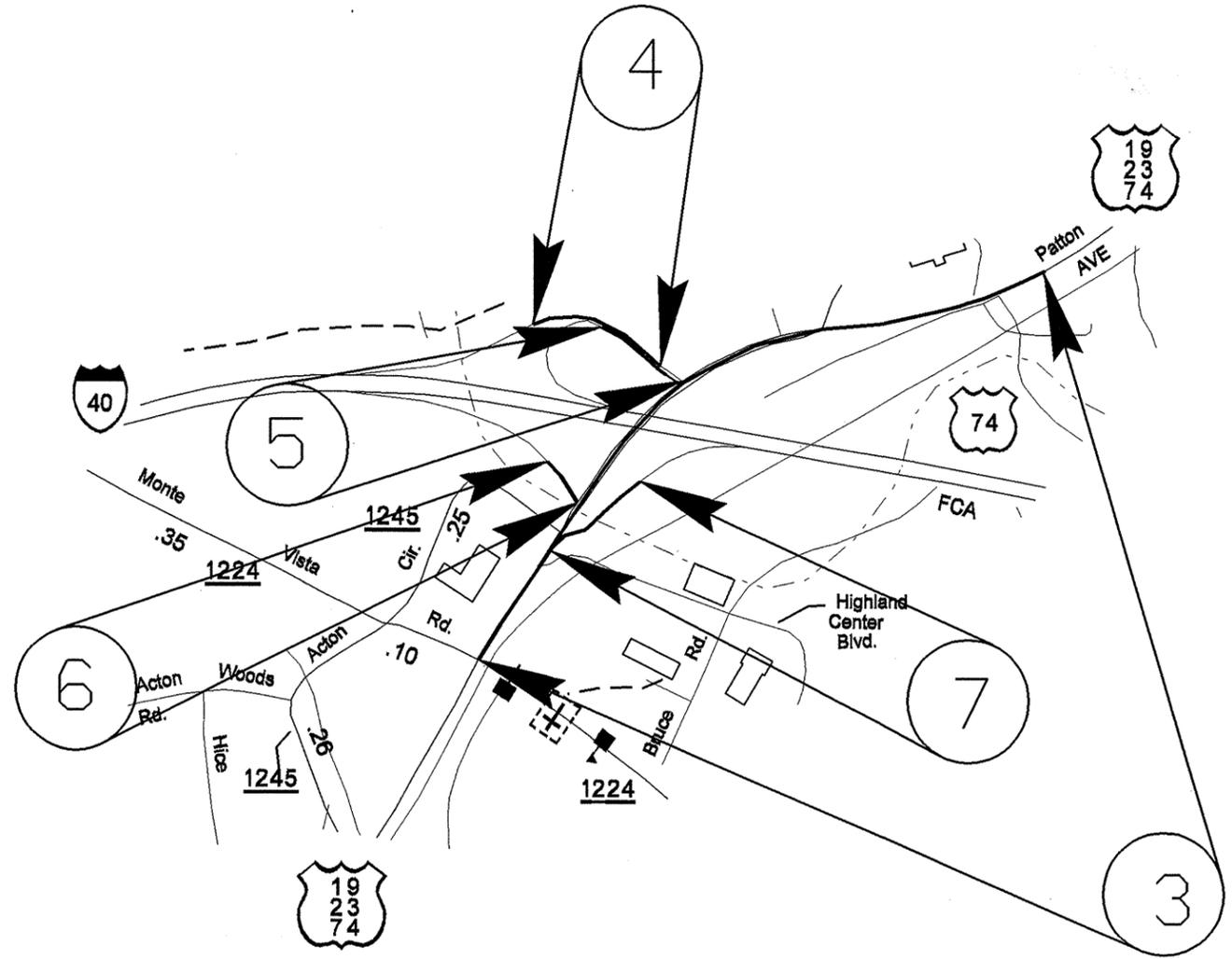
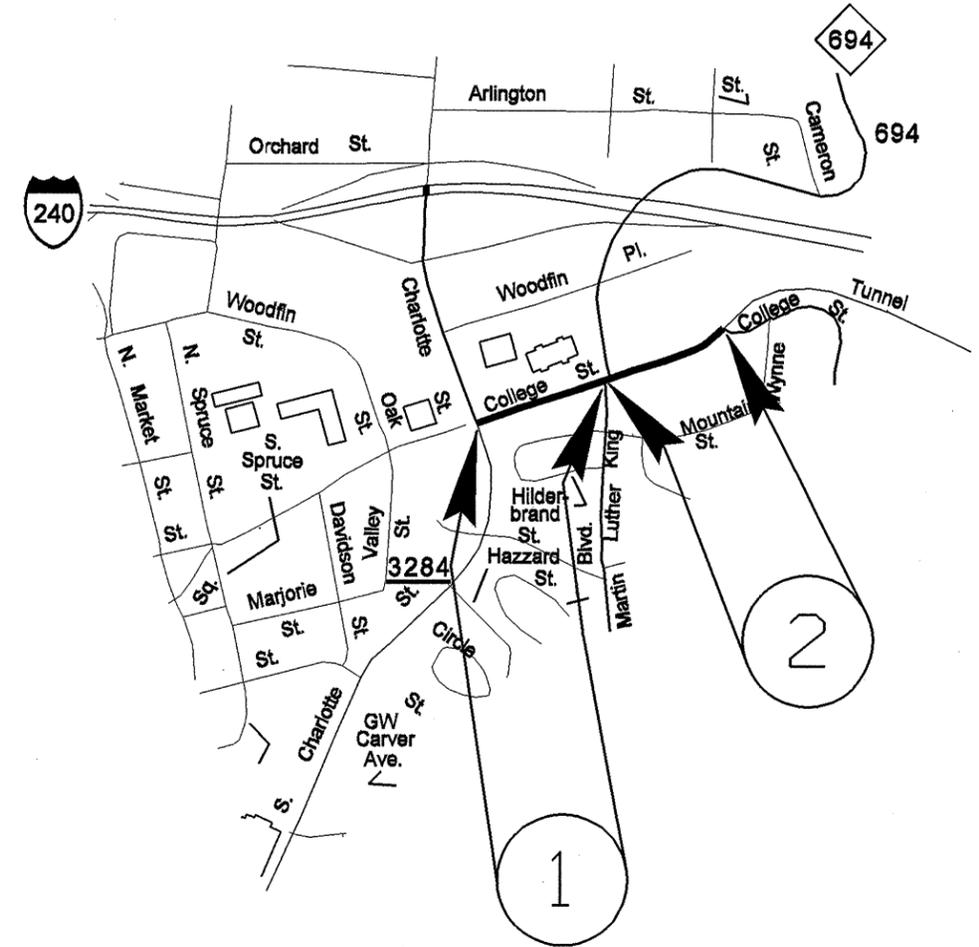
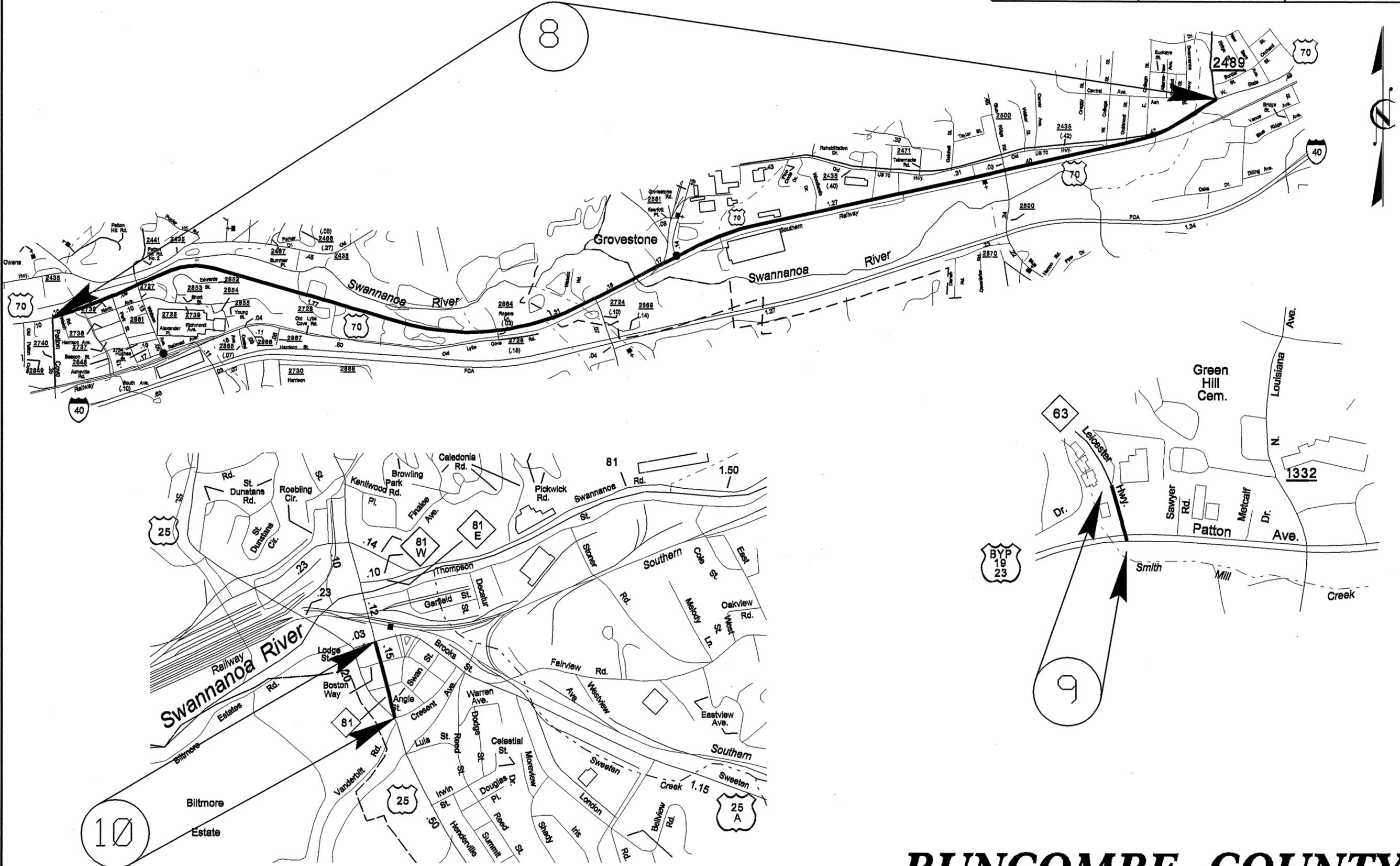


PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.8 , 13CR.20111.6	1	



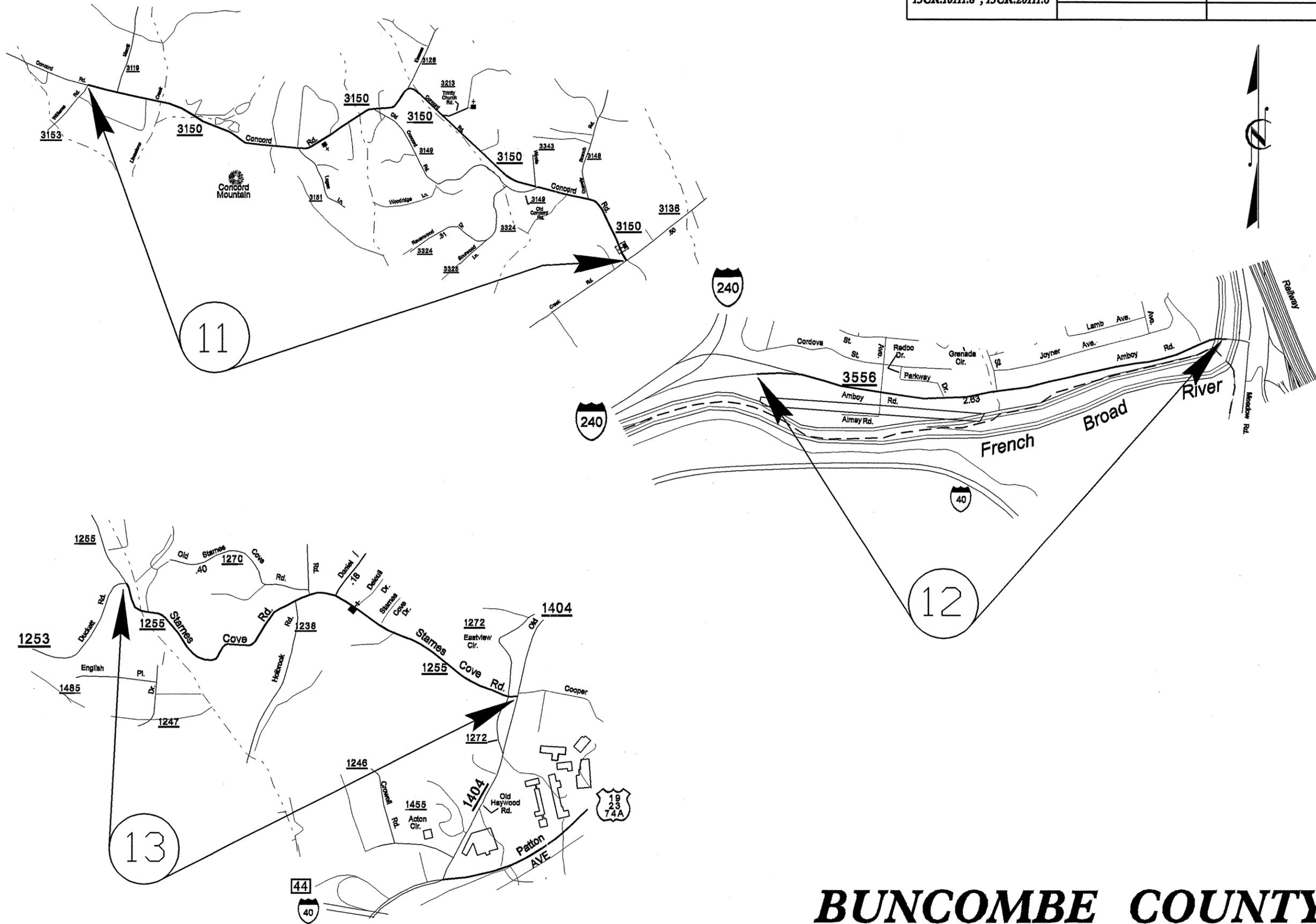
BUNCOMBE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.1011.8 , 13CR.2011.6	2	



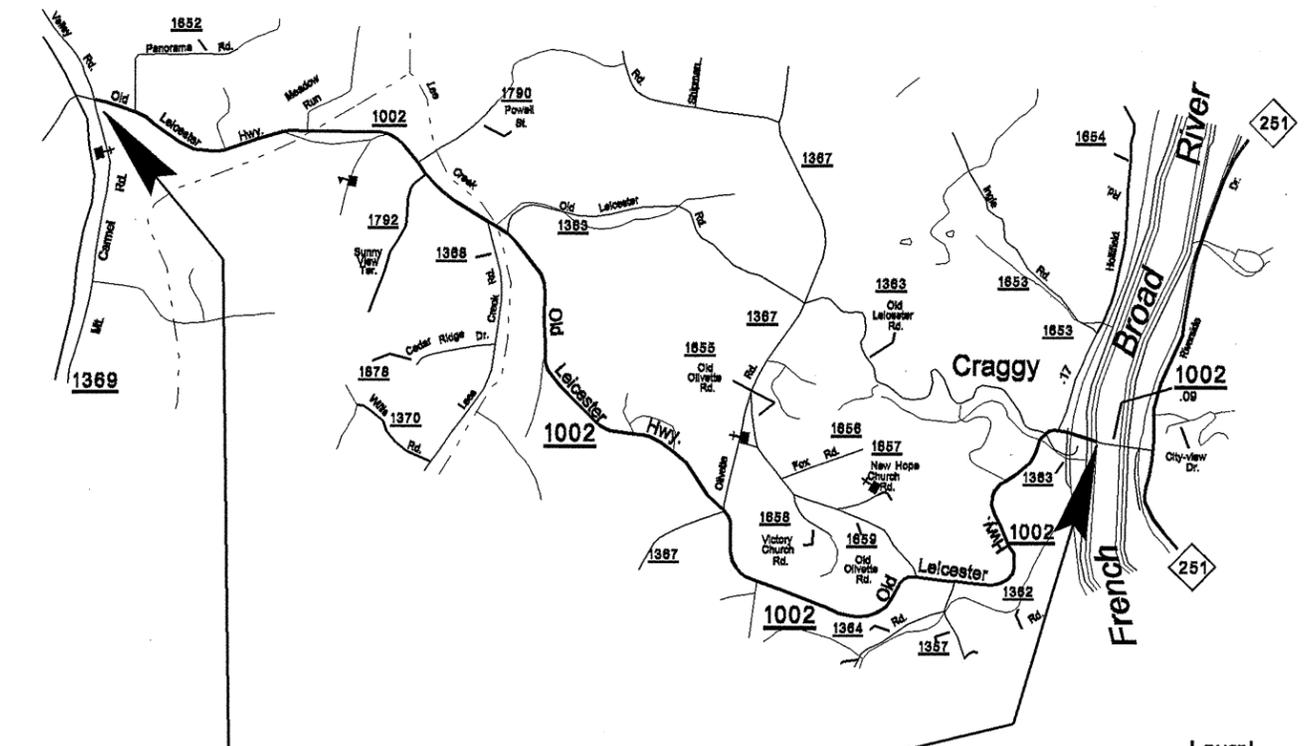
BUNCOMBE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.8 , 13CR.20111.6	3	

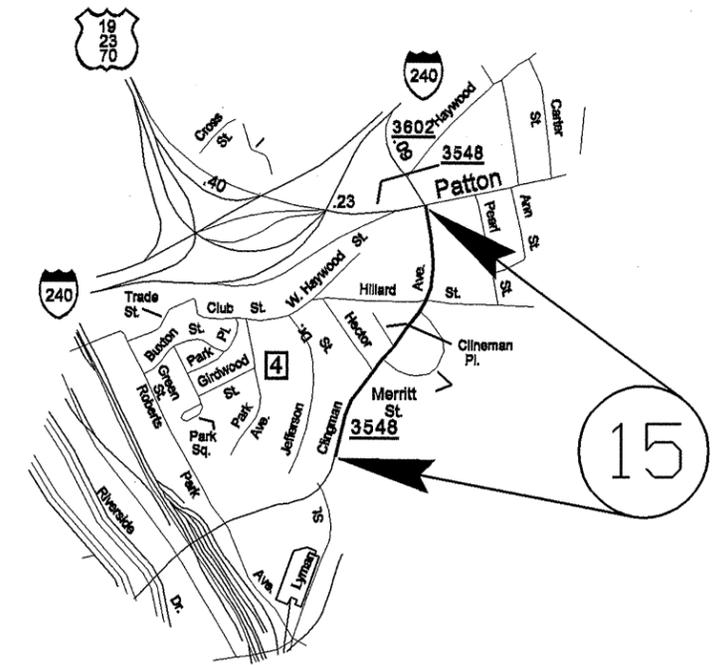


BUNCOMBE COUNTY

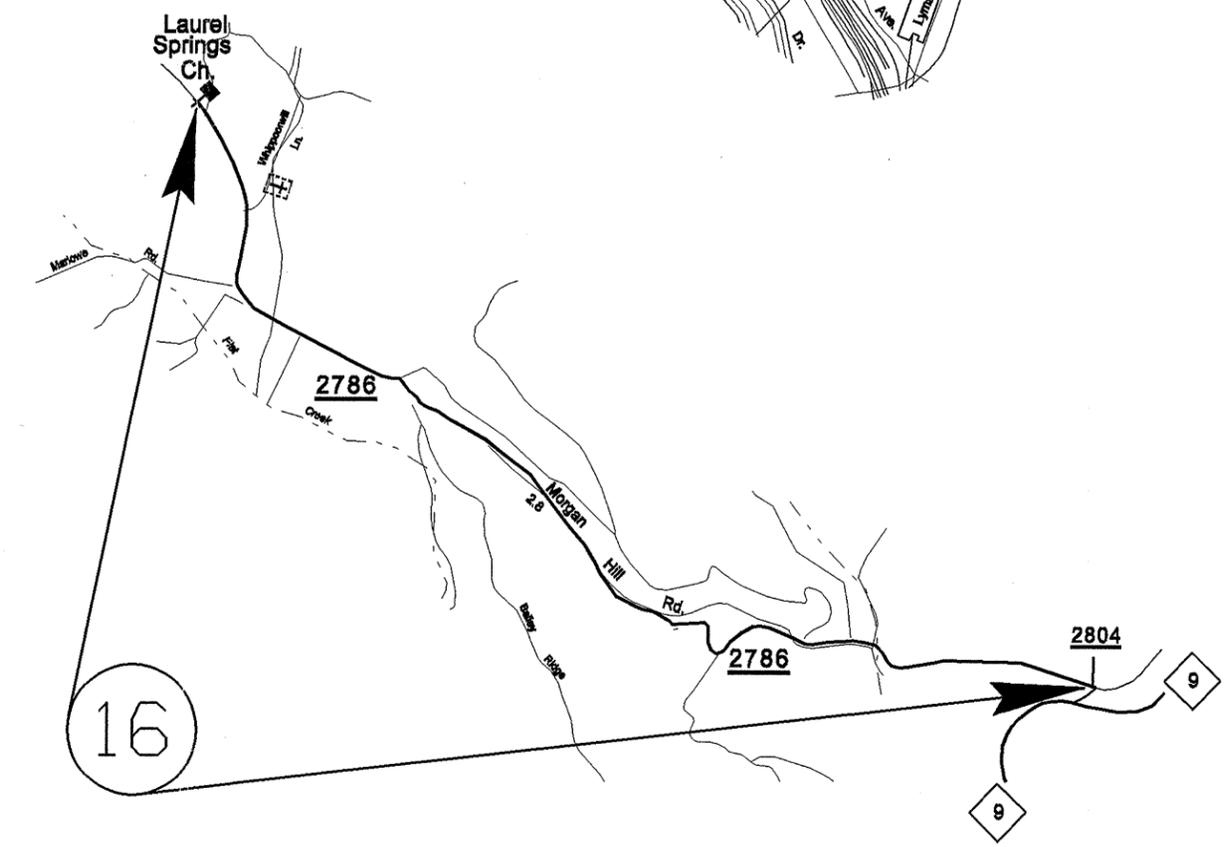
PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.8 , 13CR.20111.6	4	



14



15

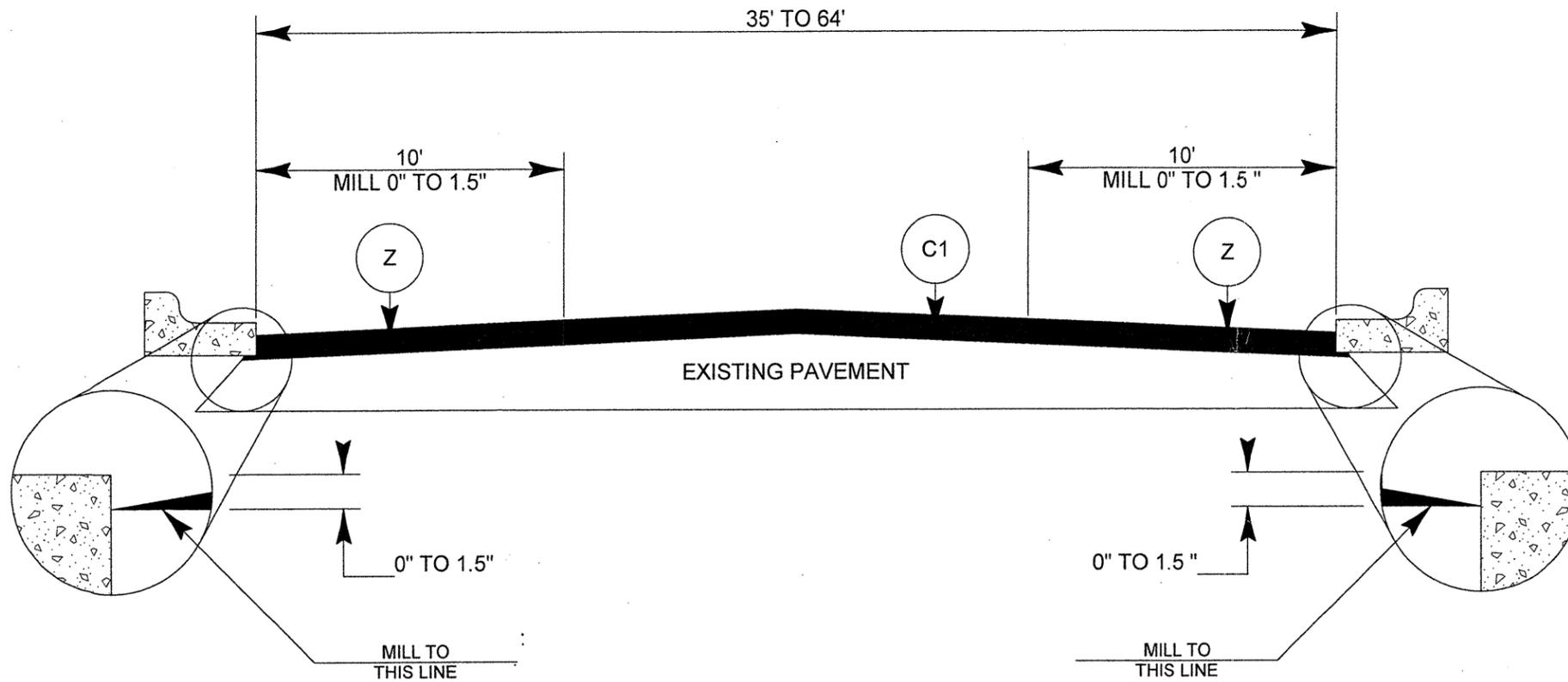


16

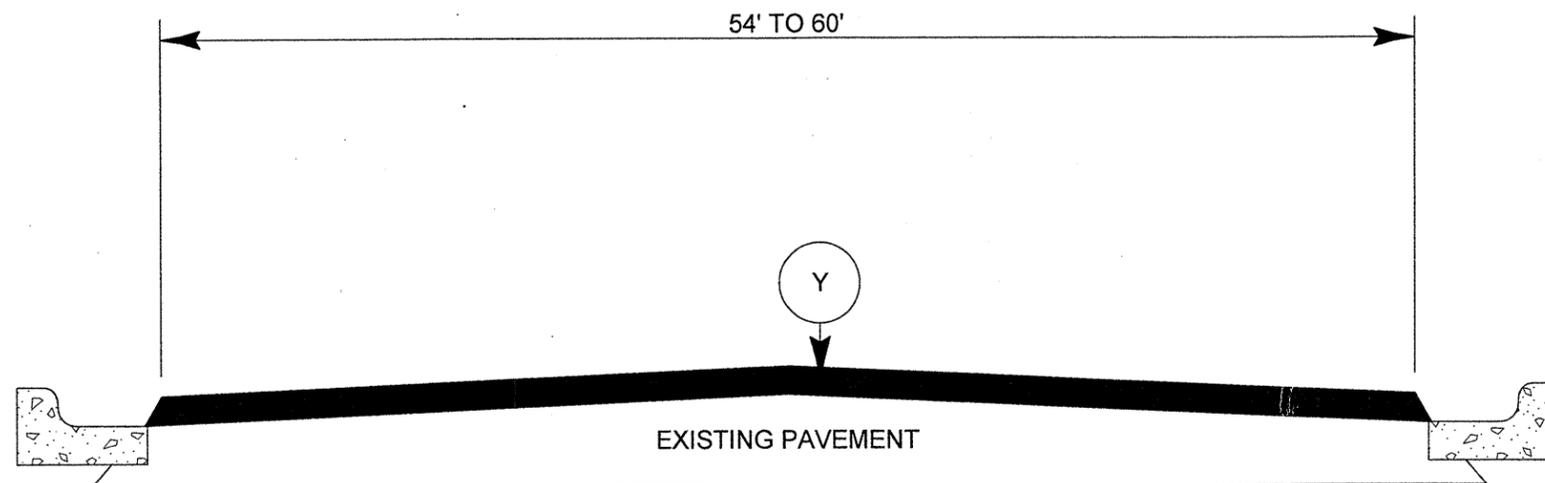
BUNCOMBE COUNTY



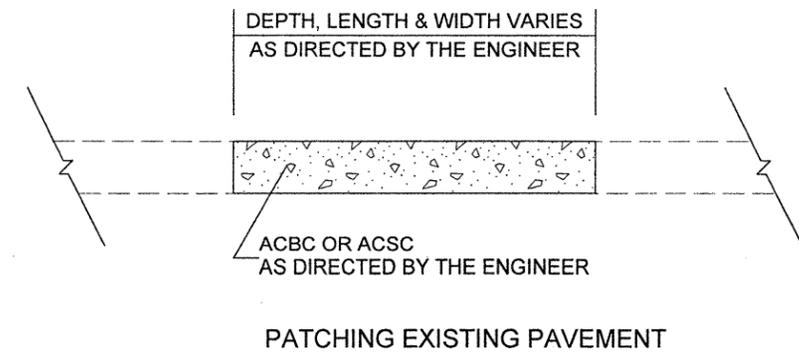
PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.8, 13CR.20111.6	5	



TYPICAL SECTION NO. 1

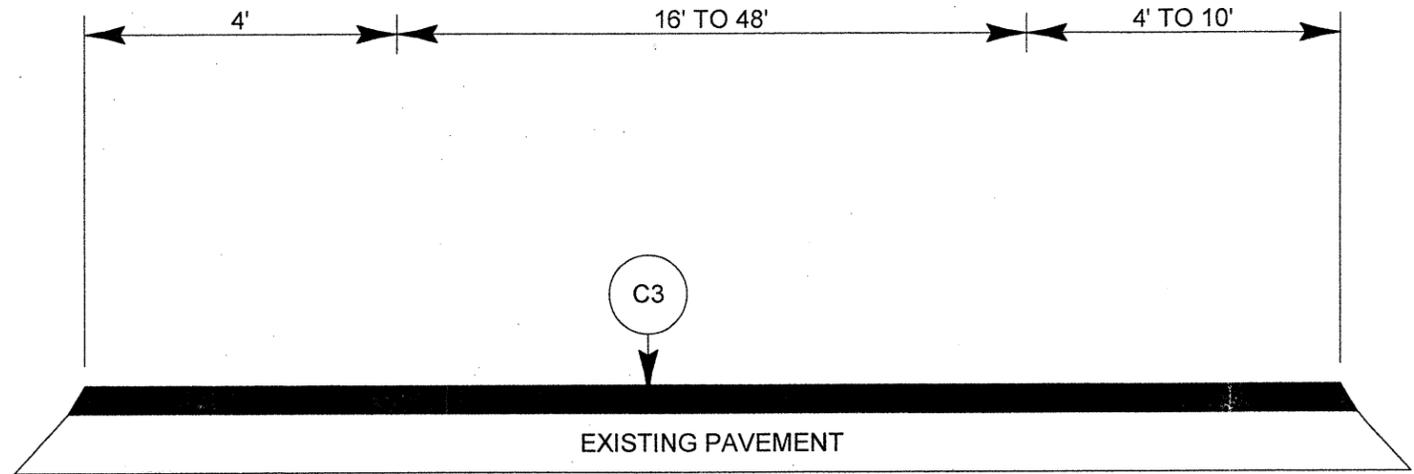


TYPICAL SECTION NO. 2

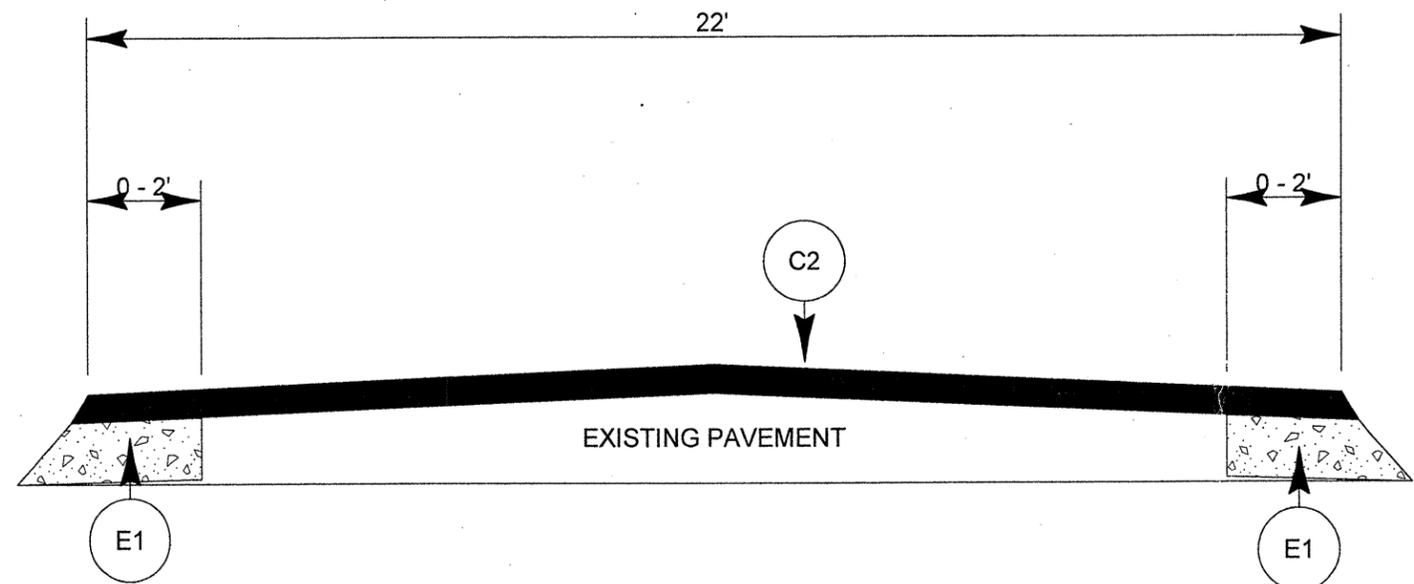


PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ACSC, TYPE S9.5B AT AN AVERAGE RATE OF 168 LBS PER SY
C2	PROP. APPROX. 1 1/2" ACSC, TYPE SF9.5A AT AN AVERAGE RATE OF 165 LBS. PER SY
C3	PROP. APPROX 1 1/2" ACSC, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SY
E1	PROP. APPROX. 6" ACBC, TYPE B25.0B AT AN AVERAGE RATE OF 684 LBS. PER SY
E2	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 114 LBS. PER SQ YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN 5 1/2" IN DEPTH
R1	EXPRESSWAY GUTTER
U1	EXISTING PAVEMENT
Y	PROP. APPROX 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS PER SQ. YARD
Z	MILL 0" TO 1 1/2"

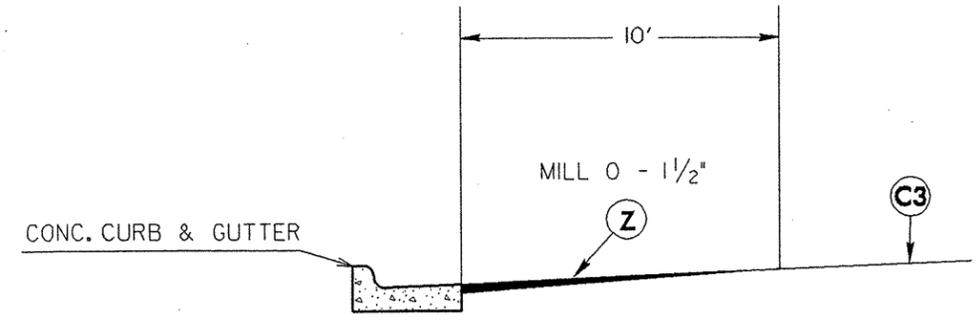
PROJECT NO. 13CR.10111.8, 13CR.20111.6	SHEET NO. 6	TOTAL SHEETS
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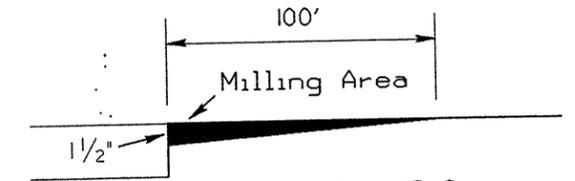
TYPICAL SECTION NO. 3



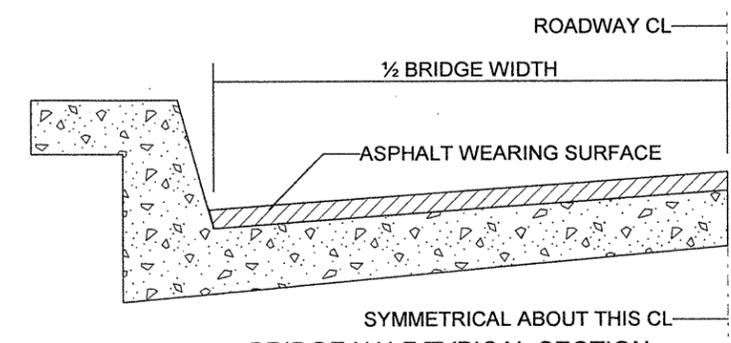
TYPICAL SECTION NO. 4



MILLING DETAIL NO. 1
TO BE USED WITH
TYPICAL NO. 3
IN LOCATION WHERE
CURB IS LOCATED



MILLING DETAIL NO. 2
MILLING DETAIL AT BRIDGE APPROACHES
COST OF MILLING IS INCIDENTAL TO OTHER ITEMS



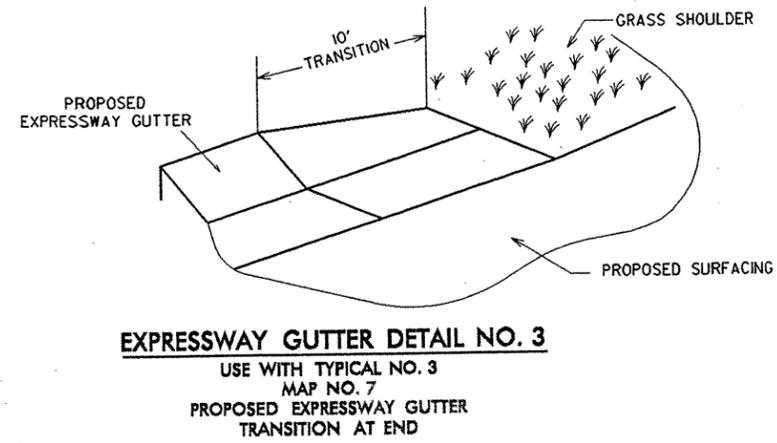
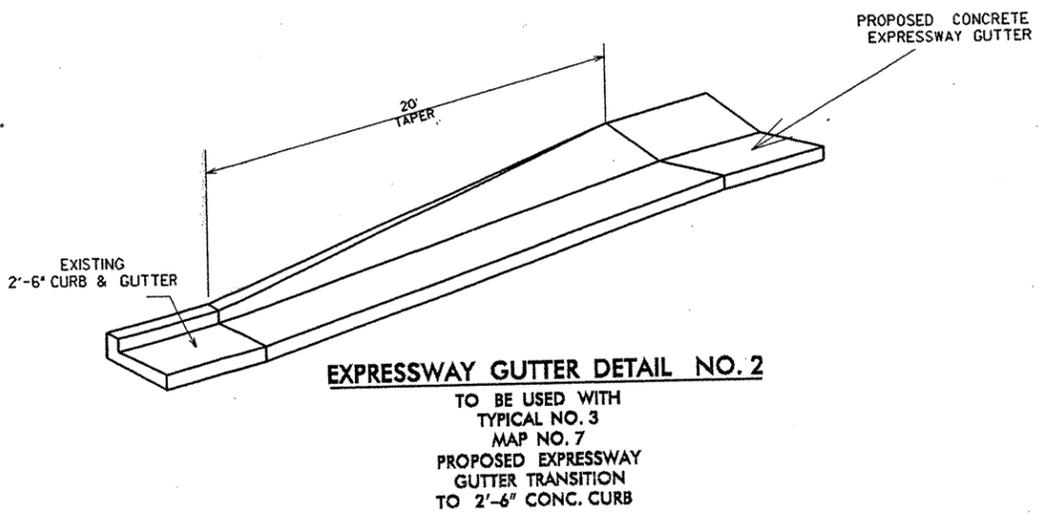
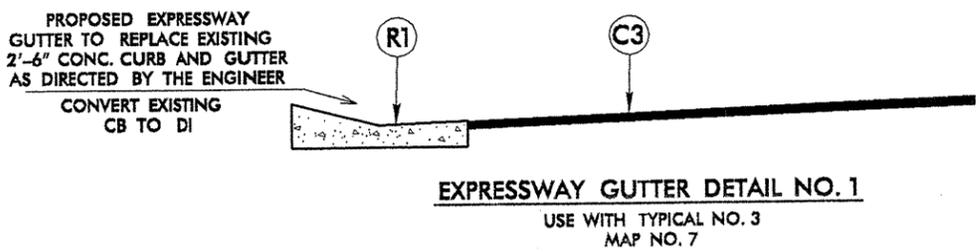
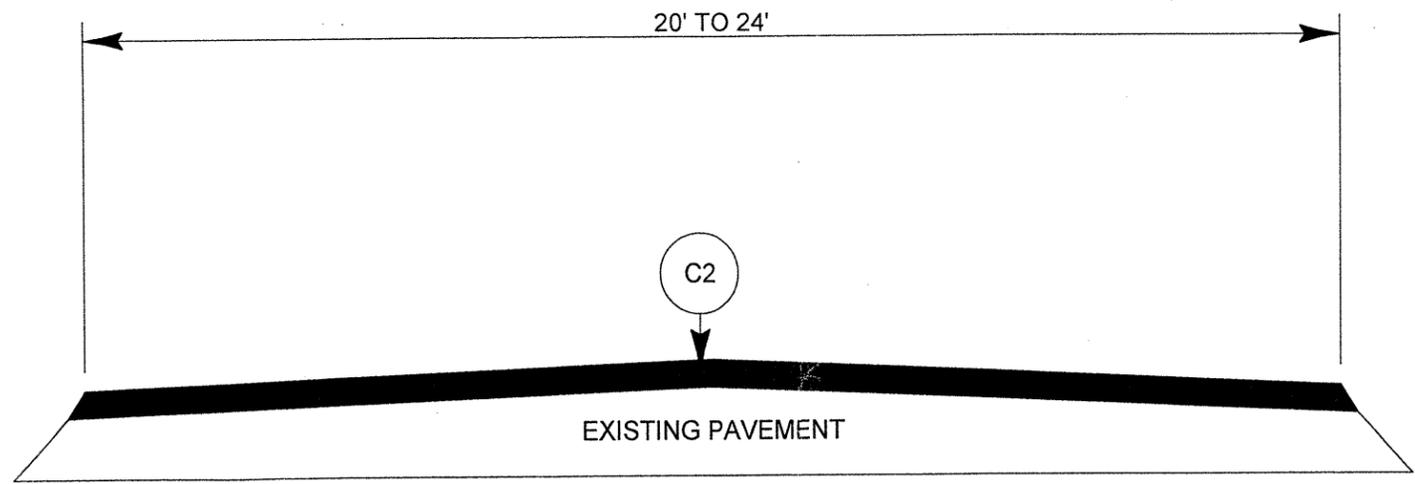
BRIDGE HALF TYPICAL SECTION
FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1 1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

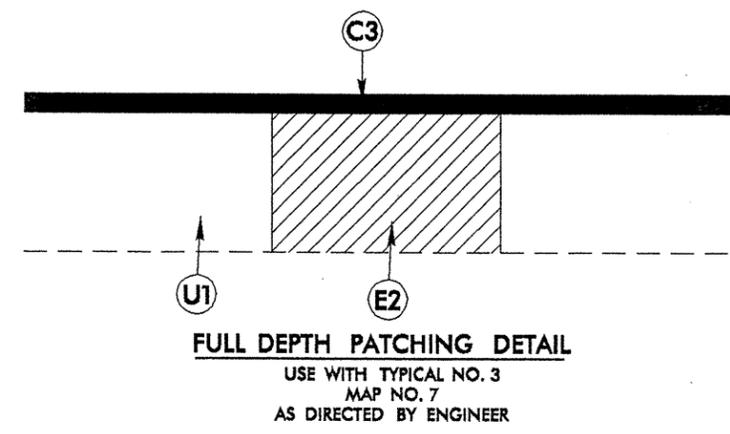
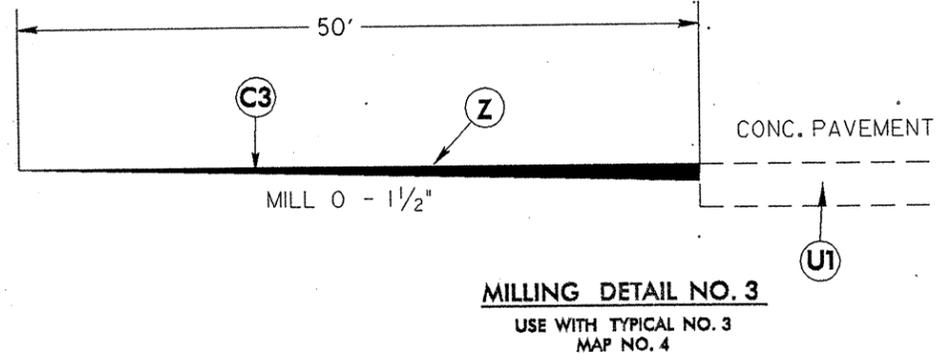
NOTES

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.
SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.
BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

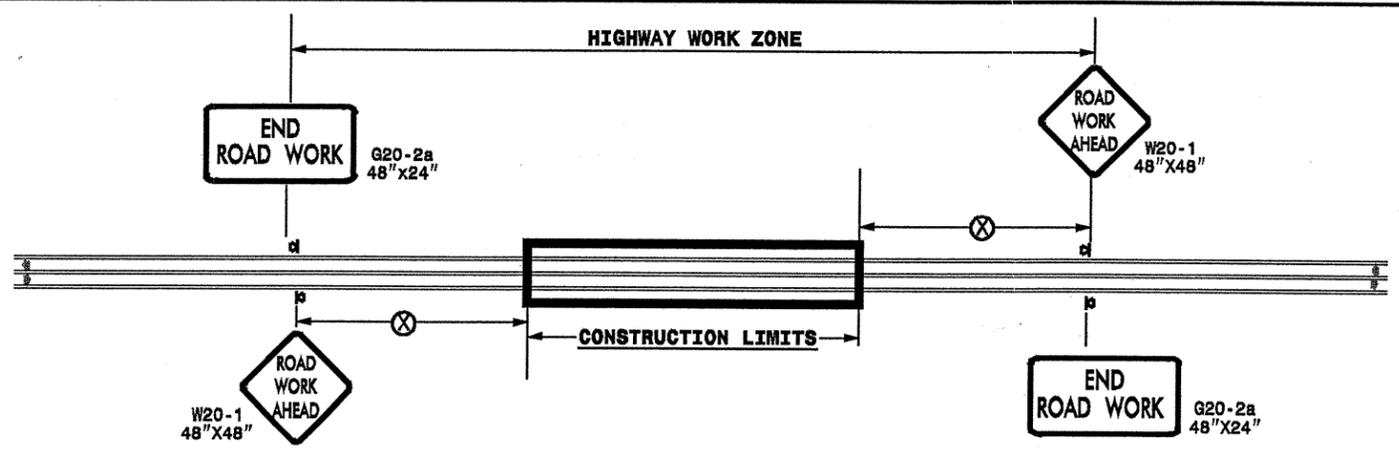
PROJECT NO. 13CR.10111.8, 13CR.20111.6	SHEET NO. 7	TOTAL SHEETS
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TYPICAL SECTION NO. 5



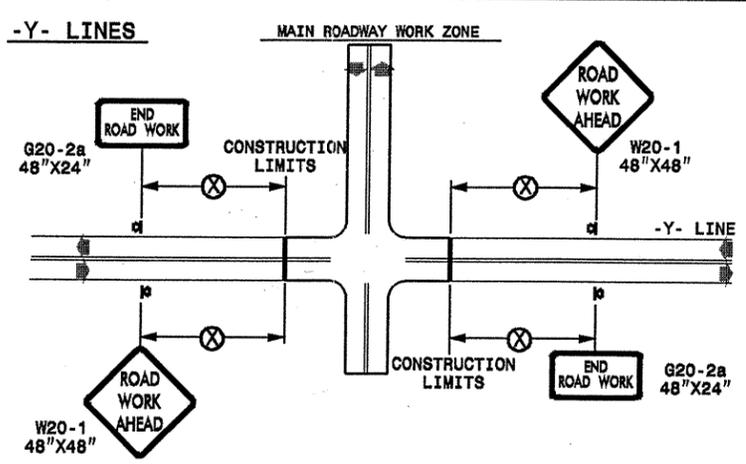
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

☒ PORTABLE SIGN

➔ DIRECTION OF TRAFFIC FLOW

DETAIL DRAWING
FOR TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS

SHEET 1 OF 1

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED ADVANCED WORK ZONE WARNING SIGNS	SCALE: NONE DATE: _____ DWG. BY: _____ DESIGN BY: _____ REVIEWED BY: _____		REVISIONS 7-98 10/01 10-98 03/04 01/01 11/04
SEAL					

16-FEB-2009 19:45
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