF9.5A TONS	PG 64-22 PLANT MIX TONS	SEED & MULCHING AC
6CR.10781.35	Robeson	1	NC 130	FROM CONST. JOINT WEST OF I-95 BRIDGE TO CONST. JOINT EAST OF I-95 BRIDGE	4	NO	0.54	46-56	13		1.08	15746			1,323		79	1.30
TOTAL FOR MAP NO. 1							0.54		13		1.08	15746			1,323		79	1.30
TOTAL FOR PROJ NO. 6CR.10781.35							0.54		13		1.08	15746			1,323		79	1.30
6CR.20781.35	Robeson	2	SR 1164	FROM US 74 TO THE PROPOSED I-74 PROJECT LIMITS	3	NO	0.8	20	19		1.6		117	30		648	44	1.94
		"	"	FROM A POINT 0.01 MILE WEST OF BRIDGE (#172) @ THE PROPOSED I-74 PROJECT LIMITS TO BRIDGE (#172) @ THE PROPOSED I-74 PROJECT LIMITS	1	NO	0.1	24	2		0.2					97	6	0.24
TOTAL FOR MAP NO. 2							0.9		21	0	1.8	0	117	30		745	50	2.18
		3	SR 1314	FROM HOKE COUNTY TO HOKE COUNTY	1	NO	1.4	20	34		2.8					1,133	74	3.39
TOTAL FOR MAP NO. 3							1.4		34	0	2.8	0	0	0		1,133	74	3.39
		4	SR 1514	FROM NC 72 TO SR 1318	1	NO	1.58	20	38		3.16					1,279	83	3.83
TOTAL FOR MAP NO. 4							1.58		38	0	3.16	0	0	0		1,279	83	3.83
		5	SR 1774	FROM SR 1318 TO SR 1505	3	NO	4	20	96		8			134		3,238	217	9.70
TOTAL FOR MAP NO. 5							4		96	0	8	0	0	134		3,238	217	9.70
		6	SR 2120	FROM SR 2100 TO NC 211	3	NO	1.15	21	28		2.3			38		978	65	2.79
TOTAL FOR MAP NO. 6							1.15		28	0	2.3	0	0	38		978	65	2.79
		7	SR 2297	FROM SR 2230 TO SR 2220	3	NO	1.3	20	31		2.6			44		1,052	70	3.15
TOTAL FOR MAP NO. 7							1.3		31	0	2.6	0	0	44		1,052	70	3.15
		8	SR 2449	FROM NC 130 TO SR 2450	2	NO	0.9	19	22		1.8			30		828	55	2.18
TOTAL FOR MAP NO. 8							0.9		22	0	1.8	0	0	30		828	55	2.18
		9	SR 2462	FROM NC 130 TO SR 2489	3	NO	1.42	18	34		2.84			48		1,045	70	3.44
TOTAL FOR MAP NO. 9							1.42		34	0	2.84	0	0	48		1,045	70	3.44
TOTAL FOR PROJ NO. 6CR.20781.35							12.65		304	0	25.3	0	117	324	0	10,298	684	30.66
GRAND TOTAL							13.19		317	0	26.38	15746	117	324	1,323	10,298	763	31.96

PROJECT NO.	SHEET NO.	TOTAL NO.
6CR.10781.35, 6CR.20781.35	5	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4685000000-E	4686000000-E	4705000000-E	4710000000-E	4721000000-E	4810000000-E		4900000000-N	
					4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	16" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO RXR 120 M EA	4" WHITE PAINT LF	4" YELLOW PAINT LF	YELLOW & YELLOW MARKERS EA	CYAN & RED MARKERS EA
6CR.10781.35	Robeson	1	NC 130	FROM CONST. JOINT WEST OF I-95 BRIDGE TO CONST. JOINT EAST OF I-95 BRIDGE	6,000	6,000				6,000	6,000	40	20
TOTAL FOR MAP NO. 1					6,000	6,000				6,000	6,000	40	20
TOTAL FOR PROJ NO. 6CR.10781.35					6,000	6,000				6,000	6,000	40	20
										12,000	60		
6CR.20781.35	Robeson	2	SR 1164	FROM US 74 TO THE PROPOSED I-74 PROJECT LIMITS						19,200	15,400		
		"	"	FROM A POINT 0.01 MILE WEST OF BRIDGE (#172) @ THE PROPOSED I-74 PROJECT LIMITS TO BRIDGE (#172) @ THE PROPOSED I-74 PROJECT LIMITS									
TOTAL FOR MAP NO. 2										19,200	15,400		
		3	SR 1314	FROM HOKE COUNTY TO HOKE COUNTY						30,000	24,000		
TOTAL FOR MAP NO. 3										30,000	24,000		
		4	SR 1514	FROM NC 72 TO SR 1318						34,000	28,900		
TOTAL FOR MAP NO. 4										34,000	28,900		
		5	SR 1774	FROM SR 1318 TO SR 1505			100	60	4	86,000	73,100		
TOTAL FOR MAP NO. 5							100	60	4	86,000	73,100		
		6	SR 2120	FROM SR 2100 TO NC 211						24,400	20,740		
TOTAL FOR MAP NO. 6										24,400	20,740		
		7	SR 2297	FROM SR 2230 TO SR 2220						28,000	23,800		
TOTAL FOR MAP NO. 7										28,000	23,800		
		8	SR 2449	FROM NC 130 TO SR 2450						19,600	16,660		
TOTAL FOR MAP NO. 8										19,600	16,660		
		9	SR 2462	FROM NC 130 TO SR 2489						30,400	25,840		
TOTAL FOR MAP NO. 9										30,400	25,840		
TOTAL FOR PROJ NO. 6CR.20781.35							100	60	4	271,600	228,440		
										500,040			
GRAND TOTAL					6,000	6,000	100	60	4	277,600	234,440	40	20
										512,040		60	

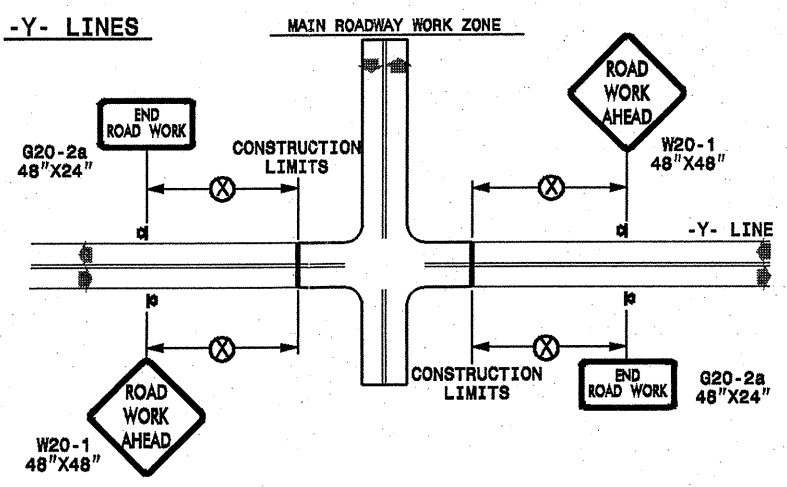
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING
FOR TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS

GENERAL NOTES

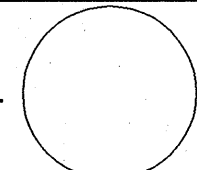

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

◀ PORTABLE SIGN

➡ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED ADVANCED WORK ZONE WARNING SIGNS		
				
SCALE: NONE		REVISIONS		
DATE: 7-98		10/01		
DWG. BY:		10-98	03/04	
DESIGN BY:		01/01	11/04	

24-JUN-2009 18:26
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 psey@more AT WZTC21602