

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
B-4128	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
GUILFORD COUNTY**

B-4128

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - THRU LANE DROPS
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS
1264.02	PLACEMENT OF OBJECT MARKERS

INDEX OF SHEETS

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TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS
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TCP-5	PROPOSED DETOUR ROUTE
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SD-1	SPECIAL SIGN DESIGN
PM-1	FINAL PAVEMENT MARKING SCHEDULE AND TEMPORARY PAVEMENT MARKING SCHEDULE
PM-2	FINAL PAVEMENT MARKING PLAN

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
 - TYPE II BARRICADE
 - TYPE III BARRICADE
 - CONE
 - DRUM SKINNY DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
 - YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - PAVEMENT MARKING SYMBOLS

TIP PROJECT:

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APPROVED: DATE: 8/31/07	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
SEAL	<u>J.S. BOURNE, PE</u> TRAFFIC CONTROL ENGINEER
	<u>J.S. KITE, PE</u> TRAFFIC CONTROL PROJECT ENGINEER
	<u>D. PARKER / J.D. KUSE, PE</u> TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	<u>D.E. RICHARDSON</u> TRAFFIC CONTROL DESIGN ENGINEER

PROJECT NOTES

PROJ. REFERENCE NO.	SHEET NO.
B-4128	TCP-2

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
-L- (SR 1549 MACKAY RD)	7AM TO 9AM (MON THRU FRI) 4PM TO 7PM (MON THRU FRI)

LANE AND SHOULDER CLOSURE REQUIREMENTS

- B) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- C) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- E) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- F) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- G) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- H) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- I) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- J) PROVIDE PERMANENT SIGNING.
- K) PROVIDE DETOUR SIGNING WITHIN AND OFF THE PROJECT LIMITS.
- L) COVER OR REMOVE ALL DETOUR SIGNS WITHIN AND OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.
- M) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

- N) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED, REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.

PAVEMENT MARKINGS AND MARKERS

- O) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
-L- (SR 1549 MACKAY RD)	POLYUREA	SNOWPLOWABLE
-Y- (WILLIAMSBOROUGH LN)	POLYUREA	SNOWPLOWABLE

- P) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
-L- (SR 1549 MACKAY RD)	PAINT	RAISED
-Y- (WILLIAMSBOROUGH LN)	PAINT	NA
DETOUR	PAINT	RAISED

- Q) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.
- R) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- S) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

LOCAL NOTE

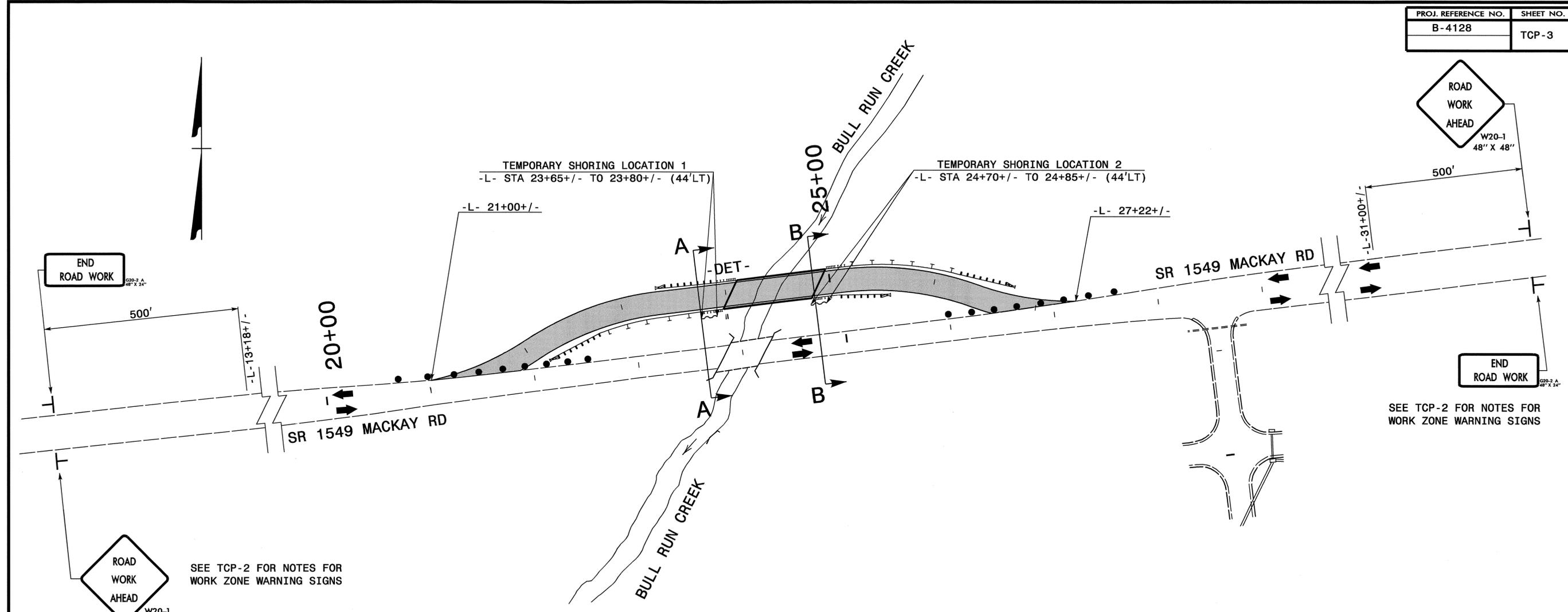
1. MAINTAIN DRIVEWAY ACCESS WITHIN PROJECT LIMITS USING INCIDENTAL STONE.
2. COMPLETE ANY PROPOSED OR TEMPORARY WIDENING IN SUCH A MANNER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE.

NOTES FOR WORK ZONE WARNING SIGNS

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

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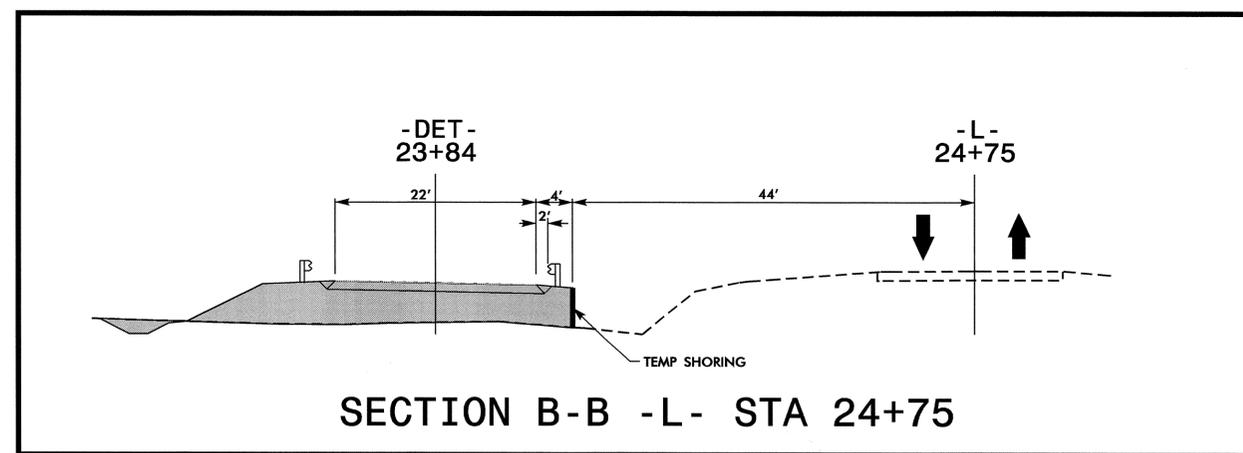
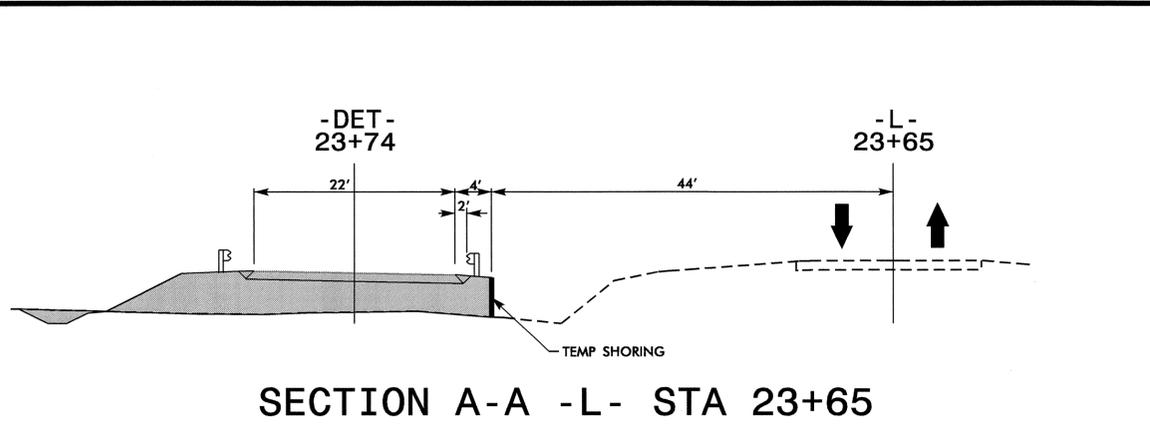


PHASE I

STEP 1 - INSTALL ALL ADVANCED WORK ZONE WARNING SIGNS.

STEP 2 - USING RSD 1101.02, SHEET 1 OF 9, CONSTRUCT -DET- FROM -L- 21+00+/- TO 27+22+/- TO EDGE AND ELEV OF EXIST PAVEMENT INCLUDING GUARDRAIL.

- INSTALL TEMPORARY SHORING.



APPROVED: *Jessica D. Kuse* DATE: 8/31/07

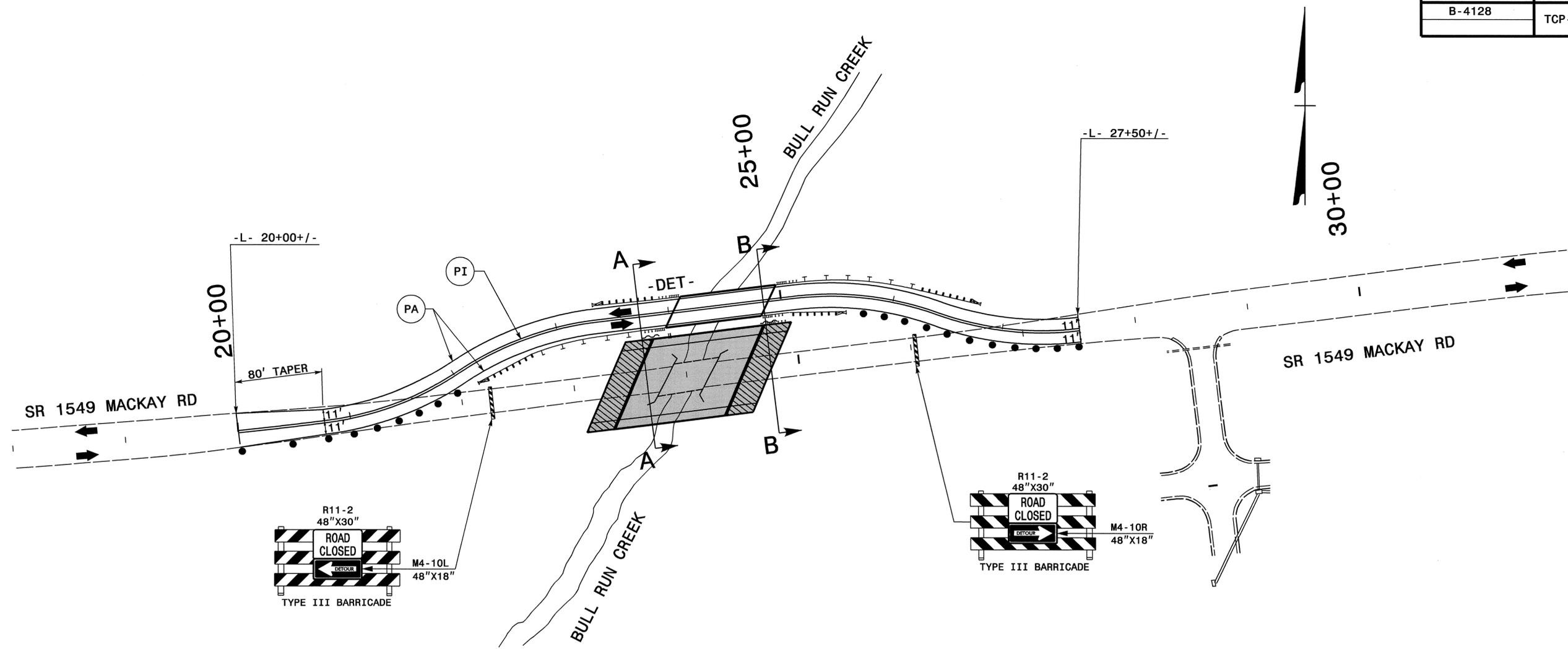
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PROFESSIONAL ENGINEER
NO. 027811
JESSICA D. KUSE

PHASE I

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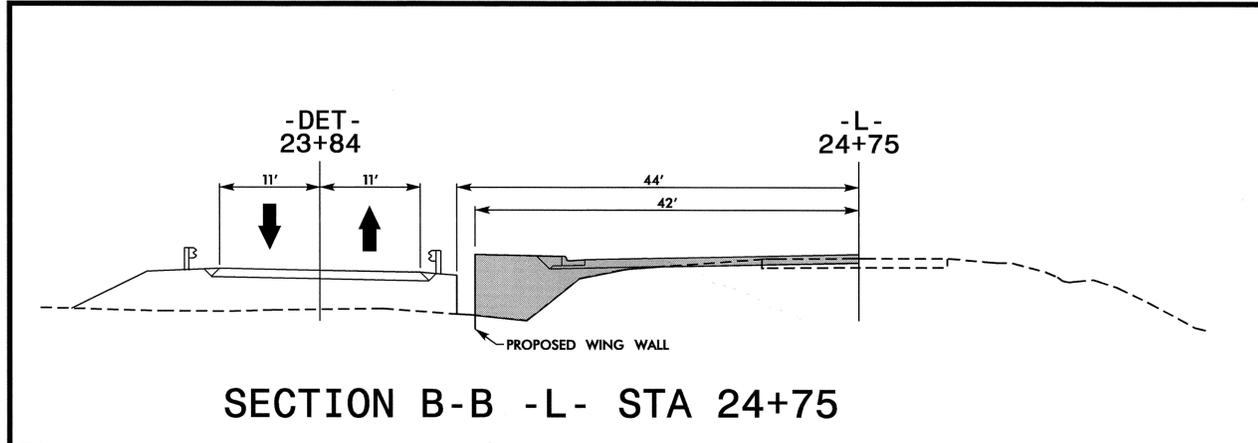
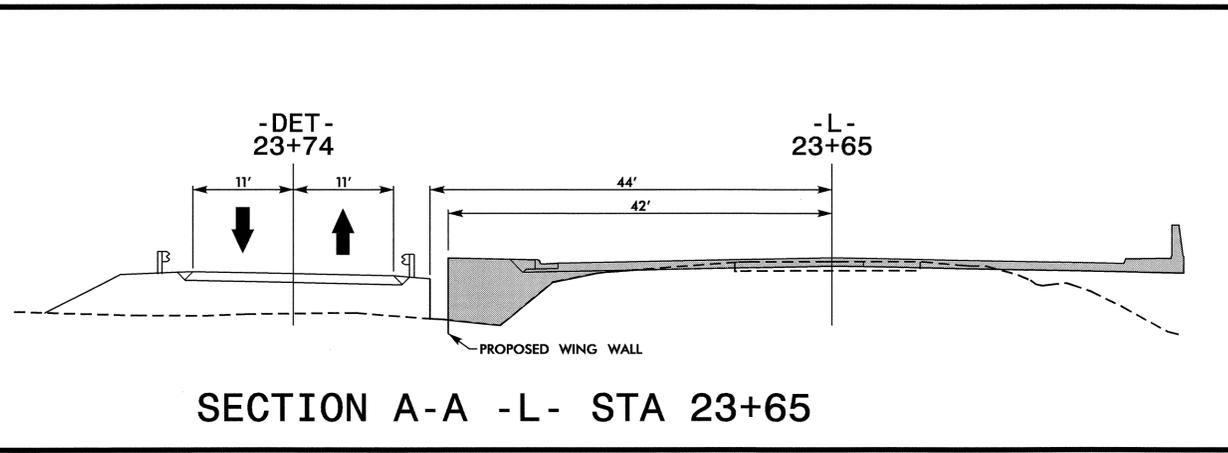


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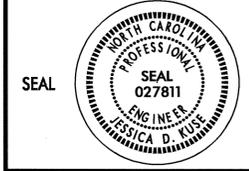
STEP 1 - USING RSD 1101.02, SHEET 1 OF 9, SIMULTANEOUSLY, PLACE PAVEMENT MARKINGS ON -DET-, REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SHIFT TRAFFIC.

STEP 2 - WITH TRAFFIC ON -DET-, CONSTRUCT PROPOSED BRIDGE ACCORDING TO STRUCTURE PLANS.

- PLACE AND COVER SIGNS FOR PROPOSED OFF SITE DETOUR. (SEE TCP-5)

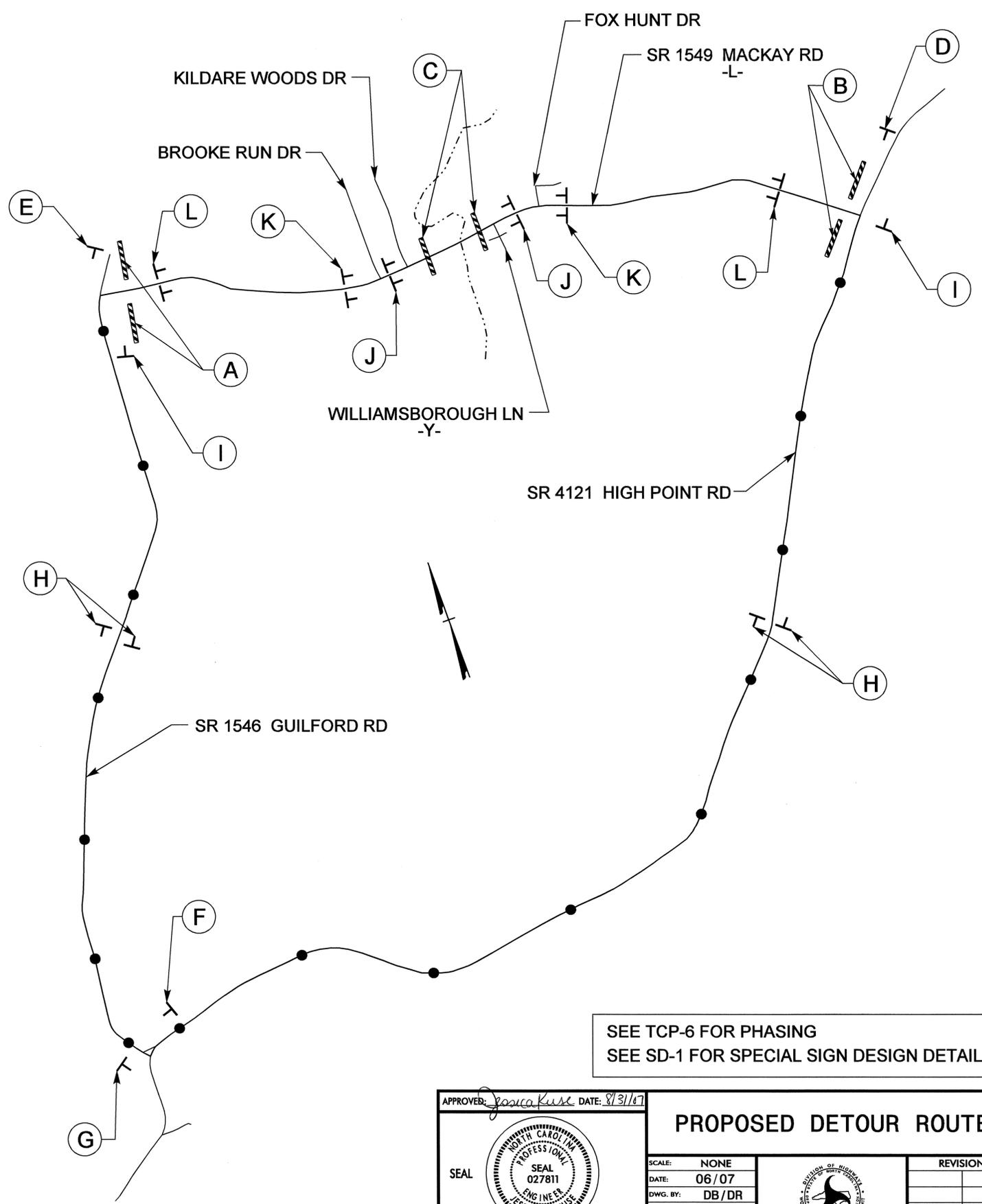
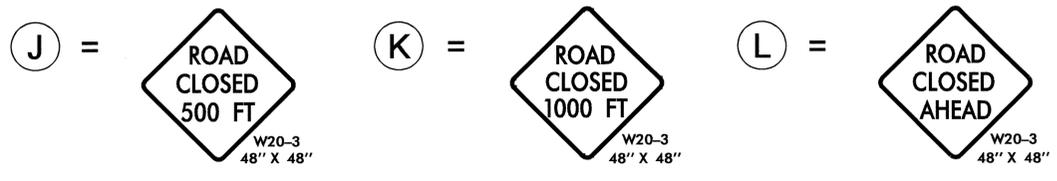
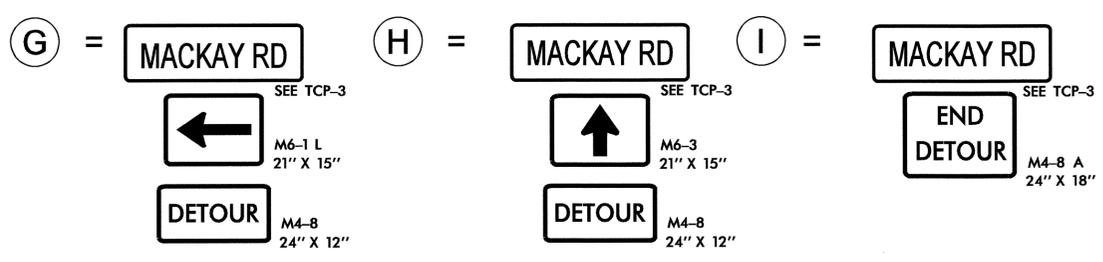
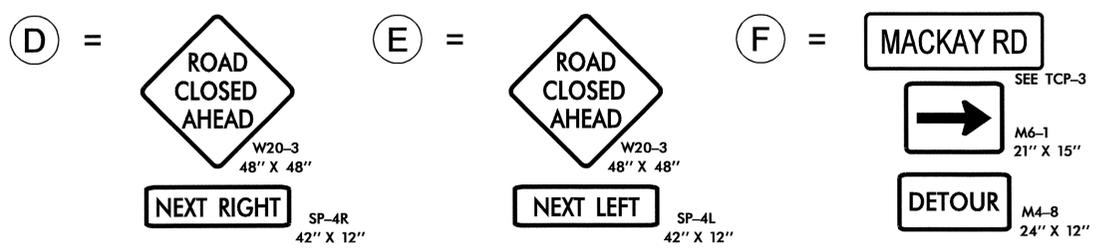
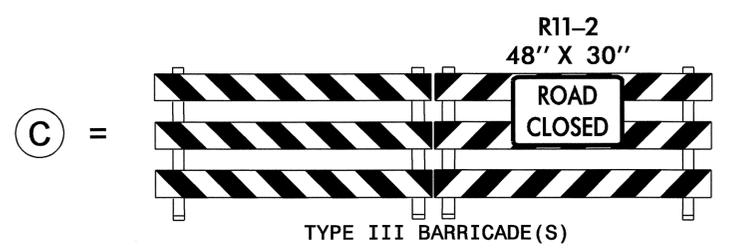
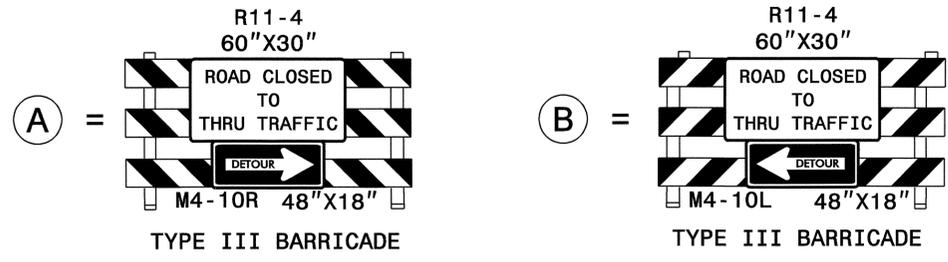


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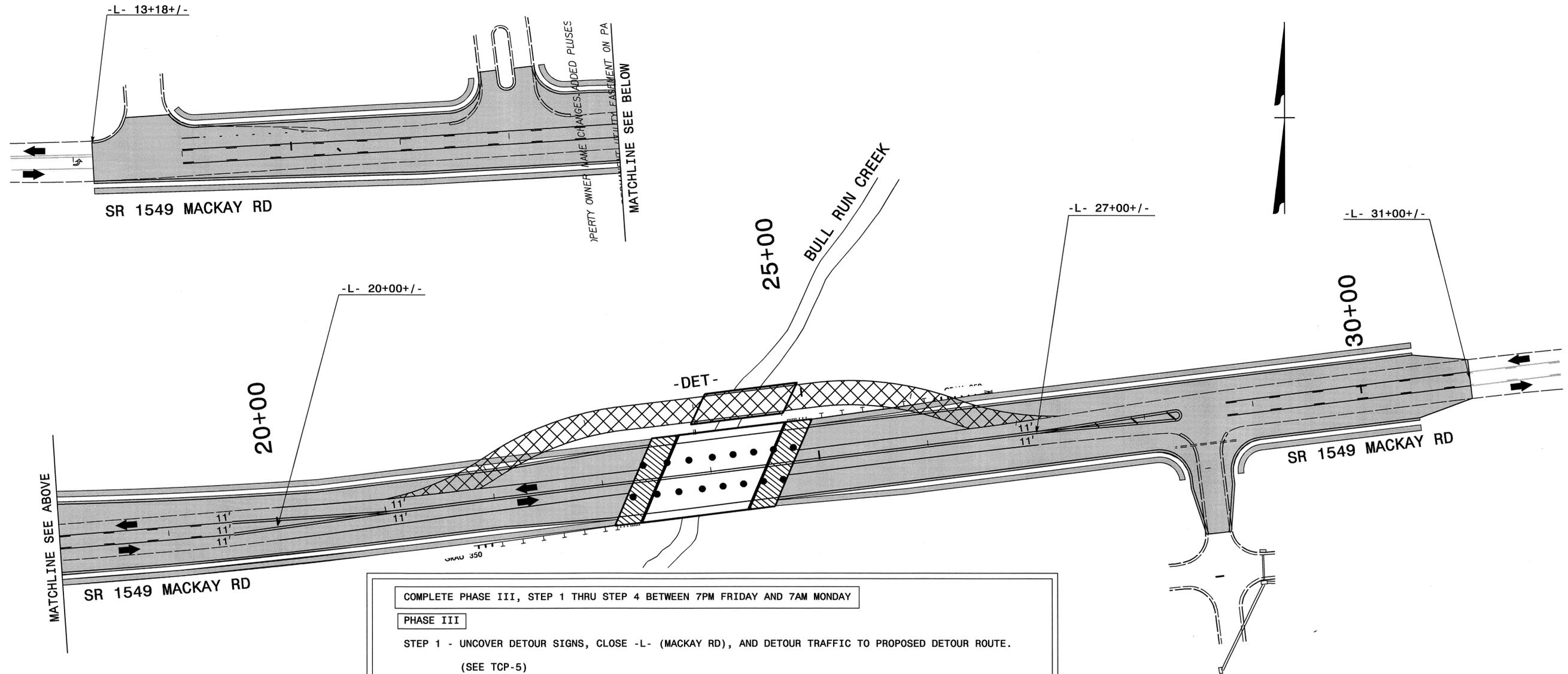
SEE TCP-6 FOR PHASING
SEE SD-1 FOR SPECIAL SIGN DESIGN DETAIL

APPROVED: *Jessica Kuse* DATE: 8/31/17

PROPOSED DETOUR ROUTE

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COMPLETE PHASE III, STEP 1 THRU STEP 4 BETWEEN 7PM FRIDAY AND 7AM MONDAY

PHASE III

- STEP 1 - UNCOVER DETOUR SIGNS, CLOSE -L- (MACKAY RD), AND DETOUR TRAFFIC TO PROPOSED DETOUR ROUTE. (SEE TCP-5)
- STEP 2 - WITH TRAFFIC DETOURED, REMOVE AND REPLACE FULL DEPTH PAVEMENT FROM -L- 20+00 TO 23+52 AND -L- 24+52 TO 26+00 UP TO, BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE (EXCLUDING CURB AND GUTTER).
 - INSTALL PROPOSED PIPES AT -L- 22+93+/- AND 25+00+/-.
 - WEDGE AND RESURFACE (EXISTING WIDTH) FROM -L- 26+00 TO 27+00 UP TO, BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE. (SEE GENERAL NOTE F.)
- STEP 3 - PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS.
- STEP 4 - COVER DETOUR SIGNS AND REOPEN -L- (MACKAY RD) TO TRAFFIC.
- STEP 5 - USING RSD 1101.02, SHEETS 1 AND 2 OF 9, CONSTRUCT FROM -L- 13+18 TO 31+00 UP TO, BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE (INCLUDING CURB AND GUTTER).
 - REMOVE PROPOSED DETOUR.
- STEP 6 - USING RSD 1101.02, SHEET 2 OF 9, PLACE FINAL LAYER OF SURFACE COURSE.
- STEP 7 - USING RSD 1101.02, SHEET 2 OF 9, PLACE FINAL PAVEMENT MARKINGS AND MARKERS. (SEE PM-2)
- STEP 8 - REMOVE ALL REMAINING TRAFFIC CONTROL DEVICES.

APPROVED: *Jessica D. Kuse* DATE: 10/23/07



PHASE III

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Temporaray Shoring No. 1

FOR TEMPORARY SHORING, SEE TEMPORARY SHORING SPECIAL PROVISION.

DO NOT USE A TEMPORARY MSE WALL FROM STATION 23+65 -L-, 44 FT LT TO STATION 23+80 -L-, 44 FT LT. STANDARD TEMPORARY SHORING OR CONTRACTOR DESIGNED SHORING IS REQUIRED. SEE TEMPORARY SHORING SPECIAL PROVISION.

WHEN USING CONTRACTOR DESIGNED SHORING FROM STATION 23+65 -L-, 44 FT LT TO STATION 23+80 -L-, 44 FT LT, USE THE FOLLOWING SOIL PARAMETERS:

UNIT WEIGHT OF SOIL ABOVE WATER TABLE, $\gamma = 120$ PCF
 UNIT WEIGHT OF SOIL BELOW WATER TABLE, $\gamma' = 60$ PCF
 FRICTION ANGLE, $\phi = 30$ DEGREES
 COHESION, $c = 0$ PSF

DO NOT USE DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 23+65 -L-, 44 FT TO STATION 23+80 -L-, 44 FT LT.

PILING FOR TEMPORARY SHORING FROM STATION 23+65 -L-, 44 FT LT TO STATION 23+80 -L-, 44 FT LT, MAY NOT PENETRATE BELOW ELEVATION 755 FT DUE TO THE PRESENCE OF AN OBSTRUCTION, VERY DENSE OR HARD SOIL, WEATHERED OR HARD ROCK.

FOR CONTRACTOR DESIGNED SHORING, SURVEY THE SHORING LOCATION TO DETERMINE EXISTING ELEVATIONS AND ACTUAL DESIGN HEIGHTS BEFORE BEGINNING DESIGN.

LOCATION NO 1 QUANTITY = 64 SF

Temporaray Shoring No. 2

FOR TEMPORARY SHORING, SEE TEMPORARY SHORING SPECIAL PROVISION.

DO NOT USE A TEMPORARY MSE WALL FROM STATION 24+70 -L-, 44 FT LT TO STATION 24+85 -L-, 44 FT LT. STANDARD TEMPORARY SHORING OR CONTRACTOR DESIGNED SHORING IS REQUIRED. SEE TEMPORARY SHORING SPECIAL PROVISION.

WHEN USING CONTRACTOR DESIGNED SHORING FROM STATION 24+70 -L-, 44 FT LT TO STATION 24+85 -L-, 44 FT LT, USE THE FOLLOWING SOIL PARAMETERS:

UNIT WEIGHT OF SOIL ABOVE WATER TABLE, $\gamma = 120$ PCF
 UNIT WEIGHT OF SOIL BELOW WATER TABLE, $\gamma' = 60$ PCF
 FRICTION ANGLE, $\phi = 30$ DEGREES
 COHESION, $c = 0$ PSF

DO NOT USE DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 24+70 -L-, 44 FT TO STATION 24+85 -L-, 44 FT LT.

PILING FOR TEMPORARY SHORING FROM STATION 24+70 -L-, 44 FT LT TO STATION 24+85 -L-, 44 FT LT, MAY NOT PENETRATE BELOW ELEVATION 755 FT DUE TO THE PRESENCE OF AN OBSTRUCTION, VERY DENSE OR HARD SOIL, WEATHERED OR HARD ROCK.

FOR CONTRACTOR DESIGNED SHORING, SURVEY THE SHORING LOCATION TO DETERMINE EXISTING ELEVATIONS AND ACTUAL DESIGN HEIGHTS BEFORE BEGINNING DESIGN.

LOCATION NO 2 QUANTITY = 86 SF

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TEMPORARY SHORING NOTES		
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