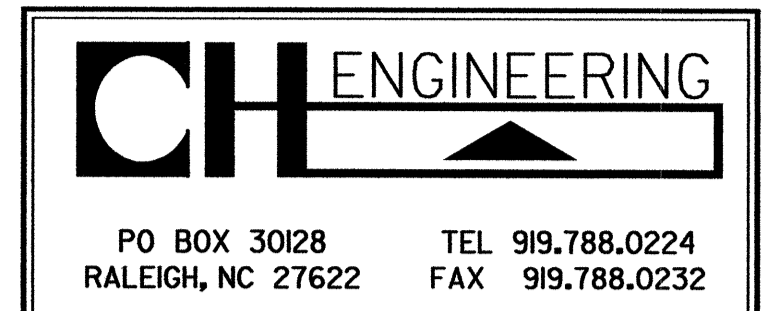


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
EDGEcombe COUNTY**

STATE PROJECT REFERENCE NO. **B-2965** SHEET NO. **TCP-1**



B-2965

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS
TCP-1A	TEMPORARY PAVEMENT MARKING SCHEDULE
TCP-2	PROJECT NOTES
TCP-3	PHASING
TCP-4	PHASE I OVERVIEW
TCP-5 THRU TCP-7	PHASE I DETAILS
TCP-8	PHASE II OVERVIEW
TCP-9 THRU TCP-12	PHASE II DETAILS
TCP-13 THRU TCP-15	PHASE III DETAILS
TCP-16 THRU TCP-17	PHASE IV DETAILS
TCP-18	TRUCK DETOUR ROUTE AND SIGNING
TCP-19	TEMPORARY OFFSITE DETOUR
TCP-20	DETAIL DRAWING FOR TWO-WAY UNDIVIDED & URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS
TCP-21	TEMPORARY SHORING DATA
PM-1 THRU PM-4	FINAL PAVEMENT MARKING SCHEDULE AND FINAL PAVEMENT MARKING PLANS
SD-1	SIGN DESIGN

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
 - TYPE II BARRICADE
 - TYPE III BARRICADE
 - CONE
 - DRUM SKINNY DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
 - YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - PAVEMENT MARKING SYMBOLS

TIP PROJECT:

PLAN REVIEWED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT

J. S. Bourne, PE TRAFFIC CONTROL ENGINEER
J. Ishak, PE TRAFFIC CONTROL PROJECT ENGINEER
H. A. Lawandos TRAFFIC CONTROL PROJ. DESIGN ENGINEER
 _____ TRAFFIC CONTROL DESIGN ENGINEER

APPROVED: Pranda S. Early
DATE: 8-18-08

SEAL

PLAN PREPARED FOR NCDOT BY:

T. R. Hepler, PE QC ENGINEER
R. B. Early, PE PROJECT ENGINEER
J. A. Phillips PROJECT DESIGNER



TEMPORARY PAVEMENT MARKING SCHEDULE

SYMBOL	DESCRIPTION	PAY ITEM/ QUANTITY BREAKDOWN PAVEMENT MARKING LINES	TOTAL QUANTITY
		PAINT (4")	
PA	WHITE EDGELINE (2X)	15840 LF	
PD	2 FT WHITE MINISKIP (2X)	208 LF	
PE	WHITE SOLID LANE LINE (2X)	1914 LF	
PF	YELLOW SKIP (2X)	1136 LF	
PH	YELLOW SINGLE CENTER LINE (2X)	7440 LF	
PI	YELLOW DOUBLE CENTER LINE (2X)	23100 LF	
			49638 LF
		PAINT (8")	
PV	YELLOW DIAGONAL (2X)	150 LF	
			150 LF
		PAINT (24")	
P4	WHITE STOPBAR (2X)	466 LF	
P5	WHITE CROSSWALK LINE (2X)	1280 LF	
			1746 LF
		PAINT MARKING SYMBOLS	
QA	LEFT TURN ARROW (2X)	38 EA	
QB	RIGHT TURN ARROW (2X)	4 EA	
QC	STRAIGHT ARROW (2X)	4 EA	
QE	STRAIGHT / RIGHT COMBO (2X)	10 EA	
			56 EA
		TEMPORARY RAISED PAVEMENT MARKERS	
MK	YELLOW & YELLOW	74 EA	
MI	CRYSTAL & RED	139 EA	
			213 EA

NOTE: FOR EACH PAINT PAVEMENT MARKING ITEM, 1X IMPLIES A SINGLE APPLICATION, 2X IMPLIES TWO APPLICATIONS, AND 3X IMPLIES THREE APPLICATIONS.

REVISIONS

8/18/2008 Y:\NCDDOT\B2965\Traffic\TrafficControl\TCP\B2965-fe_TCP01A.dgn USERNAME

CH ENGINEERING
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 RALEIGH, NC 27622 FAX 919.788.0232

APPROVED: <i>Ronda B. Early</i> DATE: 8-18-08	TEMPORARY PAVEMENT MARKING SCHEDULE											
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PROJECT NOTES

PROJ. REFERENCE NO. B-2965	SHEET NO. TCP-2
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GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

- A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:
- | | |
|--|--------------------------------------|
| <u>ROAD NAME</u> | <u>DAY AND TIME RESTRICTIONS</u> |
| -L- (US 64 BUSINESS/NC 33),
-Y1-, -Y2-, -Y3- & -Y4- | 7:30 AM - 8:30 AM
MONDAY - FRIDAY |

- B) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- C) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- G) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- H) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- J) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) IN ADVANCE AND A MINIMUM OF EVERY 500' THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- L) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- M) PROVIDE PERMANENT SIGNING.
- N) PROVIDE DETOUR SIGNING WITHIN THE PROJECT LIMITS.
- O) CONTRACTOR WILL COVER OR REMOVE ALL DETOUR SIGNS WITHIN THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.
- P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- Q) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 200' IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC CONTROL DEVICES

- R) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED, REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.
- S) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- T) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 200 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- U) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
US 64 BUSINESS/NC 33/ SOUTH MAIN STREET (-L-)	POLYUREA	RAISED REFLECTIVE
-Y- LINES	POLYUREA	RAISED REFLECTIVE

- V) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
US 64 BUSINESS/NC33/ SOUTH MAIN STREET (-L-)	PAINT	RAISED REFLECTIVE
-Y- LINES	PAINT	RAISED REFLECTIVE

- W) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.
- X) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- Y) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.
- Z) TRACE THE EXISTING AND PROPOSED MONOLITHIC ISLAND LOCATIONS WITH PROPER COLOR PAVEMENT MARKINGS PRIOR TO REMOVAL AND INSTALLATION. PLACE DRUMS TO DELINEATE ANY EXISTING AND PROPOSED MONOLITHIC ISLANDS AFTER REMOVAL AND BEFORE INSTALLATION.

TEMPORARY / FINAL SIGNALS

- AA) NOTIFY THE ENGINEER TWO (2) MONTHS BEFORE A TRAFFIC SIGNAL INSTALLATION BY OTHERS IS REQUIRED.
- BB) SHIFT AND REVISE ALL SIGNAL HEADS AS SHOWN ON THE SIGNAL PLAN.

MISCELLANEOUS

- CC) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.
- DD) CONTRACTOR SHALL MAINTAIN PEDESTRIAN TRAFFIC IN ACCORDANCE WITH THE MUTCD, TRAFFIC CONTROL PLAN AND AS DIRECTED BY THE ENGINEER.

LOCAL NOTE

- 1) USING TYPE III BARRICADES, CLOSE ALBEMARLE STREET ENTRANCE TO RIVERVIEW PARK FOR DURATION OF PROJECT UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- 2) SAFE UP DROP-OFFS WITH A 6:1 SLOPE.
- 3) DO NOT CLOSE BOTH SIDEWALK ON BOTH SIDES OF -L- (MAIN ST.) AT THE SAME TIME.

REVISIONS

8/18/2008 Y:\NCDOT\B2965\TrafficControl\TCP\B2965-Fe-TCP02.dgn
USER:NAME

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APPROVED: *Ronda B. Early* DATE: 8-18-08

SEAL

PROJECT NOTES

SCALE: NONE	REVISIONS
DATE: 08/03/08	
DWG. BY: JAP	
DESIGN BY: JAP	
REVIEWED BY: RBE	

NOTE:

USE INCIDENTAL STONE AS NECESSARY, MAINTAIN VEHICULAR ACCESS TO ALL DRIVEWAYS DURING THE LIFE OF THE CONTRACT UNLESS OTHERWISE NOTED IN THE PHASING OR DIRECTED BY THE ENGINEER.

COORDINATE WITH THE UTILITY CONTRACTOR AND WITH THE ENGINEER IN CHARGE TO PROVIDE ANY TRAFFIC CONTROL NEEDED TO PERFORM THE PROPOSED UTILITY CONSTRUCTION ON THIS PROJECT.

RETURN TRAFFIC TO THE EXISTING PATTERN AT THE END OF EACH WORK DAY UNLESS OTHERWISE STATED IN THE PHASING OR DIRECTED BY THE ENGINEER.

COMPLETE ANY PROPOSED OR TEMPORARY WIDENING IN SUCH A MANNER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE.

PHASE I

STEP 1: PRIOR TO ANY CONSTRUCTION OPERATIONS, PLACE WORK ZONE ADVANCED WARNING SIGNS ON -L- (US 64 BUS./ NC 33/ S. MAIN ST.) AND ALL -Y- LINES IN ACCORDANCE WITH NCDOT DETAIL DRAWING FOR TWO-WAY UNDIVIDED WORK ZONE SIGNS. SEE SHEET TCP-19. SEE LOCAL NOTE 1 (SEE SHEET TCP-2).

INSTALL TRUCK DETOUR SIGNS ALONG THE DESIGNATED TRUCK DETOUR AS SHOWN ON SHEET TCP-17.

USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 3 OF 9, CONSTRUCT TEMPORARY PAVEMENT FROM -L- STA. 42+09+/- TO STA. 42+30+/- AND INSTALL WATER FILLED BARRIER FROM -L- STA. 41+60+/- TO STA. 42+60+/- . INSTALL TEMPORARY SHORING FROM STA. 42+00 +/- -L- TO STA. 42+27+/- -L-. SEE SHEET TCP-7.

STEP 2: AWAY FROM TRAFFIC, BEGIN CONSTRUCTION OF PROPOSED STRUCTURE AND PROPOSED -L- FROM STA 36+00+/- TO STA 42+40+/- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE. SEE SHEET TCP-6 AND TCP-7.

USING ROADWAY STANDARD DRAWING NO. 1101.02 (SHEET 1 OF 9) AND FLAGGERS, PERFORM THE FOLLOWING (SEE SHEETS TCP-5 AND TCP-6):

- RELOCATE EXISTING SIGNAL POLES AT THE INTERSECTION OF -L- (US 64 BUS./ NC 33/ S. MAIN ST.), -Y3- (MUTUAL BLVD.) AND -Y4- (NC 122/111/US 258). SEE SIGNAL PLANS.
- RELOCATE EXISTING SIGNAL POLES AT THE INTERSECTION OF -L- (US 64 BUS./ NC 33/ S. MAIN ST.), -Y1- (SR 1308-ALBEMARLE AVENUE) AND -Y2- (ST. ANDREWS STREET). SEE SIGNAL PLANS.
- WIDEN THE FOLLOWING UP TO EDGE AND ELEVATION OF EXISTING PAVEMENT:
 - L- (US 64 BUS./ NC 33/ S. MAIN ST.) FROM STA 11+50+/- TO STA 30+00+/-
 - Y3- (MUTUAL BLVD) FROM STA 11+50+/- TO STA 14+93+/-
 - Y4- WBL (NC 122/ 111/ US 258) FROM STA 10+00+/- TO STA 14+00+/-
 - Y4- EBL (NC 122/ 111/ US 258) FROM STA 10+00+/- TO STA 16+10+/-
 - Y5- FROM STA 10+00+/- TO STA 10+40+/-
 - Y6- (DANCY STREET) FROM STA 10+00+/- TO STA 11+00+/-
 - Y7- (BLACK STREET) FROM STA 11+50+/- TO STA 13+00+/-
 - Y8- (WALSTON STREET) FROM STA 10+00+/- TO STA 11+00+/-
- CONSTRUCT TEMPORARY PAVEMENT ON THE NORTHBOUND SIDE OF -L- FROM STA 32+00+/- TO STA 33+50+/- . SEE SHEET TCP-6. SEE LOCAL NOTE 2 (SHEET TCP-2).

STEP 3: USING ROADWAY STANDARD DRAWING NO. 1101.02 (SHEET 1 OF 9), REMOVE EXISTING CONCRETE ISLANDS ON -L- (US 64 BUS./ NC 33/ S. MAIN ST.) AND -Y4- (NC 122/ 111/US 258). PAVE AND REPAIR WHERE ISLAND WAS REMOVED AND PROVIDE SUITABLE DRIVING SURFACE. SEE SHEET TCP-6.

STEP 4: USING ROADWAY STANDARD DRAWING NO. 1101.02 (SHEET 1 OF 9), REVISE SIGNAL TO TEMPORARY PATTERN AND PLACE TEMPORARY (PAINT) PAVEMENT MARKINGS ON -L- AND -Y4- AS SHOWN ON SHEETS TCP-9 & TCP-10. SHIFT TRAFFIC TO NEW PATTERN AND CLOSE SR 1558 (RIVERVIEW DRIVE) TO TRAFFIC USING TYPE III BARRICADES.

PHASE II

STEP 1: USING ROADWAY STANDARD DRAWING NO 1101.02 (SHEET 1 OF 9) FOR ALTERNATING LANE CLOSURES AND WORKING IN A CONTINUOUS MANNER, COMPLETE WIDENING UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE AND PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS IN THE FOLLOWING LOCATIONS:

- L- (US 64 BUS./ NC 33/ S. MAIN ST.) FROM STA 11+50+/- TO STA 30+00+/-
- Y3- (MUTUAL BLVD) FROM STA 11+50+/- TO STA 14+93+/-
- Y4- (NC 122/ 111/ US 258) FROM STA 10+00+/- TO STA 18+00+/- AND RIVERVIEW DRIVE
- Y5- FROM STA 10+00+/- TO STA 11+50+/-
- Y6- (DANCY STREET) FROM STA 10+00+/- TO STA 11+00+/-
- Y7- (BLACK STREET) FROM STA 11+50+/- TO STA 13+00 +/-
- Y8- (WALSTON STREET) FROM STA 10+00+/- TO STA 11+00+/-

USING ROADWAY STANDARD DRAWING NO 1101.02 (SHEET 1 OF 9) AS NEEDED, CONSTRUCT -L- SBL (US 64 BUS./ NC 33/ S. MAIN ST.) FROM STA 30+00+/- TO STA 36+00+/- .

STEP 2: USING ROADWAY STANDARD DRAWING NO. 1101.02 (SHEET 1 OF 9) AND POLICE AT THE INTERSECTION, REMOVE EXISTING CONCRETE ISLANDS ON -Y1- (SR 1308 / ALBEMARLE AVENUE) AND -Y2- (ST. ANDREWS STREET). PAVE AND REPAIR WHERE ISLAND WAS REMOVED TO PROVIDE SUITABLE DRIVING SURFACE.

COMPLETE CONSTRUCTION OF PROPOSED STRUCTURE AND PROPOSED -L- FROM STA 36+00+/- TO STA 42+40+/- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE (BEGUN IN PHASE I, STEP 2). REMOVE TEMPORARY SHORING AND WATER FILLED BARRIER.

PHASE II

STEP 2: (CONTINUED)
BEGIN PAVEMENT REMOVAL ON RIVERVIEW DRIVE AS SHOWN IN ROADWAY PLANS.

PHASE III

STEP 1: INSTALL AND COVER OFFSITE DETOUR SIGNS TO PREPARE FOR THE TEMPORARY CLOSURE OF -L- (NORTH OF -Y1- AND -Y2-) TO CONSTRUCT THE ROUNDABOUT. SEE SHEET TCP-18.

STEP 2: REVISE SIGNAL TO TEMPORARY PATTERN, PLACE TEMPORARY (PAINT) PAVEMENT MARKING AND SHIFT TRAFFIC TO A TWO-LANE, TWO-WAY PATTERN ON THE NORTH SIDE OF -Y1- (ALBEMARLE AVENUE) AND -Y2- (ST. ANDREWS STREET) AND CONSTRUCT THE FOLLOWING UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE. SEE TCP-13.

- PROPOSED WIDENING ON THE SOUTH SIDE OF -Y1- FROM STA 12+50+/- TO STA 16+00+/-
- PROPOSED WIDENING ON THE SOUTH SIDE OF -Y2- FROM STA 10+75+/- TO STA 13+15+/-

STEP 3: USING ROADWAY STANDARD DRAWING NO 1101.02 (SHEET 1 OF 9) AND WORKING IN A CONTINUOUS MANNER, CONSTRUCT THE FOLLOWING UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE: SEE TCP-14.

- -L- FROM STA 42+69+/- TO STA 44+43+/- WEDGE EXISTING INTERSECTION PAVEMENT AND FEATHER TO EXISTING PAVEMENT ALONG -L- .
- -Y1- FROM STA 12+50+/- TO STA 16+00+/- (WEDGE EXISTING)
- -Y2- FROM STA 10+75+/- TO STA 13+15+/- (WEDGE EXISTING)

STEP 4: REVISE SIGNAL TO TEMPORARY PATTERN, PLACE TEMPORARY (PAINT) PAVEMENT MARKING AND SHIFT TRAFFIC TO A TWO-LANE, TWO-WAY PATTERN ON THE SOUTH SIDE OF -Y1- (ALBEMARLE AVENUE) AND -Y2- (ST. ANDREWS STREET) AND CONSTRUCT THE FOLLOWING UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE. SEE TCP-15.

- PROPOSED WIDENING ON THE NORTH SIDE OF -Y1- FROM STA 12+50+/- TO -L-
- PROPOSED WIDENING ON THE NORTH SIDE OF -Y2- FROM STA -L- TO STA 14+00+/-
- USING ROADWAY STANDARD DRAWING NO 1101.02 (SHEET 1 OF 9) AND WORKING IN A CONTINUOUS MANNER, WEDGE AND WIDEN -L- FROM STA 43+76+/- TO STA 46+00+/- . SEE LOCAL NOTE 3.

PHASE IV

ICT #1
COMPLETE THE WORK REQUIRED OF STEPS 1 THROUGH 3 WITHIN (30) CONSECUTIVE DAYS. REFER TO SPECIAL PROVISIONS AND LIQUIDATED DAMAGES.

STEP 1: USING ROADWAY STANDARD DRAWING NO. 1101.02 (SHEET 1 OF 9), PERFORM THE FOLLOWING IN A CONTINUOUS MANNER:

- CLOSE -L- TO THRU TRAFFIC AS SHOWN ON TCP-19.
- WEDGE AND WIDEN -L- FROM STA. 30+00+/- TO STA. 32+00+/- . USING THE OFF-SITE DETOUR, SHIFT TRAFFIC TO THE PROPOSED BRIDGE AND PLACE TEMPORARY PAVEMENT MARKING. SEE SHEETS TCP-16 AND 17.
- DEACTIVATE AND REMOVE SIGNAL AT THE INTERSECTION OF -L- (US 64 BUS./ NC 33/ SOUTH MAIN STREET), -Y1- (ALBEMARLE AVENUE) AND -Y2- (ST. ANDREWS STREET).

STEP 2: AWAY FROM TRAFFIC, COMPLETE CONSTRUCTION OF THE ROUNDABOUT UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE. COMPLETE CONSTRUCTION OF PROPOSED ISLANDS ALONG -Y1-, -Y2- AND ON -L- AT ROUNDABOUT.

STEP 3: PLACE TEMPORARY PAVEMENT MARKING AS SHOWN ON SHEET PM-4. REMOVE TEMPORARY SHORING AND WATER FILLED BARRIER. OPEN -L-, -Y1- AND -Y2- TO FINAL TRAFFIC PATTERN AND COVER/REMOVE DETOUR SIGNING.

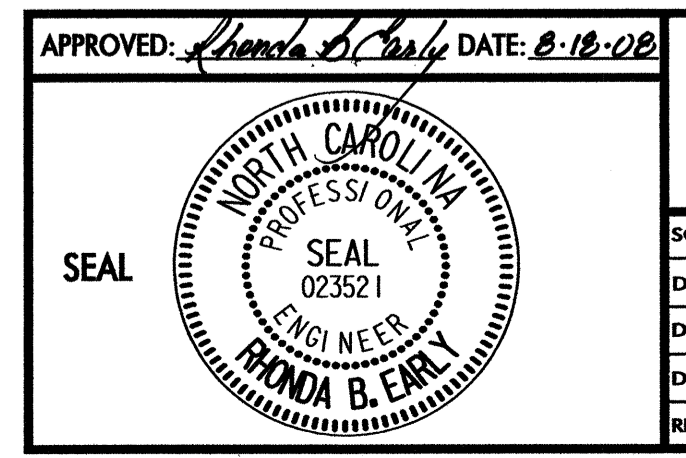
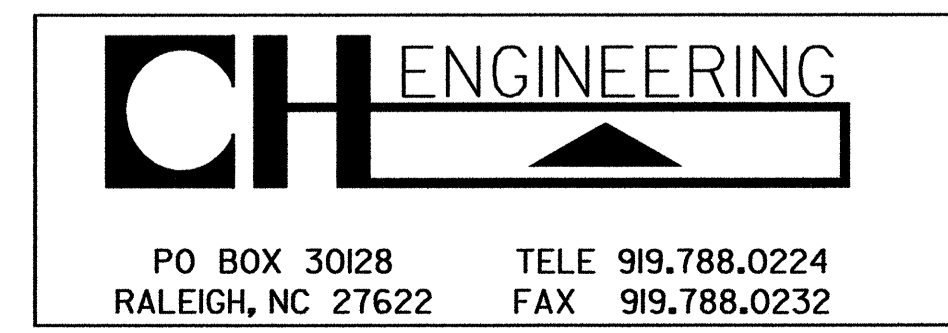
STEP 4: USING ROADWAY STANDARD DRAWING NO. 1101.02, (SHEET 1 OF 9) AS NEEDED, REMOVE TEMPORARY PAVEMENT AND COMPLETE OBLITERATION OF PAVEMENT ON RIVERVIEW DRIVE AS SHOWN IN ROADWAY PLANS. COMPLETE ANY REMAINING DRIVEWAY AND SIDEWALK CONNECTIONS. COMPLETE -L- WIDENING FROM STA. 29+00 TO STA. 36+00 NB SIDE (INCLUDING SIDEWALK). SEE SHEET TCP-16

STEP 5: USING ROADWAY STANDARD DRAWING NO. 1101.02 (SHEET 1 OF 9), PLACE FINAL SURFACE COURSE, FINAL PAVEMENT MARKINGS AND MARKERS ON -L- AND ALL -Y- LINES. SEE SHEETS PM-1 THRU PM-4.

STEP 6: REMOVE ALL TRAFFIC CONTROL DEVICES AND OPEN ALL ROADS TO THE FINAL TRAFFIC PATTERN.

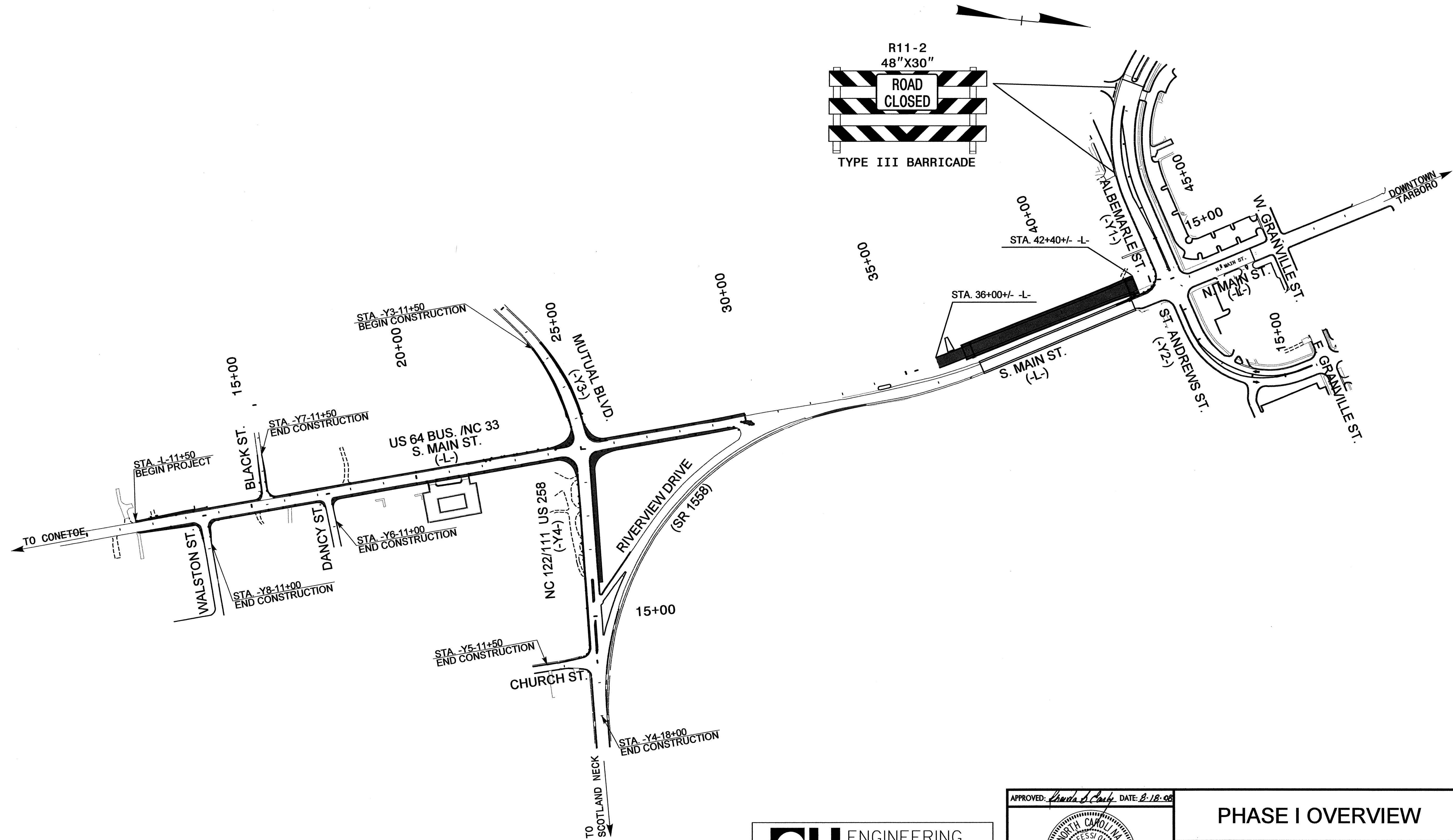
REVISIONS

8/18/2008 Y:\NCDOT\B2965\Traffic\TrafficControl\TCP\B2965-Te_TCP03phasing.dgn USERNAME



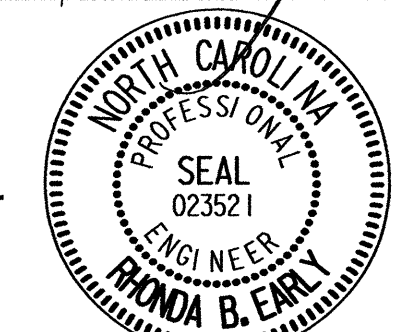
PHASING		REVISIONS	
SCALE:	DATE: 08/03/08		
DWG. BY: JAP	DESIGN BY: JAP		
REVIEWED BY: RBE			

REVISIONS
 8/18/2008
 Y:\NCDOT\B2965\TrafficControl\TCP\B2965-fe-TCP04plov.dgn
 USERNAME

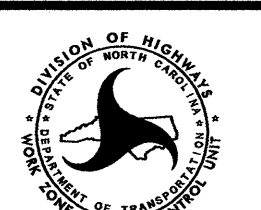


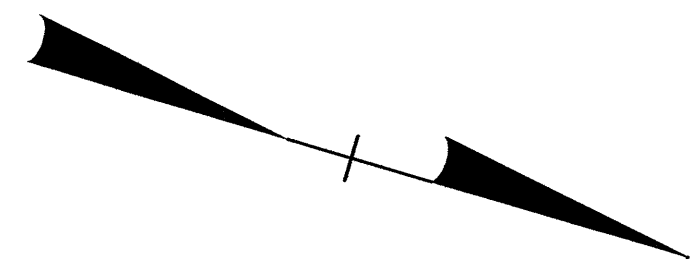
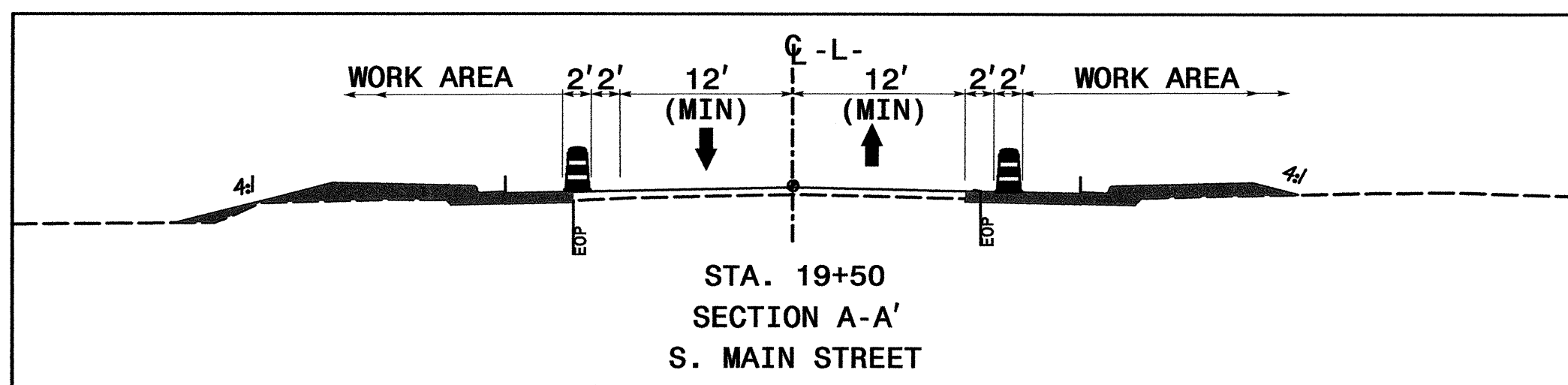
NOTE:
 - STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - SEE SHEETS TCP-6 & TCP-7 FOR SIDEWALK SIGNING.

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 RALEIGH, NC 27622 FAX 919.788.0232

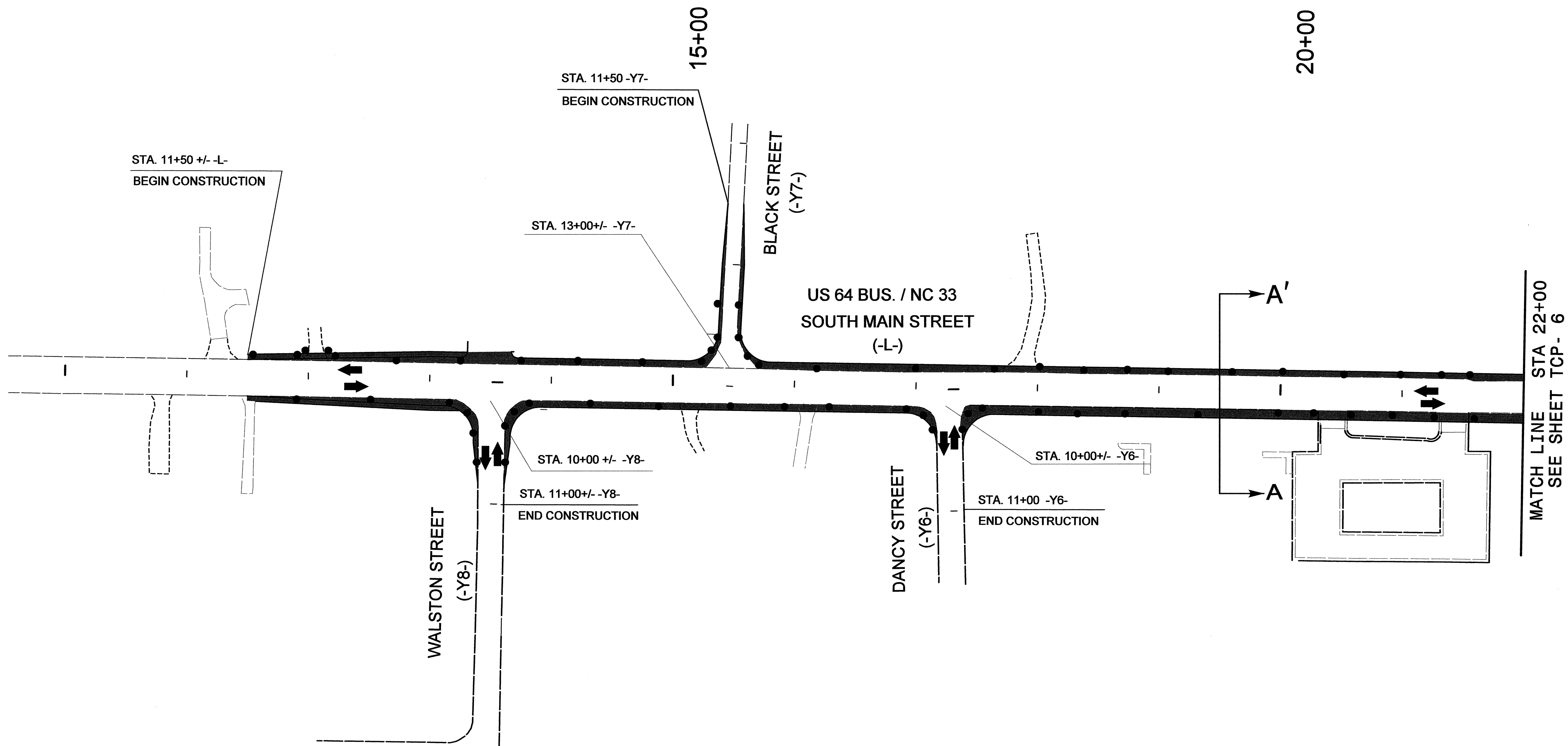
APPROVED: *Ronda B. Early* DATE: 8-18-08


PHASE I OVERVIEW

SCALE: 1" = 150'		REVISIONS <table border="1"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>								
DATE: 08/03/08										
DESIGN BY: JAP										
REVIEWED BY: RBE										



REVISIONS



8/18/2008
Y:\NC001\B2965\Traffic\TrafficControl\TCP\B2965_Te_TCP05.pldgn
USERNAME

NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE
UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

CH ENGINEERING

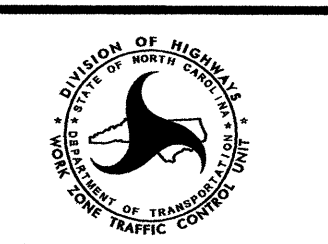
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APPROVED: *Ronda B. Early* DATE: 8-18-08

SEAL

**PHASE I
DETAIL 1**

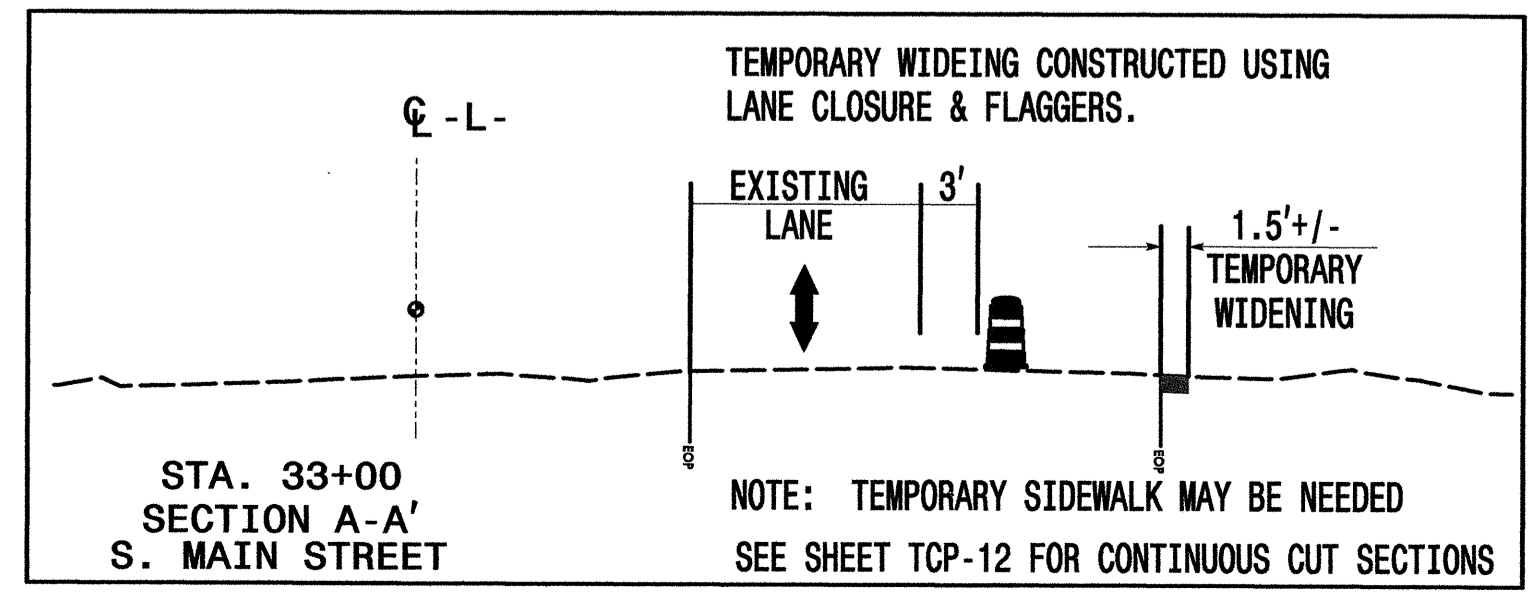
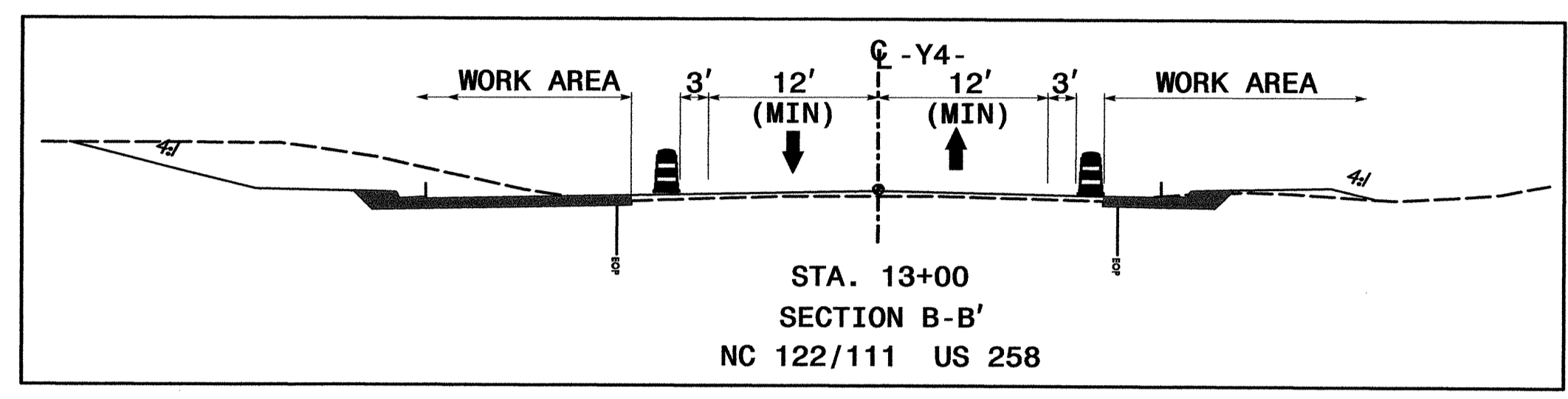
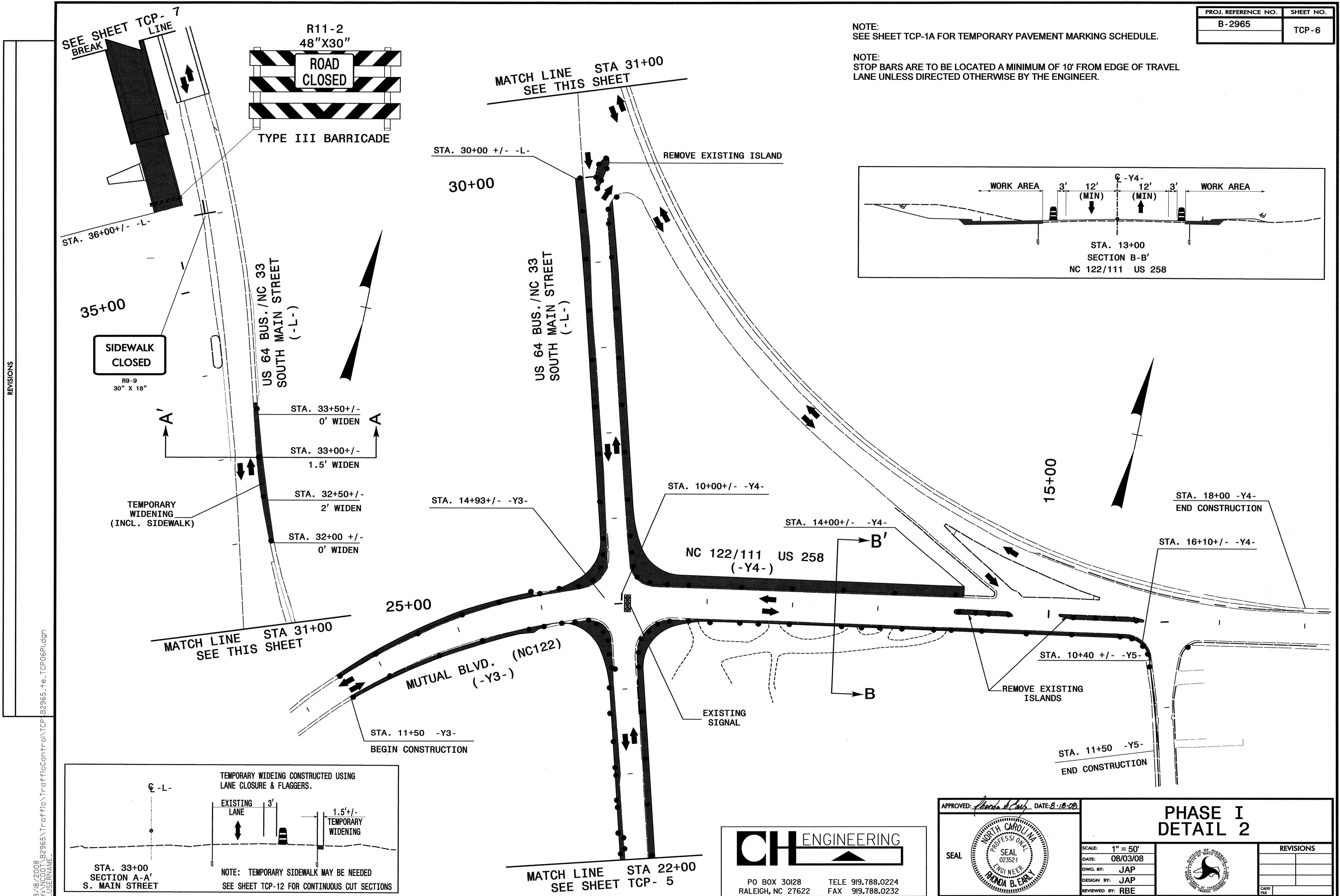
SCALE: 1" = 50'
DATE: 08/03/08
DWG. BY: JAP
DESIGN BY: JAP
REVIEWED BY: RBE



REVISIONS	

NOTE:
SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.



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APPROVED: *[Signature]* DATE: 8-18-08

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SEAL 023521
FRONDA B. EARLY

**PHASE I
DETAIL 2**

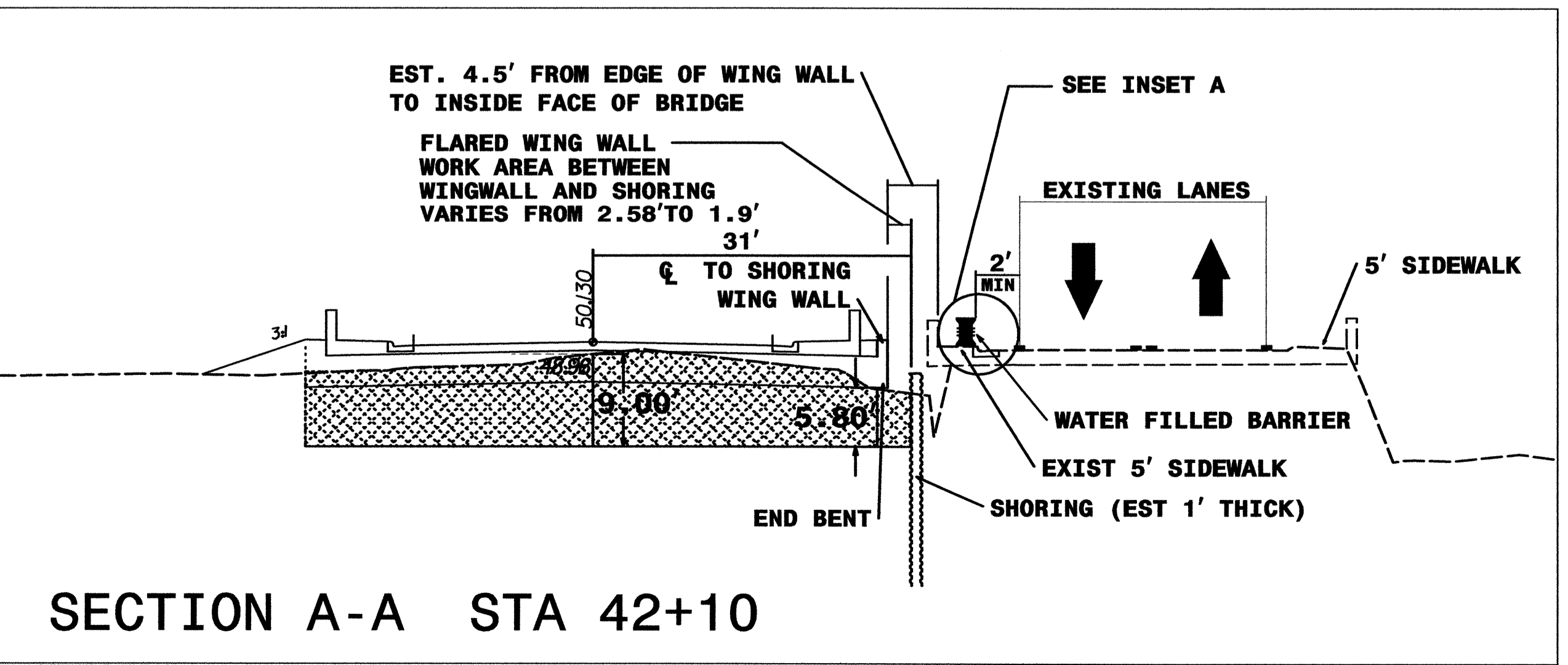
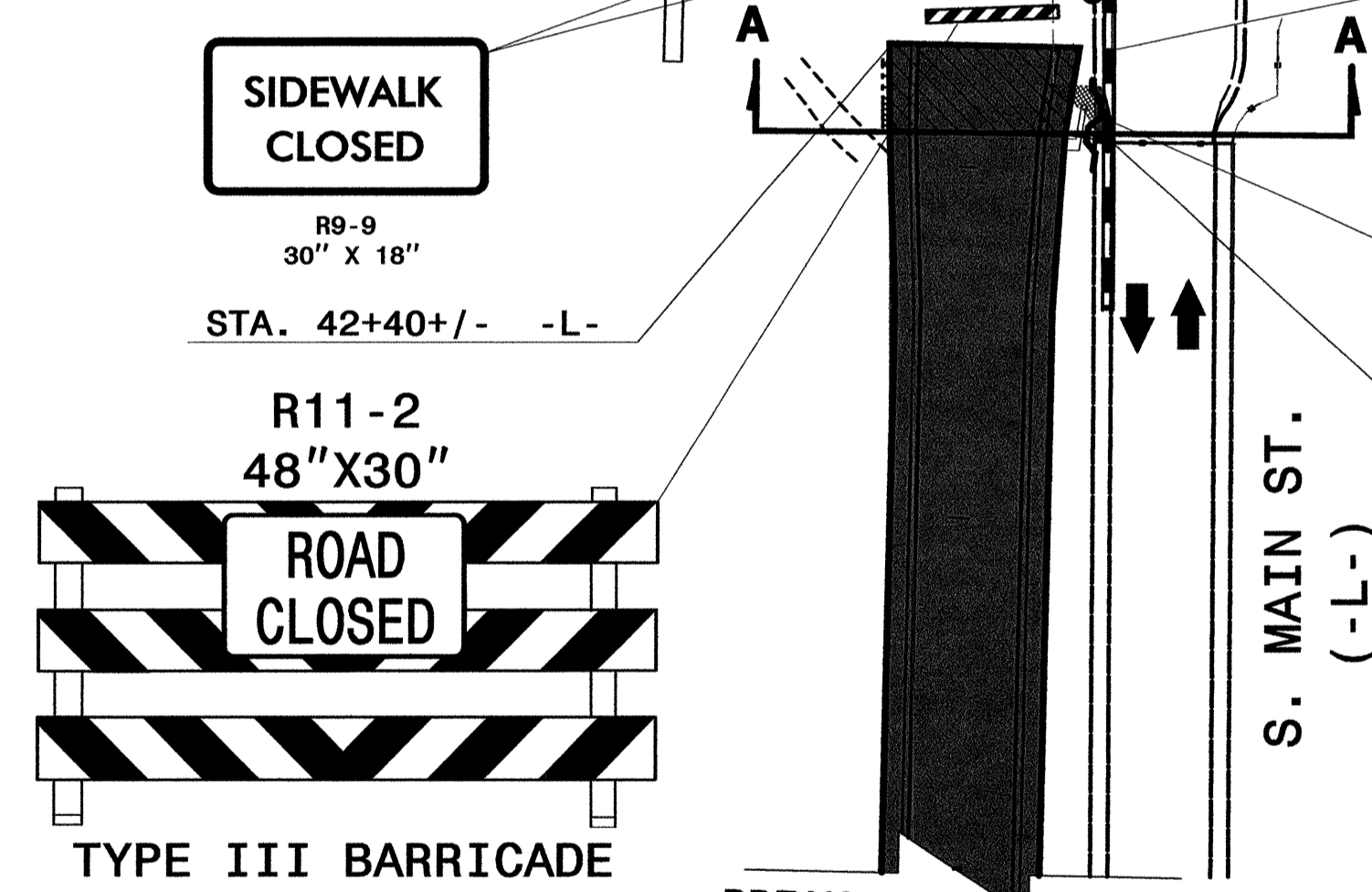
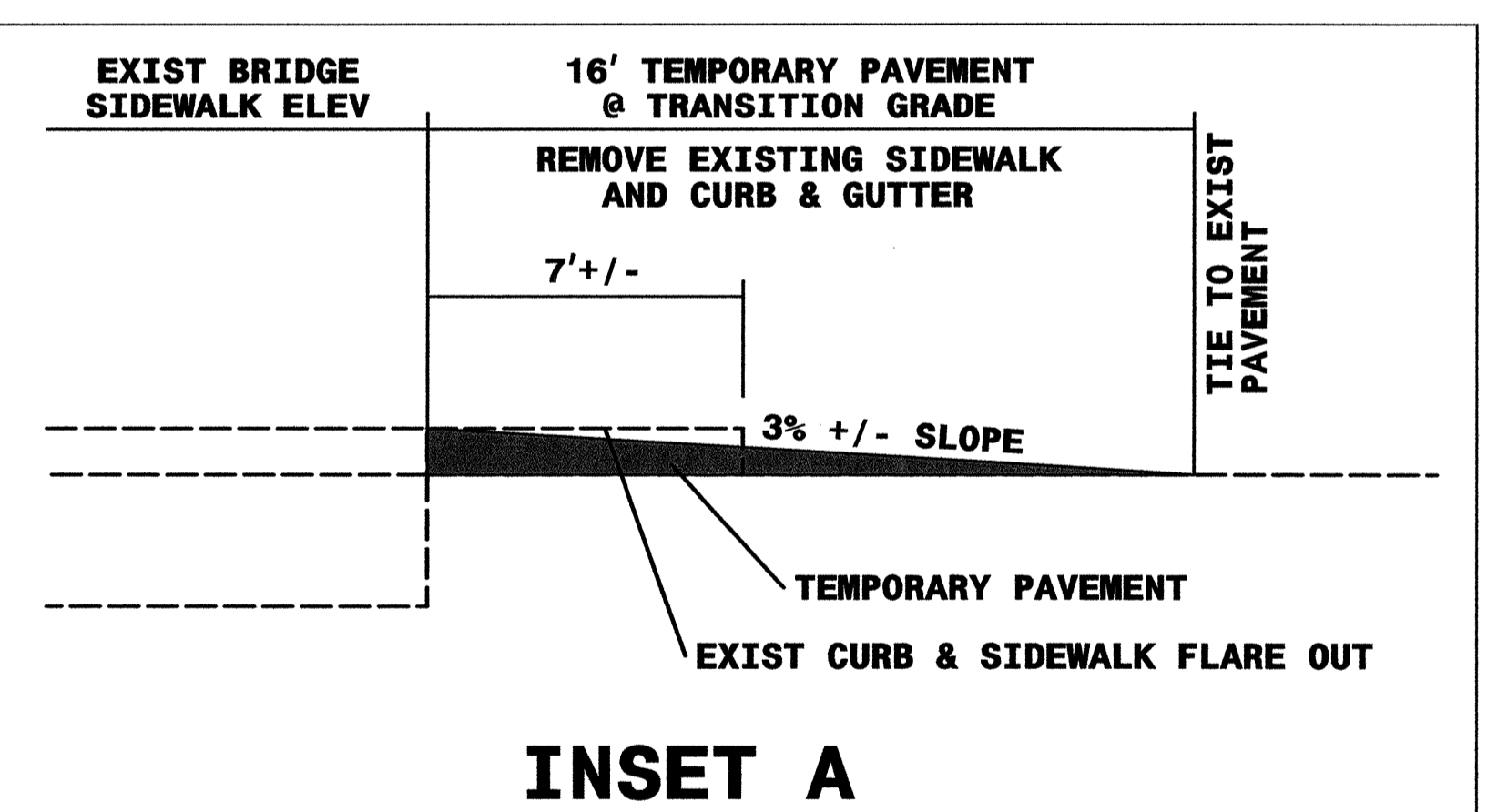
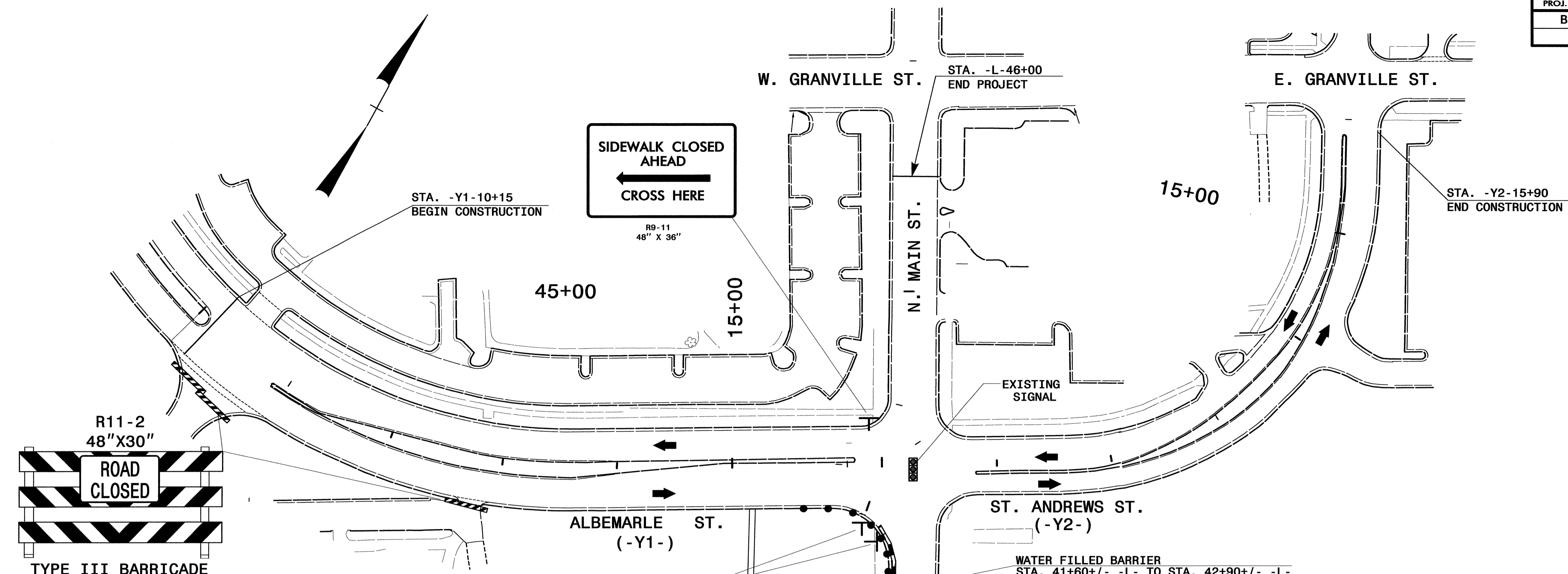
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DATE: 08/03/08
DWG. BY: JAP
DESIGN BY: JAP
REVIEWED BY: RBE

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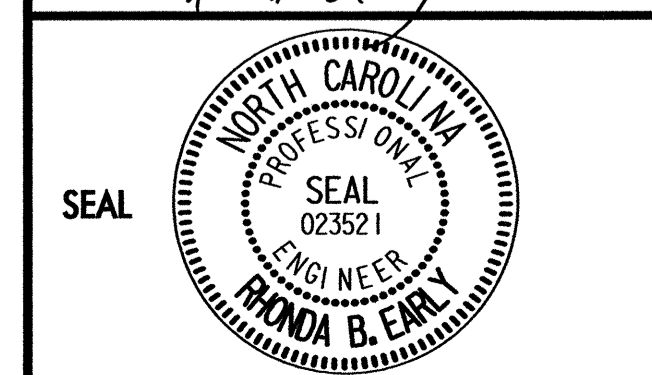
8/18/2008 Y:\NCDOT\B2965\TrafficControl\TCP\B2965_fe_TCP06P1.dgn USER:NAME



NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

NOTE:
SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

APPROVED: *Shanda B. Early* DATE: 8-12-08



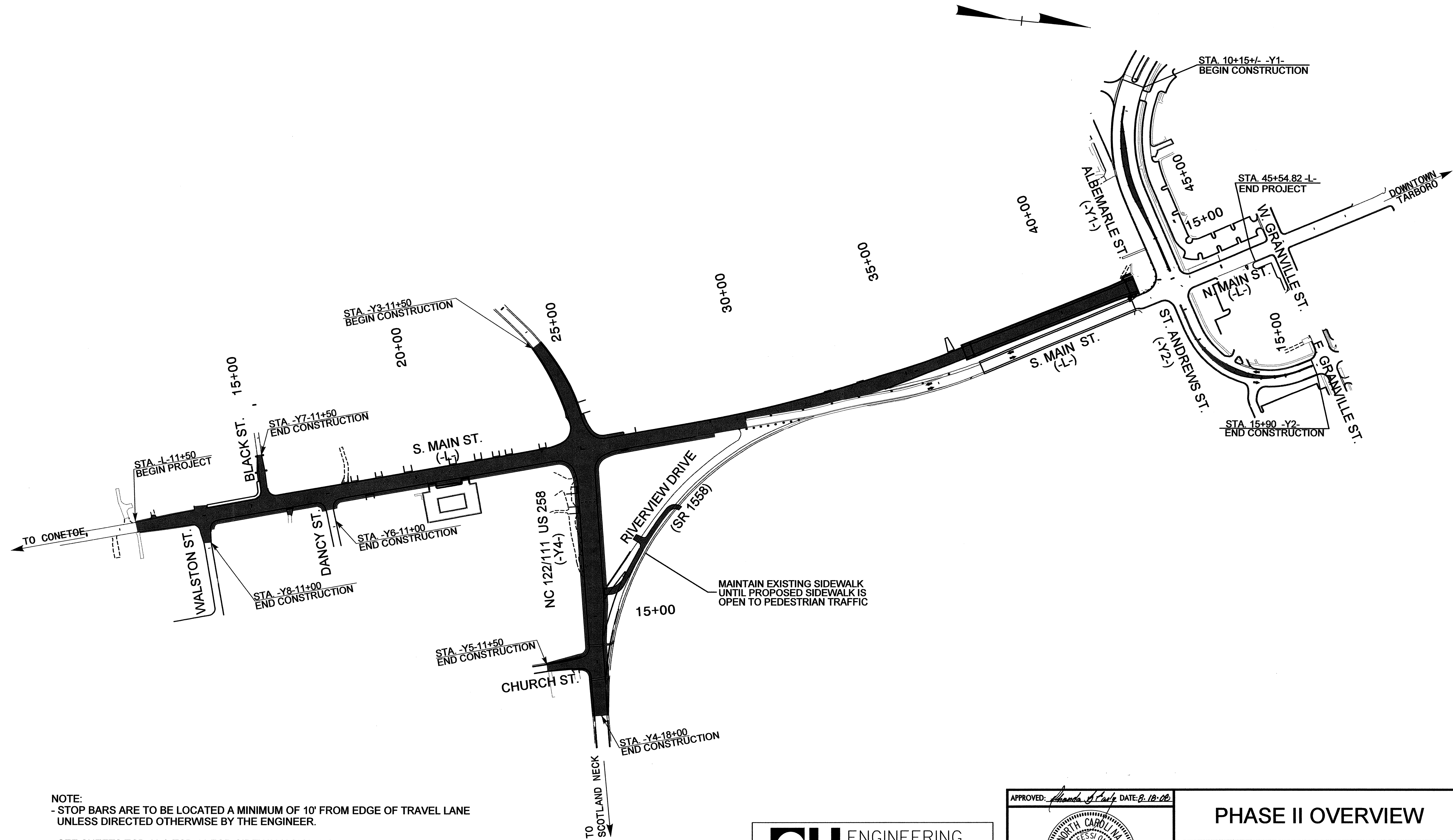
SCALE: 1" = 50'		DATE: 08/03/08		REVISIONS	
DESIGN BY: JAP		REVIEWED BY: RBE		CADD FILE	

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8/18/2008
 Y:\NCDOT\B2965\TrafficControl\TCP\B2965-fe_TCP08P2ov.dgn
 USERNAME

NOTE:
 - STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - SEE SHEETS TCP-10 & TCP-11 FOR SIDEWALK SIGNING.

MAINTAIN EXISTING SIDEWALK UNTIL PROPOSED SIDEWALK IS OPEN TO PEDESTRIAN TRAFFIC

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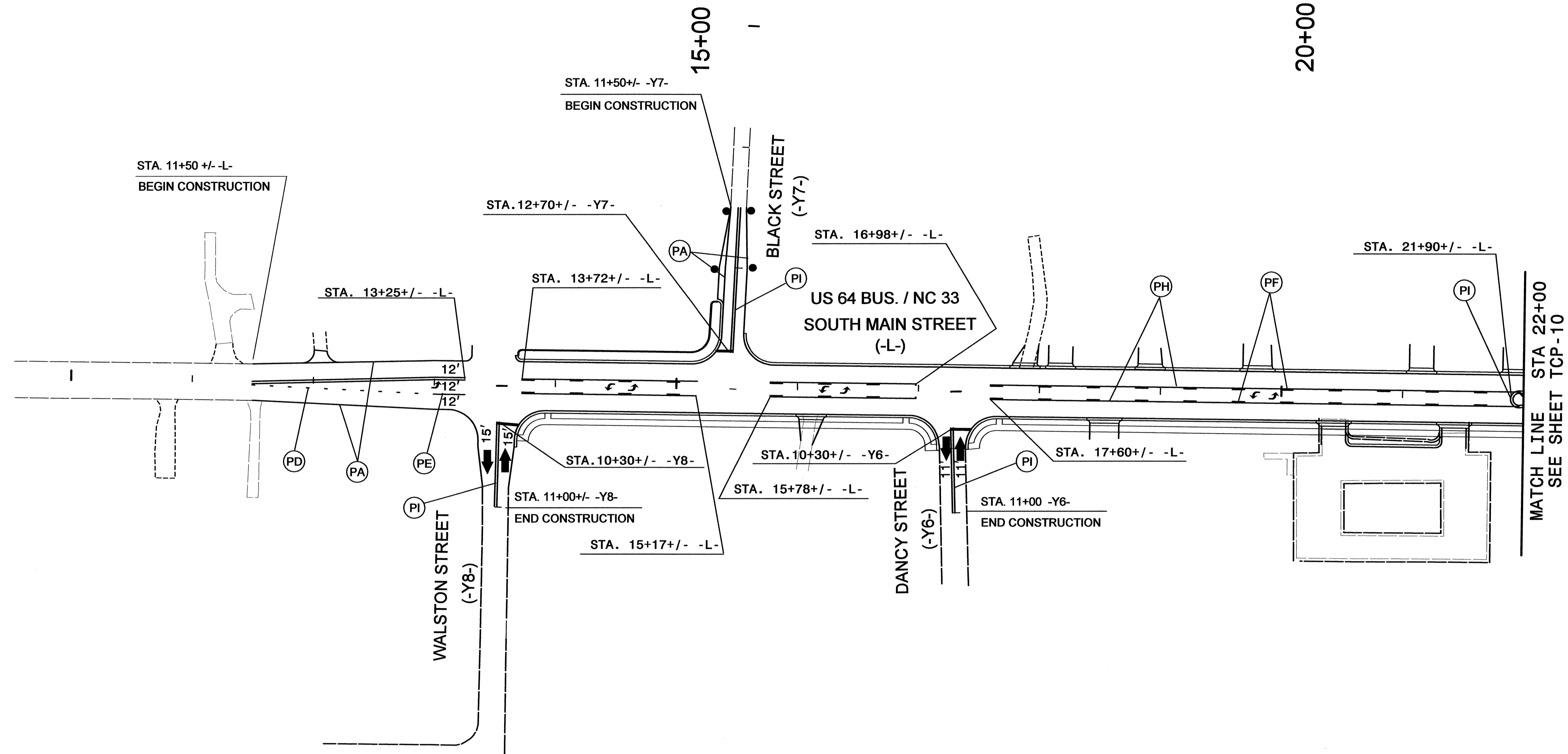
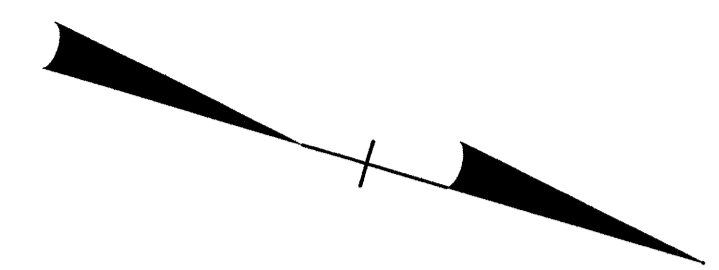
SEAL
 NORTH CAROLINA
 PROFESSIONAL
 SEAL
 023521
 ENGINEER
 RONDA B. EARLY

PHASE II OVERVIEW

SCALE: 1" = 150'
 DATE: 08/03/08
 DWG. BY: JAP
 DESIGN BY: JAP
 REVIEWED BY: RBE



REVISIONS	



8/18/2008 Y:\NCDOT\B2965\Traffic\TrafficControl\TCP\B2965-fe-TCP09pl.dgn USERNAME

NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE
UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

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APPROVED: *Ronda B. Early* DATE: 8-18-08

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**PHASE II
DETAIL 1**

SCALE: 1" = 50'

DATE: 08/03/08

DWG. BY: JAP

DESIGN BY: JAP

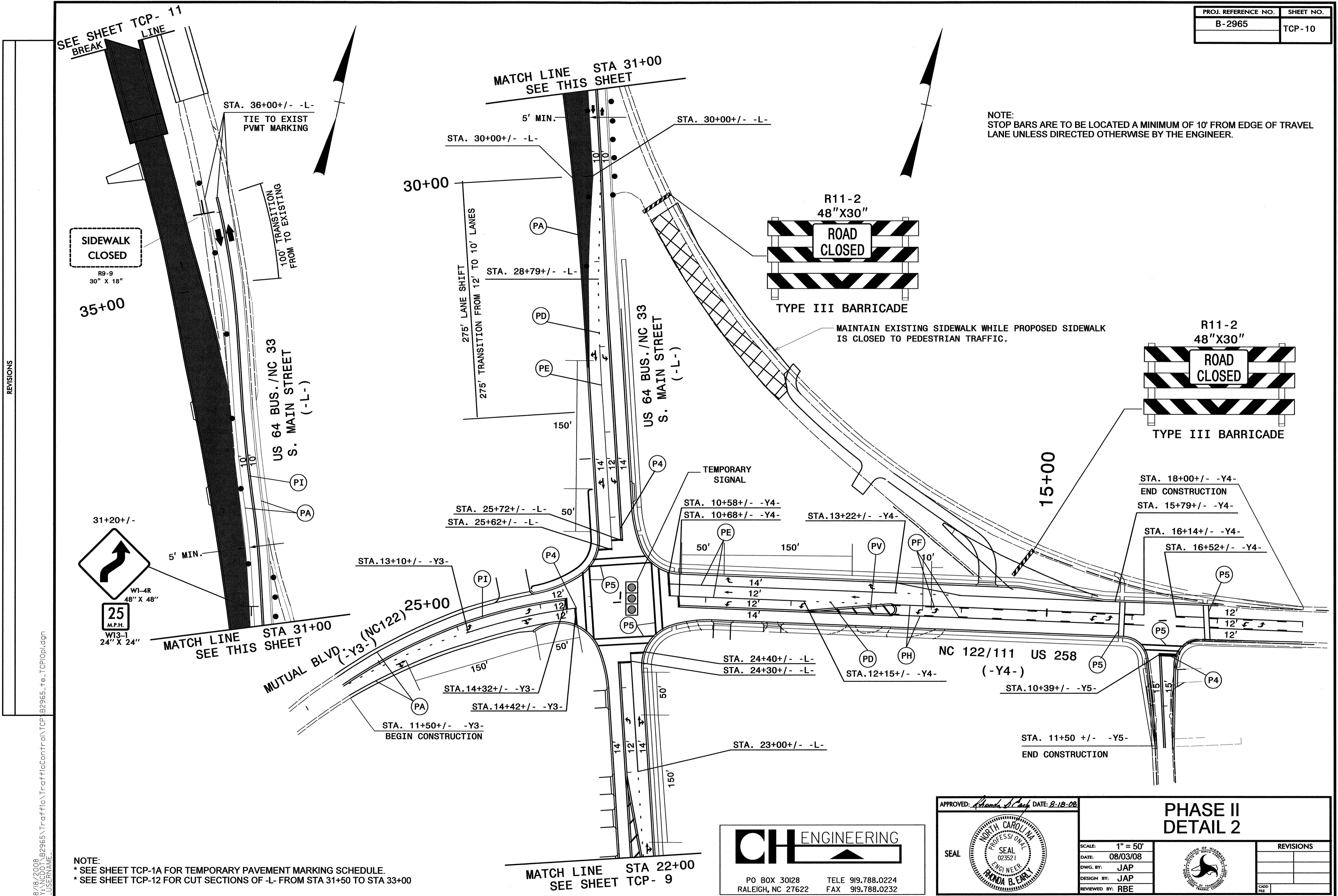
REVIEWED BY: RBE

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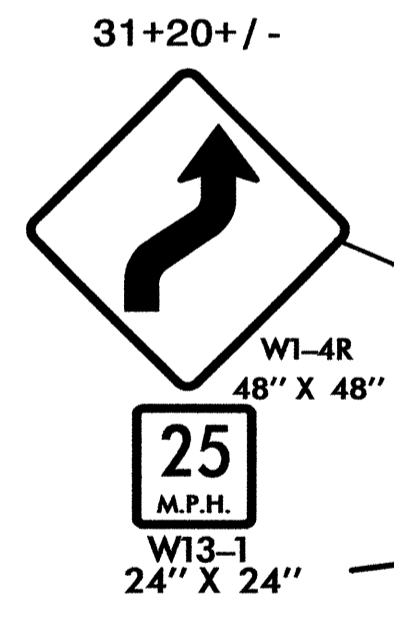
NOTE:
SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.



SIDWALK CLOSED
R9-9
30" X 18"

35+00

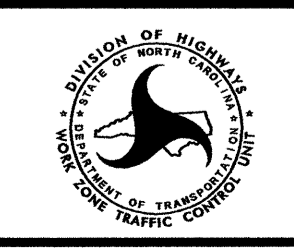


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 023521

**PHASE II
DETAIL 2**

SCALE:	1" = 50'
DATE:	08/03/08
DWG. BY:	JAP
DESIGN BY:	JAP
REVIEWED BY:	RBE

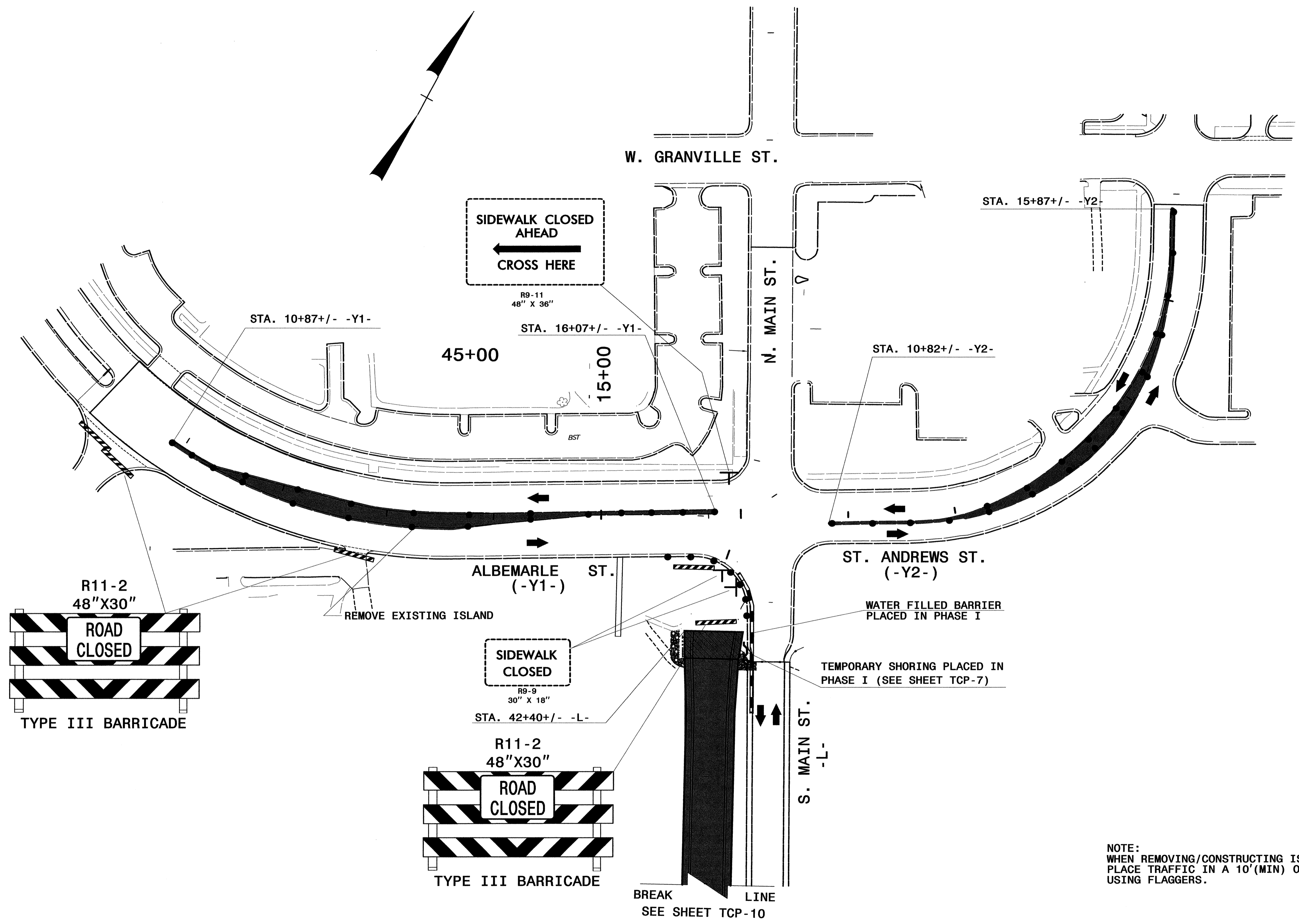


REVISIONS	

NOTE:
 * SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.
 * SEE SHEET TCP-12 FOR CUT SECTIONS OF -L- FROM STA 31+50 TO STA 33+00

REVISIONS

8/18/2008 Y:\NCDOT\B2965\Traffic\TrafficControl\TCP\B2965-fe_TCP10.dgn
 USERNAME



NOTE:
WHEN REMOVING/CONSTRUCTING ISLANDS ON -Y1- AND -Y2-,
PLACE TRAFFIC IN A 10'(MIN) ONE LANE, TWO-WAY PATTERN
USING FLAGGERS.

REVISIONS

8/18/2008
Y:\NCDOT\B2965\TrafficControl\TCP\B2965-fe-TCP\plp.dgn
-USERNAME-

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APPROVED: *Ronda B. Early* DATE: 8-18-08

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NORTH CAROLINA
PROFESSIONAL
ENGINEER
RONDA B. EARLY
023521

**PHASE II
DETAIL 3**

SCALE: 1" = 50'

DATE: 08/03/08

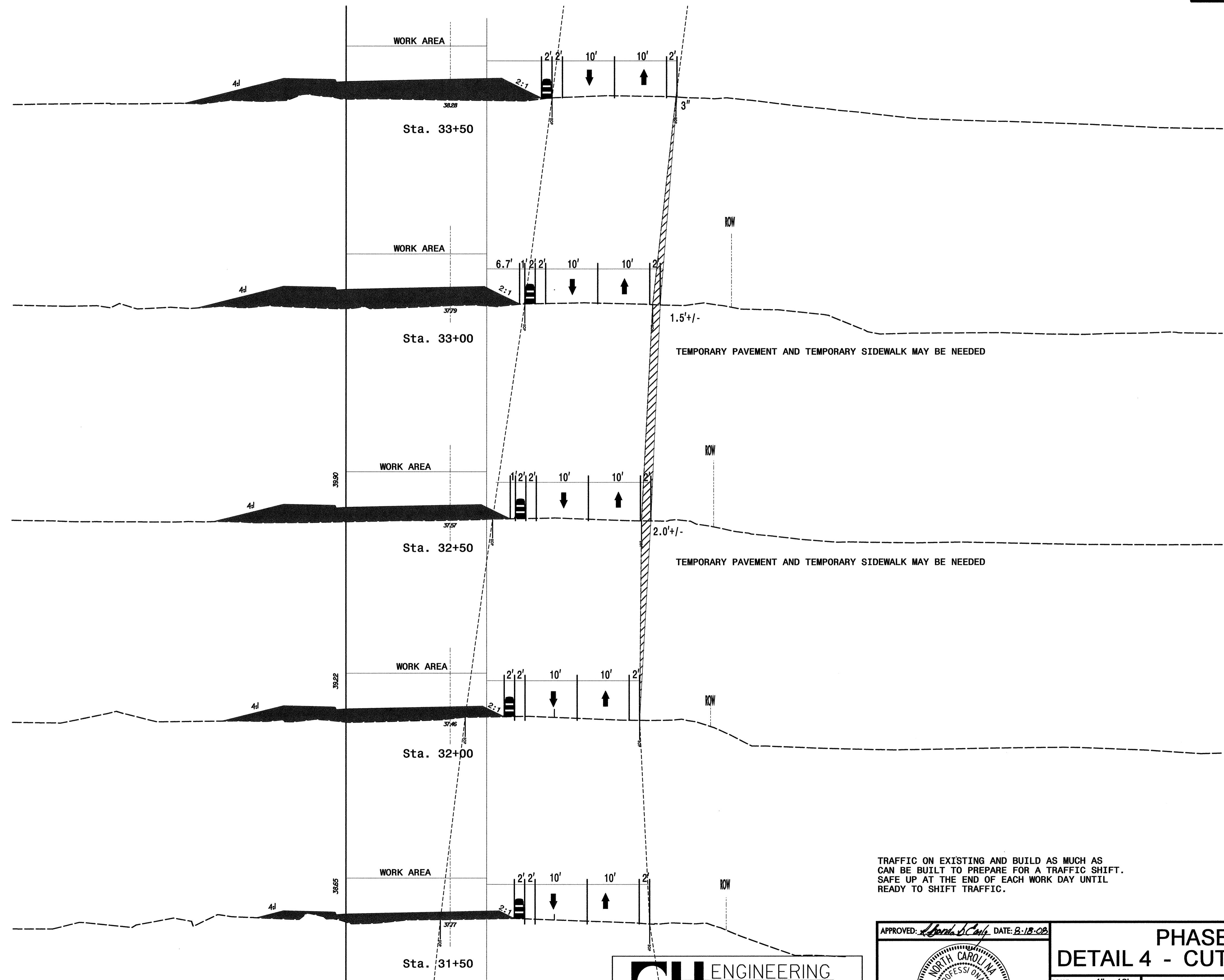
DWG. BY: JAP

DESIGN BY: JAP

REVIEWED BY: RBE

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ADD FILE



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8/18/2008
 Y:\NCDOT\B2965\TrafficControl\TCP\B2965-fe_TCP12P2xs.dgn
 USERNAME

TRAFFIC ON EXISTING AND BUILD AS MUCH AS CAN BE BUILT TO PREPARE FOR A TRAFFIC SHIFT. SAFE UP AT THE END OF EACH WORK DAY UNTIL READY TO SHIFT TRAFFIC.

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APPROVED: *[Signature]* DATE: 8-18-08

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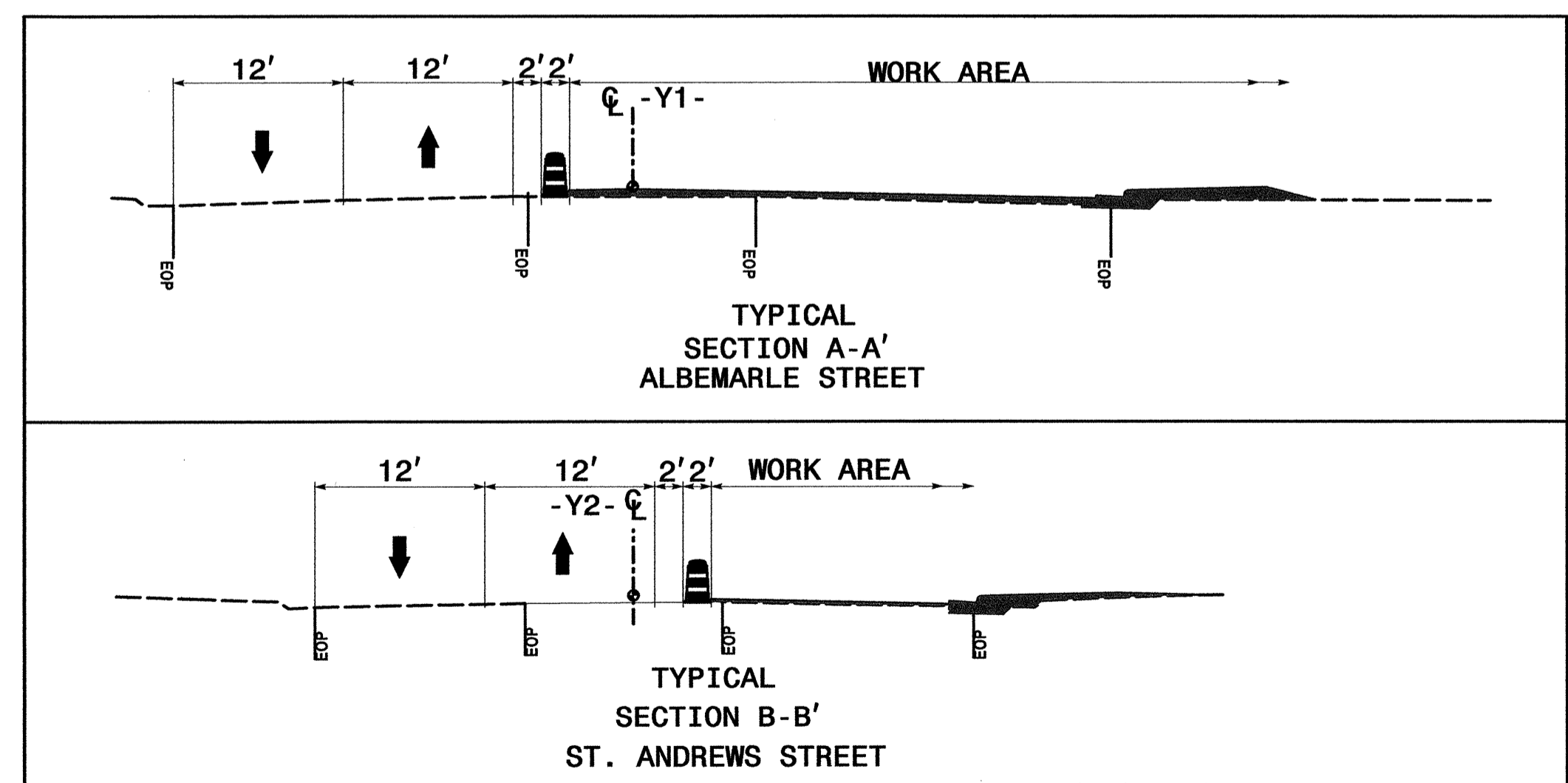
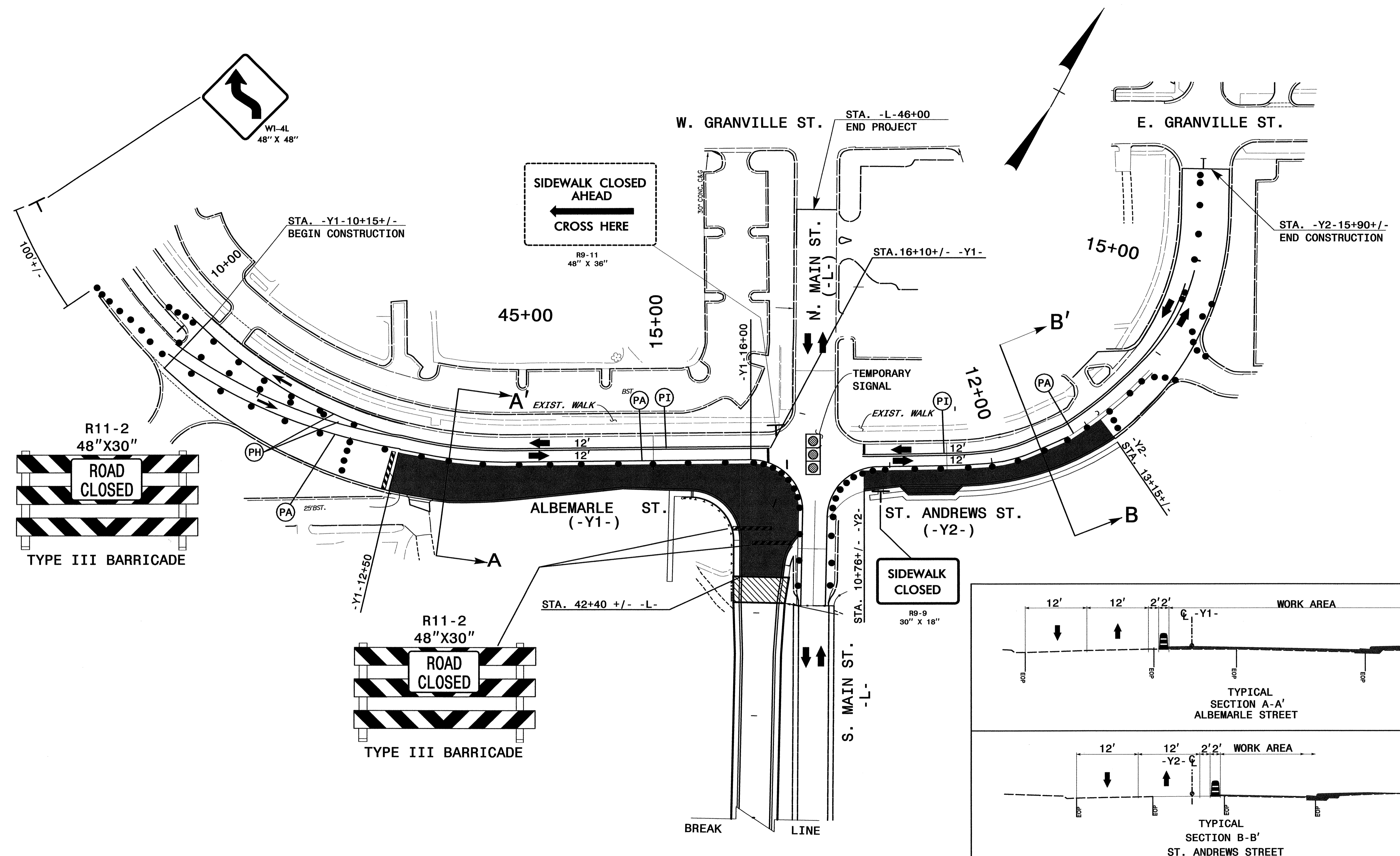
NORTH CAROLINA PROFESSIONAL ENGINEER
 SEAL 023521
 RONDA B. EARLY

**PHASE II
 DETAIL 4 - CUT SECTIONS**

SCALE: 1" = 10'
 DATE: 08/03/08
 DWG. BY: JAP
 DESIGN BY: JAP
 REVIEWED BY: RBE

REVISIONS

CADD FILE



NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

NOTE:
SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

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APPROVED: *[Signature]* DATE: 8-18-08
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 NORTH CAROLINA PROFESSIONAL ENGINEER
 RONDA B. EARLY

**PHASE III STEP 2
DETAIL 1**

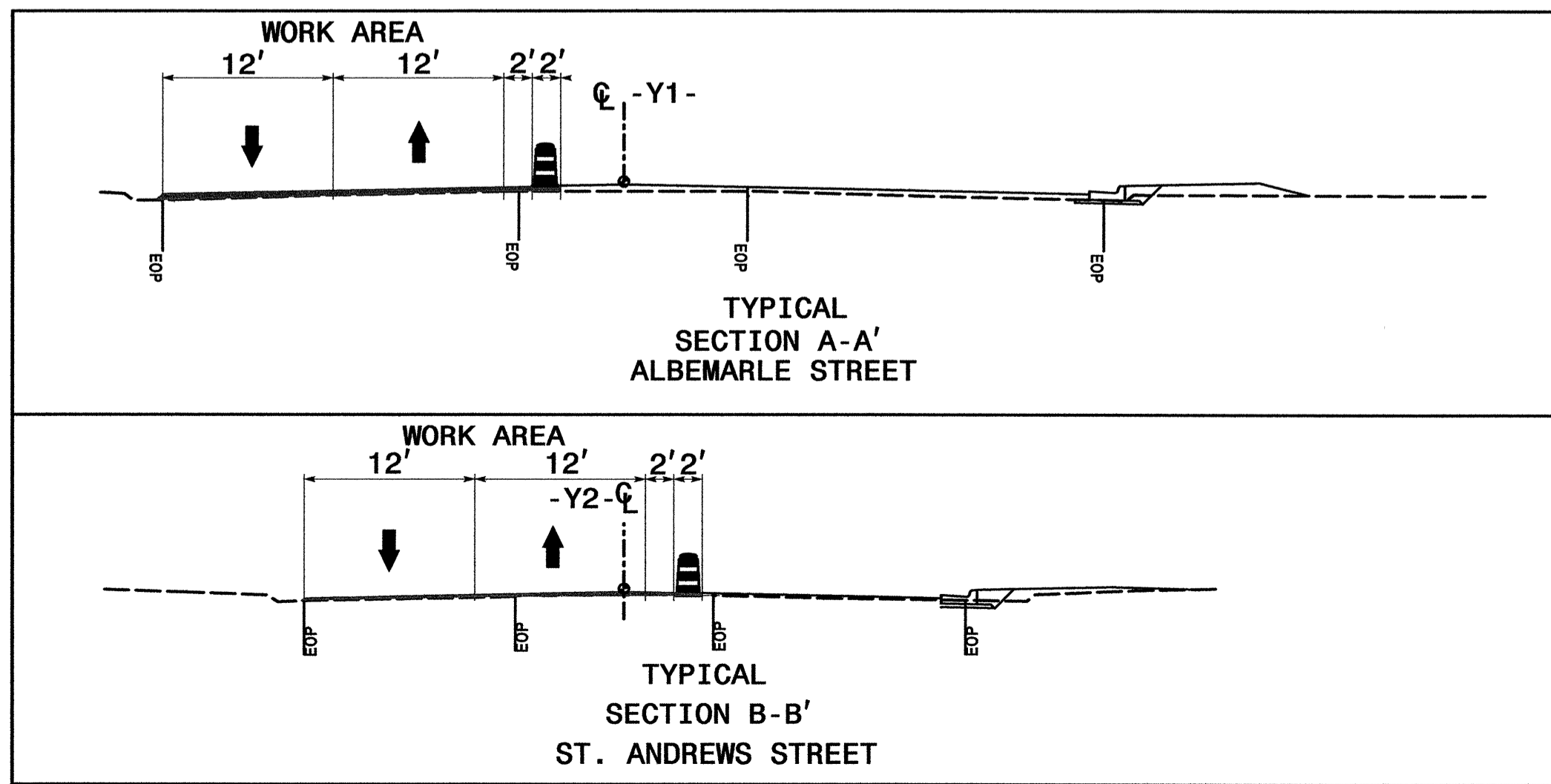
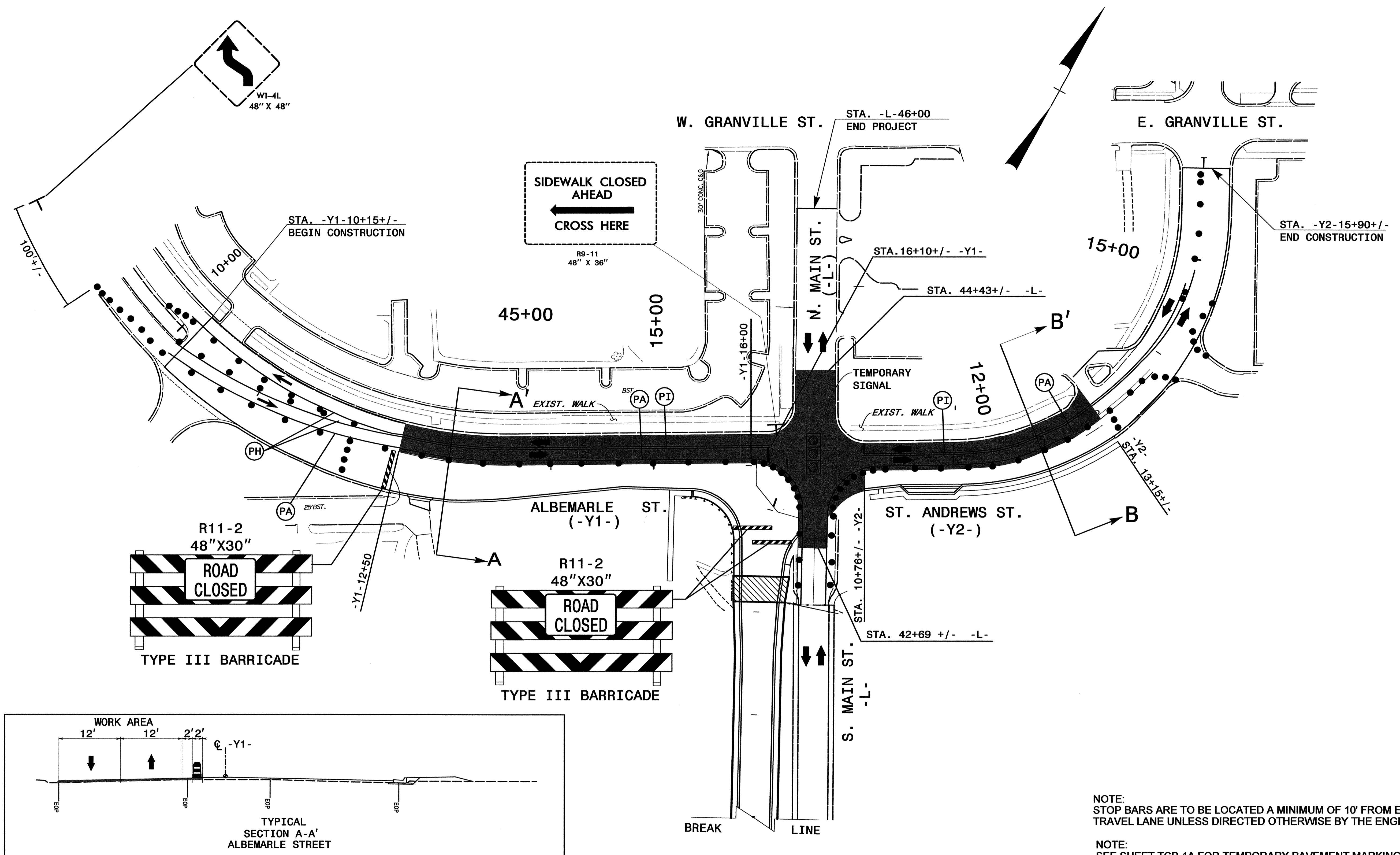
SCALE: 1" = 50'
 DATE: 08/03/08
 DWG. BY: JAP
 DESIGN BY: JAP
 REVIEWED BY: RBE

REVISIONS

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 Y:\NGDOT\B2965\Traffic\TrafficControl\TCP\B2965-fe_TCP13p3S2.dgn
 USERNAME



NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' FROM EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

NOTE:
SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

REVISIONS

8/18/2008 Y:\NCDOT\B2965\TrafficControl\TCP-B2965-fe-TCP14p3S3.dgn USERNAME

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APPROVED: *Ronda B. Early* DATE: 8-18-08

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NORTH CAROLINA PROFESSIONAL ENGINEER
SEAL 023521
RONDA B. EARLY

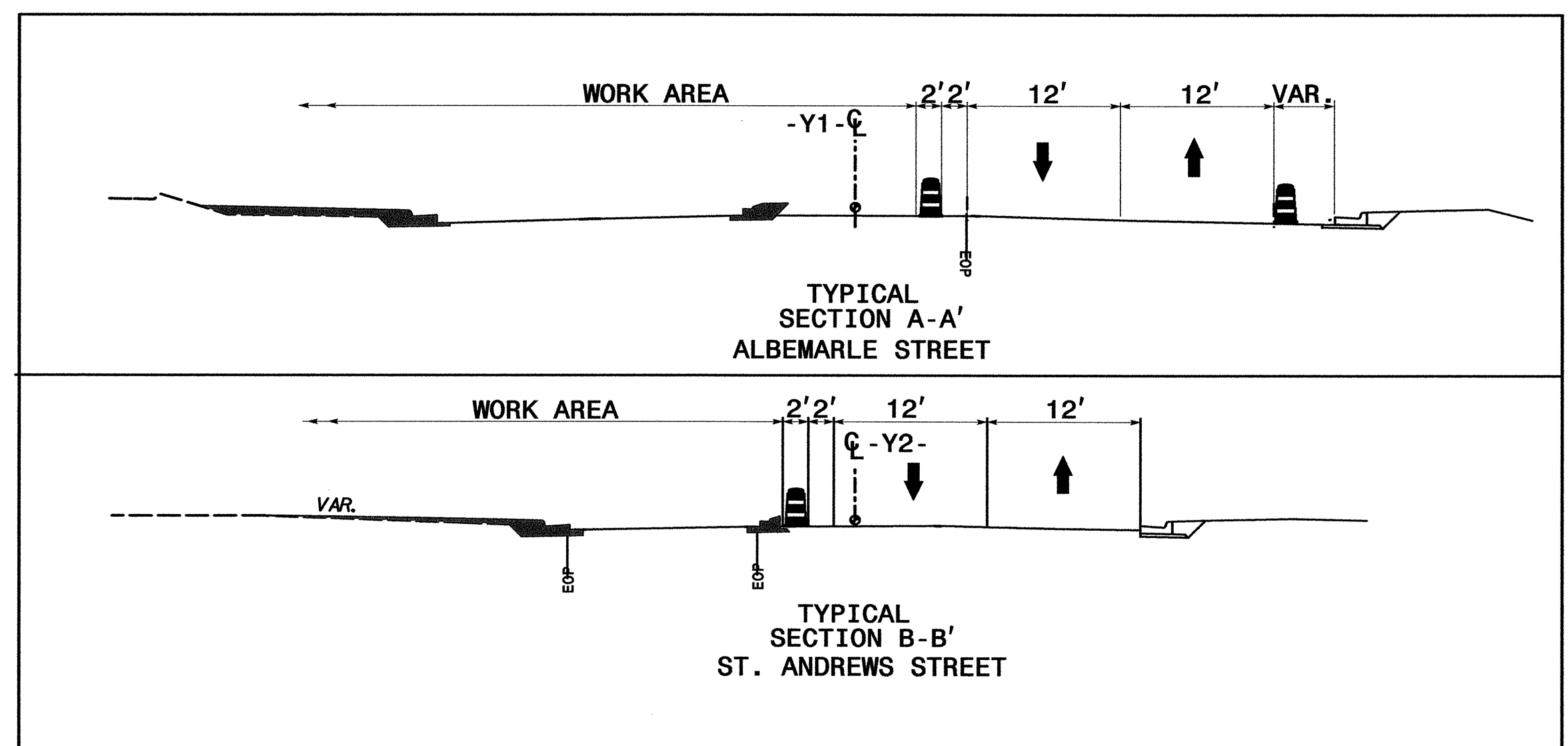
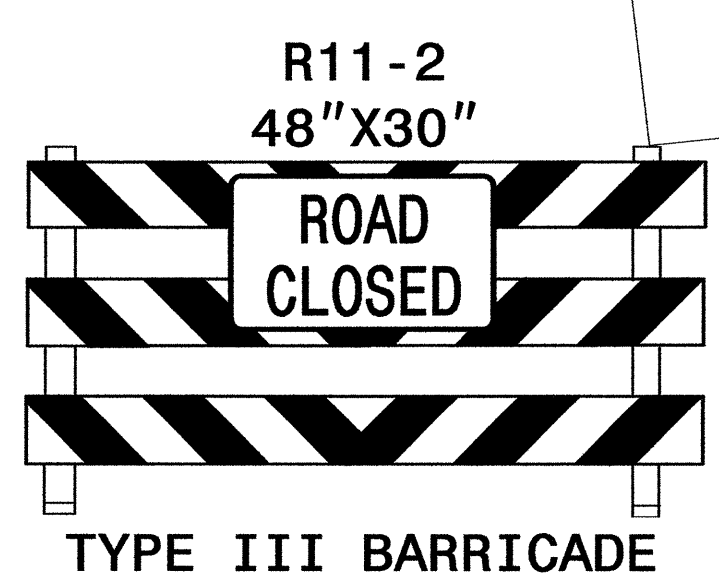
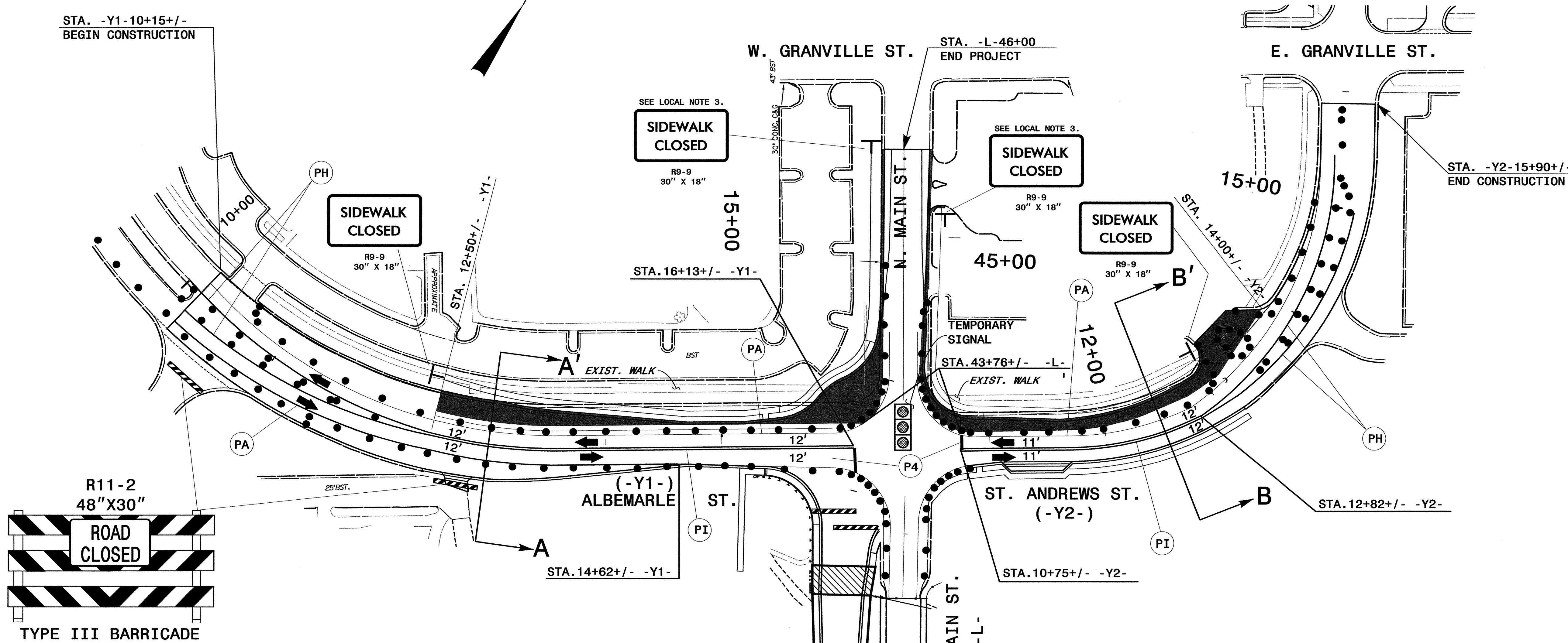
**PHASE III STEP 3
DETAIL 2**

SCALE: 1" = 50'

DATE: 08/03/08
DWG. BY: JAP
DESIGN BY: JAP
REVIEWED BY: RBE

REVISIONS	

CADD FILE



NOTE:
STOP BARS ARE TO BE LOCATED A MINIMUM OF 10' EDGE OF TRAVEL LANE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

NOTE:
SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

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SEAL 023521
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**PHASE III STEP 4
DETAIL 3**

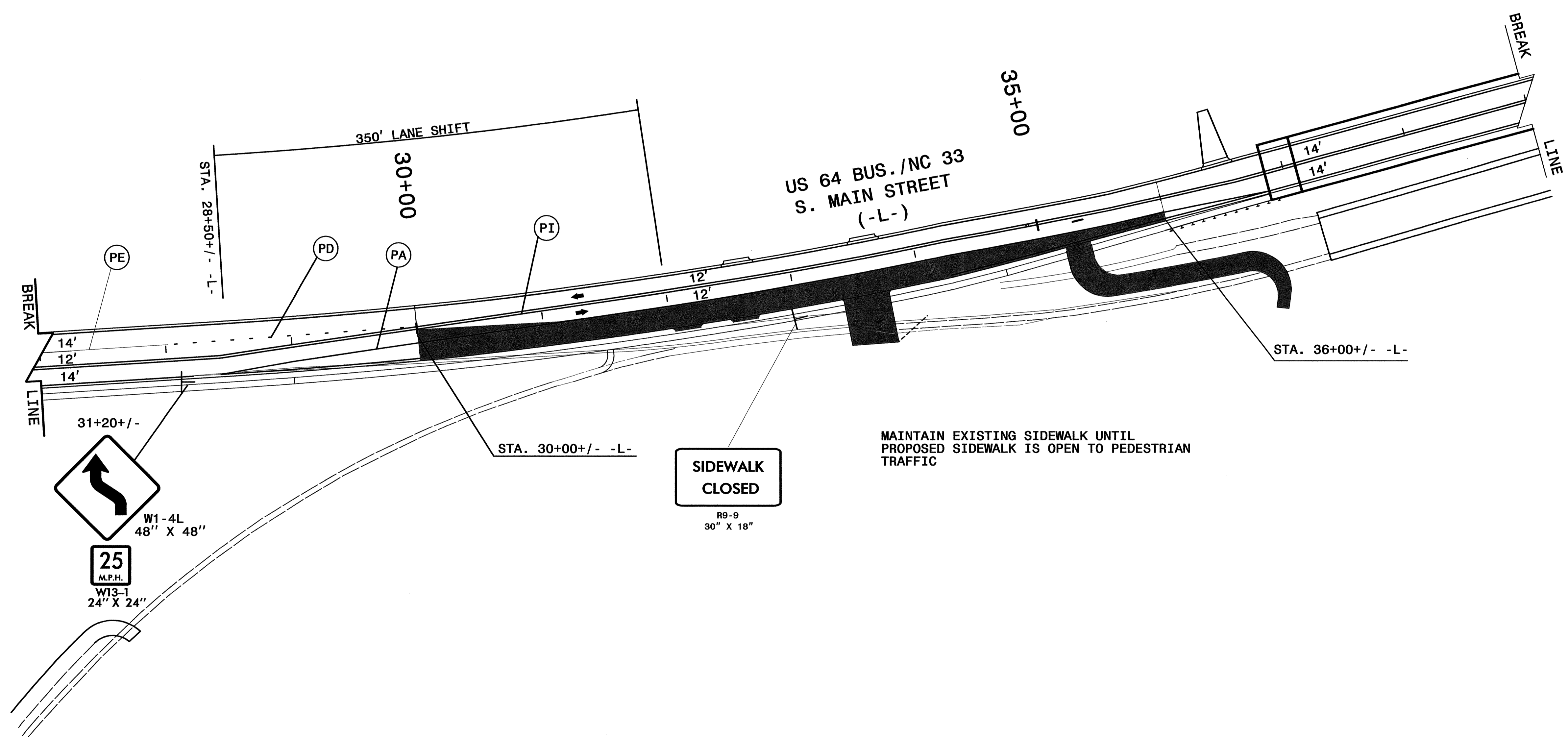
SCALE: 1" = 50'
DATE: 08/03/08
DWG. BY: JAP
DESIGN BY: JAP
REVIEWED BY: RBE

REVISIONS

CADD FILE

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8/18/2008
Y:\NCGDOT\B2965\TrafficControl\TCP\B2965-fe_TCP15p3s4.dgn
USER:NAME



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8/18/2008
Y:\NCDOT\2965\Traffic\TrafficControl\TCP\B2965-fe_TCP16p4.dgn
-USERNAME-

NOTE:
* SEE SHEET TCP-1A FOR TEMPORARY PAVEMENT MARKING SCHEDULE.

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APPROVED: *Ronda B. Early* DATE: 8-18-08

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PROFESSIONAL
SEAL
023521
ENGINEER
RONDA B. EARLY

**PHASE IV
DETAIL 1**

SCALE: 1" = 50'

DATE: 08/03/08

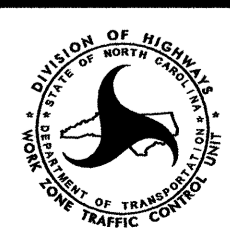
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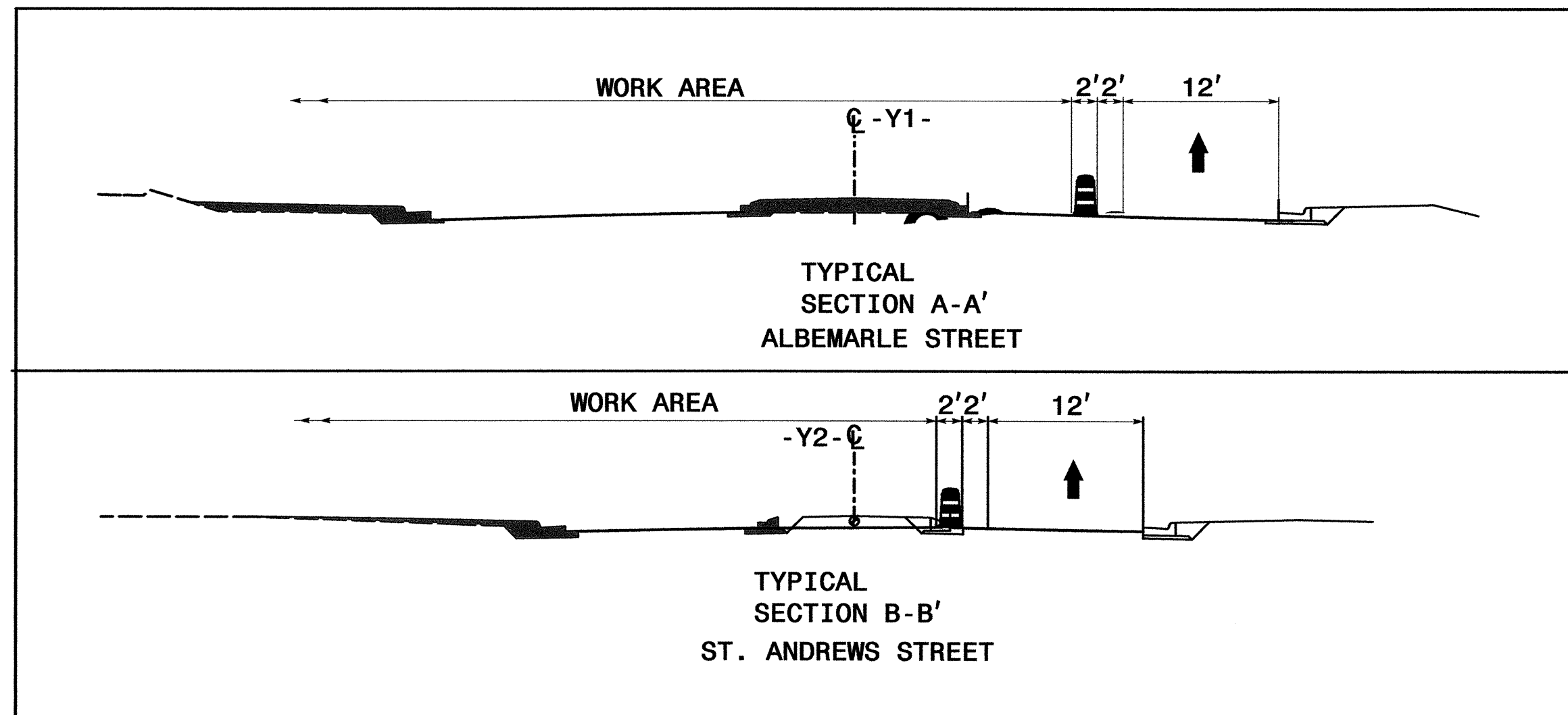
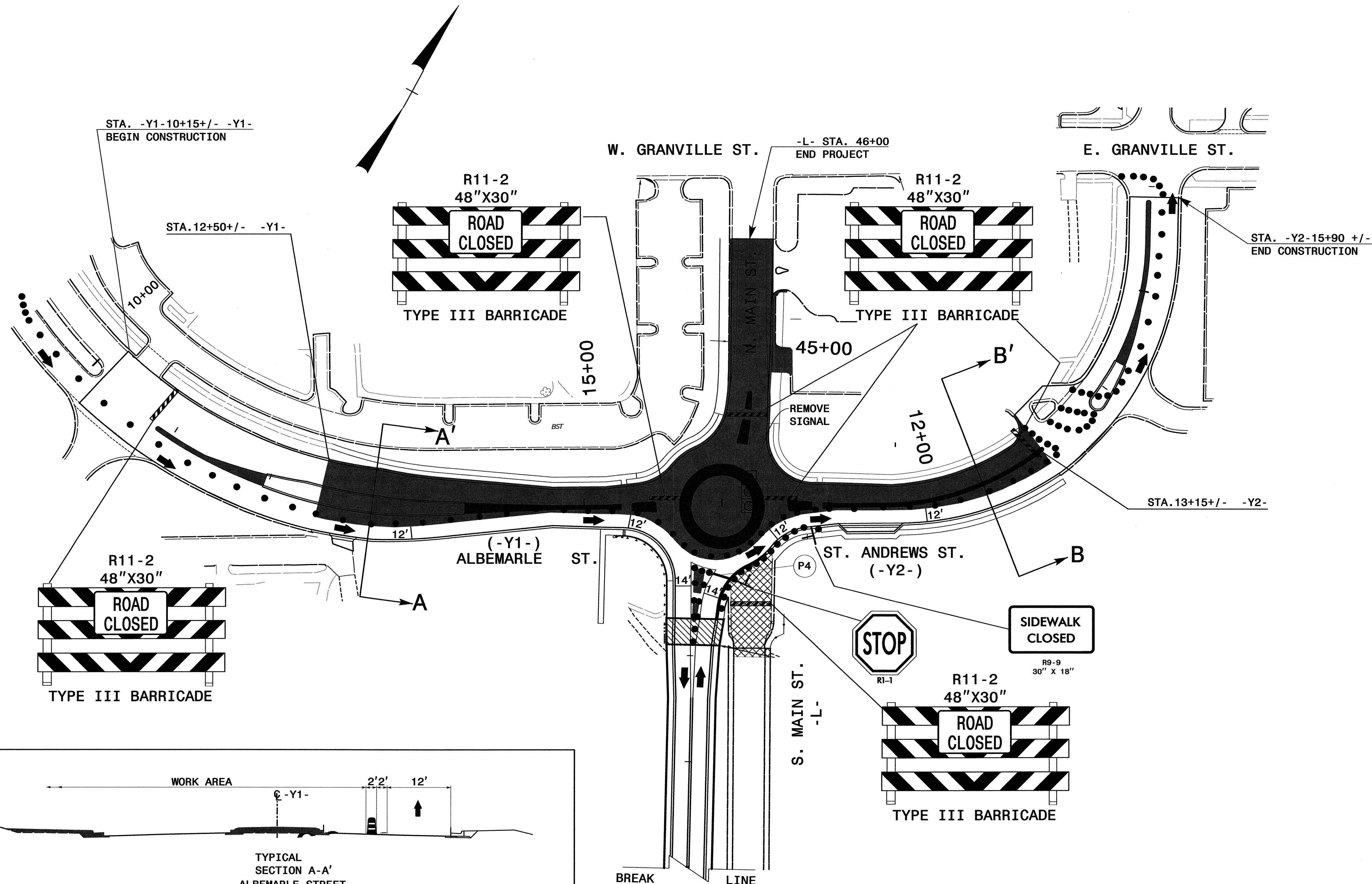
DESIGN BY: JAP

REVIEWED BY: RBE

REVISIONS

CADD FILE





REVISIONS

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APPROVED: *[Signature]* DATE: 8-18-08
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 NORTH CAROLINA PROFESSIONAL ENGINEER
 RONDA B. EARLY
 023521

**PHASE IV
DETAIL 2**

SCALE: 1" = 50'		REVISIONS
DATE: 08/03/08		
DWG. BY: JAP		
DESIGN BY: JAP		
REVIEWED BY: RBE		

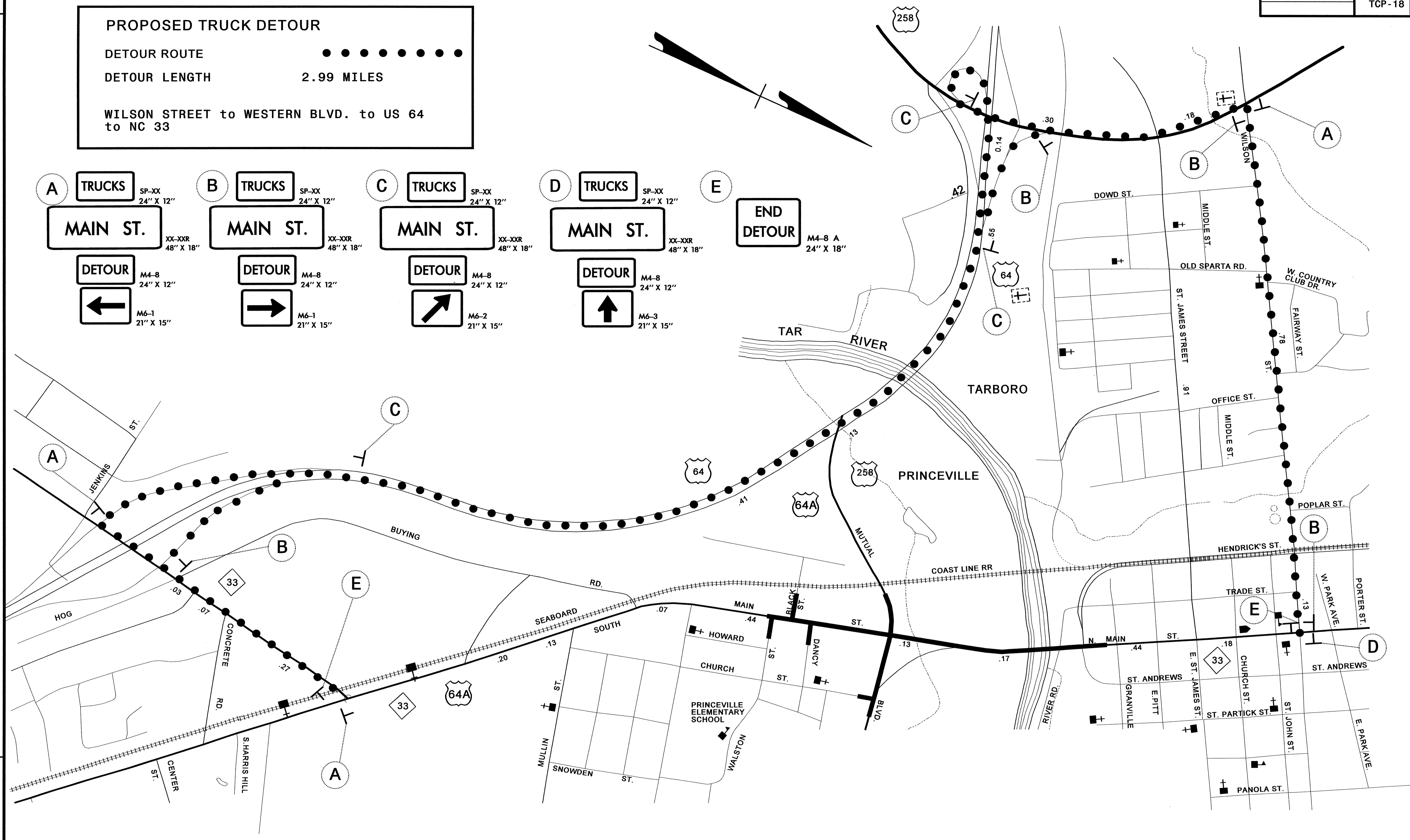
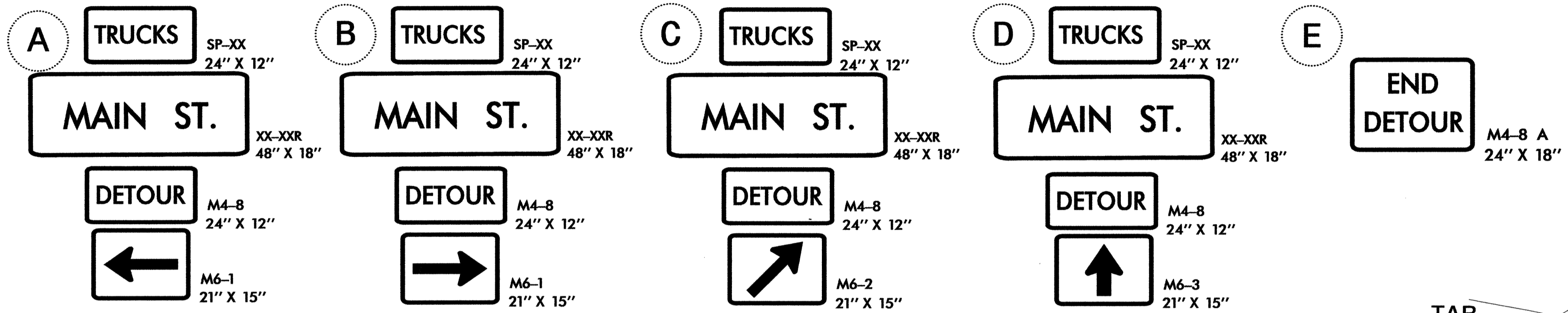
CADD FILE

PROPOSED TRUCK DETOUR

DETOUR ROUTE 

DETOUR LENGTH **2.99 MILES**

WILSON STREET to WESTERN BLVD. to US 64 to NC 33



REVISIONS

8/18/2008 Y:\NCDOT\B2965\Traffic\TrafficControl\TCP\B2965-fe_TCP18trdetour.dgn USERNAME

CH ENGINEERING

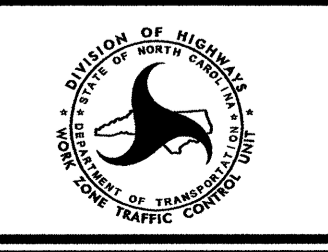
PO BOX 30128 TELE 919.788.0224
RALEIGH, NC 27622 FAX 919.788.0232

APPROVED: *Ronda B. Early* DATE: 8-19-08

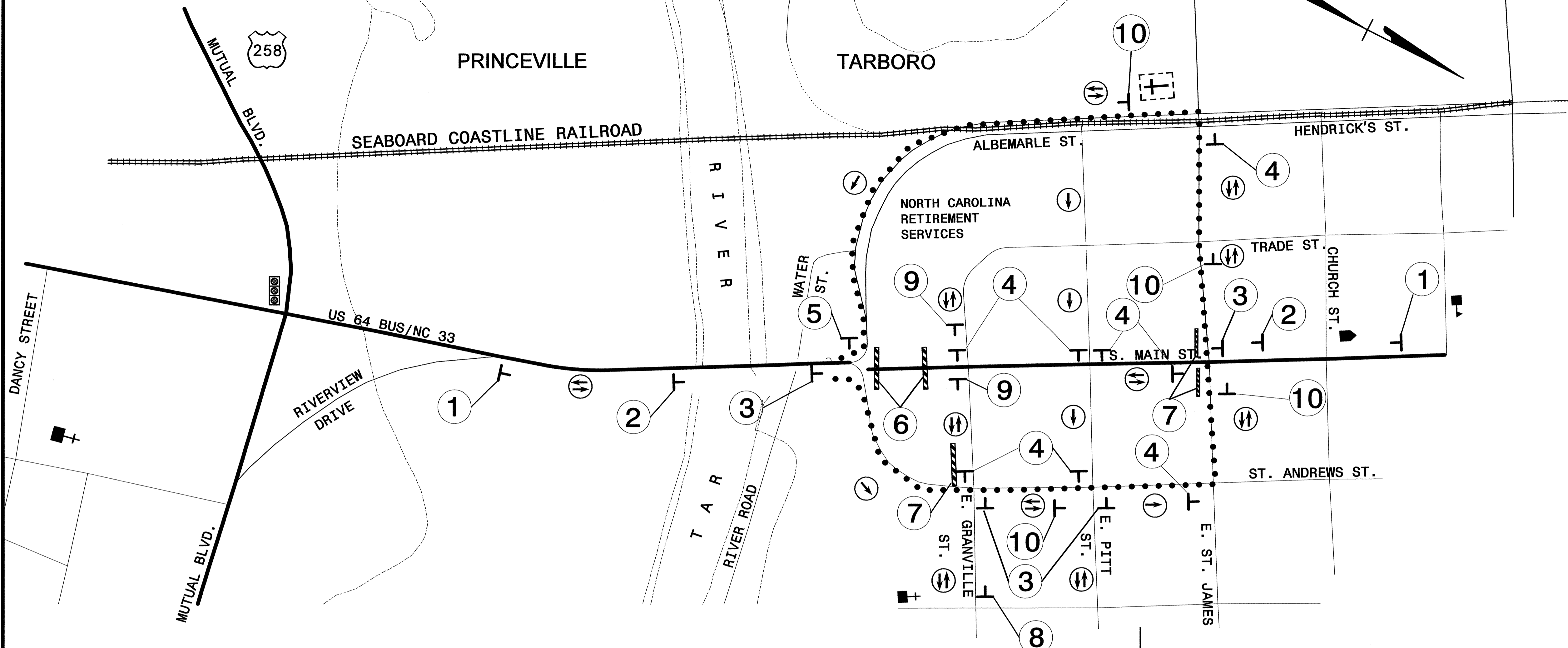
SEAL

TRUCK DETOUR ROUTE AND SIGNING

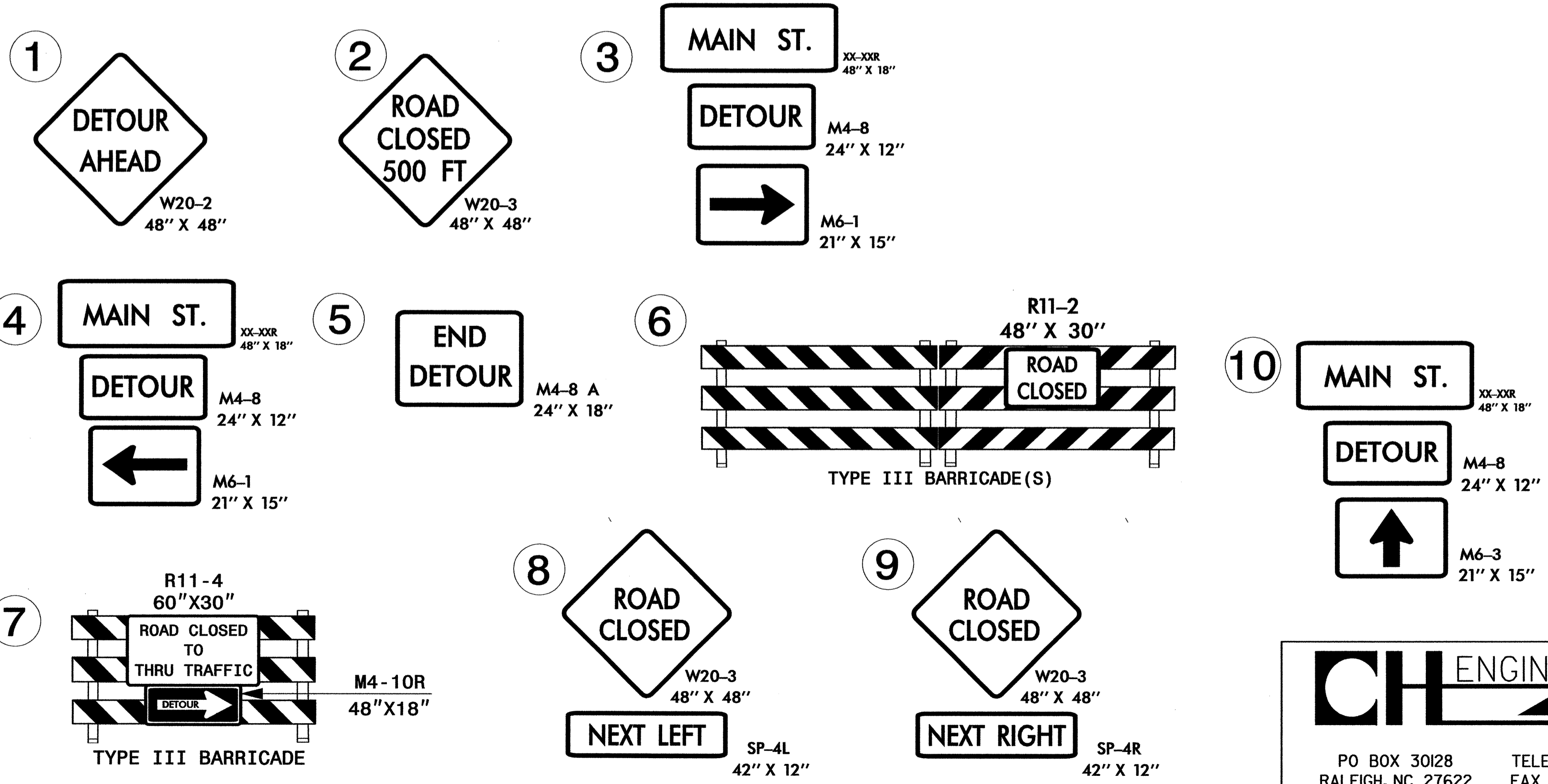
SCALE: NONE
DATE: 08/03/08
DWG. BY: JAP
DESIGN BY: JAP
REVIEWED BY: RBE



REVISIONS	



THE FOLLOWING DETOUR IS RECOMMENDED WHEN TRAFFIC IS IN A TEMPORARY PATTERN DURING THE CONSTRUCTION OF THE PROPOSED ROUNDABOUT AND NORTH MAIN STREET IS CLOSED SOUTH OF WEST GRANVILLE STREET.



PROPOSED MAIN STREET DETOUR

DETOUR ROUTE
 DETOUR LENGTH .5 MILE

ST. JAMES STEET TO ALBEMARLE STREET
 TO MAIN STREET

REVISIONS

8/18/2008 Y:\NCDOT\2965\TrafficControl\TCP\B2965-fe_TCP19detour.dgn USER:NAME

CH ENGINEERING

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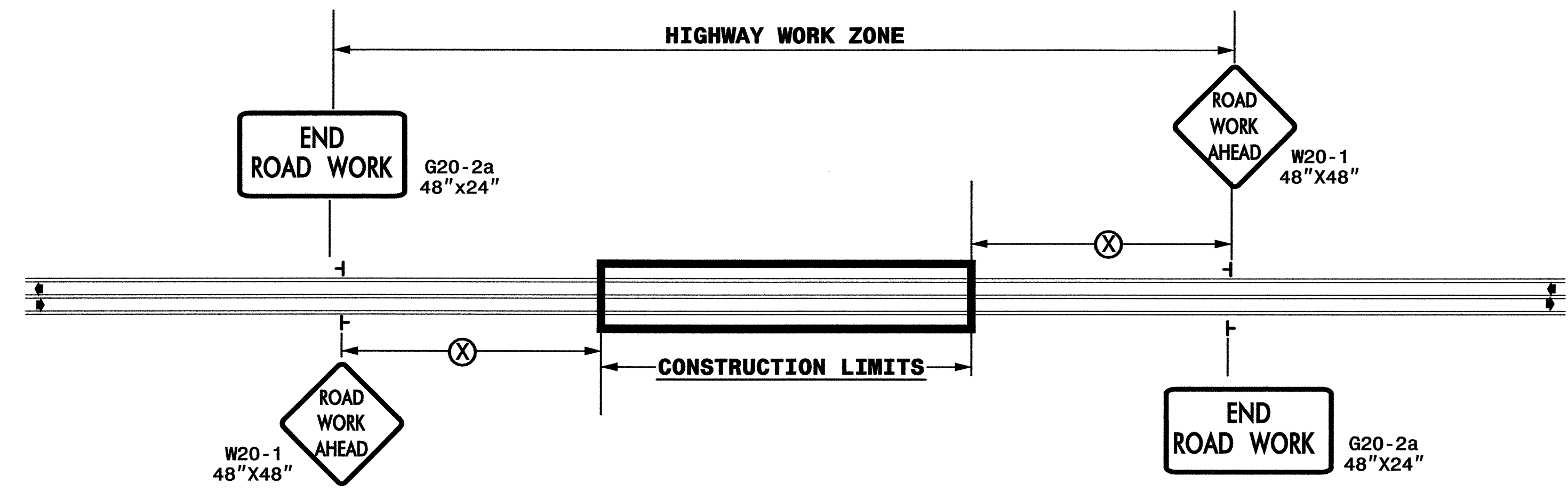
APPROVED: *Ronda B. Early* DATE: 8-18-08

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 NORTH CAROLINA PROFESSIONAL ENGINEER
 SEAL 023521
 RONDA B. EARLY

TEMPORARY OFFSITE DETOUR

SCALE:	DATE: 08/03/08		REVISIONS _____ _____ _____
DWG. BY: JAP	DESIGN BY: JAP		
REVIEWED BY: RBE			
CADD FILE			

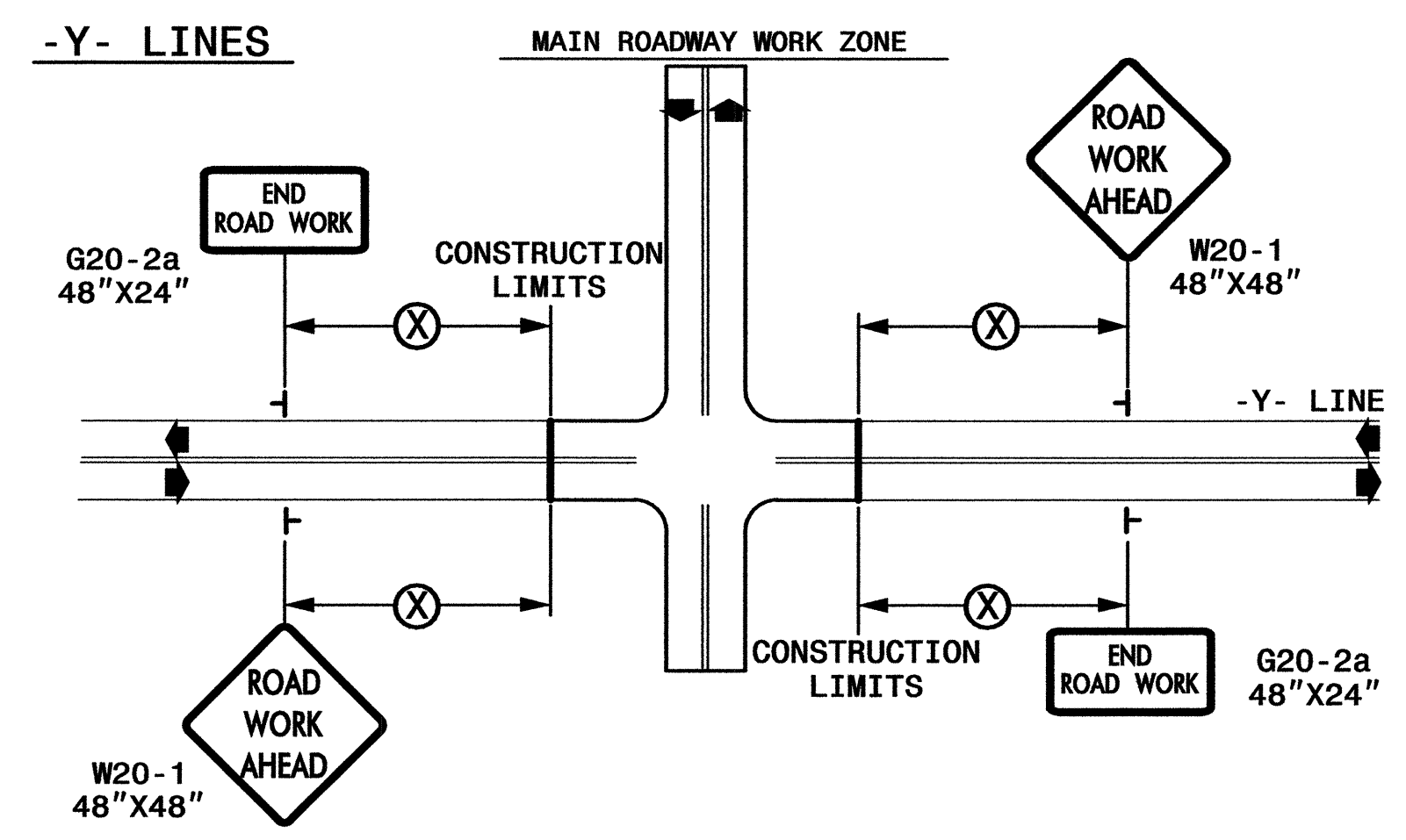
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

┆ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: *[Signature]* DATE: 8-18-08



DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS

SCALE: NONE		REVISIONS	
DATE:		7-98	10/01
DWG. BY:		10-98	03/04
DESIGN BY:		01/01	11/04
REVIEWED BY:			

PROJ. REFERENCE NO.	SHEET NO.
B-2965	TCP-21

Temporary Shoring No. 1

FOR TEMPORARY SHORING, SEE TEMPORARY SHORING SPECIAL PROVISION.

DO NOT USE TEMPORARY MSE WALL FROM STATION 42+00-L- TO STATION 42+27-L-,
31 FT. RIGHT.


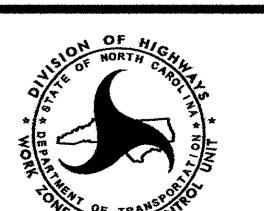
WHEN USING CONTRACTOR DESIGNED SHORING FROM STATION 42+00-L- TO STATION 42+27-L-,
DESIGN SHORING FOR THE FOLLOWING IN-SITU ASSUMED SOIL PARAMETERS:

- UNIT WEIGHT OF SOIL ABOVE WATER TABLE, $\gamma = 120$ PCF
- UNIT WEIGHT OF SOIL BELOW WATER TABLE, $\gamma' = 60$ PCF
- FRICTION ANGLE, $\phi = 30$ DEGREES
- COHESION, $c = 0$ PSF

FOR CONTRACTOR DESIGNED SHORING, SURVEY THE SHORING LOCATION TO DETERMINE
EXISTING ELEVATIONS AND ACTUAL DESIGN HEIGHTS BEFORE BEGINNING DESIGN.

THE INFORMATION SHOWN ON THIS SHEET WAS PROVIDED THROUGH A SEALED DOCUMENT.

THE DOCUMENT WAS SUBMITTED TO THE WORK ZONE TRAFFIC CONTROL UNIT ON JUNE 6, 2008 AND SEALED BY THEIN TUN ZAN, P.E., LICENSE # 30943

APPROVED: <i>Phonda B. Early</i> DATE: 6-29-08	TEMPORARY SHORING DATA	
	SCALE: NONE	
	DATE: JUNE 08	
	DWG. BY: JAP	
	DESIGN BY: JAP	
REVIEWED BY: RBE	REVISIONS	

8/29/2008
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 USERNAME