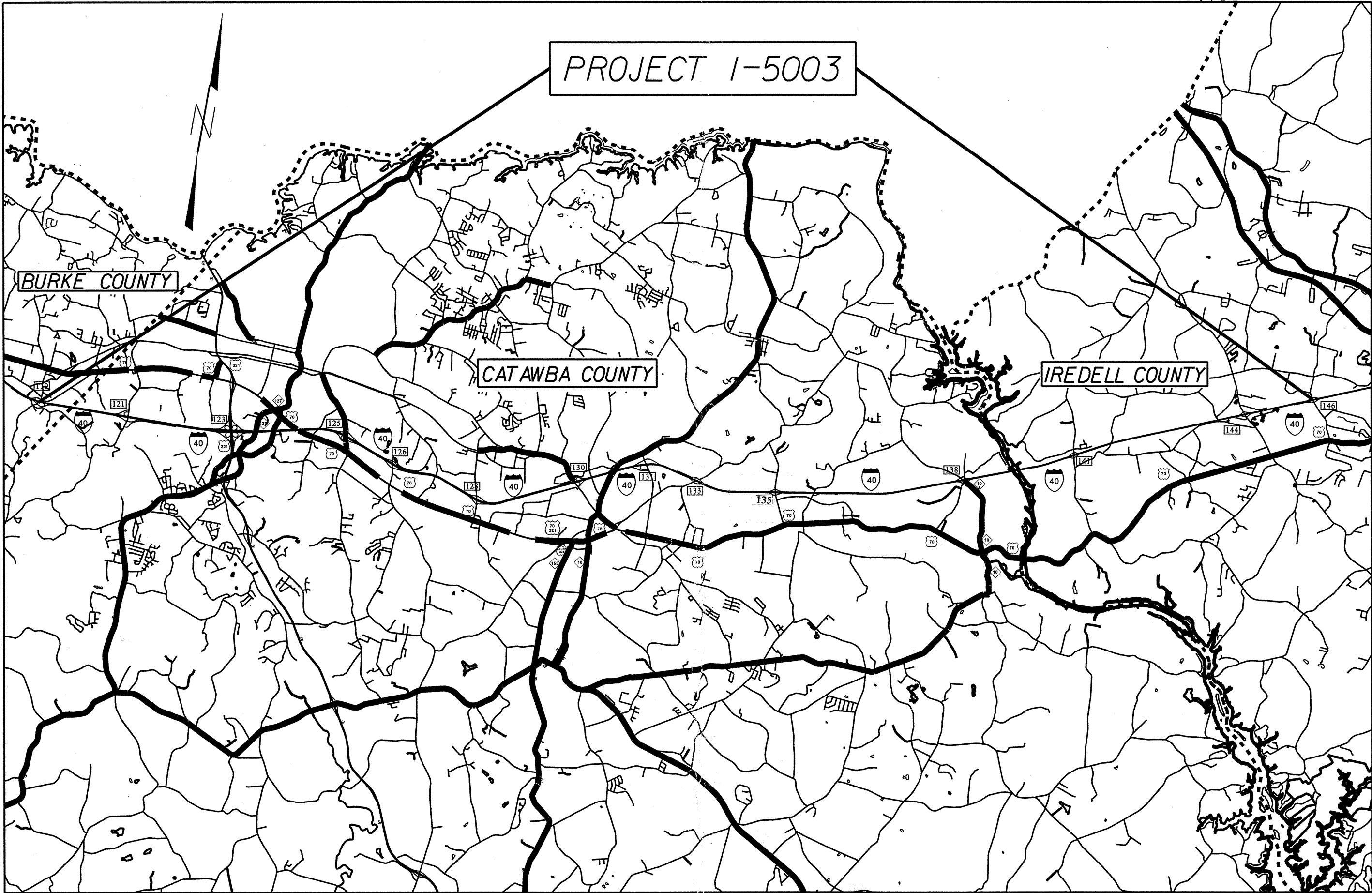
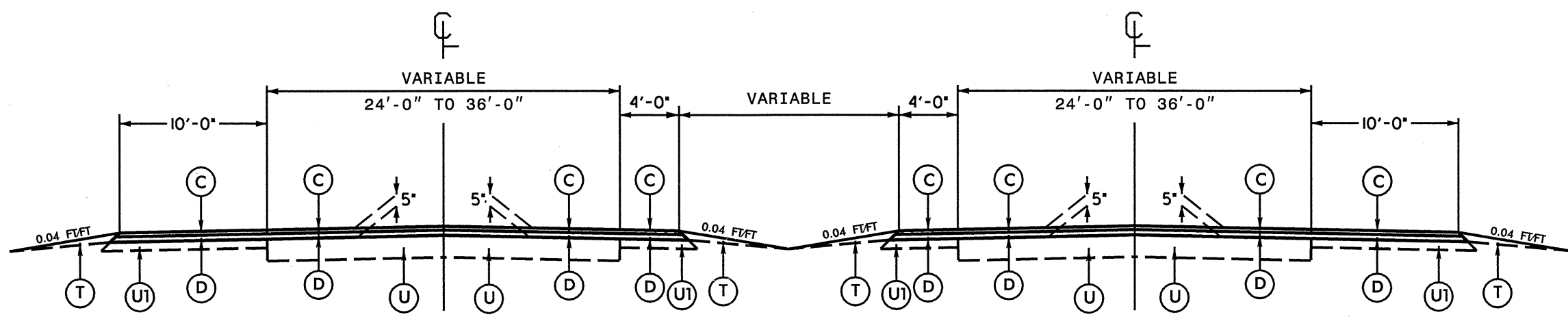


PROJECT 1-5003





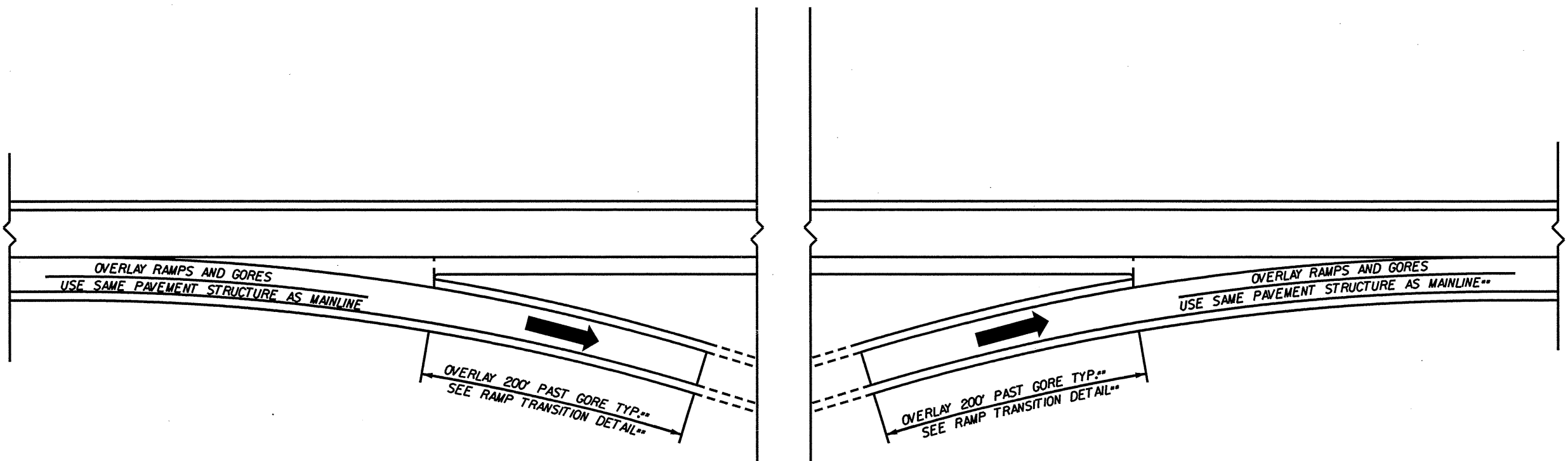
TYPICAL SECTION NO. 1

USE TYPICAL SECTION NO. 1 AS FOLLOWS
FROM MM119 IN BURKE COUNTY TO END OF CRC SECTION (EXIT 131)

PAVEMENT SCHEDULE	
C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
D	PROP. APPROX. 3" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.
U	EXISTING CONCRETE PAVEMENT.
U1	EXISTING ASPHALT PAVEMENT.
T	EARTH MATERIAL.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

5/14/98

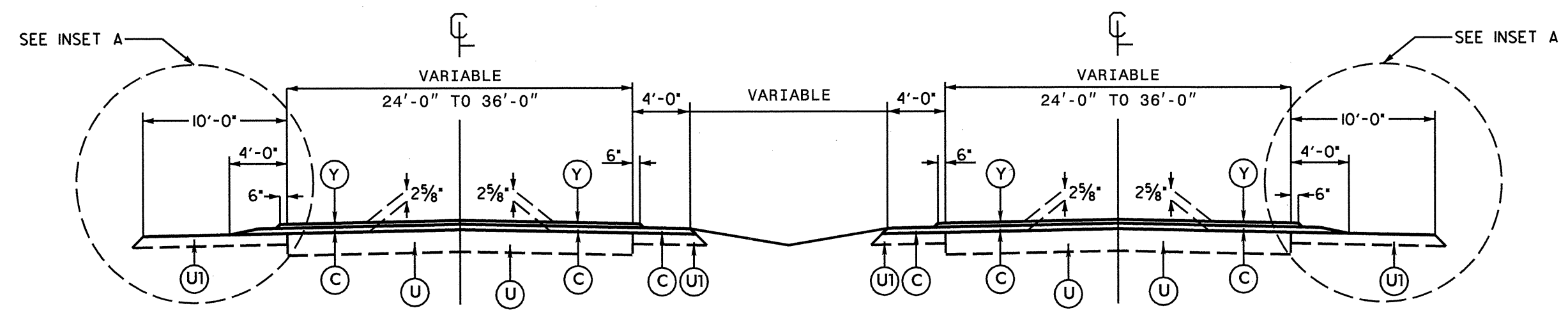


DETAIL GORE AND RAMP RESURFACING

USE DETAIL IN CONJUNCTION WITH TYPICAL SECTION NO.1

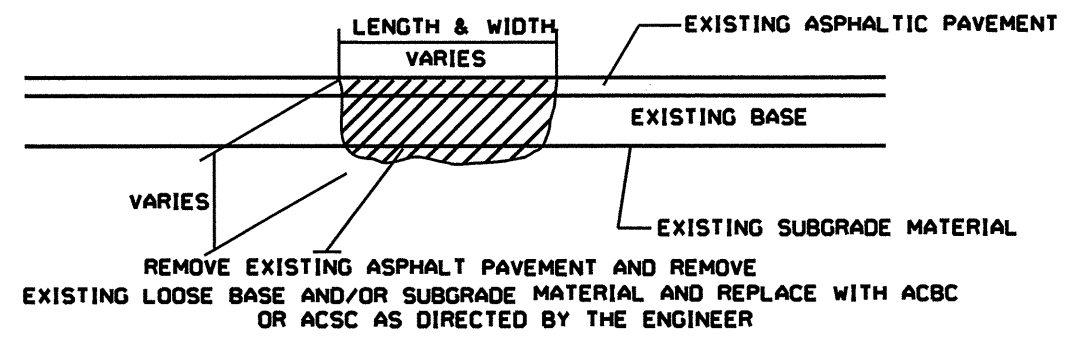
TYPICAL 1-A

*****SYTIME*****
*****1/9/2008*****



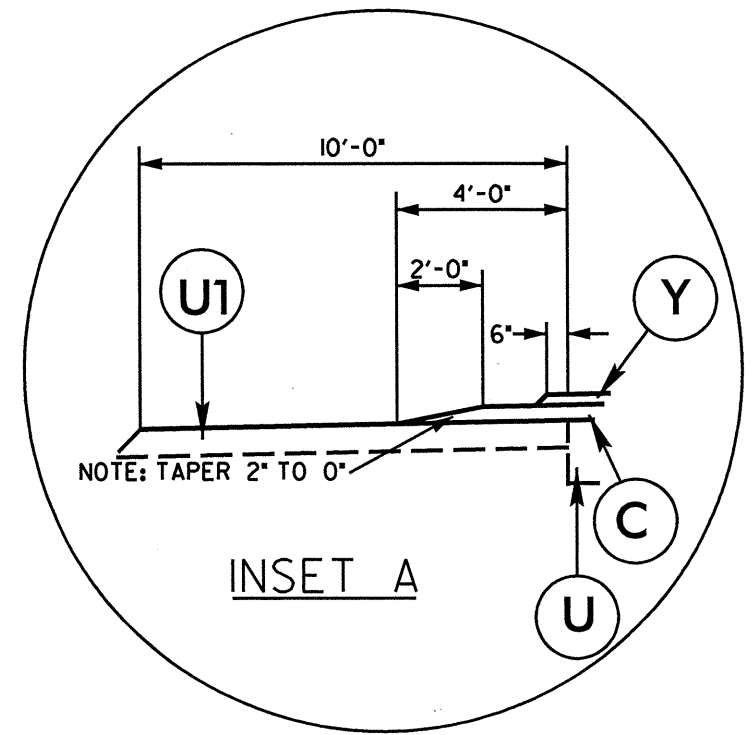
TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2 AS FOLLOWS
FROM END OF CRC SECTION (EXIT 131)
TO END OF EXISTING CONCRETE PAVEMENT



PATCHING EXISTING PAVEMENT

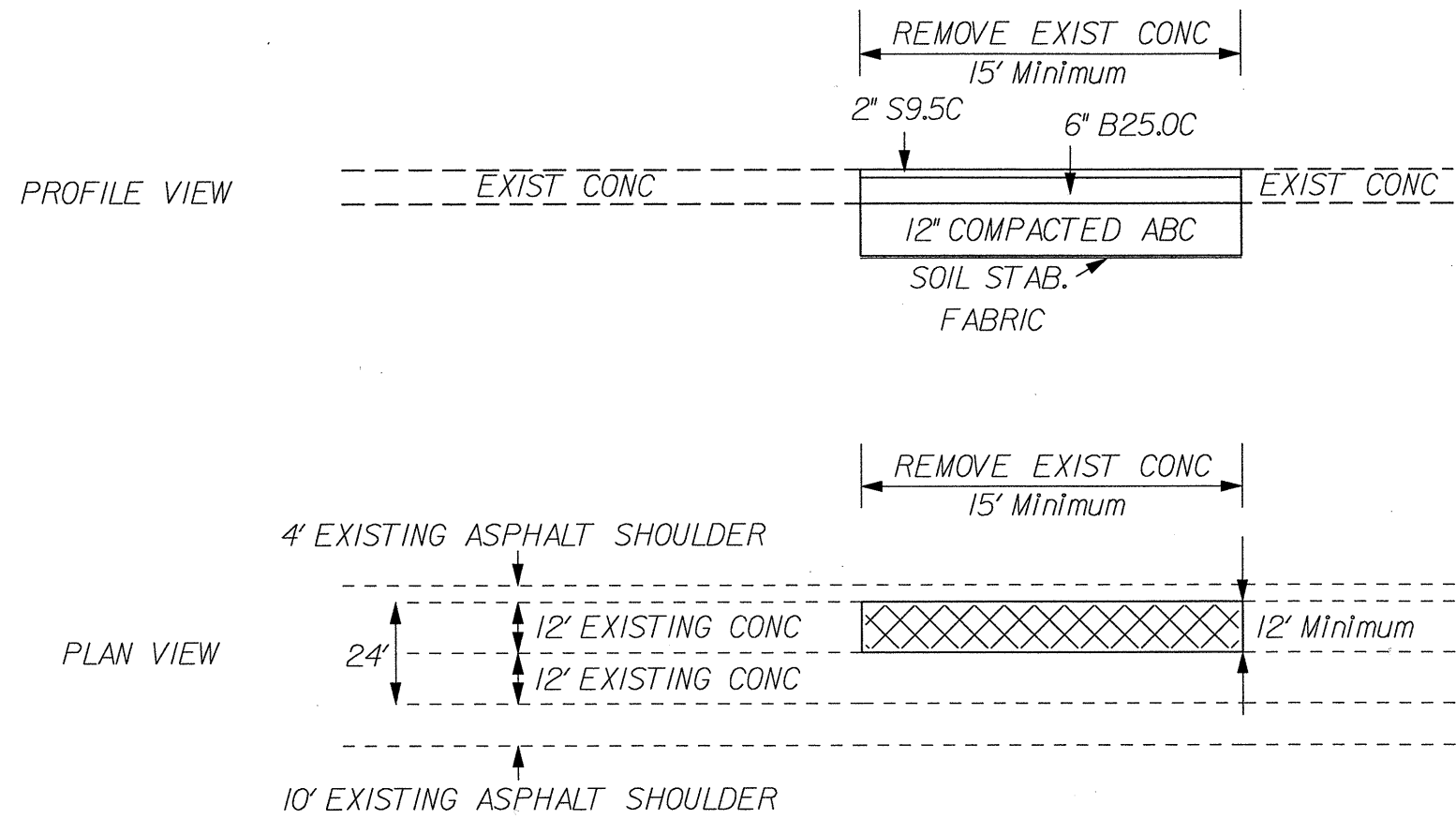
USE WITH TYPICAL NO. 2 WHERE SHOULDER PATCHING MAY BE NECESSARY



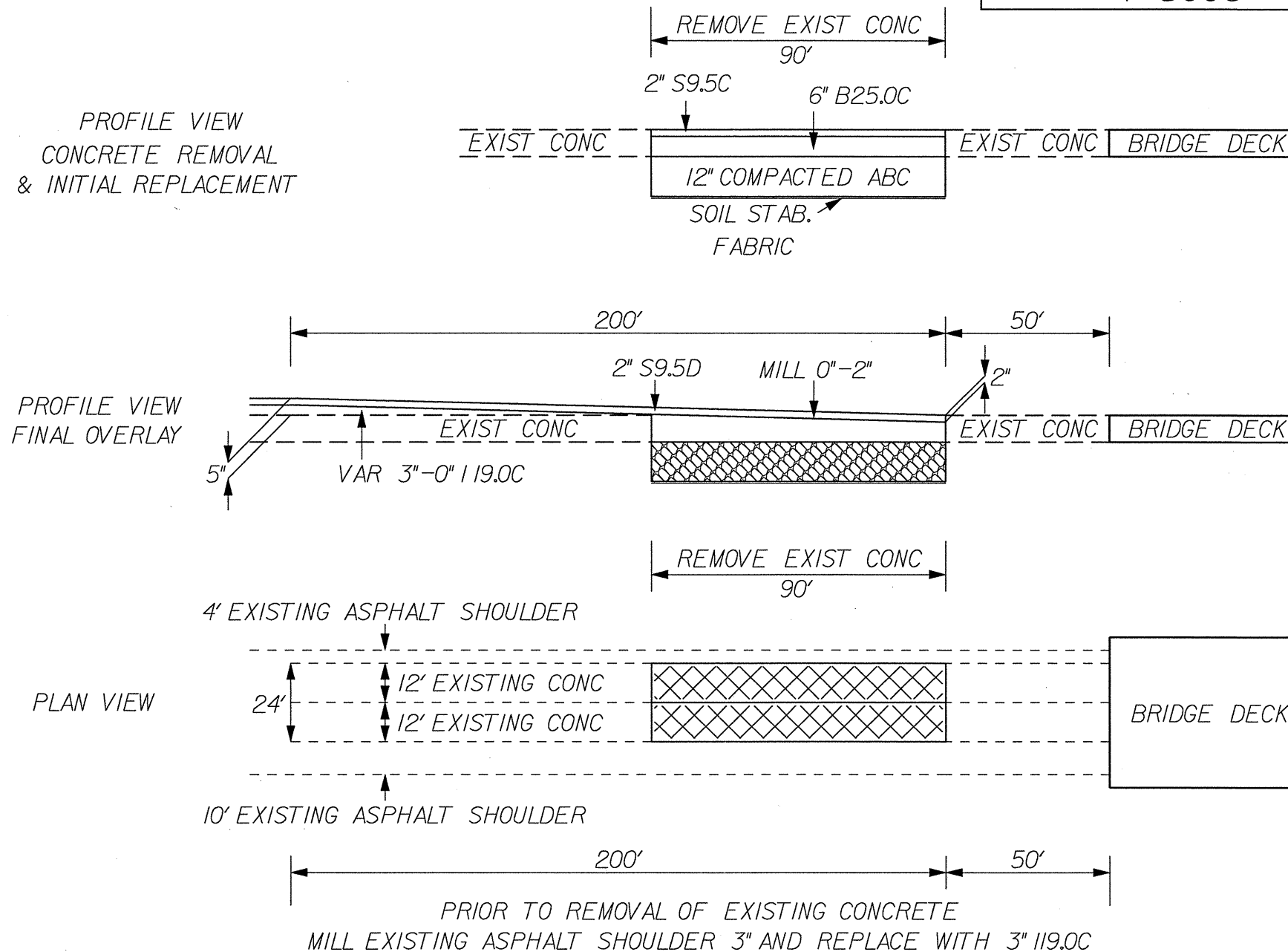
PAVEMENT SCHEDULE	
C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
U	EXISTING CONCRETE PAVEMENT.
U1	EXISTING ASPHALT PAVEMENT.
Y	PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

5/14/08
*****CUSTOMER TIME*****
*****ADDENDUMS*****



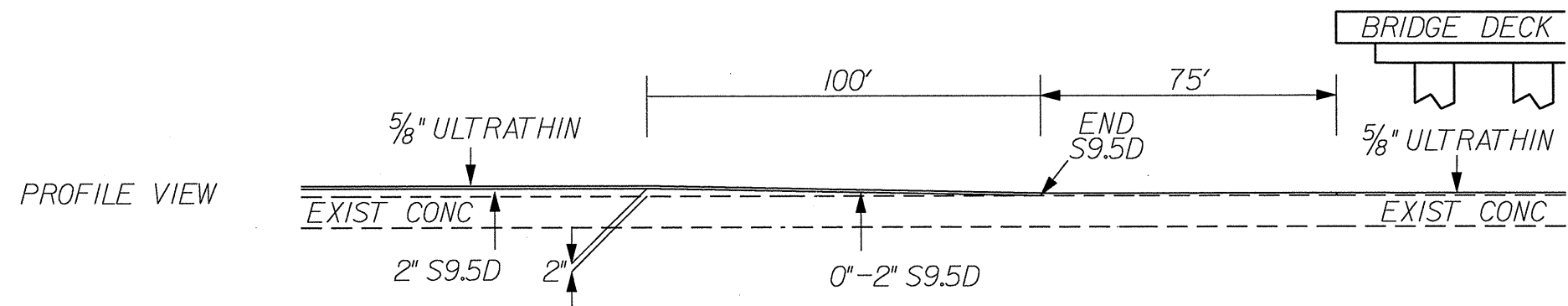
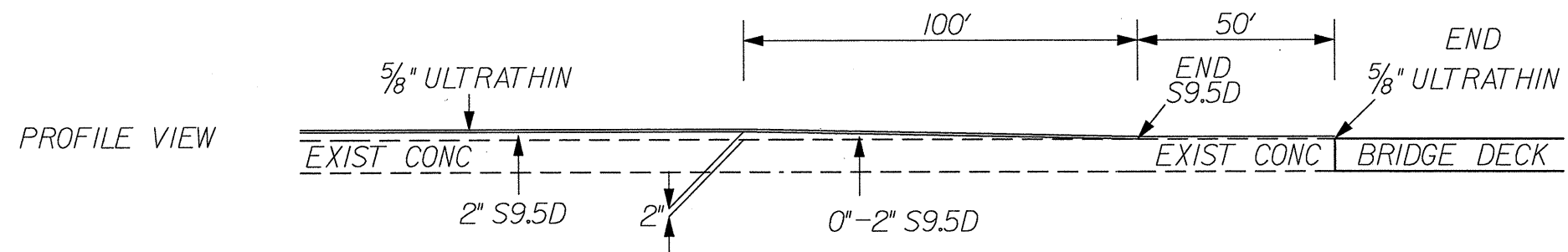
Concrete Removal and Asphalt Replacement Detail



Bridge Approach And Departure Detail A

USE DETAIL AT THE FOLLOWING LOCATIONS

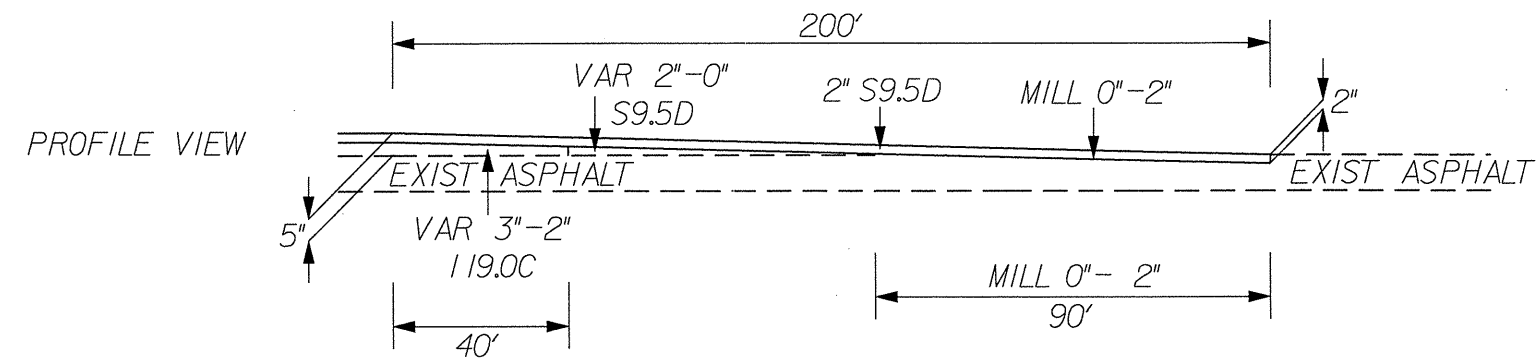
MM 119 IN BURKE COUNTY (BEGINNING OF PROJECT)
US 321, US 70, AND COUNTY HOME ROAD



Bridge Approach And Departure Detail B

USE DETAIL AT THE FOLLOWING LOCATIONS

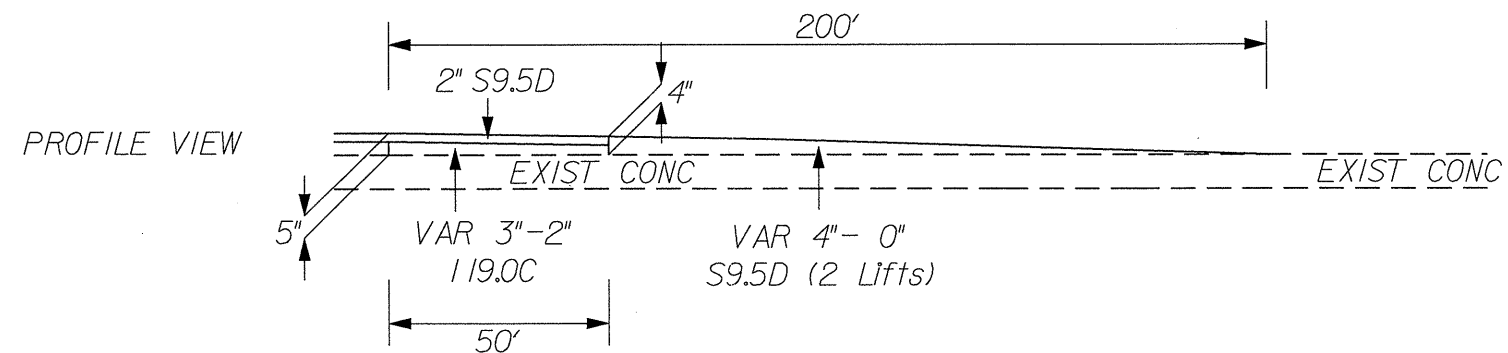
BEGINNING WITH THE OLD NC 16 UNDERPASS
TO STAMEY FARM ROAD



Ramp Pavement Transition Detail A

USE DETAIL AT THE FOLLOWING LOCATIONS

OLD SHELBY ROAD & McDONALD PARKWAY

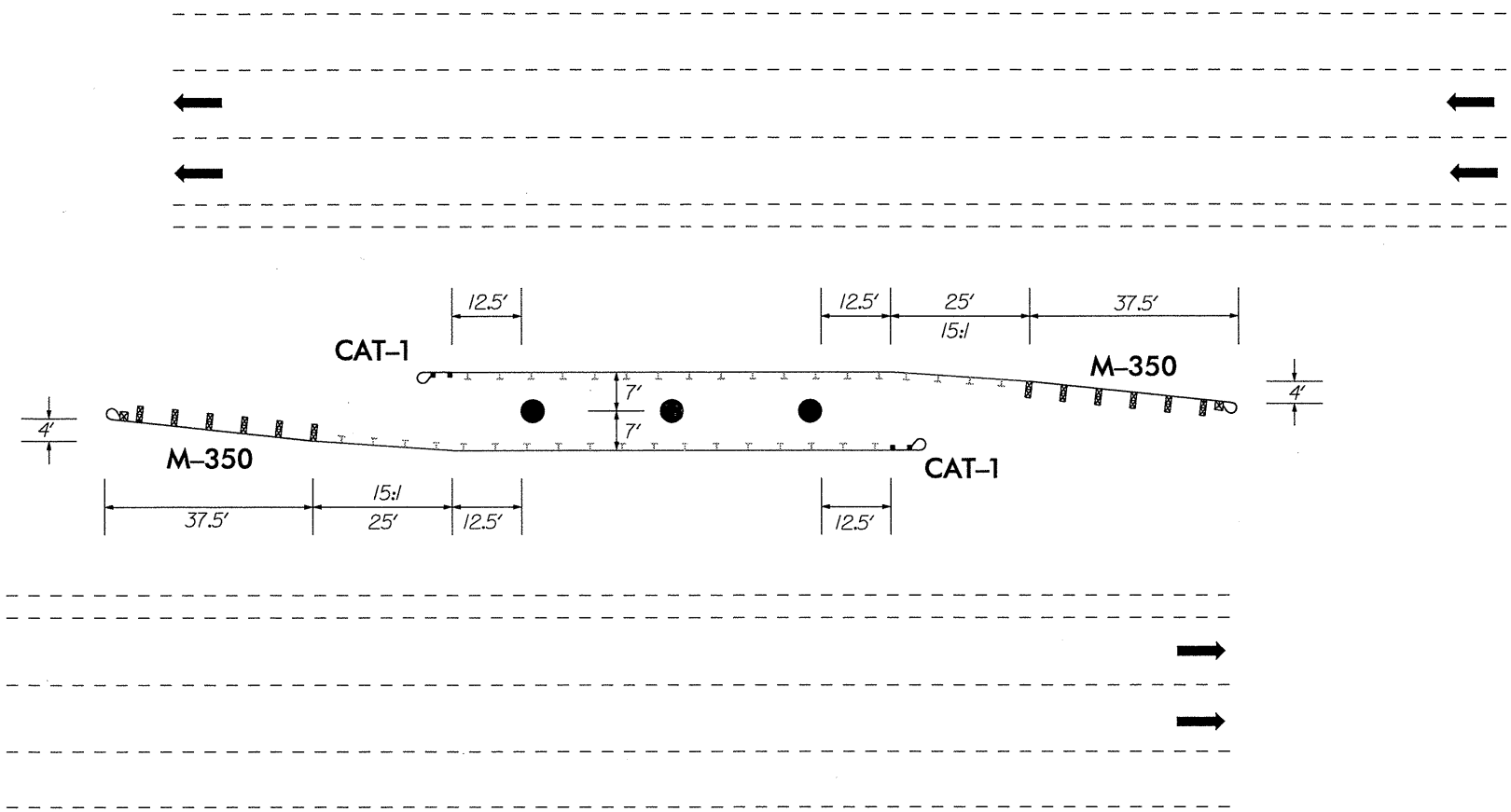


Ramp Pavement Transition Detail B

USE DETAIL AT THE FOLLOWING LOCATIONS

US 321, L-R BLVD., Exit 128, & Exit 130

PROJECT REFERENCE NO.	SHEET NO.
1-5003	10



**Detail Of Median Pier
Protection At Underpasses**

PROJECT NO.	SHEET NO.	TOTAL NO.
41455	11	

SUMMARY OF QUANTITIES

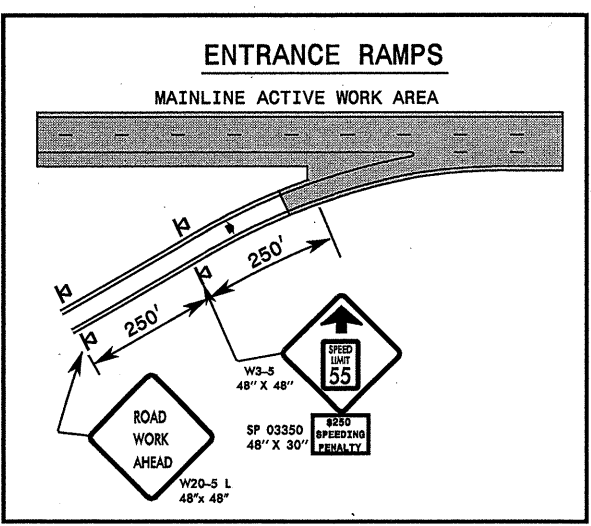
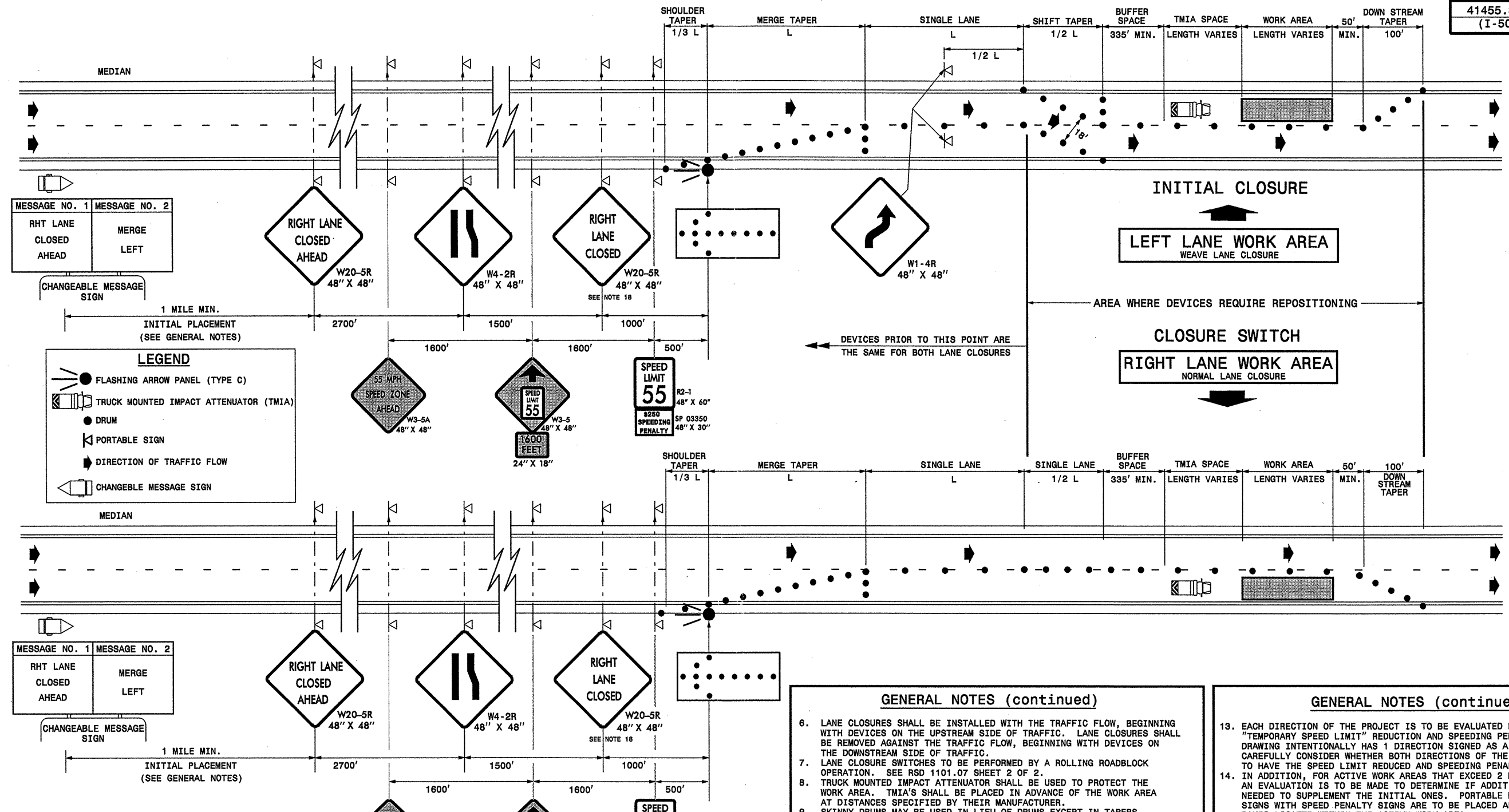
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LENGTH MI	WIDTH FT	UNDERCUT EXCAVATION CY	REMOVAL OF CONC PVMT SLABS SY	SOIL STABILIZATION FABRIC SY	AGGREGATE BASE COURSE TONS	SHOULDER BORROW CY	0" TO 2" MILLING SY	3" MILLING SY	BASE COURSE, B25.0C TONS	INTERMEDIATE COURSE, I19.0C TONS	SURFACE COURSE, S9.5C TONS	SURFACE COURSE, S9.5D TONS	PG 64-22 PLANT MIX TONS	PG 70-22 PLANT MIX TONS	PG 76-22 PLANT MIX TONS	PG 70-28 PLANT MIX TONS
41455	BURKE, CATAWBA, IREDELL	1	I-40	CONTINUOUS REINFORCED CONCRETE SECTION	1,1A	11.25	38	1,715	5,145	5,145	3,473	22,660	20,360	2,000	2,332	109,807	576	68,959	5,261	35	3,793	
41455	BURKE, CATAWBA, IREDELL	1	I-40	CONCRETE SLAB SECTION	2,2A	14.7	38	5,146	15,435	15,435	10,419				6,995		1,729	69,996	301	104	3,850	962
41455	BURKE, CATAWBA, IREDELL	1	I-40	ENTIRE MAP	1,1A,2,2A																	
TOTAL FOR PROJ NO. 41455						25.95		6,861	20,580	20,580	13,892	22,660	20,360	2,000	9,327	109,807	2,305	138,955	5,562	139	7,643	962

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LENGTH MI	WIDTH FT	PATCHING EXISTING PAVEMENT TONS	PATCHING CONC PVMT SPALLS SF	ULTRATHIN HOT MIX ASPHALT TYPE B TON	MILLED RUMBLE STRIPS LF	APPLICATION OF ULTRATHIN SY	STEEL BEAM GUARDRAIL LF	ADDITIONAL GUARDRAIL POSTS EA	GUARDRAIL ANCHOR TYPE CAT-1 EA	GUARDRAIL ANCHOR TYPE 350 EA	GUARDRAIL ANCHOR TYPE M-350 EA	REMOVE & RESET GUARDRAIL LF	PORTABLE LIGHTING LS	SEED & MULCHING AC
41455	BURKE, CATAWBA, IREDELL	1	I-40	CONTINUOUS REINFORCED CONCRETE SECTION	1,1A	11.25	38		750		245,000							40,200		70
41455	BURKE, CATAWBA, IREDELL	1	I-40	CONCRETE SLAB SECTION	2,2A	14.7	38	250	2,250	18,506	315,100	508,920								
41455	BURKE, CATAWBA, IREDELL	1	I-40	ENTIRE MAP	1,1A,2,2A								6,000	15	13	73	9		1	
TOTAL FOR PROJ NO. 41455						25.95		250	3,000	18,506	560,100	508,920	6,000	15	13	73	9	40,200	1	70

*GUARDRAIL TO BE REPLACED AT VARIOUS LOCATIONS AS DIRECTED BY ENGINEER

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4400000000-E	4405000000-E	4415000000-N	4420000000-N	4430000000-N	4480000000-N	4688000000-E	4690000000-E	4700000000-E	4725000000-E	4810000000-E			4820000000-E	4845000000-N	4900000000-N	4905000000-N	4510000000-N	4410000000-E	4445000000-E
					STATIONARY WORK ZONE SIGN SF	PORTABLE WORK ZONE SIGN SF	FLASHING ARROW PANELS, TYPE C EA	CHANGEABLE MESSAGE SIGNS EA	DRUMS EA	TRUCK MTD IMPACT ATTN 60 MPH EA	6" WIDE THERMO 90 MILS LF	6" WIDE THERMO 120 MILS LF	12" X 90 M WHITE THERMO LF	THERMO MERGE ARROW 90 M EA	4" WHITE PAINT LF	4" YELLOW PAINT LF	8" WHITE PAINT LF	PAINT MERGE ARROW EA	CYAN & RED MARKERS EA	SNOW PLOWABLE MARKERS EA	POLICE HRS	WORK ZONE SIGNS (BARRICADE MOUNTED) SF	BARRICADES (TYPE III) LF	
41455	Burke, Catawba, Iredell	1	I-40	I-40 FROM MP 119 IN BURKE CO. TO MAP 146 IN IREDELL CO.	704	170	4	4.00	300	4.00	574,772.00	69,347.00	42,000	12	705,580	564,506	42,000	24	275	6,100	60		48	48
TOTAL FOR PROJ NO. 41455					704	170	4	4	300	4	574,772	69,347	42,000	12	705,580	564,506	42,000	24	275	6,100	60		48	48
GRAND TOTAL					704	170	4	4	300	4	574,772	69,347	42,000	12	705,580	564,506	42,000	24	275	6,100	60		48	48



- GENERAL NOTES**
- WEAVE LANE CLOSURES ARE TO BE USED ONLY ON DIVIDED, CONTROLLED ACCESS ROADWAYS, WITH POSTED SPEED LIMITS OF 55 MPH OR GREATER.
 - FLASHING ARROW PANELS SHALL BE PLACED ON THE SHOULDER (PAVED OR UNPAVED). THE LOCATION OF THE ARROW PANEL SHALL MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE. LANE CLOSURES SHALL BE EXTENDED IF NEEDED, WITHIN THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE TO THE BEGINNING OF THE LANE CLOSURE OR FLASHING ARROW PANEL IS MET. SEE ROADWAY STANDARD DRAWING (RSD) 1101.11 SHEET 2 FOR STOPPING SIGHT DISTANCE & BUFFER SPACE TABLES.
 - THE MAXIMUM SPACING OF DRUMS IN TAPERS SHALL BE EQUAL IN FEET TO THE POSTED SPEED LIMIT. THE MAXIMUM SPACING OF DRUMS ALONG THE BUFFER SPACE, AND WORK AREA, SHALL BE EQUAL IN FEET TO (2) TIMES THE POSTED SPEED LIMIT.
 - SEE RSD 1101.11 SHEETS 1 & 4 FOR VALUES OF "L" AND SIGN SPACING DISTANCES.
 - SEE RSD 1101.02 SHEETS 6 & 7 FOR TREATMENT OF LANE CLOSURES THRU INTERCHANGES.

- GENERAL NOTES (continued)**
- LANE CLOSURES SHALL BE INSTALLED WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. LANE CLOSURES SHALL BE REMOVED AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
 - LANE CLOSURE SWITCHES TO BE PERFORMED BY A ROLLING ROADBLOCK OPERATION. SEE RSD 1101.07 SHEET 2 OF 2.
 - TRUCK MOUNTED IMPACT ATTENUATOR SHALL BE USED TO PROTECT THE WORK AREA. TMIA'S SHALL BE PLACED IN ADVANCE OF THE WORK AREA AT DISTANCES SPECIFIED BY THEIR MANUFACTURER.
 - SKINNY DRUMS MAY BE USED IN LIEU OF DRUMS EXCEPT IN TAPERS.
 - CHANGEABLE MESSAGE SIGN SHALL BE PLACED EITHER IN THE MEDIAN OR ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. THE SIGN SHOULD INITIALLY BE LOCATED APPROXIMATELY 2 MILES IN ADVANCE OF THE MERGE TAPER. IF IT IS ANTICIPATED THAT TRAFFIC WILL BACK UP TO WHERE THE SIGN IS LOCATED, THE SIGN SHOULD THEN INITIALLY BE PLACED APPROXIMATELY 1 MILE PRIOR TO ANTICIPATED BACKUPS. BACKUPS SHOULD BE MONITORED SUCH THAT FOR FUTURE LANE CLOSURES, THE SIGN IS PLACED APPROXIMATELY 1 MILE PRIOR TO WHERE TRAFFIC IS ANTICIPATED TO BACK UP.
 - CHANGEABLE MESSAGE SIGN MESSAGES OTHER THAN THE ONES SHOWN MAY BE PORTRAYED AS DEEMED NECESSARY BY THE ENGINEER. NO MORE THAN 2 MESSAGE DISPLAYS SHOULD BE USED WITH ANY CYCLE.
 - THIS DRAWING IS INTENDED TO SHOW THE LOCATIONS AND SIGNING REQUIRED FOR A "TEMPORARY SPEED LIMIT" REDUCTION AND SPEEDING PENALTY ON A FREEWAY WHICH IS TO BE REDUCED TO 55 MPH. REFER TO THE RSD 1101.02 SHEET 3 OF 9 FOR ADDITIONAL LANE CLOSURE REQUIREMENTS AND GENERAL NOTES.

- GENERAL NOTES (continued)**
- EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "TEMPORARY SPEED LIMIT" REDUCTION AND SPEEDING PENALTY. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED AND SPEEDING PENALTY ADDED.
 - IN ADDITION, FOR ACTIVE WORK AREAS THAT EXCEED 2 MILE IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF ADDITIONAL SIGNS ARE NEEDED TO SUPPLEMENT THE INITIAL ONES. PORTABLE MOUNTED W3-5 SIGNS WITH SPEED PENALTY SIGNS ARE TO BE PLACED ALONG ENTRANCE RAMPS LOCATED WITHIN THE ACTIVE WORK AREA.
 - THE "TEMPORARY SPEED LIMIT" REDUCTION AND SPEEDING PENALTY ARE ONLY IN EFFECT WHEN WORKERS ARE PRESENT WHILE A LANE CLOSURE IS IN PLACE. THE SPEED LIMIT AND SPEED PENALTY SIGNS ARE TO BE REMOVED WHEN THIS CONDITION DOES NOT EXIST. OTHER PERTINENT SIGNS MAY BE DISPLAYED AT THE DIRECTION OF THE ENGINEER IN COORDINATION WITH THE WORK ZONE TRAFFIC CONTROL UNIT (919-250-4159). AT THE COMPLETION OF THE PROJECT, THE ENGINEER SHALL NOTIFY THE REGIONAL TRAFFIC ENGINEER TO REScind THE ORDINANCE.
 - IF A LANE CLOSURE REMAINS INSTALLED WHILE WORKERS ARE NOT PRESENT, AN ADVISORY SPEED PLAQUE MAY BE ADDED TO THE LAST "RIGHT LANE CLOSED" SIGN.
 - WHEN "TEMPORARY SPEED LIMIT" REDUCTIONS ARE IN EFFECT, THE CONTRACTOR IS TO COVER ANY EXISTING SPEED LIMIT SIGNS LOCATED WITHIN THE ACTIVE WORK AREA THAT CONFLICT WITH THE "TEMPORARY SPEED LIMIT" REDUCTION.
 - THE \$250 SPEEDING PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A "TEMPORARY SPEED LIMIT" REDUCTION.

APPROVED: _____ DATE: _____

SEAL

LANE CLOSURES FOR DIVIDED ROADWAYS WITH TEMPORARY SPEED LIMIT REDUCTION AND SPEEDING PENALTY

SCALE: NONE
 DATE: 07-07
 DWG. BY: PS
 DESIGN BY: CL
 REVIEWED BY: CL

REVISIONS
 01-08

30-JAN-2008 10:51
 \\DOT\DFSR00101\groups\WZTC\design\group4\resurfacing\resurfacing2007\div12\c20820_414553gv1\cp_sprdim_10mph_low.dgn
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