

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
B-4192	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
McDOWELL COUNTY**

B-4192

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1170.01	PORTABLE CONCRETE BARRIER
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.12	PAVEMENT MARKINGS - BRIDGES
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

**TEMPORARY PAVEMENT
MARKING SCHEDULE**

SYMBOL	DESCRIPTION	PAY ITEM	QUANTITY BREAKDOWN	TOTAL QUANTITY
PAVEMENT MARKING LINES				
		PAINT (4")		5520 FT
PA	WHITE EDGELINE (2X)		2760 FT	
PI	YELLOW DOUBLE CENTERLINE (2X)		2760 FT	
PAINT (24")				
P4	WHITE STOP BAR (2X)		50 FT	50 FT

**FINAL PAVEMENT
MARKING SCHEDULE**

SYMBOL	DESCRIPTION	PAY ITEM	QUANTITY BREAKDOWN	TOTAL QUANTITY
PAVEMENT MARKING LINES				
		PAINT (4")		5520 FT
PA	WHITE EDGELINE (2X)		2760 FT	
PI	YELLOW DOUBLE CENTERLINE (2X)		2760 FT	

INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, INDEX OF SHEETS, AND TEMPORARY AND FINAL PAVEMENT MARKING SCHEDULE
TCP-2	PROJECT NOTES
TCP-3	PHASING
TCP-4	PHASE I OVERVIEW
TCP-5	PHASE II OVERVIEW
TCP-6	DETAIL DRAWING FOR ADVANCED WORK ZONE WARNING SIGNS

LEGEND

- GENERAL**
- ← DIRECTION OF TRAFFIC FLOW
 - ↑ NORTH ARROW
 - PROPOSED PVMT. - - - - - EXIST. PVMT.
 - PORTABLE CONCRETE BARRIER (PCB)
 - WORK AREA
 - ▨ REMOVAL OF EXISTING PAVEMENT/STRUCTURE
- TRAFFIC CONTROL DEVICES**
- I TYPE I BARRICADE
 - II TYPE II BARRICADE
 - III TYPE III BARRICADE
 - ▲ CONE
 - DRUM
 - ⚡ FLASHING ARROW PANEL (TYPE C)
 - ⊗ TYPE 'B' WARNING LIGHT
 - T STATIONARY SIGN
 - ⚡ PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - ⚡ WARNING FLAGS
 - ~ CRASH CUSHION
 - ◀ CHANGEABLE MESSAGE SIGN
 - ⊞ TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - ⊞ POLICE
 - ⊞ FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
 - ◆ YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - ↔ PAVEMENT MARKING SYMBOLS

TIP PROJECT:

99999SYTIME99999
99999DGN99999
99999SERVNAME99999

PLAN REVIEWED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT	APPROVED: <i>Stacey H. Bailey</i> DATE: 8/10/06	PLAN PREPARED FOR N.C.D.O.T. BY: GREENHORNE & O'MARA, INC. 5565 CENTERVIEW DRIVE, SUITE 107 RALEIGH, NORTH CAROLINA 27606 (919) 851-1919
J. STUART BOURNE, P.E. TRAFFIC CONTROL ENGINEER	SEAL	STACEY H BAILEY, P.E. PROJECT ENGINEER
JOSEPH ISHAK, P.E. TRAFFIC CONTROL PROJECT ENGINEER		MICHELLE WARD DESIGN ENGINEER
HABIB A. LAWANDOS TRAFFIC CONTROL PROJECT DESIGN ENGINEER		MICHELLE WARD DESIGN TECHNICIAN
WALID DAKDUK TRAFFIC CONTROL DESIGN ENGINEER		

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PROJECT NOTES

PROJ. REFERENCE NO. B-4192	SHEET NO. TCP-2
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GREENHORNE & O'MARA, INC.
5565 CENTERVIEW DRIVE, SUITE 107
RALEIGH, NORTH CAROLINA 27606
(919) 851-1919

GENERAL NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT STOP TRAFFIC FOR MORE THAN 15 MINUTES AS FOLLOWS:

ROAD NAME	OPERATION
1. -L- (SR 1103/BAT CAVE RD)	TRAFFIC SHIFTS

B) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- C) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT (12m) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING REPLACEMENT DETAIL FOR RSD 1101.04, SHEET TCP-9, UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- G) DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.
- H) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.
- I) DO NOT INSTALL MORE THAN 1/2 MILE OF LANE CLOSURE ON SR 1103 (BAT CAVE RD), MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- J) DO NOT INSTALL MORE THAN ONE LANE CLOSURE, IN ANY ONE DIRECTION, ON SR 1103 (BAT CAVE RD).

PAVEMENT EDGE DROP OFF REQUIREMENTS

- K) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

 BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

 BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

 BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- L) DO NOT EXCEED A DIFFERENCE OF 2 inches IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT (150m) IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- M) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- N) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

- O) PROVIDE PERMANENT SIGNING.
- P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC BARRIER

- Q) INSTALL MOVABLE/PORTABLE CONCRETE BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE MOVABLE/PORTABLE CONCRETE BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

ONCE MOVABLE/PORTABLE CONCRETE BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE MOVABLE/PORTABLE CONCRETE BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET MOVABLE/PORTABLE CONCRETE BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

- R) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF THE MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
LESS THAN 50 MPH	20 FT
50 MPH OR HIGHER	30 FT

INSTALL MOVABLE/PORTABLE CONCRETE BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE MOVABLE/PORTABLE CONCRETE BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP CLOSED THE SECTION OF THE ROADWAY UNTIL THE BARRIER CAN BE PLACED OR AFTER BARRIER IS REMOVED.

TRAFFIC CONTROL DEVICES

- S) WHEN USING ROADWAY STANDARD NO. 1101.02, SHEET 1 OF 7, DRUMS MAY BE USED IN LIEU OF CONES ON SR 1103 (BAT CAVE RD).
- T) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT (3m) ON-CENTER IN RADII, AND 3 FT (1m) OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.
- U) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

- V) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

PAVEMENT MARKINGS AND MARKERS

- W) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. -L- (SR 1103) BAT CAVE RD	PAINT	NONE





- X) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. -L- (SR 1103) BAT CAVE RD	PAINT	NONE

- Y) PLACE AT LEAST TWO APPLICATIONS OF PAINT PAVEMENT MARKINGS ON THE FINAL WEARING SURFACE ON NEW ASPHALT PAVEMENT. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.
- Z) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- AA) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.
- BB) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

TEMPORARY/FINAL SIGNALS

- CC) SHIFT AND REVISE ALL SIGNAL HEADS AS SHOWN ON THE SIGNAL PLANS.

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*****SYTIME*****
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PHASING

PROJ. REFERENCE NO.	SHEET NO.
B-4192	TCP-3

GREENHORNE & O'MARA, INC.
 5565 CENTERVIEW DRIVE, SUITE 107
 RALEIGH, NORTH CAROLINA 27606
 (919) 851-1919

NOTES:

RETURN TRAFFIC TO THE EXISTING PATTERN AT THE END OF EACH WORK DAY UNLESS OTHERWISE NOTED IN THE PHASING OR DIRECTED BY THE ENGINEER.

USING INCIDENTAL STONE AS NECESSARY, MAINTAIN VEHICULAR ACCESS TO ALL RESIDENCES DURING THE LIFE OF THE CONTRACT UNLESS OTHERWISE NOTED IN THE PHASING OR DIRECTED BY THE ENGINEER. MAINTAIN TRAFFIC FROM DRIVEWAYS BETWEEN THE TWO TEMPORARY SIGNALS TO A POINT BEYOND THE SIGNAL.

COMPLETE ANY PROPOSED WIDENING IN SUCH A MANNER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE

PHASE I (SEE PHASE I OVERVIEW, SHEET TCP-4)

STEP 1:
INSTALL WORK ZONE SIGNS ACCORDING TO RSD 1110.01 AND SHEET TCP-6.

STEP 2:
USING RSD 1101.02, SHEET 1 OF 9, INSTALL PCB FROM STA 14+95 +/- -L- TO STA 15+65 +/- -L- AND INSTALL TEMPORARY BARRIER SUPPORTED SHORING FROM STA 14+95 +/- -L- TO STA 15+15 +/- -L-.

STEP 3:
AWAY FROM TRAFFIC, CONSTRUCT PHASE I AND PHASE II OF THE CULVERT AT STA 12+00 +/- -DET- (STA 14+75 +/- -L-).

USING RSD 1101.02, SHEET 1 OF 9, WHEN NECESSARY, CONSTRUCT -DET- FROM STA 13+40 +/- TO STA 13+83 +/- INCLUDING THE TEMPORARY GRAVEL ON THE RIGHT SIDE OF -L- FROM -L- STA. 16+00 +/- TO -L- STA. 16+50 +/- AS SHOWN ON ROADWAY PLANS.

STEP 4:
INSTALL TEMPORARY SIGNALS AND TEMPORARY MARKING SHOWN ON PHASE II OVERVIEW, AND USING RSD 1101.02 SHEET, 1 OF 9, SHIFT TRAFFIC TO ONSITE DETOUR AND ACTIVATE TEMPORARY SIGNALS. (SEE PHASE II OVERVIEW, SHEET TCP-5)

PHASE II (SEE PHASE II OVERVIEW, SHEET TCP-5)

STEP 1:
AWAY FROM TRAFFIC REMOVE PCB AND BARRIER SUPPORTED SHORING FROM STA 14+95 +/- -L- TO STA 15+65 +/- -L-. REMOVE EXISTING BRIDGE AND CONSTRUCT PHASE III AND PHASE IV OF THE CULVERT, AND CONSTRUCT -L- FROM STA 14+12 +/- -L- TO STA 15+57 +/- -L-, UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.

STEP 2:
COVER OR DEACTIVATE TEMPORARY SIGNALS AND USING RSD 1101.02, SHEET 1 OF 9, CONSTRUCT THE FOLLOWING: (REACTIVATE OR UNCOVER TEMPORARY SIGNALS FOR USE DURING NON-WORKING PERIODS.)

STA 10+61.00 -L- TO STA 14+12 +/- -L-
 STA 15+57 +/- -L- TO STA 17+50.00 -L-

STEP 3:
USING RSD 1101.02 SHEET 1 OF 9, PLACE TEMPORARY MARKINGS ON -L-, DEACTIVATE AND REMOVE TEMPORARY SIGNALS, AND SHIFT TRAFFIC TO THE FINAL PATTERN. (SEE RSD 1205.02, SHEET 1 OF 3, FOR PAVEMENT MARKING ON 2-LANE ROADS - USE DOUBLE YELLOW CENTERLINE FOR CENTERLINE MARKING)

PHASE III

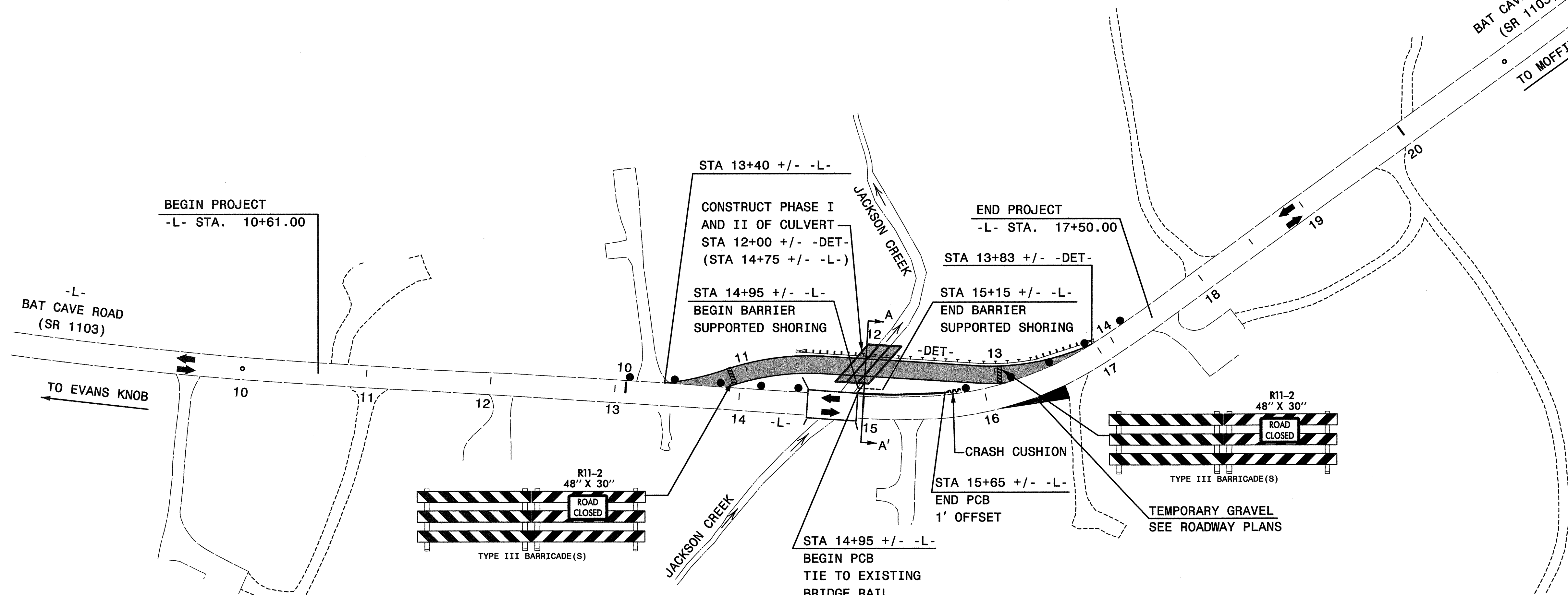
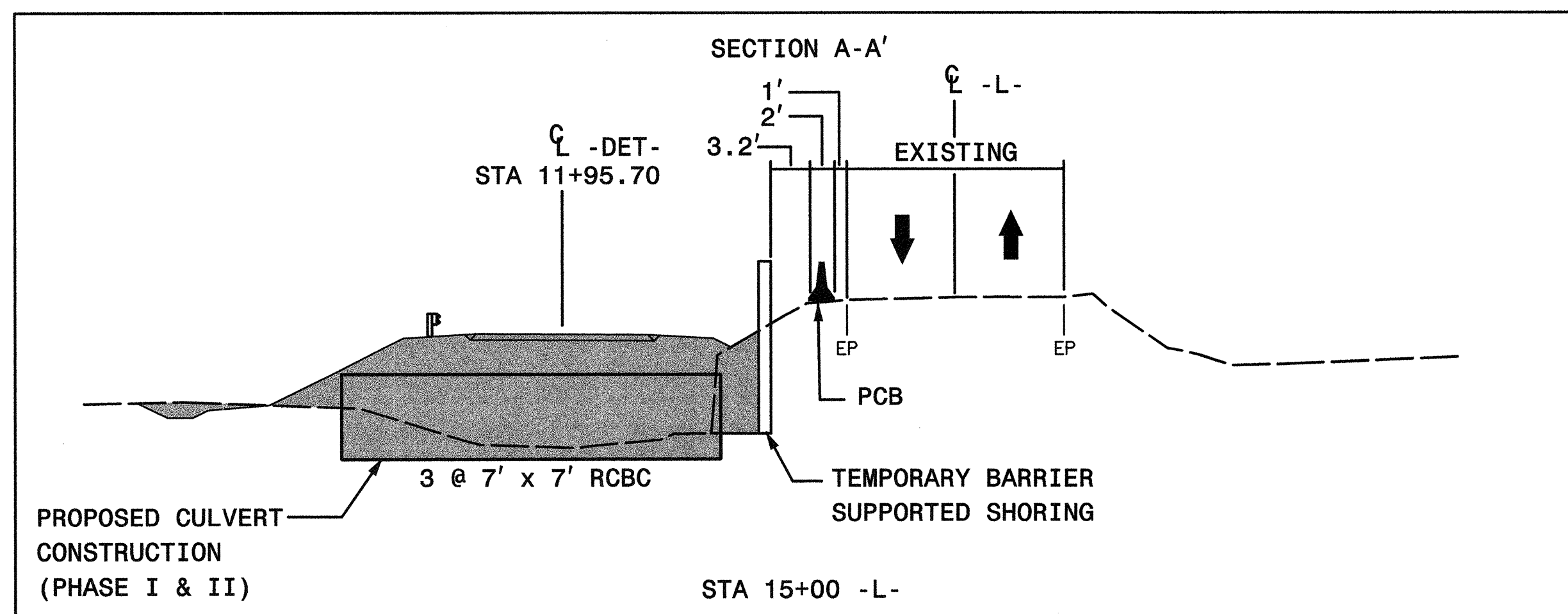
STEP 1:
USING RSD 1101.02, SHEET 1 OF 9, WHEN NECESSARY, COMPLETE THE FOLLOWING:

- REMOVE ASPHALT AND AGGREGATE BASE COURSE FROM -DET- ALIGNMENT AND THE TEMPORARY GRAVEL CONSTRUCTED IN PHASE I
- REGRADE SHOULDERS AND SLOPES ON -L- (LT SIDE) ACCORDING TO THE ROADWAY PLANS
- COMPLETE ANY OTHER WORK NOT COMPLETED IN PHASE I OR PHASE II

STEP 2:
USING RSD 1101.02 SHEET 1 OF 9, PLACE THE FINAL LAYER OF SURFACE COURSE AND FINAL PAVEMENT MARKINGS (PAINT), REMOVE ALL REMAINING TEMPORARY TRAFFIC CONTROL DEVICES, AND OPEN -L- (SR 1103) TO THE FINAL PATTERN. (SEE RSD 1205.02, SHEET 1 OF 3, FOR PAVEMENT MARKING ON 2-LANE ROADS - USE DOUBLE YELLOW CENTERLINE FOR CENTERLINE MARKING)

***** SYSTEM *****
 ***** USER *****

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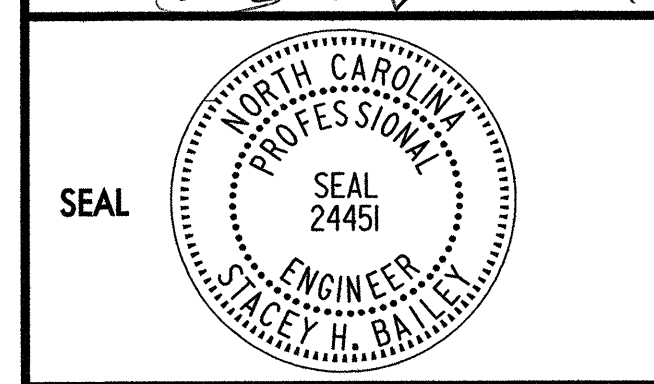


SOIL PARAMETERS

f = 30 degrees
 c = 0 psf
 Unit weight above the water table = 120 pcf
 Unit weight below the water table = 60 pcf

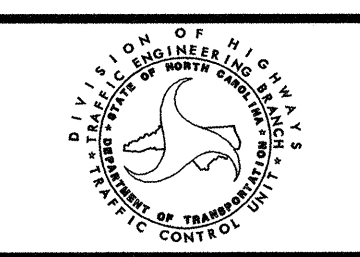
- LEGEND**
- = PROPOSED CONSTRUCTION
 - = TEMPORARY GRAVEL

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PHASE I OVERVIEW

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 REVIEWED BY: SHB

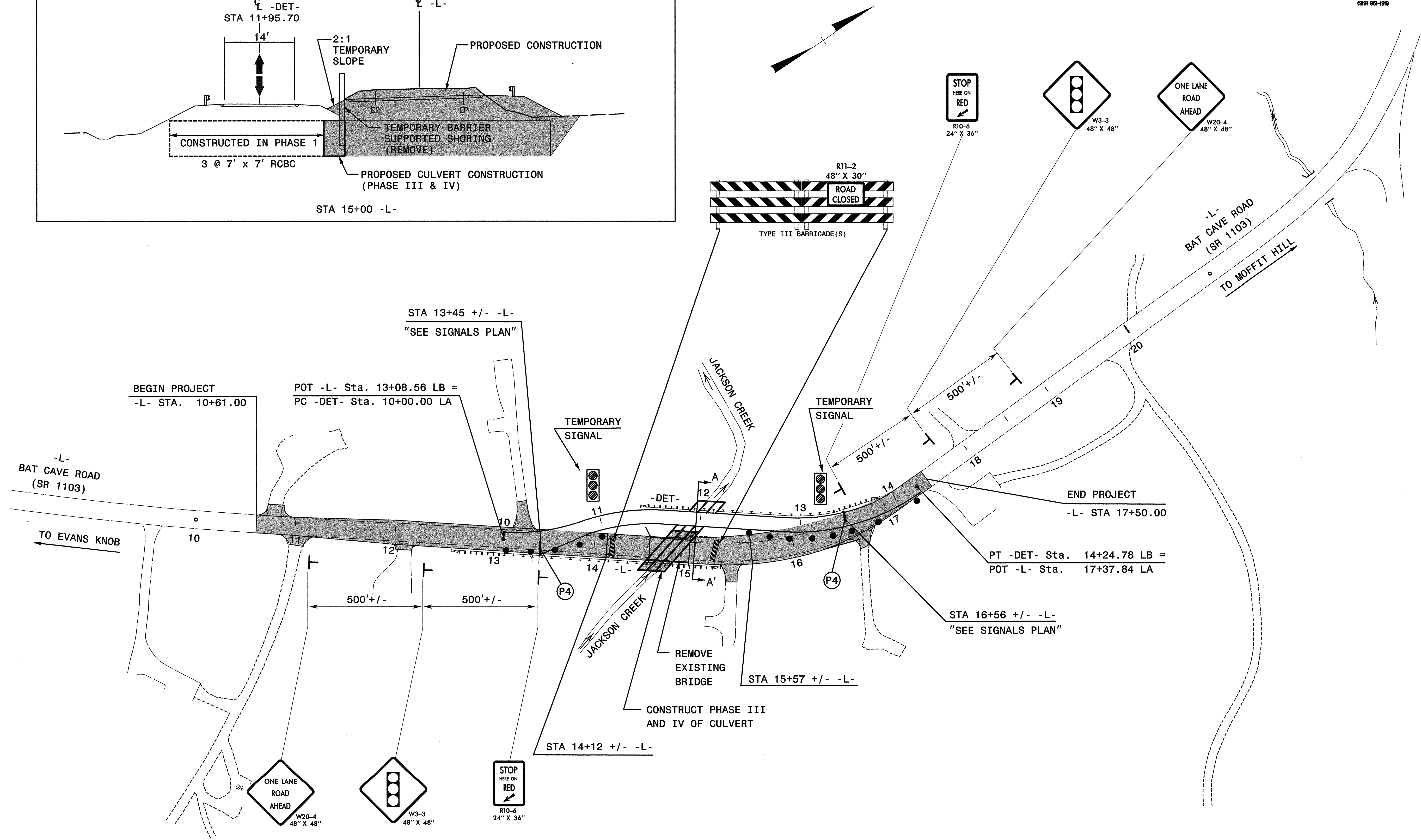
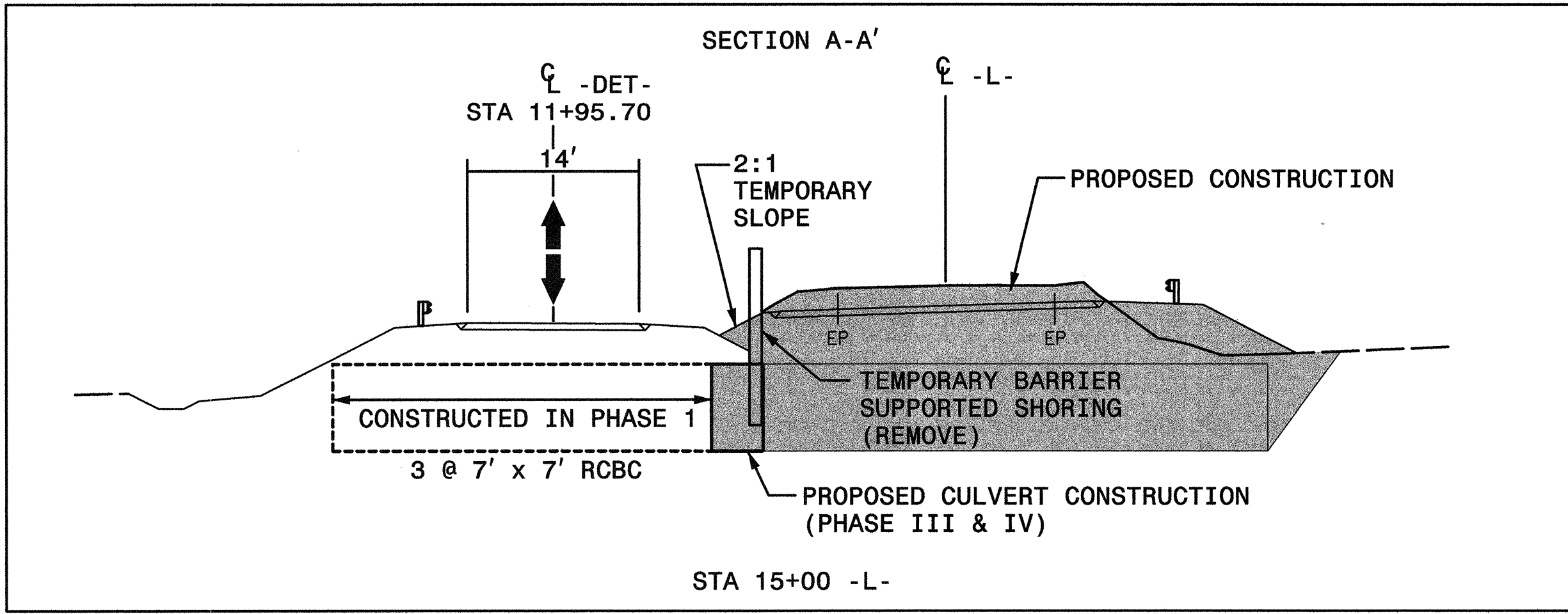


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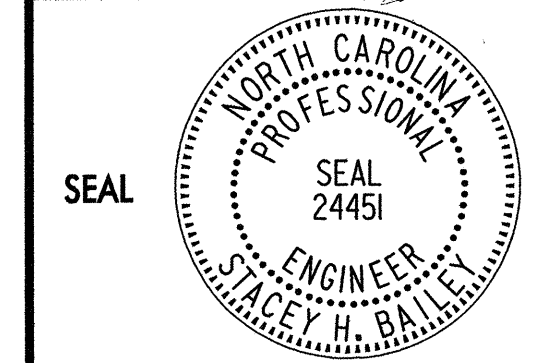
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LEGEND
 = PROPOSED CONSTRUCTION

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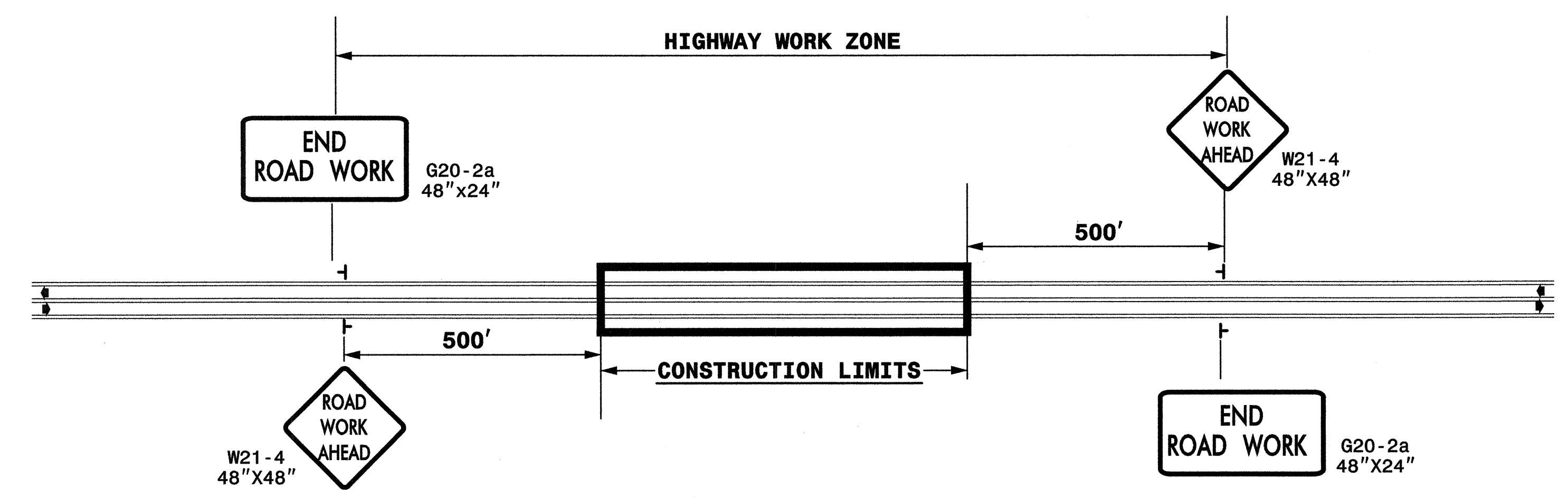
PHASE II OVERVIEW

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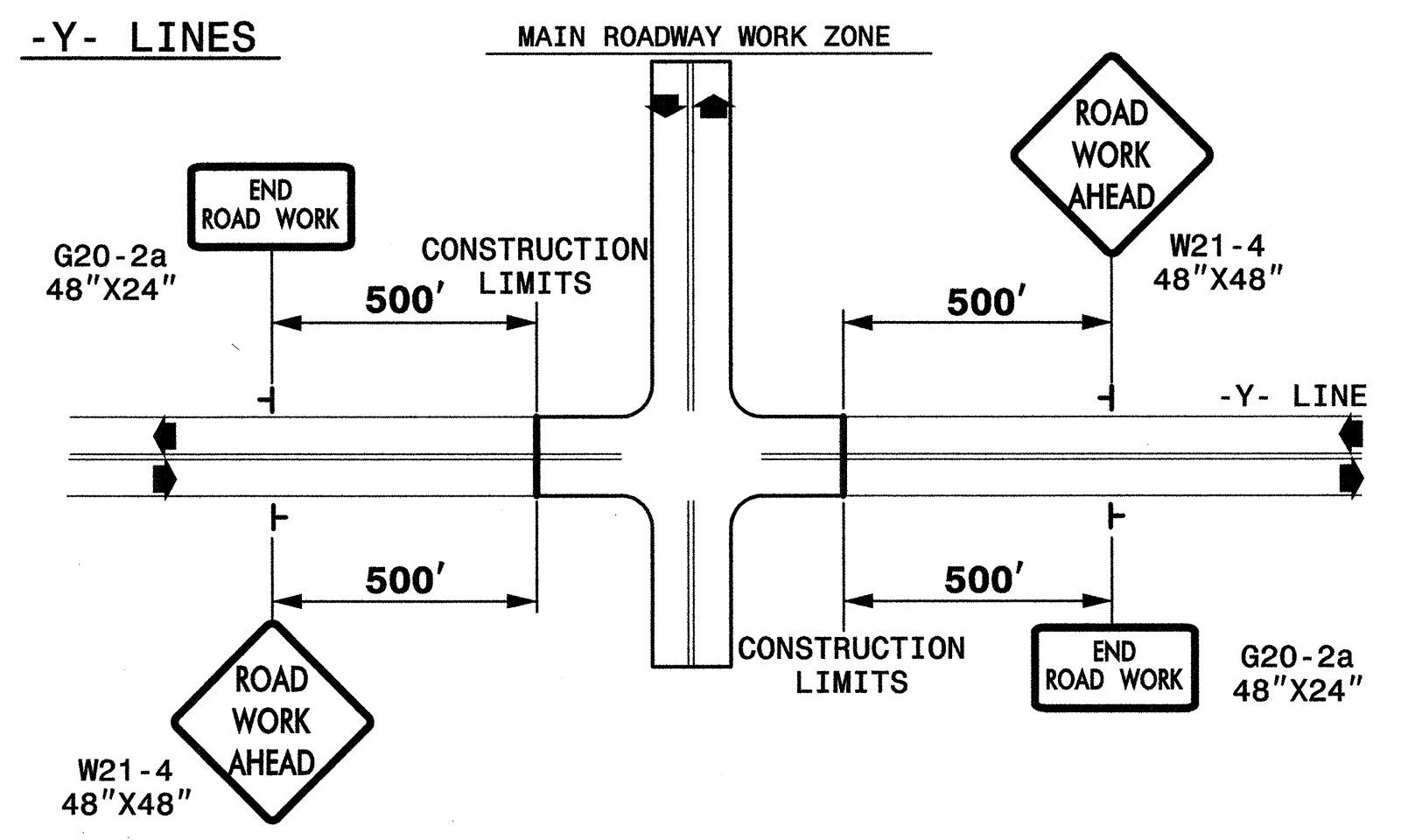
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TWO-WAY UNDIVIDED ** (L-LINES)



STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

└ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: <i>[Signature]</i> DATE: 8/10/06	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
	SCALE: NONE	REVISIONS
	DATE: 8-06	7-98 10/01
	DWG. BY: NCDOT	10-98 03/04
	DESIGN BY: NCDOT	01/01 11/04
REVIEWED BY:	CADD FILE: B4192WZSIGNS.DGN	

\$\$\$\$SYTIME\$\$\$\$
\$\$\$\$USERNAME\$\$\$\$