

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
1-3600	TCP-1

**PLAN FOR PROPOSED  
TRAFFIC CONTROL, MARKING & DELINEATION  
DAVIE COUNTY**

I - 3600

**ROADWAY STANDARD DRAWINGS**

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"-ROADWAY DESIGN UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUMS
1145.01	BARRICADES TYPE-III
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - INTERCHANGES
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
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1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

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**LEGEND**

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
  - NORTH ARROW
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  - WORK AREA
  - REMOVAL OF EXISTING PAVEMENT
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- TYPE I BARRICADE
  - TYPE II BARRICADE
  - TYPE III BARRICADE
  - CONE
  - DRUM
  - FLASHING ARROW PANEL (TYPE C)
  - TYPE 'B' WARNING LIGHT
  - STATIONARY SIGN
  - PORTABLE SIGN
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  - WARNING FLAGS
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  - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
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- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
  - YELLOW/YELLOW PAVEMENT MARKER
  - CRYSTAL/RED PAVEMENT MARKER
  - PAVEMENT MARKING SYMBOLS

TIP PROJECT:

08-JAN-2007 09:21 \\dot\dfsroot\projstore\proj\13600\trafficcontrol\tcp\3600\_Tc\_Title.dgn AT W:\C2\2424 J.R.SPENCE

APPROVED:	PLAN PREPARED BY: N.C.D.O.T. TRAFFIC CONTROL, MARKING & DELINEATION UNIT
DATE: 8/24/07	
SEAL	J. S. BOURNE, P.E. TRAFFIC CONTROL ENGINEER
	G. L. GETTIER, P.E. TRAFFIC CONTROL PROJECT ENGINEER
	J. W. GILSTRAP TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	J. R. SPENCE TRAFFIC CONTROL DESIGN ENGINEER

# GENERAL NOTES

# PROJECT NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

### TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. I-40, ALL ON/OFF RAMP AND REST AREAS	NOON FRIDAY TO MIDNIGHT SUNDAY

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
1. I-40, ALL ON/OFF RAMP AND REST AREAS

#### HOLIDAY

- FOR ANY EVENT THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31ST TO 7:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A SATURDAY OR A SUNDAY, THEN UNTIL 7:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 7:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 7:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE DAY AFTER INDEPENDENCE DAY.  
IF INDEPENDENCE DAY IS ON A SATURDAY OR SUNDAY, THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 7:00 P.M. TUESDAY.
- FOR THANKSGIVING, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 7:00 P.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 P.M. THE FOLLOWING MONDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT STOP TRAFFIC OR CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. I-40, ALL ON/OFF RAMP AND REST AREAS	NOON FRIDAY TO MIDNIGHT SUNDAY

D) DO NOT STOP TRAFFIC FOR MORE THAN THE FOLLOWING TIME LIMITS:

ROAD NAME	TIME LIMIT	OPERATION
1. I-40, ALL ON/OFF RAMP AND REST AREAS	5 MINUTES	TRAFFIC SHIFTS

E) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

### LANE AND SHOULDER CLOSURE REQUIREMENTS

F) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.

- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT (12m) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER SEE DETAIL ON SHEET TCP-14 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT (1.5m) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT (3m) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

I) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

J) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT (5m) OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.

K) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

### PAVEMENT EDGE DROP OFF REQUIREMENTS

L) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES (50mm) ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES (75mm) ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

M) DO NOT EXCEED A DIFFERENCE OF 1.5 inches (40mm) IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT (150m) IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

### TRAFFIC PATTERN ALTERATIONS

N) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

### SIGNING

DRAWING NUMBERS 1101.02, SHEET 9 OF 9 AND /OR 1101.03, SHEET 9 OF 9 100 FT (31m) FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

- P) PROVIDE PERMANENT SIGNING.
- Q) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- R) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) 500 FT (150m) IN ADVANCE OF THE UNEVEN AREA.
- S) INSTALL BLACK ON ORANGE "BUMP" SIGNS (W8-1) 500 FT (150m) IN ADVANCE OF THE UNEVEN AREA.

### TRAFFIC BARRIER

T) PROTECT THE APPROACH END OF PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

OFFSET THE APPROACH END OF PORTABLE CONCRETE BARRIER A MINIMUM OF 40 FT (12m) FROM ONCOMING TRAFFIC OR PROTECT AT ALL TIMES BY A TEMPORARY CRASH CUSHION.

INSTALL PORTABLE CONCRETE BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE PORTABLE CONCRETE BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP CLOSED THE SECTION OF THE ROADWAY UNTIL THE BARRIER CAN BE PLACED OR AFTER BARRIER IS REMOVED.

### TRAFFIC CONTROL DEVICES

U) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT (3m) ON-CENTER IN RADIUS, AND 3 FT (1m) OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.

V) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

W) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 1000 FT (300m) CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

### PAVEMENT MARKINGS AND MARKERS

X) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. ALL ROADS	WIDE POLYUREA	SNOWPLOWABLE

Y) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. ALL ROADS	WIDE PAINT COLD APPLIED PLASTIC ( TYPE 4, TAPE )	TEMPORARY RAISED

Z) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

AA) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.

BB) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

### MISCELLANEOUS

CC) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH WORK AREA/WORK ZONE.

DD) A "ROLLING ROAD BLOCK" MAY BE USED ON I-40. USE ROADWAY STANDARD DRAWING NUMBERS 1101.02, SHEET 9 OF 9 AND/OR 1101.03, SHEET 9 OF 9. CLOSE THE ON-RAMP OR STOP TRAFFIC WHEN THE " ROLLING ROAD BLOCK " IS USED IN THE VICINITY OF THE RAMPS.

EE) INSTALL PROPOSED GUARDRAIL AS DIRECTED BY THE ENGINEER.

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REVIEWED BY: JWG			



### PHASE III CON'D

STEP 3: - USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3, 6 & 7 OF 9 AND 1101.04:

- SHIFT TRAFFIC ONTO THE INSIDE SHOULDERS, AS SHOWN ON SHEET TCP-7, DETAIL 8.
- WHEN REQUIRED, PLACE INTERMEDIATE PAVEMENT MARKINGS (6"/12" PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR ACCESS TO THE AFFECTED REST AREA RAMPS, UTILIZING THE 2-FOOT ASPHALT LATERAL RAMP-DOWN, AND OPEN TO TRAFFIC.

NOTE: PLACE DRUMS TO DELINEATE THE INTERMEDIATE RAMP TRAFFIC PATTERN.

- PAVE THE OUTSIDE LANES OF I-40 (-EBL-/-WBL-) UP THROUGH THE FIRST LAYER OF SURFACE COURSE [INCLUDING PROPOSED/TEMPORARY TIE-INS TO EXISTING PAVEMENT], AS SHOWN IN THE CONSTRUCTION PLANS AND ON SHEET TCP-7, DETAILS 8 & 9.
- PLACE INTERMEDIATE PAVEMENT MARKINGS, (6"/12" PAINT AND 6" REMOVABLE TAPE ON EXISTING STRUCTURES), AND TEMPORARY RAISED PAVEMENT MARKERS (SEE SHEET TCP-7, DETAIL 9.)

NOTE: CONTRACTOR SHALL PLACE 6" REMOVABLE TAPE ON THE LAST 280 FEET OF EACH SECTION TO EXPEDITE THE TRAFFIC SHIFT DURING THE CONSTRUCTION OF THE NEXT SECTION OF I-40 (-EBL-/-WBL-) (SEE SHEET TCP-7, DETAIL 9).

STEP 4: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 3 OF 9;

- SHIFT TRAFFIC TO A ONE-LANE, ONE-WAY PATTERN ON THE NEWLY COMPLETED OUTSIDE LANES OF I-40 (-EBL-/-WBL-) AND OPEN RAMP (WHEN REQUIRED) TO TRAFFIC. (SEE SHEET TCP-7, DETAIL 10).
- PAVE THE FIRST LAYER OF SURFACE COURSE (SEE CONSTRUCTION PLANS AND SHEET TCP-7, DETAIL 10).
- PLACE INTERMEDIATE PAVEMENT MARKINGS, (6" PAINT AND 6" REMOVABLE TAPE ON EXISTING STRUCTURES), (SEE SHEET TCP-7, DETAIL 10.)

NOTE: CONTRACTOR SHALL PLACE 6" REMOVABLE TAPE ON THE LAST 280 FEET OF EACH SECTION TO EXPEDITE THE TRAFFIC SHIFT DURING THE CONSTRUCTION OF THE NEXT SECTION OF I-40 (-EBL-/-WBL-) (SEE SHEET TCP-7, DETAILS 9 & 10).

STEP 5: - OPEN I-40 (-EBL-/-WBL-) TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY (SEE SHEET TCP-7, DETAIL 11) AND REPEAT PHASE III, STEPS 1 THRU 5, UNTIL THE FIRST LAYER OF SURFACE COURSE HAS BEEN PAVED FOR THE ENTIRE PROJECT.

### PHASE IIIA (OPTION)

#### CONSTRUCTION OF PROPOSED I-40 (-EBL-/-WBL-) UP TO AND INCLUDING THE FIRST LAYER OF SURFACE COURSE.

WORK IN A CONTINUOUS MANNER TO COMPLETE THE FOLLOWING WORK IN PHASE IIIA (OPTION), STEPS 1A THRU 5A. THE CONTRACTOR SHALL ONLY WORK IN THOSE SECTIONS OF I-40 (-EBL-/-WBL-) THAT CAN BE COMPLETED FROM MIDNIGHT SUNDAY THRU NOON ON FRIDAY.

NOTE: CONTRACTOR SHALL REDUCE SPEED LIMIT ONLY IN ACTIVE WORK AREAS UTILIZING "REDUCE SPEED AHEAD" SIGNS WITH ORANGE PANELS AND SPEED LIMIT FLIP SIGNS OR "SPEED LIMIT 60" (R2-1) SIGNS, AS SHOWN ON SHEETS TCP-8, TCP-12, AND TCP-13.

NOTE: CONTRACTOR SHALL BEGIN INSTALLATION OR INSTALL ATR AS REQUIRED (SEE CONSTRUCTION PLANS).

NOTE: THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE REST AREAS AT ALL TIMES. HOWEVER, THE CONTRACTOR SHALL CLOSE THE REST AREA TO PERFORM THE PAVING OPERATIONS IN PHASE IIIA (OPTION) INSTEAD OF USING THE "YIELD" SIGNS AS SHOWN ON ROADWAY STANDARD DRAWINGS NO. 1101.02, SHEET 6 OF 9. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE REST AREA FOR ONLY ONE WORK PERIOD (FROM MIDNIGHT SUNDAY TO NOON FRIDAY). ALL REQUIRED PAVING ON I-40 AND THE ACCELERATION/DECELERATION LANES IN THE REST AREA SHALL BE COMPLETED IN THIS WORK PERIOD. THE REST AREA IS DEFINED AS FROM THE BEGINNING OF THE DECELERATION LANE TO THE END OF THE ACCELERATION LANE.

STEP 1A: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 6 & 7 OF 9 AND 1101.04, REMOVE THE EXISTING WHITE EDGELINE AND PLACE INTERMEDIATE PAVEMENT MARKINGS, (6" PAINT LINES, 12" PAINT GORE LINES, AND 6" REMOVABLE TAPE LINES ON EXISTING STRUCTURES), AND UTILIZING 280-FOOT TAPERS FOR TRANSITIONS, SHIFT TRAFFIC 8 FEET TO THE OUTSIDE SHOULDERS INTO THE INTERMEDIATE TRAFFIC PATTERNS, AS SHOWN ON SHEET TCP-7A, DETAIL 7A.

STEP 2A: - PAVE THE INSIDE LANES AND SHOULDERS (24 FEET) OF I-40 (-EBL-/-WBL-) UP THROUGH THE FIRST LAYER OF SURFACE COURSE, [INCLUDING PROPOSED/TEMPORARY TIE-INS TO EXISTING PAVEMENT], AS SHOWN IN THE CONSTRUCTION PLANS AND TCP-7A, DETAIL 7A.

- WHEN REQUIRED, CONSTRUCT TEMPORARY 1 FOOT & 2-FOOT LATERAL ASPHALT RAMP-DOWNS FROM THE PROPOSED SURFACE COURSE TO THE EXISTING PAVEMENT AT THE FOLLOWING LOCATIONS. (SEE THE CONSTRUCTION PLANS AND SHEET TCP-7A, DETAIL 7A.)  
NOTE: RAMP-DOWNS WILL BE UTILIZED TO ALLOW ACCESS TO THE EXIT/ENTRANCE RAMPS OF THE REST AREAS IN PHASE IIIA (OPTION), STEP 3A.

I-40/REST AREAS: STA. 109+00+/- (-EBL-) TO STA. 118+00+/- (-EBL-)  
STA. 134+00+/- (-EBL-) TO STA. 149+00+/- (-EBL-)  
STA. 105+00+/- (-WBL-) TO STA. 121+00+/- (-WBL-)  
STA. 138+00+/- (-WBL-) TO STA. 146+00+/- (-WBL-)

- PLACE INTERMEDIATE PAVEMENT MARKINGS, (6" PAINT ON SURFACE COURSE AND 6" REMOVABLE TAPE ON STRUCTURES), ON THE NEWLY COMPLETED SURFACE COURSE FOR THE INTERMEDIATE TRAFFIC PATTERN (ONE 12-FOOT TRAVEL LANE) AND 280-FOOT TAPERS IN EACH DIRECTION OF I-40 (-EBL-/-WBL-). SEE SHEET TCP-7A, DETAIL 7A.

STEP 3A: - USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3, 6 & 7 OF 9 AND 1101.04:

-- SHIFT TRAFFIC ONTO THE INSIDE SHOULDERS, AS SHOWN ON SHEET TCP-7A, DETAIL 8A.

-- WHEN REQUIRED, PLACE INTERMEDIATE PAVEMENT MARKINGS (6"/12" PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR ACCESS TO THE AFFECTED REST AREA RAMPS, UTILIZING THE 1 FOOT & 2-FOOT ASPHALT LATERAL RAMP-DOWN, AND OPEN TO TRAFFIC.

NOTE: PLACE DRUMS TO DELINEATE THE INTERMEDIATE RAMP TRAFFIC PATTERN.

-- PAVE THE OUTSIDE LANES OF I-40 (-EBL-/-WBL-) UP THROUGH THE FIRST LAYER OF SURFACE COURSE [INCLUDING PROPOSED/TEMPORARY TIE-INS TO EXISTING PAVEMENT], AS SHOWN IN THE CONSTRUCTION PLANS AND ON SHEET TCP-7A, DETAILS 8A & 9A.

-- PLACE INTERMEDIATE PAVEMENT MARKINGS, (6"/12" PAINT AND 6" REMOVABLE TAPE ON EXISTING STRUCTURES), AND TEMPORARY RAISED PAVEMENT MARKERS (SEE SHEET TCP-7A DETAIL 9A.)

NOTE: CONTRACTOR SHALL PLACE 6" REMOVABLE TAPE ON THE LAST 280 FEET OF EACH SECTION TO EXPEDITE THE TRAFFIC SHIFT DURING THE CONSTRUCTION OF THE NEXT SECTION OF I-40 (-EBL-/-WBL-) (SEE SHEET TCP-7A, DETAIL 9A).

STEP 4A: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 3 OF 9;

-- SHIFT TRAFFIC TO A ONE-LANE, ONE-WAY PATTERN ON THE NEWLY COMPLETED OUTSIDE LANES OF I-40 (-EBL-/-WBL-) AND OPEN RAMP (WHEN REQUIRED) TO TRAFFIC. (SEE SHEET TCP-7A, DETAIL 10A).

-- PLACE INTERMEDIATE PAVEMENT MARKINGS, (6" PAINT AND 6" REMOVABLE TAPE ON EXISTING STRUCTURES), (SEE SHEET TCP-7A, DETAIL 10A.)

NOTE: CONTRACTOR SHALL PLACE 6" REMOVABLE TAPE ON THE LAST 280 FEET OF EACH SECTION TO EXPEDITE THE TRAFFIC SHIFT DURING THE CONSTRUCTION OF THE NEXT SECTION OF I-40 (-EBL-/-WBL-) (SEE SHEET TCP-7A, DETAILS 9A & 10A).

STEP 5A: - OPEN I-40 (-EBL-/-WBL-) TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY (SEE SHEET TCP-7A, DETAIL 11A) AND REPEAT PHASE IIIA (OPTION), STEPS 1A THRU 5A, UNTIL THE FIRST LAYER OF SURFACE COURSE HAS BEEN PAVED FOR THE ENTIRE PROJECT.

### PHASE IV

#### PAVE THE PROPOSED FINAL LAYER OF SURFACE COURSE OF I-40 (-EBL-/-WBL-).

WORK IN A CONTINUOUS MANNER TO COMPLETE THE FOLLOWING WORK IN PHASE IV, STEP 1. THE CONTRACTOR SHALL ONLY WORK IN THOSE SECTIONS OF I-40 (-EBL-/-WBL-) THAT CAN BE COMPLETED FROM MIDNIGHT SUNDAY THRU NOON ON FRIDAY.

NOTE: CONTRACTOR SHALL REDUCE SPEED LIMIT ONLY IN ACTIVE WORK AREAS UTILIZING "REDUCE SPEED AHEAD" SIGNS WITH ORANGE PANELS AND SPEED LIMIT FLIP SIGNS OR "SPEED LIMIT 60" (R2-1) SIGNS, AS SHOWN ON SHEETS TCP-8, TCP-12, AND TCP-13.

NOTE: CONTRACTOR SHALL COMPLETE INSTALLATION OR INSTALL ATR AS REQUIRED (SEE CONSTRUCTION PLANS).

STEP 1: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 6 & 7 OF 9:

-- PAVE THE FINAL LAYER OF SURFACE COURSE AND PLACE WIDE POLYUREA PAVEMENT MARKINGS, SNOWPLOWABLE PAVEMENT MARKERS AND PERMANENT PAVEMENT MARKERS ON STRUCTURES (IF APPLICABLE). (SEE CONSTRUCTION PLANS).

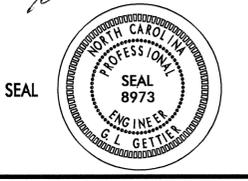
-- MILL RUMBLE STRIPS ON I-40 (-EBL-/-WBL-) (SEE CONSTRUCTION PLANS).

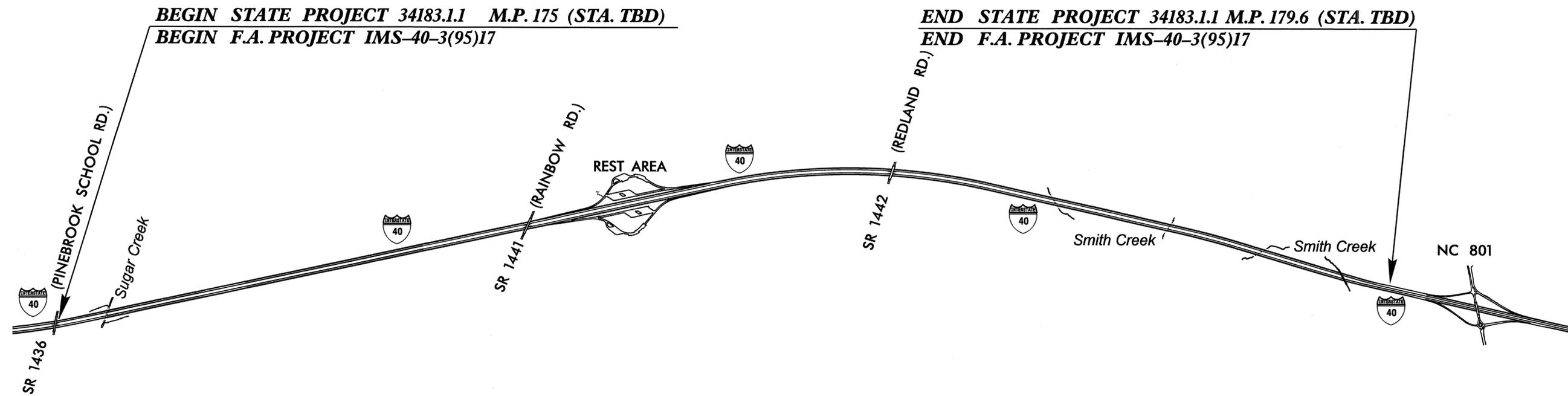
-- COMPLETE CONSTRUCTION AND INSTALLATION OF PROPOSED GROUND-MOUNTED SIGNS. (SEE SIGNING PLANS.)

-- OPEN I-40 (-EBL-/-WBL-) TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY (SEE SHEET TCP-7, DETAIL 11) AND REPEAT PHASE IV, STEP 1 UNTIL THE FINAL LAYER OF SURFACE COURSE HAS BEEN PAVED FOR THE ENTIRE PROJECT.

STEP 2: - WHEN ALL CONSTRUCTION IS COMPLETED, REMOVE ALL TRAFFIC CONTROL DEVICES WITHIN THE PROJECT LIMITS AND OPEN I-40 (-EBL-/-WBL-) TO THE EXISTING TRAFFIC PATTERN.

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## PROJECT OVERVIEW

THE FOLLOWING IS ONLY A GENERAL PHASING OUTLINE. IT DOES NOT SUPERCEDE THE PROJECT NOTES ON SHEET TCP-2 AND PROJECT PHASING ON SHEETS TCP-3 & 3A.

- PHASE I**
- INSTALL ADVANCE WORK ZONE WARNING SIGNS AND CHANGEABLE MESSAGE SIGNS.
  - INSTALL ALL REDUCE SPEED LIMIT SIGNS (FLIP SIGNS) AND NO PARKING SIGNS.
  - SHIFT TRAFFIC ONTO THE OUTSIDE LANES OF EASTBOUND AND WESTBOUND I-40. CONSTRUCT THE PROPOSED BEGINNING & ENDING TAPERS AND THE PROPOSED 10' INSIDE SHOULDERS.
  - SHIFT TRAFFIC ONTO THE INSIDE LANES OF EASTBOUND AND WESTBOUND I-40. CONSTRUCT THE PROPOSED BEGINNING & ENDING TAPERS AND THE PROPOSED 10' OUTSIDE SHOULDERS.
  - RETURN TRAFFIC TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY.

- PHASE II**
- SHIFTING TRAFFIC TO THE SHOULDER, AS REQUIRED, INSTALL PORTABLE CONCRETE BARRIER AND REMOVE & REPLACE EXISTING CONCRETE PAVEMENT ON EASTBOUND AND WESTBOUND I-40 AS REQUIRED BY THE CONSTRUCTION PLANS.
  - RETURN TRAFFIC TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY.

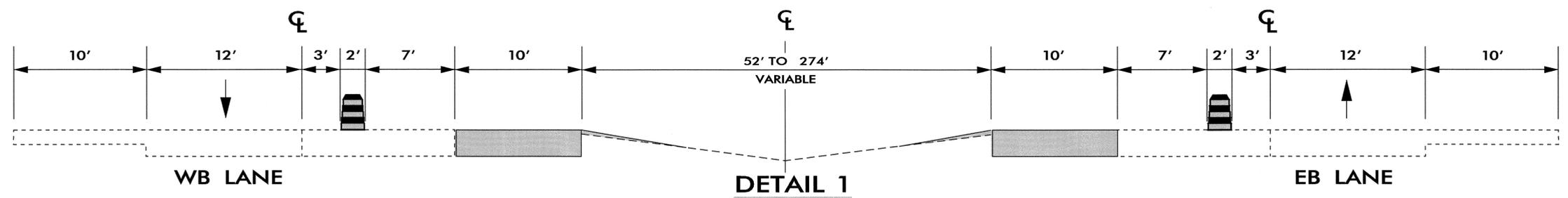
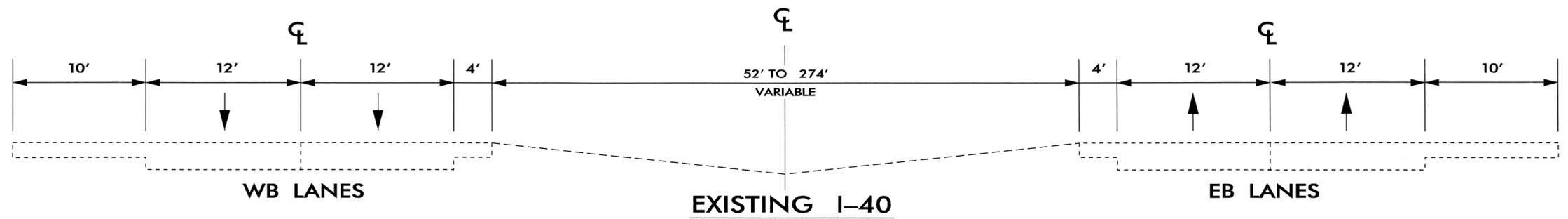
- PHASE III**
- SHIFT TRAFFIC ONTO THE OUTSIDE SHOULDERS OF EASTBOUND AND WESTBOUND I-40. PAVE THE INSIDE LANES OF I-40 UP THROUGH THE BINDER COURSE.
  - SHIFT TRAFFIC ONTO THE INSIDE SHOULDERS OF EASTBOUND AND WESTBOUND I-40. PAVE THE OUTSIDE LANES OF I-40 UP THROUGH THE FIRST LAYER OF SURFACE COURSE.
  - SHIFT TRAFFIC ONTO THE OUTSIDE LANES OF EASTBOUND AND WESTBOUND I-40. PAVE THE FIRST LAYER OF SURFACE COURSE ON THE INSIDE LANES OF I-40.
  - RETURN TRAFFIC TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY.

- PHASE IIIA (OPTION)**
- SHIFT TRAFFIC ONTO THE OUTSIDE SHOULDERS OF EASTBOUND AND WESTBOUND I-40. PAVE THE INSIDE LANES OF I-40 UP THROUGH THE FIRST LAYER OF SURFACE COURSE.
  - SHIFT TRAFFIC ONTO THE INSIDE SHOULDERS OF EASTBOUND AND WESTBOUND I-40. PAVE THE OUTSIDE LANES OF I-40 UP THROUGH THE FIRST LAYER OF SURFACE COURSE.
  - RETURN TRAFFIC TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY.

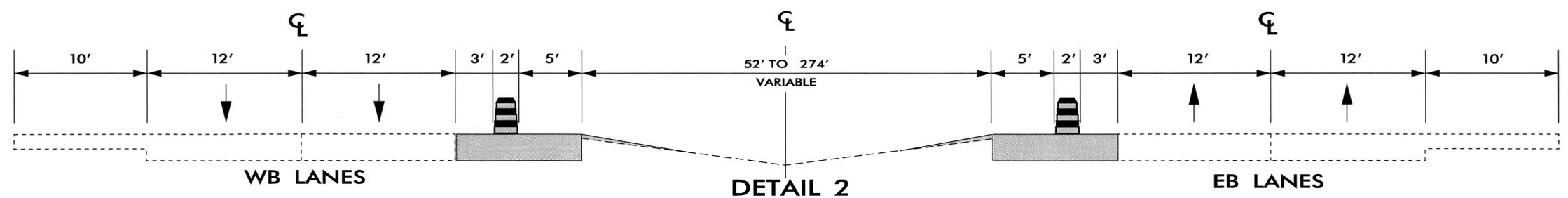
- PHASE IV**
- SHIFT TRAFFIC AS REQUIRED, PAVE THE FINAL LAYER OF SURFACE COURSE, PLACE POLYUREA PAVEMENT MARKINGS, INSTALL SNOWPLOWABLE RAISED PAVEMENT MARKERS, AND MILL RUMBLE STRIPS ON THE EASTBOUND AND WESTBOUND LANES OF I-40.
  - RETURN TRAFFIC TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY.

22-MAR-2007 11:05  
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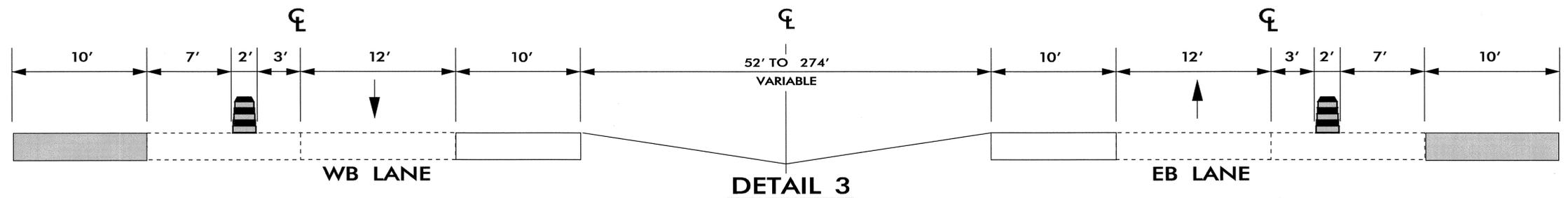
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SCALE: NONE		REVISIONS																	
DATE: 06/04																			
DWG. BY: TAG																			
DESIGN BY: TAG																			
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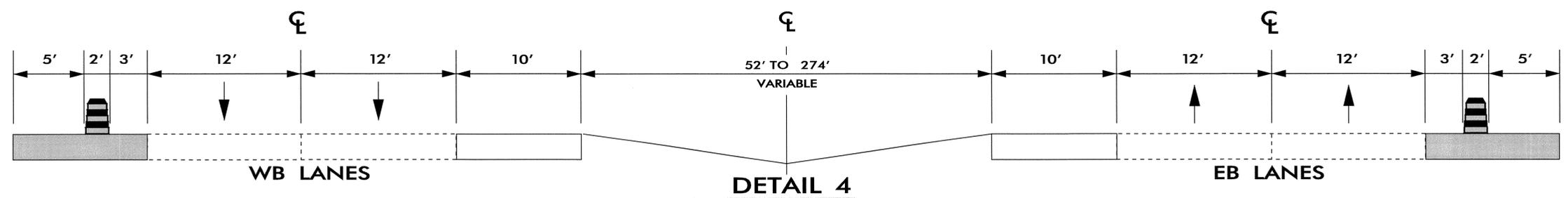
- CONSTRUCT 10' FULL DEPTH PAVEMENT (INSIDE SHOULDERS)



- WEEKEND PATTERN



- CONSTRUCT 10' FULL DEPTH PAVEMENT (OUTSIDE SHOULDERS)



- WEEKEND PATTERN

APPROVED: *[Signature]* DATE: *[Date]*

SEAL

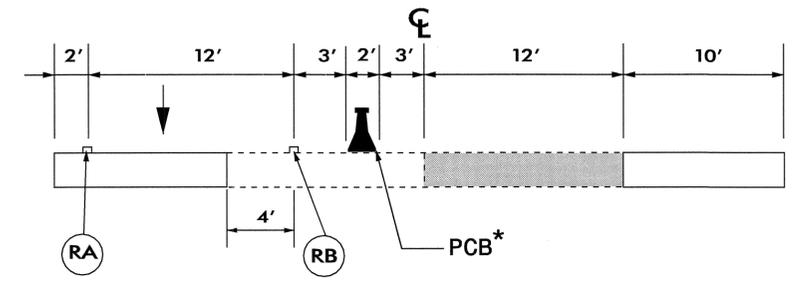
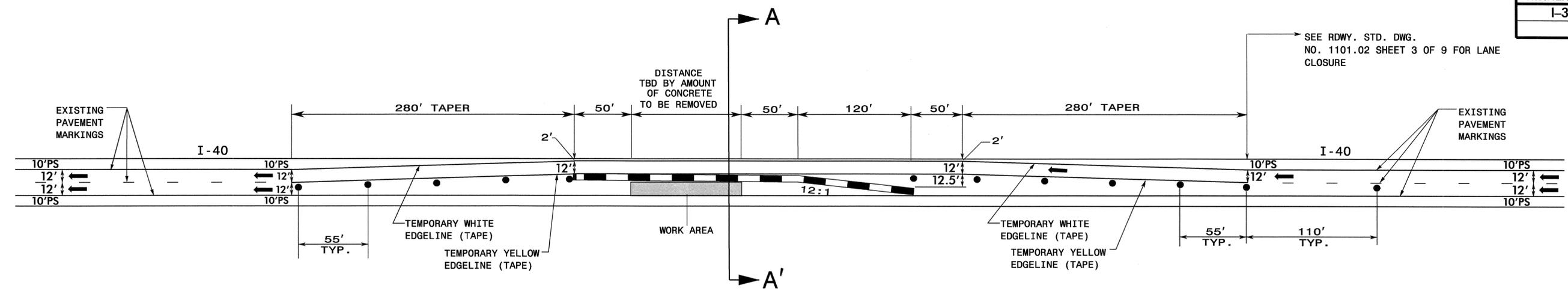
**DETAILS: EXISTING THRU 4**

SCALE: NONE  
 DATE: JANUARY 2002  
 DWG. BY: KSR  
 DESIGN BY: KSR  
 REVIEWED BY: JWG



REVISIONS	

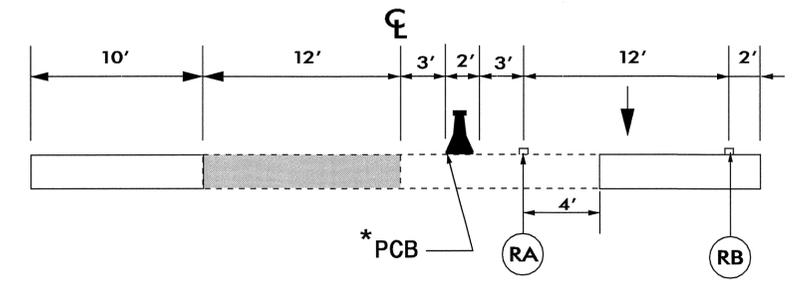
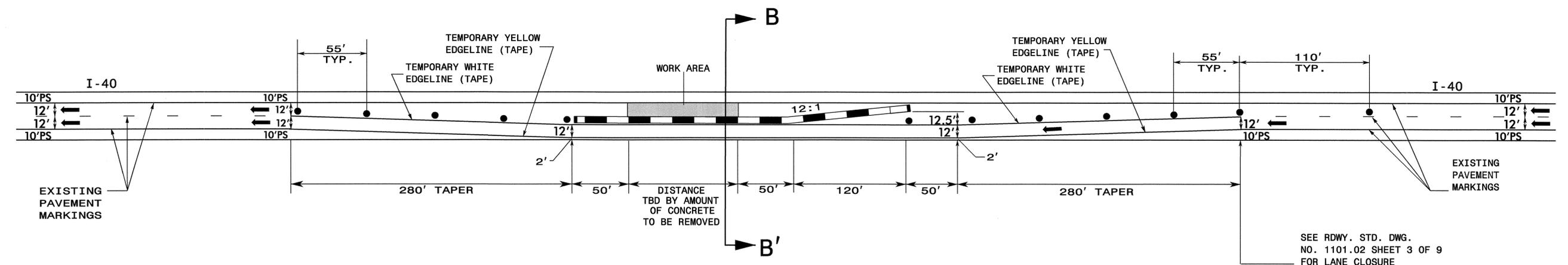
08-JAN-2007 09:23  
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 jrspence AT WZTC224244



A-A'

DETAIL 5 - PLAN AND SECTION VIEWS

\* NOTE - PORTABLE CONCRETE BARRIER WILL NOT BE PLACED ACROSS A BRIDGE UNLESS 18 FEET OF CLEARANCE CAN BE MAINTAINED FOR WIDE LOADS.



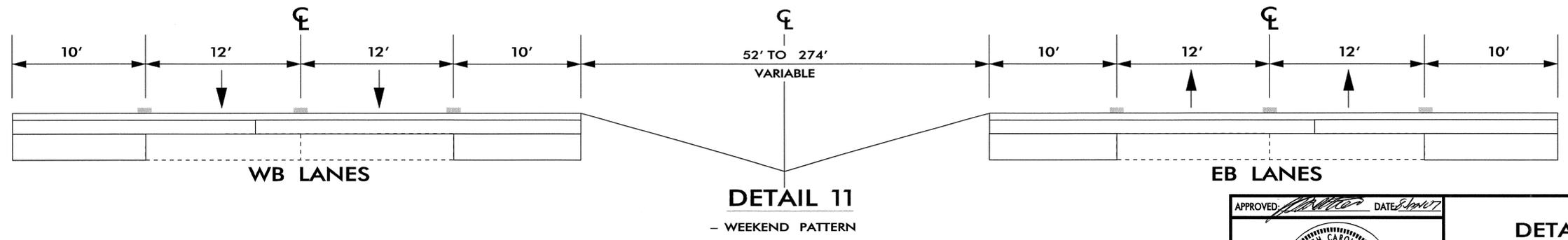
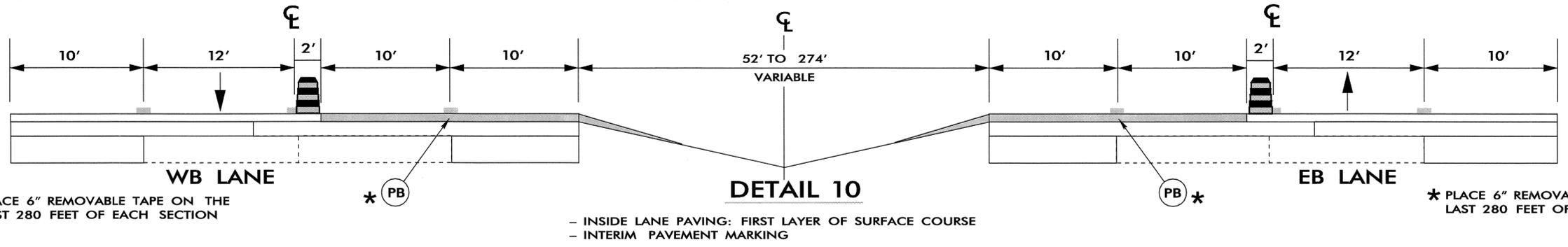
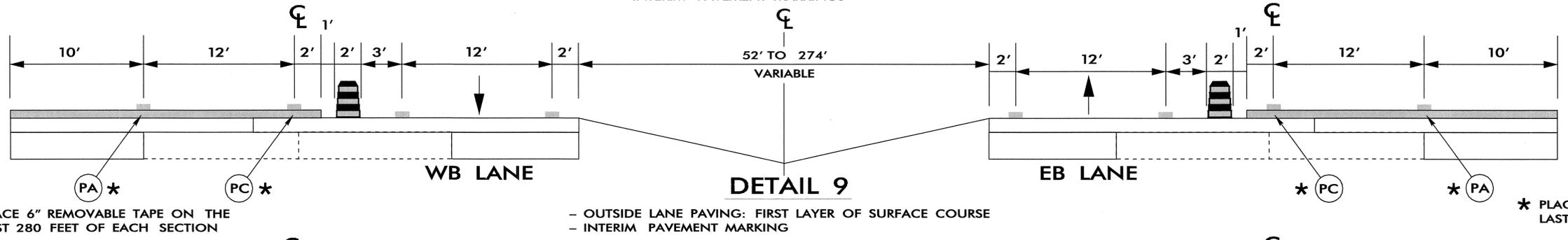
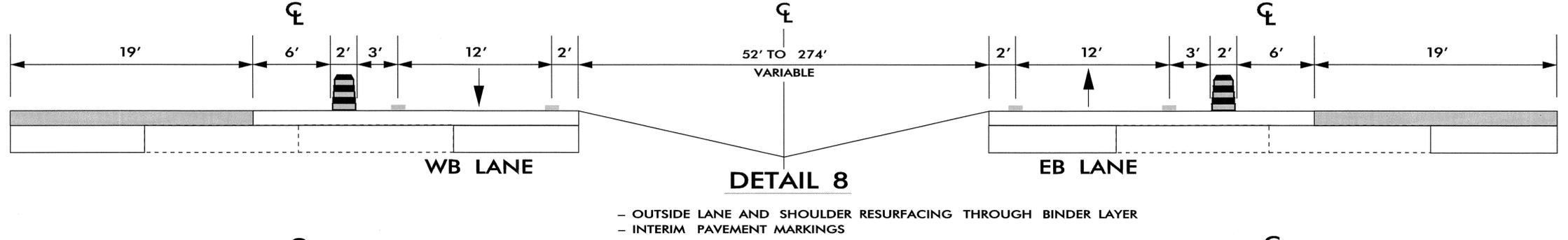
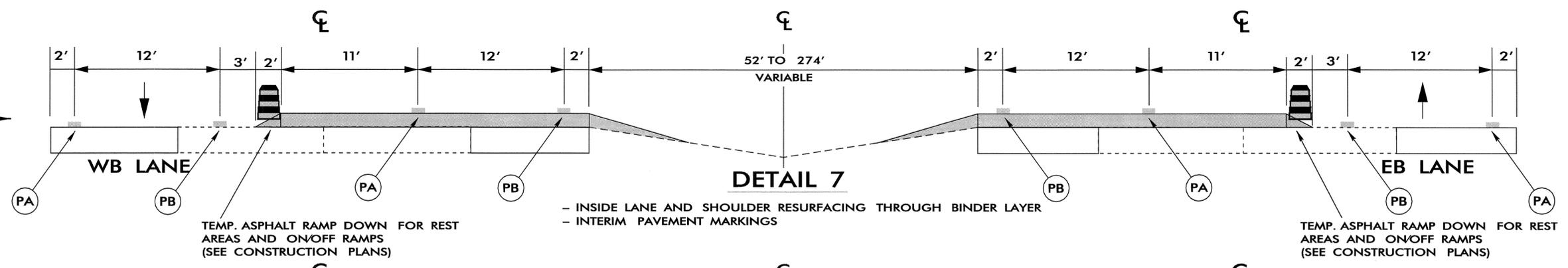
B-B'

DETAIL 6 - PLAN AND SECTION VIEWS

\* NOTE - PORTABLE CONCRETE BARRIER WILL NOT BE PLACED ACROSS A BRIDGE UNLESS 18 FEET OF CLEARANCE CAN BE MAINTAINED FOR WIDE LOADS.

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APPROVED:	DATE: 8/20/04	DETAILS: 5 THRU 6 (CONCRETE PAVEMENT REMOVAL)	
SCALE: NONE	REVISIONS		
DATE: 0804			
DWG. BY: TAG			
DESIGN BY: TAG			
REVIEWED BY: JWG			



THIS WORK MUST BE FINISHED BY FRIDAY AT NOON WITH TRAFFIC RETURNED TO THE EXISTING 2-LANE, ONE-WAY PATTERNS.

08-JAN-2007 09:24 \\dot\dfsroot\01\projstore\proj\13600\trafficcontrol\tcp\13600\_tc\_sections.dgn jrspece AT WZTC224244

APPROVED: *[Signature]* DATE: *Jan 7*

SEAL

**DETAILS: 7 THRU 11**

SCALE:	NONE
DATE:	08/04
DWG. BY:	KSR
DESIGN BY:	KSR
REVIEWED BY:	JWG

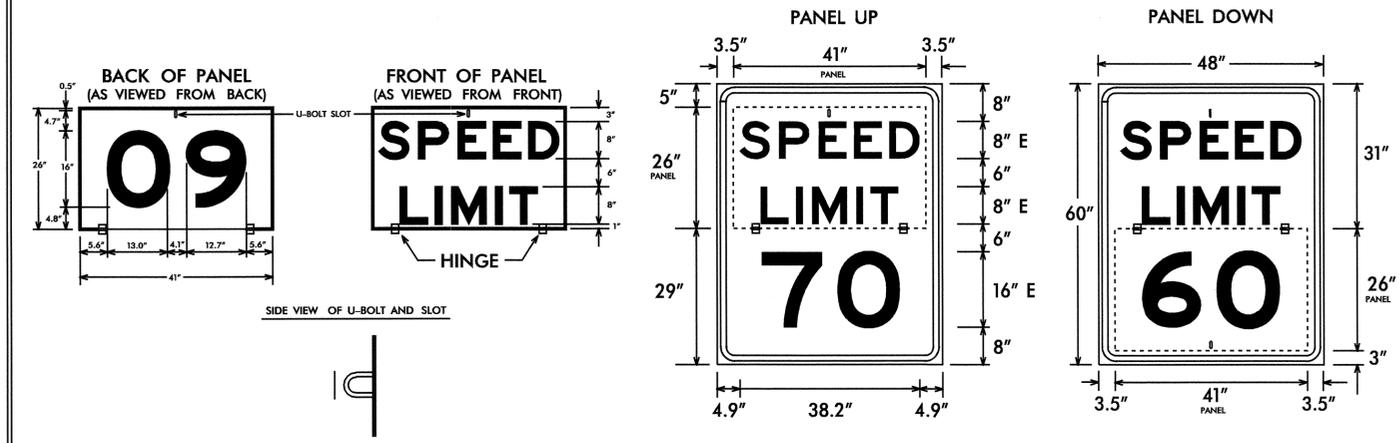
REVISIONS	

CADD FILE



### DETAIL OF 60" x 48" SPEED LIMIT (FLIP) SIGN

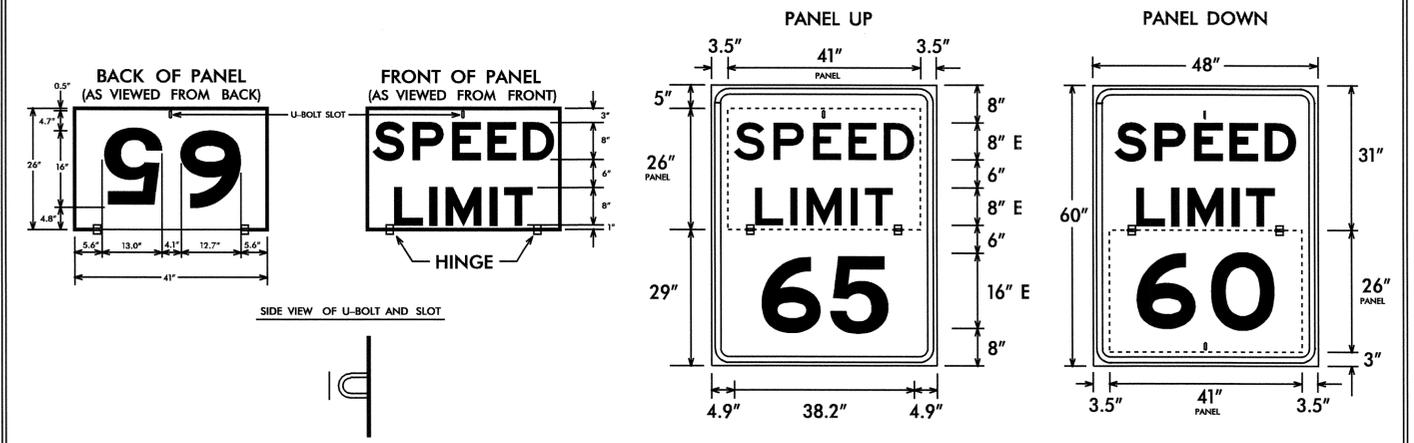
FOR CONSTRUCTION ONLY



file: Speed\_Limit\_flip\_sign.dgn

### DETAIL OF 60" x 48" SPEED LIMIT (FLIP) SIGN

FOR CONSTRUCTION ONLY



file: Speed\_Limit\_flip\_sign.dgn

08-JAN-2007 09:26 \\dot\dfsroot\proj\pstore\proj\pstore\trafficcontrol\tcp\fc-signsdesigns.dgn jr-spence AT WZTC224244

APPROVED:  DATE: 8/1/07 	<b>SIGN DESIGNS</b>									
	SCALE: NONE DATE: 8/04 DWG. BY: JLE DESIGN BY: JLE REVIEWED BY: JWG		<table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table>	REVISIONS						
REVISIONS										





### SP 03353

SIGN NUMBER: SP-03353 TYPE: A QUANTITY: 1 SIGN WIDTH: 4'-0" HEIGHT: 4'-0" TOTAL AREA: 16.0 Sq.Ft. BORDER TYPE: FLUSH RECESS: 0.59" WIDTH: 0.75" RADIUS: 1.38" NO. Z BARS: N/A LENGTH: N/A	BACKG COLOR: Fluorescent Orange COPY COLOR: Black SYMBOL	DESIGN BY: CL DOWNEY PROJECT ID: ALL PROJECTS	CHECKED BY: CHECKED DIV: DIV	STD #: W20-1 DATE: Aug 20, 2003
--	--	--	---------------------------------	------------------------------------

USE NOTES: 2, 4  
 1. Legend and border shall be direct applied Type VII reflective sheeting.  
 2. Legend and border shall be direct applied non-reflective sheeting.  
 3. Shields shall be Type VII reflective sheeting on 0.032" (0.8mm) aluminum and demountable.  
 4. Background shall be Type VII reflective sheeting.  
 5. Background shall be Type I reflective sheeting.  
 6. Center arrow(s) vertically on sign.  
 7. Bottom panel shall be yellow Type III sheeting. Legend shall be direct applied black non-reflective sheeting. Yellow panel is:

Letter spacings are to start of next letter										Series/Size Text Length	
B	E	G	I	N						22.4	C7
22.4	5.3	4.6	5.4	2.5	3.8	22.4					21.6
R	O	A	D							23.4	C7
23.4	5	5.2	5.6	3.8	23.4						19.6
W	O	R	K							22.6	C7
22.6	6.4	5.6	5.2	4	22.6						21.2

Spacing Factor is 1 unless specified otherwise  
 FILENAME: SPECISNAK  
 NORTH CAROLINA D.O.T. SIGN DETAIL

### SP 03350

SIGN NUMBER: SP-03350 TYPE: D QUANTITY: 1 SIGN WIDTH: 4'-0" HEIGHT: 2'-6" TOTAL AREA: 10.0 Sq.Ft. BORDER TYPE: FLUSH RECESS: 0.4" WIDTH: 0.5" RADIUS: 1.5" NO. Z BARS: N/A LENGTH: N/A	BACKG COLOR: White COPY COLOR: Black SYMBOL	DESIGN BY: CL DOWNEY PROJECT ID:	CHECKED BY: DIV DIV: DIV	STD #: REGULATORY DATE: Aug 18, 2003
---	---	-------------------------------------	-----------------------------	---

USE NOTES: 2, 4  
 1. Legend and border shall be direct applied Type III reflective sheeting.  
 2. Legend and border shall be direct applied non-reflective sheeting.  
 3. Shields shall be Type III reflective sheeting on 0.032" (0.8mm) aluminum and demountable.  
 4. Background shall be Type III reflective sheeting.  
 5. Background shall be Type I reflective sheeting.  
 6. Center arrow(s) vertically on sign.  
 7. Bottom panel shall be yellow Type III sheeting. Legend shall be direct applied black non-reflective sheeting. Yellow panel is:

Letter spacings are to start of next letter										Series/Size Text Length	
S	2	5	0							15.1	C6
15.1	5.3	4.6	4.2	3.5	15.4						17.6
S	P	E	E	D	I	N	G			8.1	C6
8.1	4.8	4.5	4	4	4.6	2.1	4.4	3.4	8.2		31.8
P	E	N	A	L	T	Y				11.9	C6
11.9	4.6	4	4.3	4.7	3.4	3.3	3.7	8.2			28

Spacing Factor is 1 unless specified otherwise  
 FILENAME: 250  
 NORTH CAROLINA D.O.T. SIGN DETAIL

STATE OF NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.

DETAIL DRAWING FOR  
 WORK ZONE SIGNS  
 \$250 PENALTY SIGN

#### GENERAL NOTES FOR THE "BEGIN ROAD WORK" SIGN

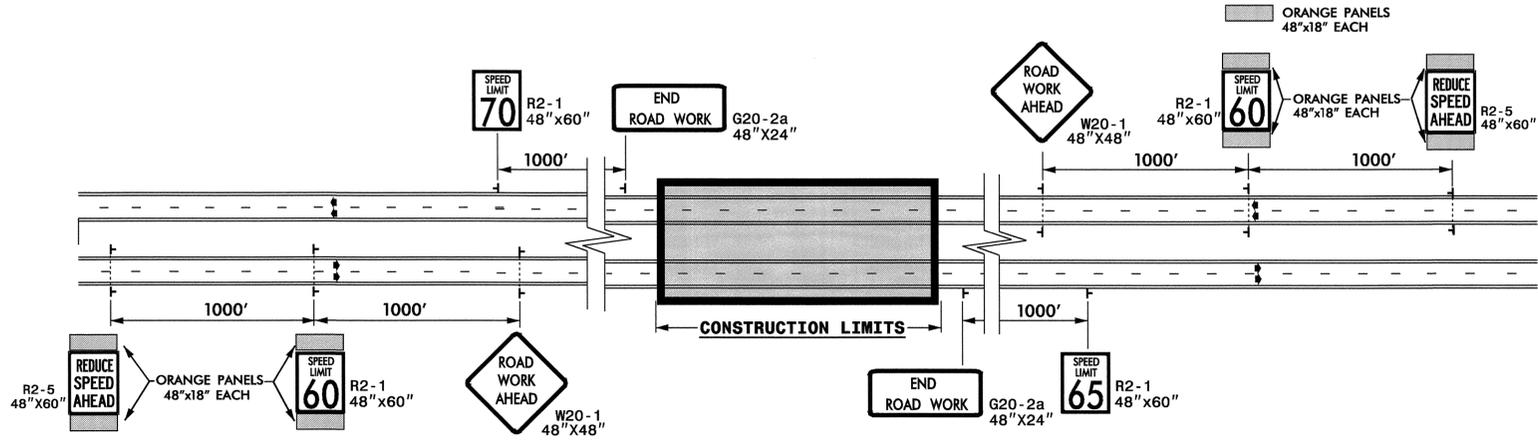
- SIGN SP-03353 "BEGIN ROAD WORK" ONLY APPLIES TO FULL CONTROL AND PARTIAL CONTROL OF ACCESS ROADWAYS
- WHEN USED, INSTALL SIGN SP-03353 "BEGIN ROAD WORK" ACCORDING TO DETAIL A ON SHEET TCP-10.

#### GENERAL NOTES FOR THE "\$250 SPEEDING PENALTY" SIGN

- SIGN SP-03350 "\$250 SPEEDING PENALTY" IS USED ONLY WHEN ORDINANCED BY THE TRAFFIC ENGINEERING AND SAFETY SYSTEMS BRANCH.
- SIGN SP-03350 "\$250 SPEEDING PENALTY" ONLY APPLIES TO FULL CONTROL AND PARTIAL CONTROL OF ACCESS ROADWAYS
- WHEN USED, MOUNT SIGN SP-03350 "\$250 SPEEDING PENALTY" BELOW SIGN R2-1 "SPEED LIMIT XX" (SEE DETAIL A ON SHEET TCP-10) AND SIGN W20-1 "ROAD WORK AHEAD" (SEE DETAIL C ON SHEET TCP-10).

APPROVED:	DATE: 8/20/03	DETAIL DRAWING FOR ADVANCED WORK ZONE WARNING SIGN DESIGNS	
	SCALE: NONE		REVISIONS
	DATE: 0803		04/04
	DESIGN BY:		
	REVIEWED BY:		

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 j-spence AT WTC224244



**GENERAL NOTES**

- FOR UNDIVIDED AND TWO LANE-TWO WAY ROADWAYS, SIGNS ARE REQUIRED ONLY ON THE RIGHT SIDE OF THE ROADWAY.
- SEE SHEET TCP-10 AND TCP-11 FOR ALL OTHER WORK ZONE SIGNS SIZE AND SPACING OTHER THAN THE "REDUCE SPEED AHEAD" SIGN.

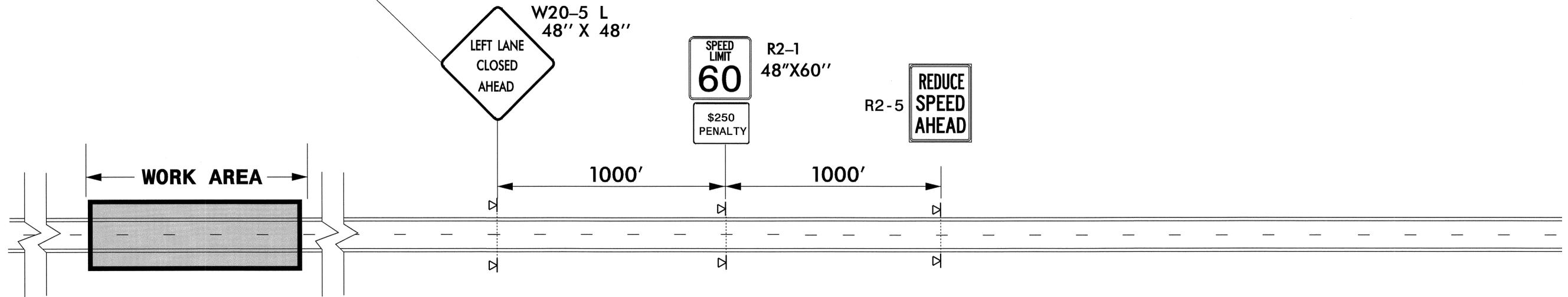
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

DETAIL DRAWING FOR  
WORK ZONE SIGNS  
"REDUCE SPEED AHEAD SIGN"

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j-spence AT W 12/22/24

APPROVED: <i>[Signature]</i>	DATE: <i>2/20/07</i>	<p align="center"><b>DETAIL DRAWING FOR WORK ZONE SIGNS</b></p>	
SCALE: NONE		REVISIONS	
DATE:		7-98	100
DWG. BY:		10-98	500
DESIGN BY:		12-99	0304
REVIEWED BY:			

SEE ROADWAY STANDARD DRAWING 1101.02,  
SHEET 3 OF 9, FOR ADDITIONAL SIGNS FOR  
TEMPORARY LEFT/RIGHT LANE CLOSURES



**GENERAL NOTES**

-THE SIZE OF ALL "REDUCE SPEED AHEAD" SIGNS (R2-5) IS 48" X 60".

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APPROVED:	DATE: 1/10/02	<b>DETAIL: TEMPORARY SPEED LIMIT REDUCTION FOR WORK AREAS</b>	
	SCALE: NONE		REVISIONS
	DATE: JANUARY 2002		
	DWG. BY: JLE		
	DESIGN BY: JLE		
REVIEWED BY: JWG			CADD FILE