

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

| | |
|-----------------------------|-----------|
| STATE PROJECT REFERENCE NO. | SHEET NO. |
| 3 6 7 2 7 . 3 . 5 | TCP-1 |

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
GUILFORD COUNTY**

WBS 36727.3.5

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"-ROADWAY DESIGN UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

| STD. NO. | TITLE |
|----------|---------------------------------|
| 1101.11 | TRAFFIC CONTROL DESIGN TABLES |
| 1110.02 | PORTABLE WORK ZONE SIGNS |
| 1115.01 | FLASHING ARROW PANELS |
| 1130.01 | DRUMS |
| 1135.01 | CONES |
| 1145.01 | BARRICADES |
| 1165.01 | TRUCK MOUNTED IMPACT ATTENUATOR |

INDEX OF SHEETS

| SHEET NO. | TITLE |
|----------------------|--|
| TCP-1 | LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS |
| TCP-2 | GENERAL NOTES |
| TCP-3 | PLAN OVERVIEW |
| TCP-4A THRU TCP-4N | BRIDGE #169 (GROOMETOWN RD.) |
| TCP-5A THRU TCP-5G | BRIDGE #197 (S. HOLDEN RD.) |
| TCP-6A THRU TCP-6G | BRIDGE #15 (US-220) |
| TCP-7A THRU TCP-7B | BRIDGE #202 (W. VANDALIA RD.) |
| TCP-8A THRU TCP-8H | BRIDGE #225 (REHOBETH CHURCH RD.) |
| TCP-9A THRU TCP-9I | BRIDGE #H261 (CREEK RIDGE RD.) |
| TCP-10 | SHOULDER CLOSURE |
| TCP-11A THRU TCP-11B | SPECIAL SIGN DETAILS |

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
- TRAFFIC CONTROL DEVICES**
- TYPE III BARRICADE
 - DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)

PROJECT:

24-OCT-2006 13:46 \\dot\dfsroot\ncspr\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp1.dgn
K Broadwell AT WZTC224240

| | |
|---------------|---|
| APPROVED: | PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT |
| DATE: 9/25/06 | |
| SEAL | J. S. BOURNE, P.E. TRAFFIC CONTROL ENGINEER |
| | G. L. GETTIER, P.E. TRAFFIC CONTROL PROJECT ENGINEER |
| | JESSE GILSTRAP TRAFFIC CONTROL PROJECT DESIGN ENGINEER |
| | KEN BROADWELL <i>KPB</i> TRAFFIC CONTROL DESIGN ENGINEER |

GENERAL NOTES

| | |
|---------------------|-----------|
| PROJ. REFERENCE NO. | SHEET NO. |
| WBS 36727.3.5 | TCP-2 |

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

| ROAD NAME | DAY AND TIME RESTRICTIONS |
|--|---|
| 1. I-85 BUSINESS (ONE LANE) I-85 (ONE LANE) | MONDAY THRU SATURDAY 6:00AM-9:00AM; 4:00PM-8:00PM SUNDAY 11:00AM-8:00PM |
| 2. I-85 BUSINESS (2 OR 3 LANES) I-85 (2 OR 3 LANES) | MONDAY THRU FRIDAY 6:00AM-8:00PM SATURDAY 8:00AM - MIDNIGHT SUNDAY ANYTIME |
| 3. W. VANDALIA ROAD | MONDAY THRU FRIDAY 6:00AM-9:00AM 4:00PM-6:00PM |

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

| ROAD NAME |
|---|
| 1. I-85 BUSINESS & ASSOCIATED RAMPS & LOOPS |
| 2. I-85 AND ASSOCIATED RAMPS AND LOOPS |

HOLIDAY

1. FOR ANY EVENT THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00AM DECEMBER 31st TO 8:00PM JANUARY 2nd. IF NEW YEAR'S DAY IS ON A SATURDAY OR A SUNDAY, THEN UNTIL 8:00PM THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 6:00AM THURSDAY AND 8:00PM MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00AM FRIDAY TO 8:00 TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00AM THE DAY BEFORE INDEPENDENCE DAY AND 8:00PM THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A SATURDAY OR SUNDAY, THEN BETWEEN THE HOURS OF 6:00AM THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:00PM THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 6:00AM FRIDAY AND 8:00PM TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00AM TUESDAY TO 8:00PM MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00AM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00PM THE FOLLOWING MONDAY AFTER THE WEEK OF CHRISTMAS.
9. FOR THE BIENNIAL EVENT AT THE FURNITURE MARKET IN HIGH POINT, BETWEEN THE HOURS OF 6:00AM THE DAY BEFORE THE BEGINNING OF THE EVENT TO 8:00PM THE DAY AFTER THE EVENT.
10. FOR THE GREENSBORO PGA GOLF TOURNAMENT BETWEEN THE HOURS OF 6:00AM THE TUESDAY OF THE WEEK OF THE TOURNAMENT UNTIL 8:00PM THE FOLLOWING MONDAY AFTER THE TOURNAMENT.
11. FOR ANY ACC OR NCAA BASKETBALL TOURNAMENT HELD AT GREENSBORO COLISEUM, BETWEEN THE HOURS OF 6:00AM THE TUESDAY OF THE WEEK OF THE TOURNAMENT UNTIL 8:00PM THE FOLLOWING MONDAY AFTER THE TOURNAMENT.

C) DO NOT STOP TRAFFIC OR CLOSE ROADS AS FOLLOWS:

| ROAD NAME | DAY AND TIME RESTRICTIONS |
|--|--|
| 1. I-85 BUSINESS | ANYTIME |
| 2. I-85 & ALL RAMPS | ANYTIME |
| 3. W. VANDALIA ROAD | ANYTIME |
| 4. GROOMETOWN ROAD RAMPS | ANYTIME |
| 6. S. HOLDEN ROAD RAMPS | ANYTIME |
| 7. S. HOLDEN ROAD LOOPS | MONDAY THRU SATURDAY 6:00AM-8:00PM SUNDAY 11:00AM-8:00PM |
| 8. US-220 LOOPS | MONDAY THRU THURSDAY 6:00AM-8:00PM 6:00AM FRIDAY THRU 6:00AM MONDAY |
| 9. US-220 RAMPS | ANYTIME |
| 10. REHOBETH CHURCH ROAD RAMPS | ANYTIME |
| 11. REHOBETH CHURCH ROAD LOOPS | MONDAY THRU SATURDAY 6:00AM-8:00PM SUNDAY 11:00AM-8:00PM |
| 12. I-40 EB ON-RAMP TO I-85 BUS. SB | MONDAY THRU SATURDAY 6:00AM-8:00PM SUNDAY 11:00AM-8:00PM |
| 13. RAMP FROM RANDLEMAN RD. TO I-85 BUS. SB | MONDAY THRU SATURDAY 6:00AM-8:00PM SUNDAY 11:00AM-8:00PM |

D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- E) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT (12M) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING SHEET TCP-10 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT (1.5M) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT (3M) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

I) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.

J) DO NOT INSTALL MORE THAN ONE LANE CLOSURE, IN ANY ONE DIRECTION, ON I-85 AND/OR I-85 BUS.

TRAFFIC PATTERN ALTERATIONS

K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

L) PROVIDE DETOUR SIGNING WITH IN AND OFF THE PROJECT LIMITS.

M) COVER OR REMOVE ALL DETOUR SIGNS WITH IN AND/OR OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.

N) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

O) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.

P) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

Q) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT (150M) CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

MISCELLANEOUS

R) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH WORK AREAS AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

S) COORDINATE WITH THE ENGINEER TO UTILIZE OVERHEAD DYNAMIC MESSAGE SIGNS (DMS), IF AVAILABLE, FOR ADVANCE WARNING TO MOTORIST OF: "ROAD WORK AHEAD AT MP XXX", "LEFT/RIGHT (NUMBER) LANES CLOSED AHEAD AT MP XXX", "(ROAD NAME) EXIT XXX CLOSED USE EXIT XXX". INCLUDE INTERSTATE DESIGNATION IN MESSAGES IF DMS IS LOCATED ON MULTI-ROUTE INTERSTATES.

T) INSTALL CHANGEABLE MESSAGE SIGNS IN ADVANCE OF THE PORTABLE WORK ZONE AS SHOWN ON THE TCP SHEETS OR AS DIRECTED BY THE ENGINEER.

U) RETURN TRAFFIC TO ITS EXISTING LANE/TRAFFIC CONFIGURATION AT THE END OF EACH WORK PERIOD.

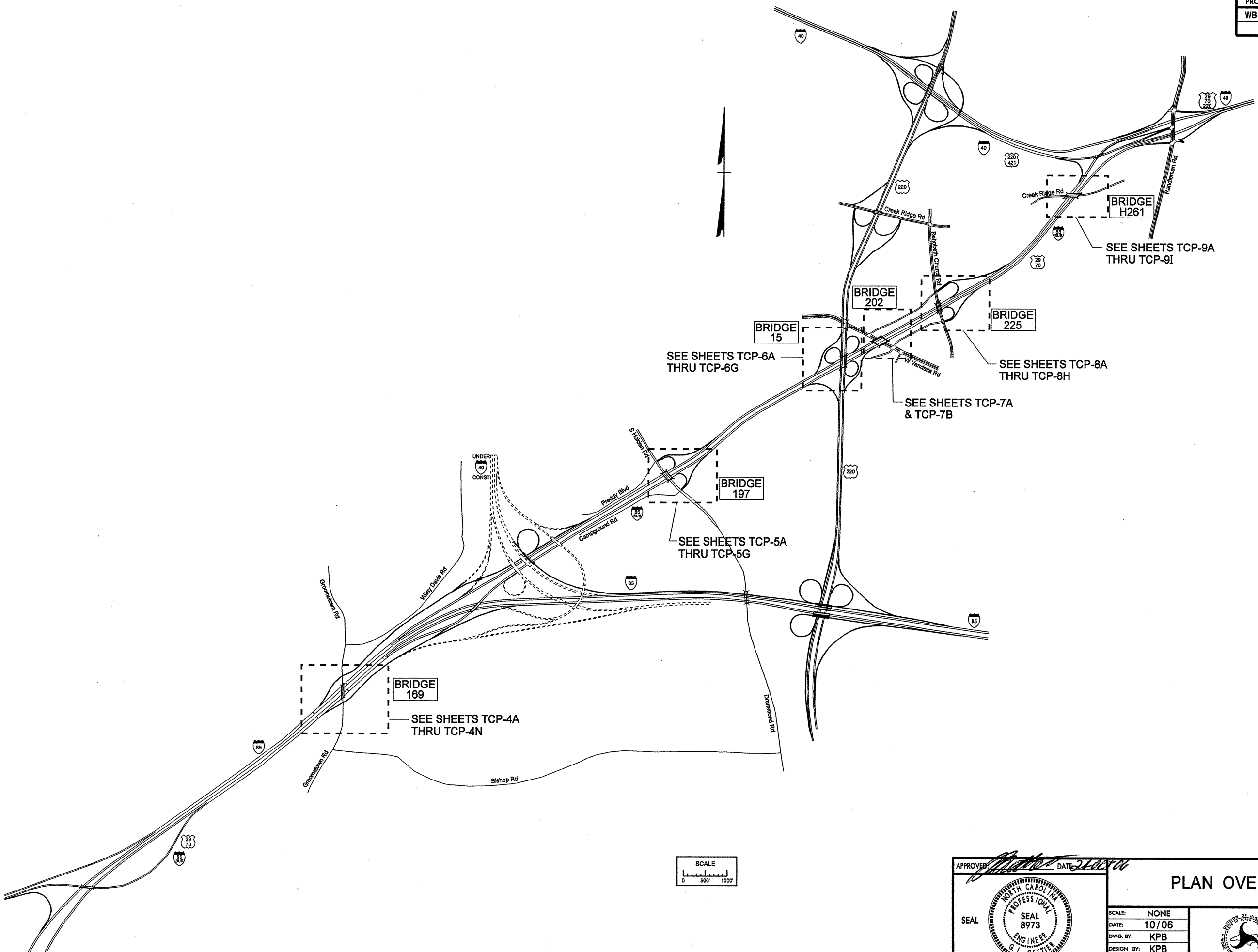
V) DO NOT PERFORM WORK FROM THE ROADWAY ON TOP OF THE STRUCTURE.

W) UPON COMPLETION OF THE WORK AT EACH BRIDGE LOCATION, REMOVE ALL TRAFFIC CONTROL DEVICES.

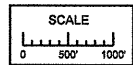
X) EXISTING SHOULDER MOUNTED ROADWAY SIGNS THAT CONFLICT WITH THE DETOUR SIGNING SHALL BE COVERED AS APPROVED AND DIRECTED BY THE ENGINEER AT NO EXPENSE TO THE DEPARTMENT.

27-DEC-2006 08:52 \\dot\dfs\cor\ol\sp\ro\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1r-top2.dgn kbroodwell AT WZTC224240

| APPROVED: DATE: 29 DEC 06 | <h2 style="margin: 0;">GENERAL NOTES</h2> | | | | |
|--|---|-----------|--|--|--|
| SCALE: NONE DATE: 12/06 DWG. BY: KPB DESIGN BY: KPB REVIEWED BY: JWG | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th style="text-align: left;">REVISIONS</th> </tr> <tr> <td style="height: 20px;"> </td> </tr> <tr> <td style="height: 20px;"> </td> </tr> <tr> <td style="height: 20px;"> </td> </tr> </table> | REVISIONS | | | |
| REVISIONS | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |



24-OCT-2006 13:46
 I:\nsproj\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp3.dgn
 kbp odswell AT W.1222.1210



| | | | |
|------------------|----------------|----------------------|-----------|
| APPROVED: | DATE: 2/10/06 | PLAN OVERVIEW | |
| | | | |
| SCALE: NONE | DATE: 10/06 | | REVISIONS |
| DWG. BY: KPB | DESIGN BY: KPB | | |
| REVIEWED BY: JWG | | | |

PHASING

PHASE 1

STEP 1: PERFORM PAINT REMOVAL AND PAINTING OPERATIONS AS SHOWN IN THE CONTRACT AND CONSTRUCTION PLANS IN ACCORDANCE WITH THE "NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES - JULY 2006" AND USING SHEETS TCP-4A THRU TCP-4N AS FOLLOWS:

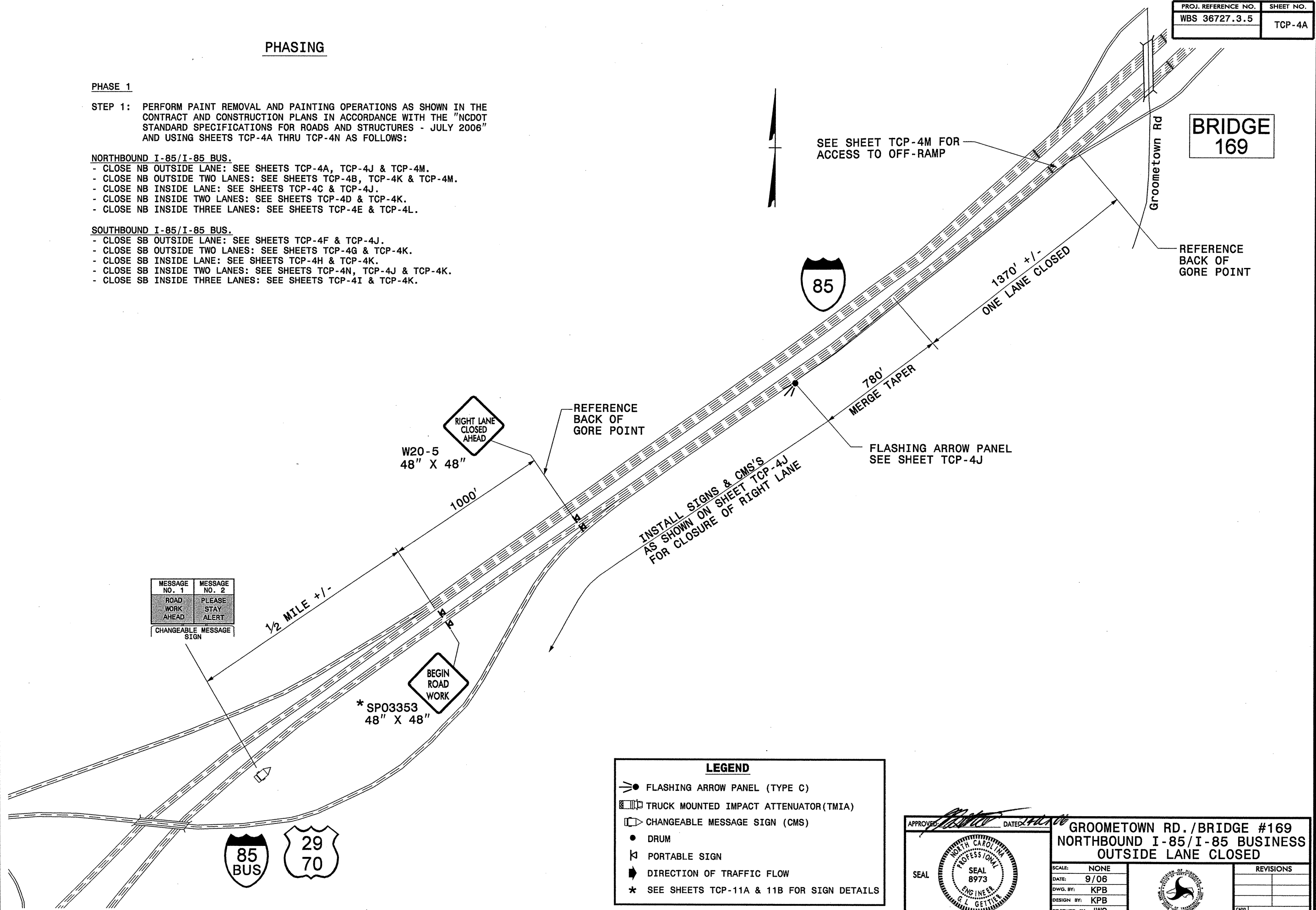
NORTHBOUND I-85/I-85 BUS.

- CLOSE NB OUTSIDE LANE: SEE SHEETS TCP-4A, TCP-4J & TCP-4M.
- CLOSE NB OUTSIDE TWO LANES: SEE SHEETS TCP-4B, TCP-4K & TCP-4M.
- CLOSE NB INSIDE LANE: SEE SHEETS TCP-4C & TCP-4J.
- CLOSE NB INSIDE TWO LANES: SEE SHEETS TCP-4D & TCP-4K.
- CLOSE NB INSIDE THREE LANES: SEE SHEETS TCP-4E & TCP-4L.

SOUTHBOUND I-85/I-85 BUS.

- CLOSE SB OUTSIDE LANE: SEE SHEETS TCP-4F & TCP-4J.
- CLOSE SB OUTSIDE TWO LANES: SEE SHEETS TCP-4G & TCP-4K.
- CLOSE SB INSIDE LANE: SEE SHEETS TCP-4H & TCP-4K.
- CLOSE SB INSIDE TWO LANES: SEE SHEETS TCP-4N, TCP-4J & TCP-4K.
- CLOSE SB INSIDE THREE LANES: SEE SHEETS TCP-4I & TCP-4K.

24-OCT-2006 13:46
 \\dot\dfsroot\01\anspro\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4a.dgn
 kbroodwell AT WTC2240



| LEGEND | |
|--------|---|
| | FLASHING ARROW PANEL (TYPE C) |
| | TRUCK MOUNTED IMPACT ATTENUATOR(TMIA) |
| | CHANGEABLE MESSAGE SIGN (CMS) |
| | DRUM |
| | PORTABLE SIGN |
| | DIRECTION OF TRAFFIC FLOW |
| | * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS |

| | | | | |
|-----------|------------------|--|--|-----------|
| APPROVED: | DATE: 9/06 | GROOMETOWN RD./BRIDGE #169 NORTHBOUND I-85/I-85 BUSINESS OUTSIDE LANE CLOSED | | |
| | SCALE: NONE | | | |
| | DWG. BY: KPB | | | REVISIONS |
| | DESIGN BY: KPB | | | |
| | REVIEWED BY: JWG | | | |

**BRIDGE
169**

SEE SHEET TCP-4M FOR
ACCESS TO OFF-RAMP

REFERENCE
BACK OF
GORE POINT



1760' +/-
TWO LANES CLOSED

780'
MERGE TAPER

2ND FLASHING ARROW PANEL
SEE SHEET TCP-4K



W20-5A
48" X 48"

REFERENCE
BACK OF
GORE POINT

1000'

1560'
ONE LANE CLOSED

1/2 MILE +/-

780'
MERGE TAPER

1ST FLASHING ARROW PANEL
SEE SHEET TCP-4K



* SP03353
48" X 48"

| | |
|-----------------|-------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | PLEASE STAY ALERT |

CHANGEABLE MESSAGE SIGN



INSTALL SIGNS & CMS'S AS
SHOWN ON SHEET TCP-4K
FOR CLOSURE OF RIGHT
TWO LANES

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

| | | | |
|------------------|----------------|--|--|
| APPROVED: | DATE: 10/06 | GROOMETOWN RD./BRIDGE #169 NORTHBOUND I-85/I-85 BUSINESS OUTSIDE TWO LANES CLOSED | |
| | SCALE: NONE | | |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| REVIEWED BY: JWG | | REVISIONS | |

24-OCT-2006 13:47
 \\dot\dfsroot\0\nsproj\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4b.dgn
 kbroodwell AT WZTC224240



**BRIDGE
169**

Groomtown Rd



FLASHING ARROW PANEL
SEE SHEET TCP-4J

780'
MERGE TAPER

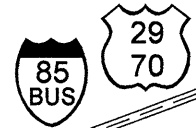
LANE CLOSED

400'

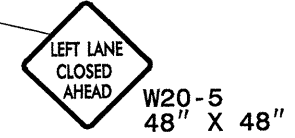
REFERENCE
BACK OF
GORE POINT

INSTALL SIGNS AND CMS'S
AS SHOWN ON SHEET TCP-4J
FOR CLOSURE OF LEFT LANE

| | |
|-------------------------|-------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | PLEASE STAY ALERT |
| CHANGEABLE MESSAGE SIGN | |



REFERENCE
BACK OF
GORE POINT



1000' +/-



LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

1/2 MILE +/-

| | | | |
|-----------|------------------|---|-----------|
| APPROVED: | DATE: 2/10/07 | GROOMETOWNE RD./BRIDGE #169 NORTHBOUND I-85/I-85 BUSINESS INSIDE LANE CLOSED | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 13:47
 \\dot.dfsr0010\inspr01\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4c.dgn
 kbrodwell AT WZTC224240

BRIDGE 169

REFERENCE BACK OF GORE POINT

Groomtown Rd

2ND FLASHING ARROW PANEL
SEE SHEET TCP-4K

1560'
ONE LANE CLOSED

780'
MERGE TAPER

TWO LANES CLOSED

400'

780'
MERGE TAPER

1ST FLASHING ARROW PANEL
SEE SHEET TCP-4K

INSTALL SIGNS AND CMS'S
AS SHOWN ON SHEET TCP-4K
FOR CLOSURE OF LEFT TWO LANES

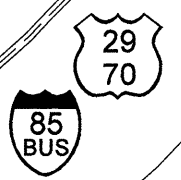
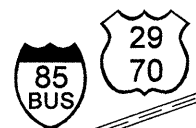
REFERENCE BACK OF GORE POINT

LEFT TWO LANES CLOSED AHEAD
W20-5A
48" X 48"

1000' +/-

BEGIN ROAD WORK
SP03353*
48" X 48"

| | |
|-------------------------|-------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | PLEASE STAY ALERT |
| CHANGEABLE MESSAGE SIGN | |



LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

| | | | |
|-----------|------------------|--|-----------|
| APPROVED: | DATE: 10/06 | GROOMTOWN RD./BRIDGE #169 NORTHBOUND I-85/I-85 BUSINESS INSIDE TWO LANES CLOSED | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 13:47
 \\dot.dfsroot\01\inspr\01\special\36727.3\1\Traffic\36727.3.5\greensboro\bp-40-2006-l-tcp4d.dgn
 kbrodwell AT WZTC224240

1/2 MILE +/-

**BRIDGE
169**

REFERENCE
BACK OF
GORE POINT

Groometown Rd



1560'
TWO LANES CLOSED

780'
MERGE TAPER

780'
MERGE TAPER

400'

3RD FLASHING ARROW PANEL
SEE SHEET TCP-4L

2ND FLASHING ARROW PANEL
SEE SHEET TCP-4L

780'
MERGE TAPER

1560'
ONE LANE CLOSED

780'
MERGE TAPER

REFERENCE
BACK OF
GORE POINT

LEFT
THREE LANES
CLOSED
AHEAD
SP*
48" X 48"

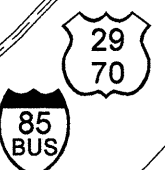
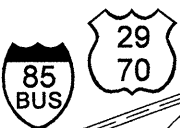
BEGIN
ROAD
WORK
SP03353*
48" X 48"

| | |
|-------------------------|---------------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| STAY IN YOUR LANE | LEFT 3 LANES CLOSED |
| CHANGEABLE MESSAGE SIGN | |

1ST FLASHING ARROW PANEL
SEE SHEET TCP-4L

INSTALL SIGNS AND CMS'S
AS SHOWN ON SHEET TCP-4L
FOR CLOSURE OF LEFT
THREE LANES

| | |
|-------------------------|-------------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | PLEASE STAY ALERT |
| CHANGEABLE MESSAGE SIGN | |



LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ▣ TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- ◻ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- ⚡ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

APPROVED: *[Signature]* DATE: 10/06

SEAL

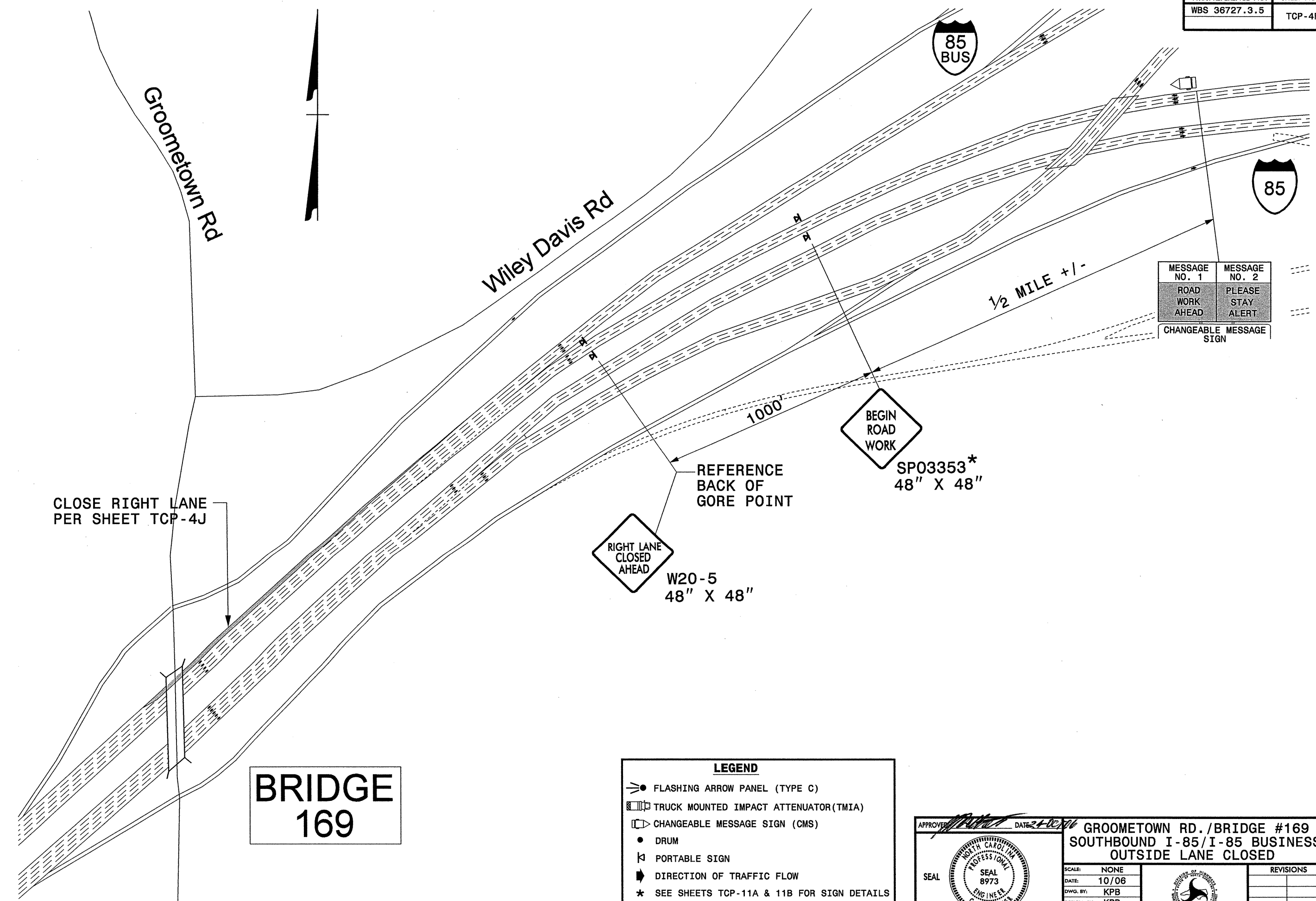
**GROOMETOWN RD./BRIDGE #169
NORTHBOUND I-85/I-85 BUSINESS
INSIDE THREE LANES CLOSED**

| | |
|--------------|-------|
| SCALE: | NONE |
| DATE: | 10/06 |
| DWG. BY: | KPB |
| DESIGN BY: | KPB |
| REVIEWED BY: | JWG |

| REVISIONS |
|-----------|
| |
| |
| |

24-OCT-2006 13:47
 \\dot.dfrroot\01\ansproj\special\36727.3.1\tr\at\fic\36727.3.5\greensboro.bp-40-2006-1-tcp4e.dgn
 kbroadwell AT WZTC24240

1/2 MILE +/-



| | |
|----------------------------------|------------------------------------|
| MESSAGE NO. 1 ROAD WORK AHEAD | MESSAGE NO. 2 PLEASE STAY ALERT |
| CHANGEABLE MESSAGE SIGN | |

CLOSE RIGHT LANE PER SHEET TCP-4J

**BRIDGE
169**

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

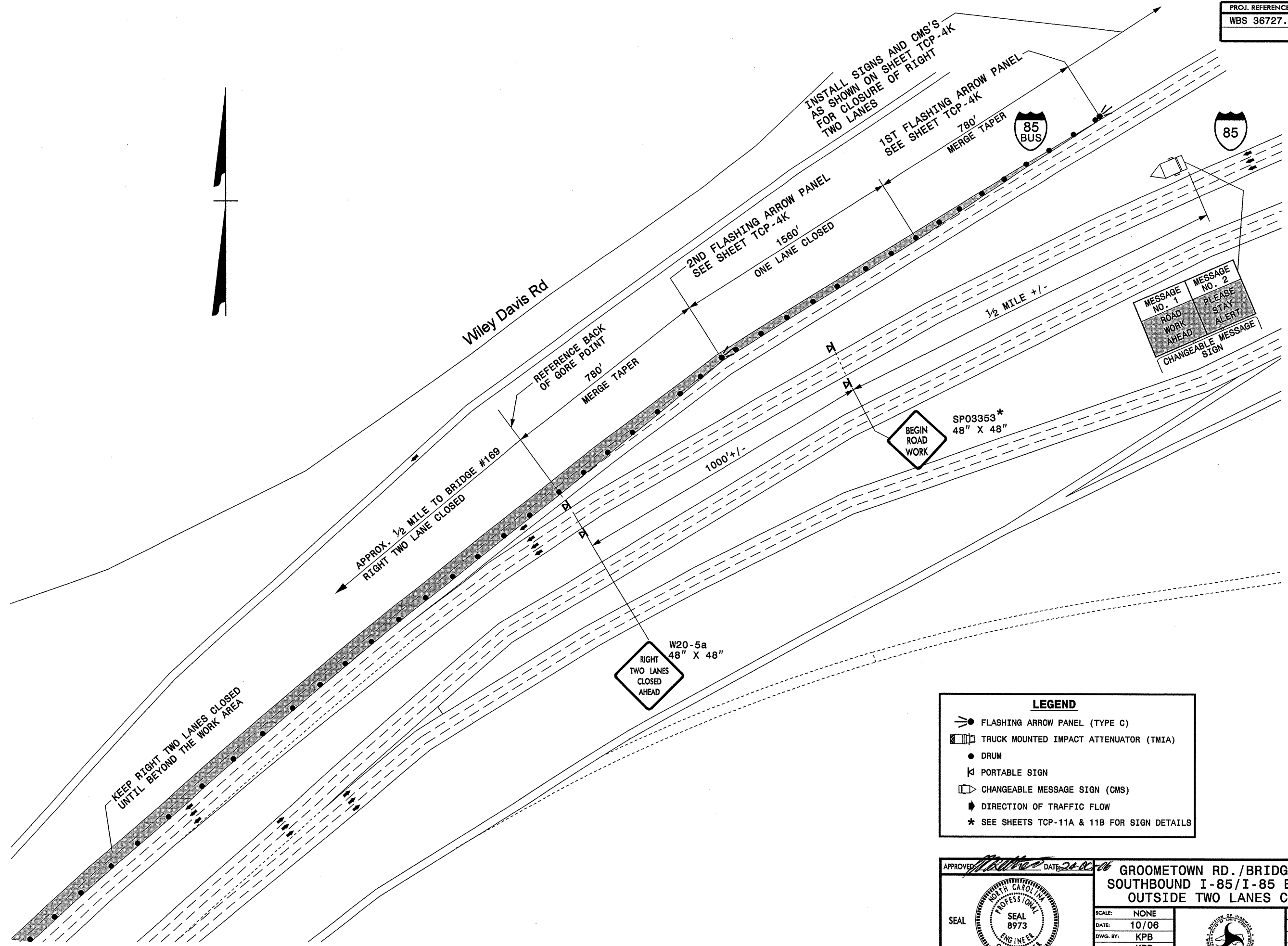
APPROVED: DATE: 10/06/06

SEAL

**GROOMETOWN RD./BRIDGE #169
SOUTHBOUND I-85/I-85 BUSINESS
OUTSIDE LANE CLOSED**

| | | |
|------------------|--|-----------|
| SCALE: NONE | | REVISIONS |
| DATE: 10/06 | | |
| DWG. BY: KPB | | |
| DESIGN BY: KPB | | |
| REVIEWED BY: JWG | | |

24-OCT-2006 13:47
 \\dot\dfsroot\0\insproj\special\36727.3\1\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4f.dgn
 kbrodwell AT WZTC224240



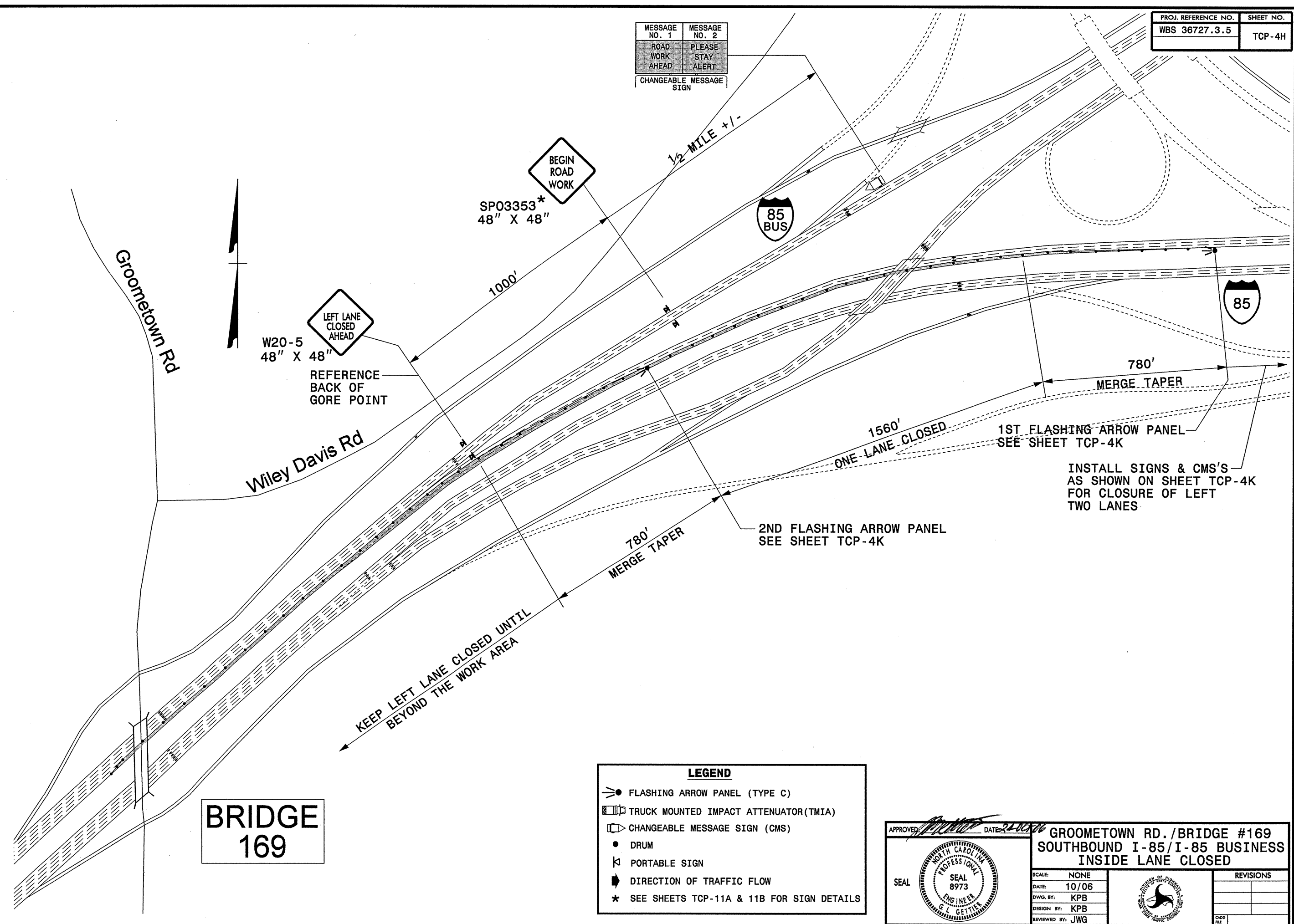
LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

| | | | |
|-----------|------------------|---|-----------|
| APPROVED: | DATE: 2-10-06 | GROOMTOWN RD./BRIDGE #169 SOUTHBOUND I-85/I-85 BUSINESS OUTSIDE TWO LANES CLOSED | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 13:48
 \\dot\dfs\00101\nspr\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-i-tcp4g.dgn
 kbr\oadwell AT WZTC24240

| | |
|-------------------------|-------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | PLEASE STAY ALERT |
| CHANGEABLE MESSAGE SIGN | |



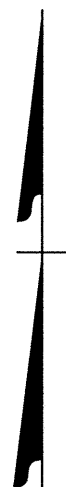
**BRIDGE
169**

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

| | | | |
|-----------|------------------|--|-----------|
| APPROVED: | DATE: 2-2-06 | GROOMETOWN RD./BRIDGE #169 SOUTHBOUND I-85/I-85 BUSINESS INSIDE LANE CLOSED | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 13:48
 C:\p01\10181\001\10181.dwg
 48" x 48" AT WZTC224240



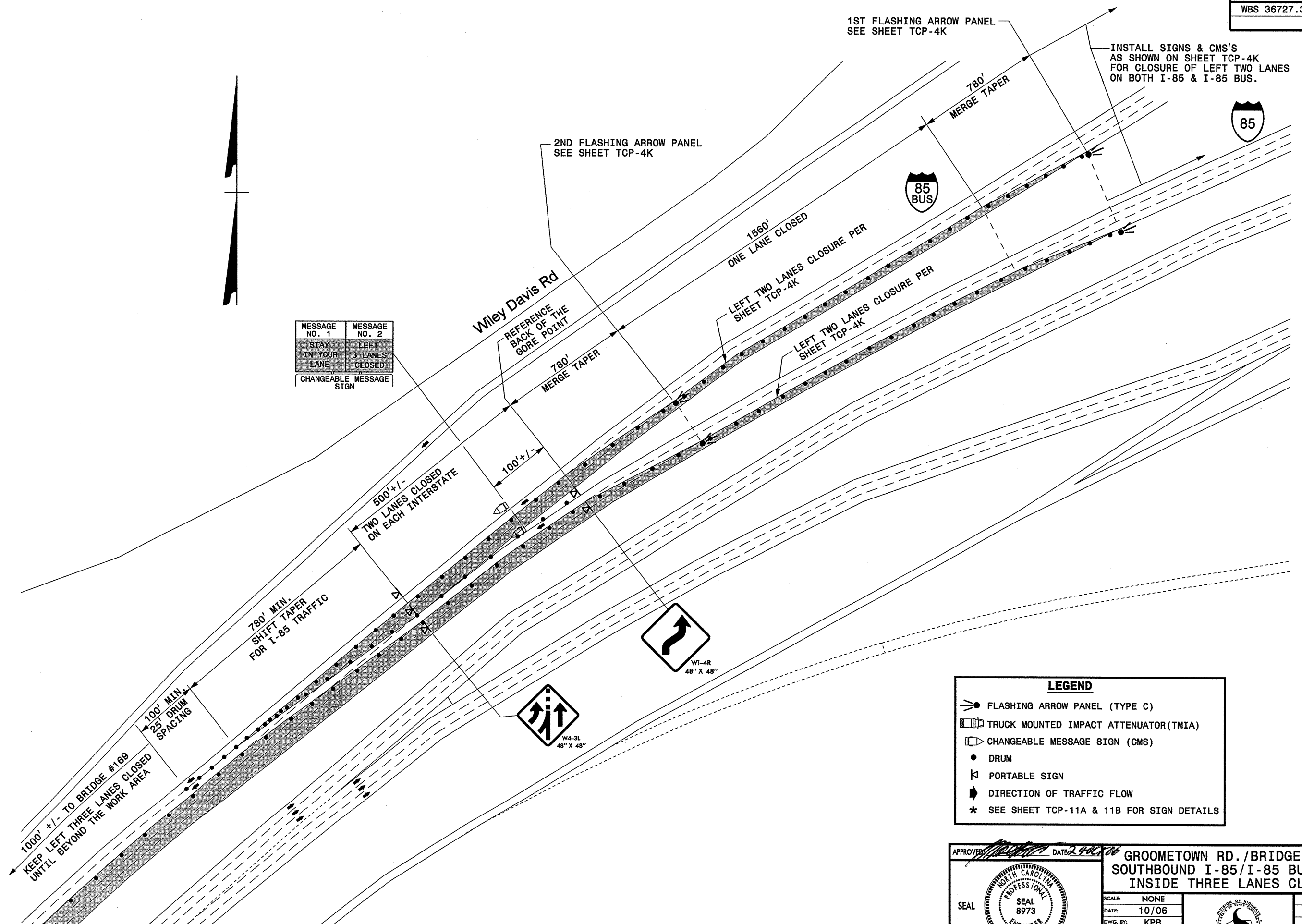
1ST FLASHING ARROW PANEL
SEE SHEET TCP-4K

INSTALL SIGNS & CMS'S
AS SHOWN ON SHEET TCP-4K
FOR CLOSURE OF LEFT TWO LANES
ON BOTH I-85 & I-85 BUS.

2ND FLASHING ARROW PANEL
SEE SHEET TCP-4K

Wiley Davis Rd
REFERENCE
BACK OF THE
GORE POINT

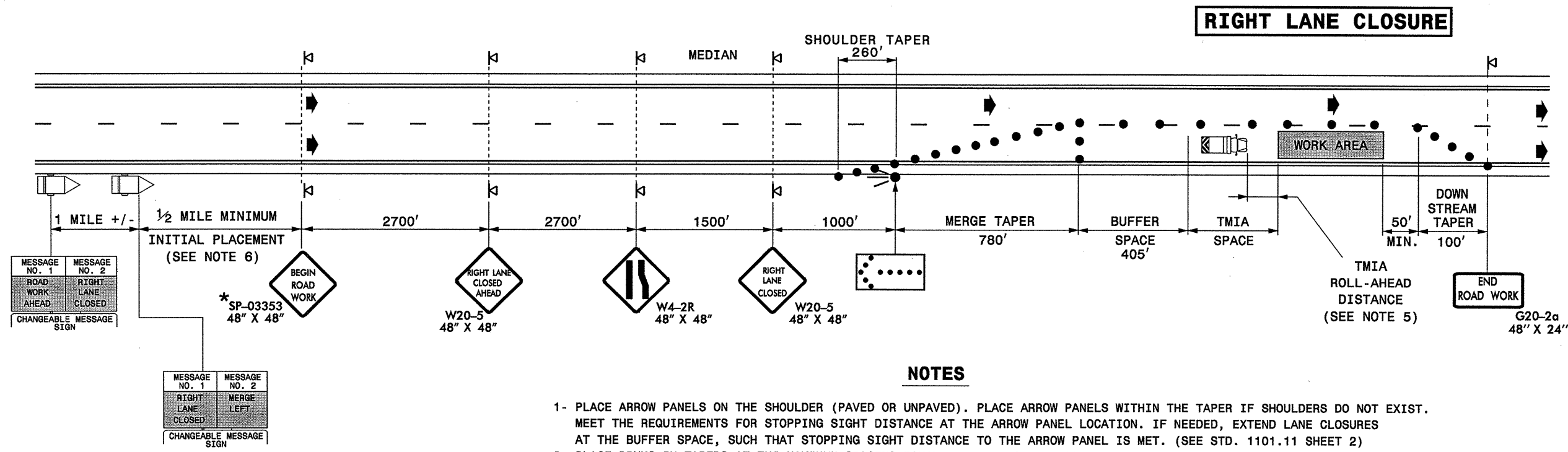
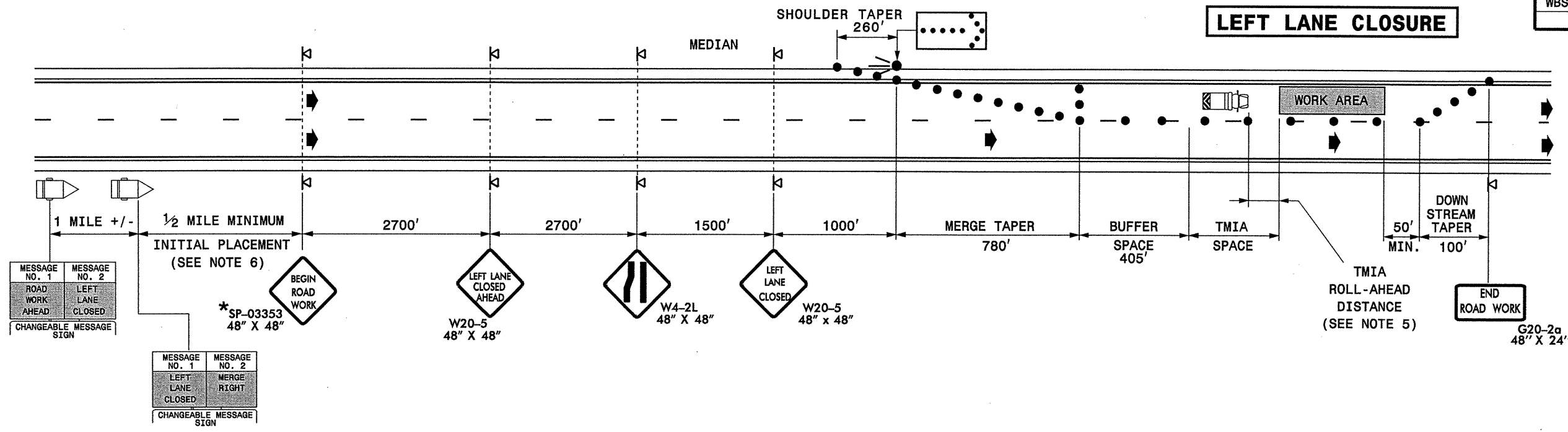
| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| STAY IN YOUR LANE | LEFT 3 LANES CLOSED |
| CHANGEABLE MESSAGE SIGN | |



| LEGEND | |
|--------|--|
| | FLASHING ARROW PANEL (TYPE C) |
| | TRUCK MOUNTED IMPACT ATTENUATOR (TMIA) |
| | CHANGEABLE MESSAGE SIGN (CMS) |
| | DRUM |
| | PORTABLE SIGN |
| | DIRECTION OF TRAFFIC FLOW |
| | * SEE SHEET TCP-11A & 11B FOR SIGN DETAILS |

| APPROVED: | DATE: 10/06 | GROOMTOWN RD./BRIDGE #169 SOUTHBOUND I-85/I-85 BUSINESS INSIDE THREE LANES CLOSED | | | | | | |
|-----------|--|---|-----------|--|--|--|--|--|
| | SCALE: NONE DATE: 10/06 DWG. BY: KPB DESIGN BY: KPB REVIEWED BY: JWG | | | | | | | |
| | | <table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | REVISIONS | | | | | |
| REVISIONS | | | | | | | | |
| | | | | | | | | |
| | | | | | | | | |

24-OCT-2006 13:48
 \\dot\dfsroot\0\inspro\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-tcp4i.dgn
 kbrodwell AT WZTC224240



NOTES

- 1- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 2- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- SEE SHEET TCP-4M FOR TREATMENT OF LANE CLOSURES AT EXIT RAMP.
- 4- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 5- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 6- PLACE CHANGEABLE MESSAGE SIGNS (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. IF TRAFFIC BACKS UP TO WHERE THE CMS IS LOCATED THEN RELOCATE THE CMS 1/2 MILE FROM THE ANTICIPATED BACK UP.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ☐ TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- ☐ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- ⚡ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

APPROVED: *[Signature]* DATE: 2/20/06

**GROOMETOWN RD./BRIDGE #169
TEMPORARY LANE CLOSURE
ONE LANE CLOSED**

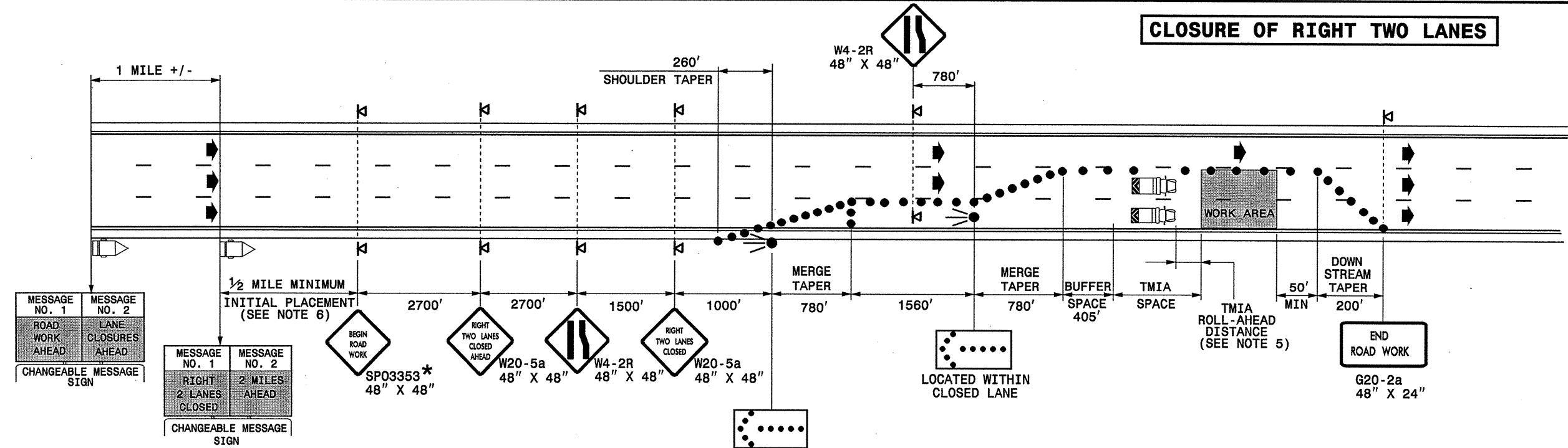
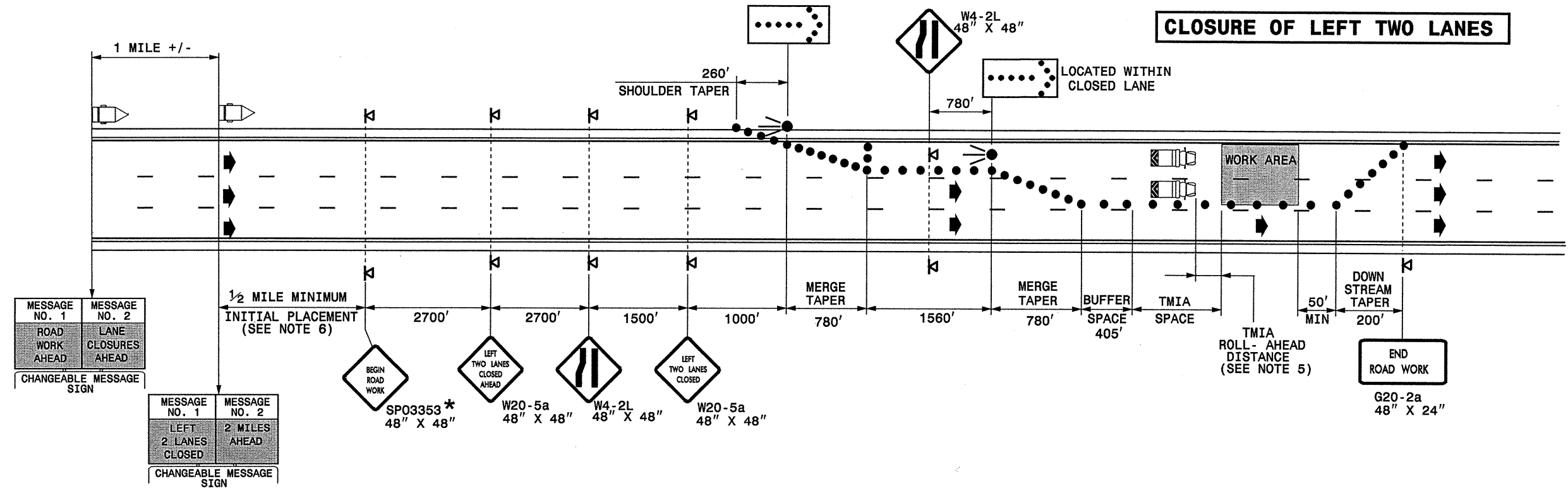
SCALE: NONE
DATE: 10/06
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

SEAL: *[Professional Seal]*
SEAL 8973
ENGINEER
G. L. GETTIE

REVISIONS

| | |
|--|--|
| | |
| | |

24-OCT-2006 13:48 \\dot\dfsroot\01\p\proj\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4j.dgn kbradwell AT W2TC224240



GENERAL NOTES

1- REFER TO NOTES ON SHEET TCP-4J.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

APPROVED: DATE: 10/06/06

**GROOMTOWN RD./BRIDGE # 169
TEMPORARY LANE CLOSURES
TWO LANES CLOSED**

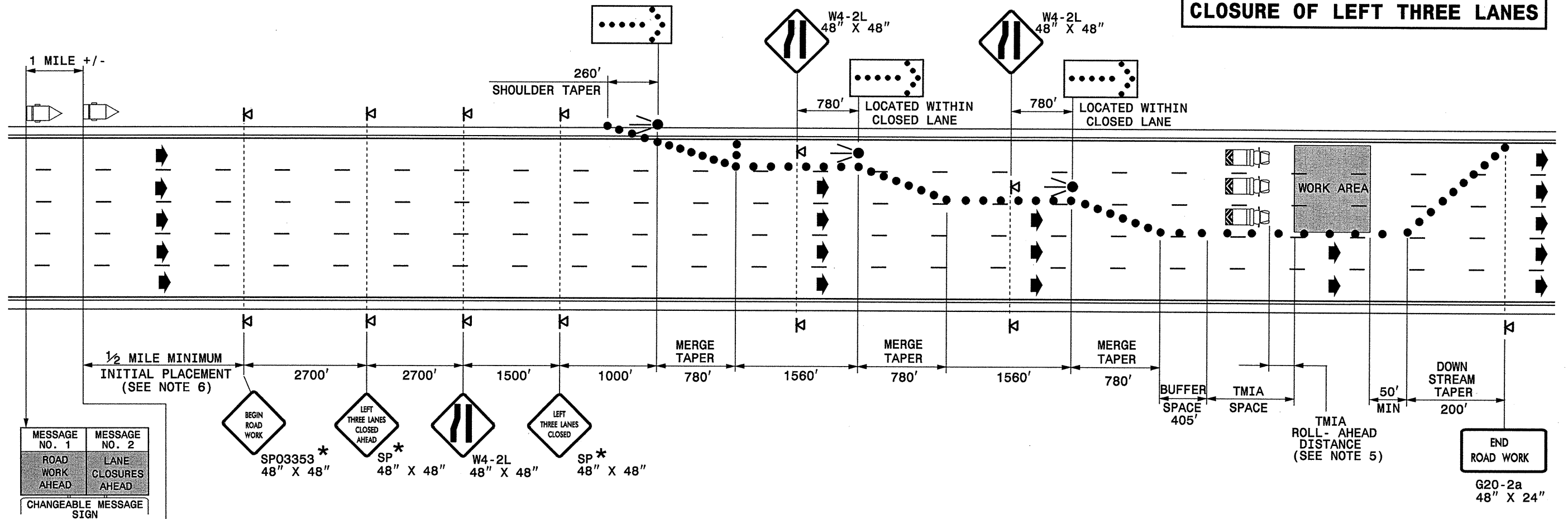
SCALE: NONE
DATE: 10/06
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

SEAL:

| REVISIONS |
|-----------|
| |
| |
| |

24-OCT-2006 13:49 \\dot\dfsroot\0\inspro\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4k.dgn kbroadwell AT WZTC224240

CLOSURE OF LEFT THREE LANES



| | |
|----------------------------------|--------------------------------------|
| MESSAGE NO. 1 ROAD WORK AHEAD | MESSAGE NO. 2 LANE CLOSURES AHEAD |
|----------------------------------|--------------------------------------|

CHANGEABLE MESSAGE SIGN

| | |
|--------------------------------------|--------------------------------|
| MESSAGE NO. 1 LEFT 3 LANES CLOSED | MESSAGE NO. 2 2 MILES AHEAD |
|--------------------------------------|--------------------------------|

CHANGEABLE MESSAGE SIGN

GENERAL NOTES

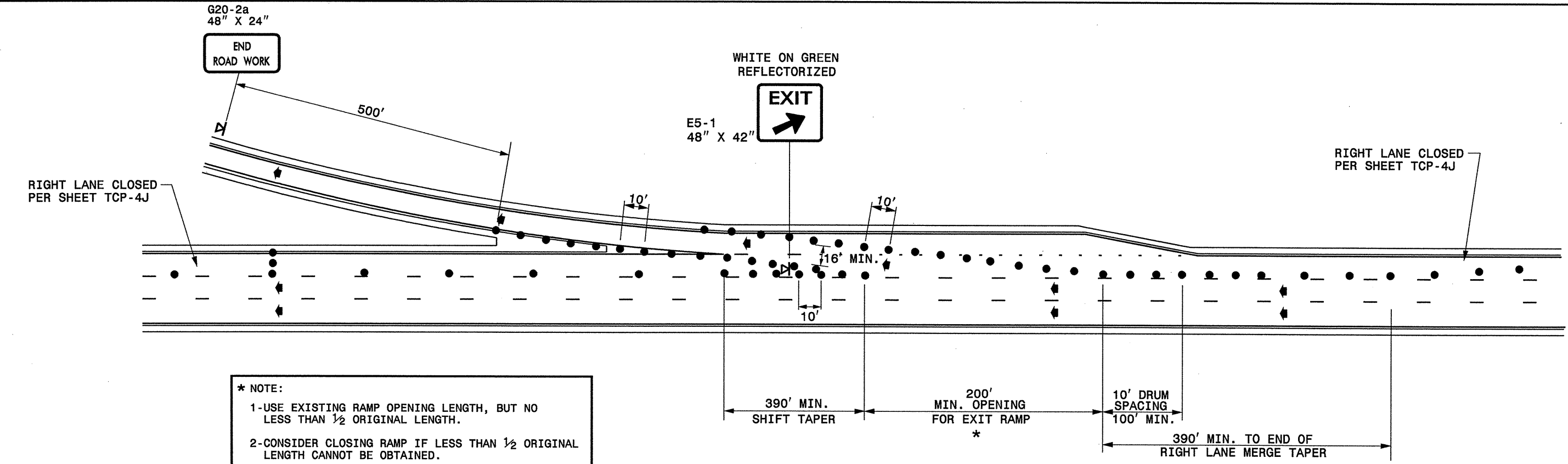
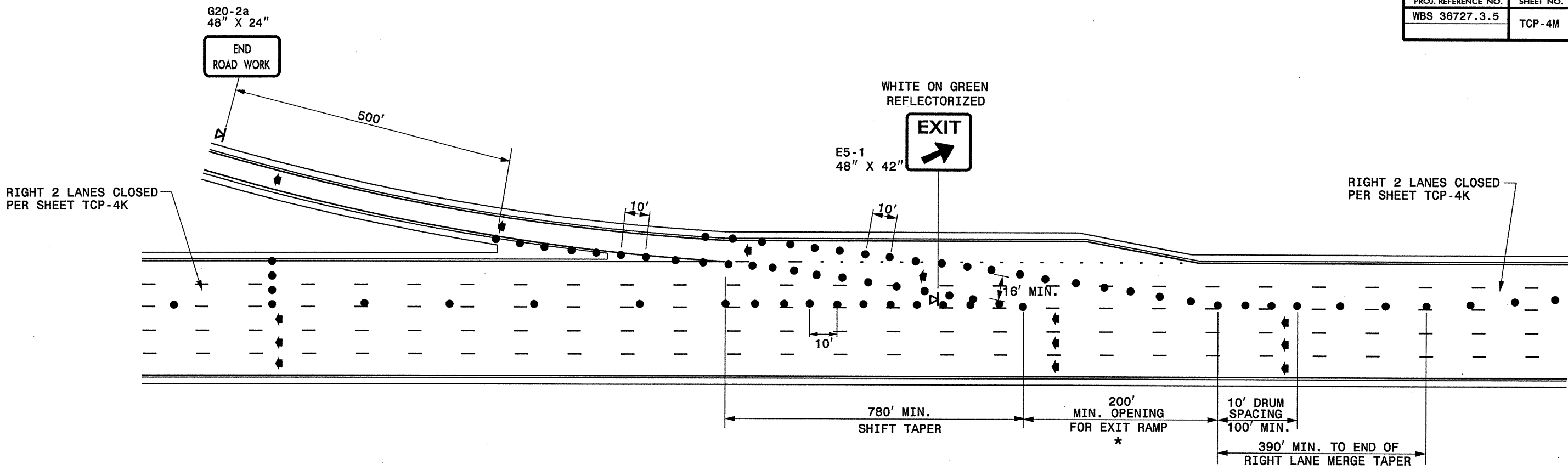
1-REFER TO NOTES ON SHEET TCP-4J.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

| | | | | |
|--|---------------|--|--|--|
| APPROVED: SEAL | DATE: 2-10-06 | GROOMTOWN RD./BRIDGE #169 TEMPORARY LANE CLOSURE INSIDE THREE LANES CLOSED | | |
| SCALE: NONE DATE: 10/06 DWG. BY: KPB DESIGN BY: KPB REVIEWED BY: JWJ | | REVISIONS <table border="1" style="width: 100%; height: 40px;"> <tr> <td> </td> <td> </td> </tr> </table> | | |
| | | | | |

24-OCT-2006 13:49
 \\dot\dfsroot\inspro\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-tcp-4l.dgn
 kbroodwell AT WZTC224240



*** NOTE:**
 1-USE EXISTING RAMP OPENING LENGTH, BUT NO LESS THAN 1/2 ORIGINAL LENGTH.
 2-CONSIDER CLOSING RAMP IF LESS THAN 1/2 ORIGINAL LENGTH CANNOT BE OBTAINED.

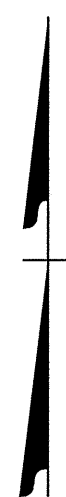
LEGEND

- DRUM
- ⊠ PORTABLE SIGN
- ◄ DIRECTION OF TRAFFIC FLOW

GENERAL NOTES
 1-MOUNT EXIT SIGNS A MINIMUM OF 7 FEET ABOVE THE PAVEMENT ELEVATION.

| APPROVED: | DATE: 10/06/06 | GROOMETOWN RD./BRIDGE #169 TEMPORARY LANE CLOSURES THRU EXIT RAMPS | | | | | | | |
|------------------|----------------|---|--|-----------|--|--|--|--|--|
| | SCALE: NONE | | | | | | | | |
| | DATE: 10/06 | | | | | | | | |
| | DWG. BY: KPB | | | | | | | | |
| | DESIGN BY: KPB | | | | | | | | |
| REVIEWED BY: JWG | | <table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | | REVISIONS | | | | | |
| REVISIONS | | | | | | | | | |
| | | | | | | | | | |
| | | | | | | | | | |

24-OCT-2006 13:49
 \\dot\dfsroot\01\inspro\special\36727.3.5\greensboro\bp-40-2006-1-top4m.dgn
 kbrodwell AT WZIC224240



INSTALL SIGNS & CMS'S
AS SHOWN ON SHEET TCP-4J
FOR CLOSURE OF LEFT
LANE ON I-85 BUS.

FLASHING ARROW PANEL
SEE SHEET TCP-4J

Wiley Davis Rd
REFERENCE
BACK OF THE
GORE POINT

| | |
|----------------------------|---------------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| STAY IN YOUR LANE | LEFT 2 LANES CLOSED |
| CHANGEABLE MESSAGE SIGN | |



INSTALL SIGNS & CMS'S
AS SHOWN ON SHEET TCP-4K
FOR CLOSURE OF LEFT TWO LANES
ON I-85

1ST FLASHING ARROW PANEL
SEE SHEET TCP-4K

2ND FLASHING ARROW PANEL
SEE SHEET TCP-4K

LEFT TWO LANES CLOSURE PER
SHEET TCP-4K

1560'
ONE LANE CLOSED

780'
MERGE TAPER

100'+/-

780'
MERGE TAPER

500'+/-
TWO LANES CLOSED
ON I-85 & ONE LANE
CLOSED ON I-85 BUS.

780' MIN.
SHIFT TAPER
FOR I-85 TRAFFIC

200' MIN.
25' DRUM
SPACING

900' +/- TO BRIDGE #169
KEEP LEFT TWO LANES CLOSED
UNTIL BEYOND THE WORK AREA



LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEET TCP-11A & 11B FOR SIGN DETAILS

APPROVED: DATE: 10/06

**GROOMETOWN RD./BRIDGE #169
SOUTHBOUND I-85/I-85 BUSINESS
INSIDE TWO LANES CLOSED**

| | | |
|------------------|--|-----------|
| SCALE: NONE | | REVISIONS |
| DATE: 10/06 | | |
| DWG. BY: KPB | | |
| DESIGN BY: KPB | | |
| REVIEWED BY: JWG | | |

SEAL:

24-OCT-2006 13:49
 \\dot\dfsroot\inspro\special\36727.3\N traffic\36727.3.5\greensboro\bp-40-2006-1-tcp4n.dgn
 Kbroodwell AT WZTC224240

PHASING

PHASE 1

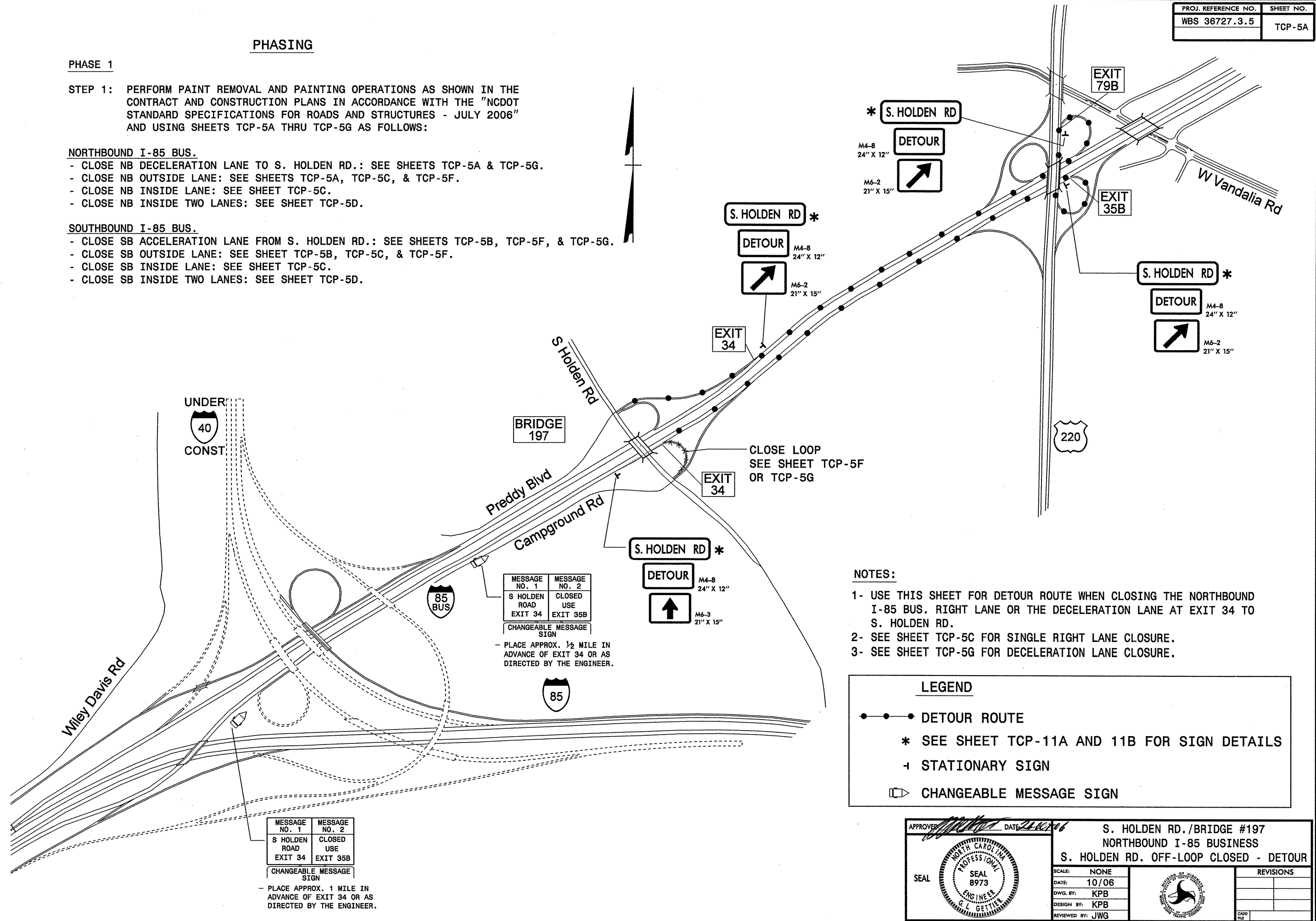
STEP 1: PERFORM PAINT REMOVAL AND PAINTING OPERATIONS AS SHOWN IN THE CONTRACT AND CONSTRUCTION PLANS IN ACCORDANCE WITH THE "NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES - JULY 2006" AND USING SHEETS TCP-5A THRU TCP-5G AS FOLLOWS:

NORTHBOUND I-85 BUS.

- CLOSE NB DECELERATION LANE TO S. HOLDEN RD.: SEE SHEETS TCP-5A & TCP-5G.
- CLOSE NB OUTSIDE LANE: SEE SHEETS TCP-5A, TCP-5C, & TCP-5F.
- CLOSE NB INSIDE LANE: SEE SHEET TCP-5C.
- CLOSE NB INSIDE TWO LANES: SEE SHEET TCP-5D.

SOUTHBOUND I-85 BUS.

- CLOSE SB ACCELERATION LANE FROM S. HOLDEN RD.: SEE SHEETS TCP-5B, TCP-5F, & TCP-5G.
- CLOSE SB OUTSIDE LANE: SEE SHEET TCP-5B, TCP-5C, & TCP-5F.
- CLOSE SB INSIDE LANE: SEE SHEET TCP-5C.
- CLOSE SB INSIDE TWO LANES: SEE SHEET TCP-5D.



UNDER
40
CONST

BRIDGE
197

CLOSE LOOP
SEE SHEET TCP-5F
OR TCP-5G

| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| S HOLDEN ROAD EXIT 34 | CLOSED USE EXIT 35B |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1/2 MILE IN ADVANCE OF EXIT 34 OR AS DIRECTED BY THE ENGINEER.

NOTES:

- 1- USE THIS SHEET FOR DETOUR ROUTE WHEN CLOSING THE NORTHBOUND I-85 BUS. RIGHT LANE OR THE DECELERATION LANE AT EXIT 34 TO S. HOLDEN RD.
- 2- SEE SHEET TCP-5C FOR SINGLE RIGHT LANE CLOSURE.
- 3- SEE SHEET TCP-5G FOR DECELERATION LANE CLOSURE.

LEGEND

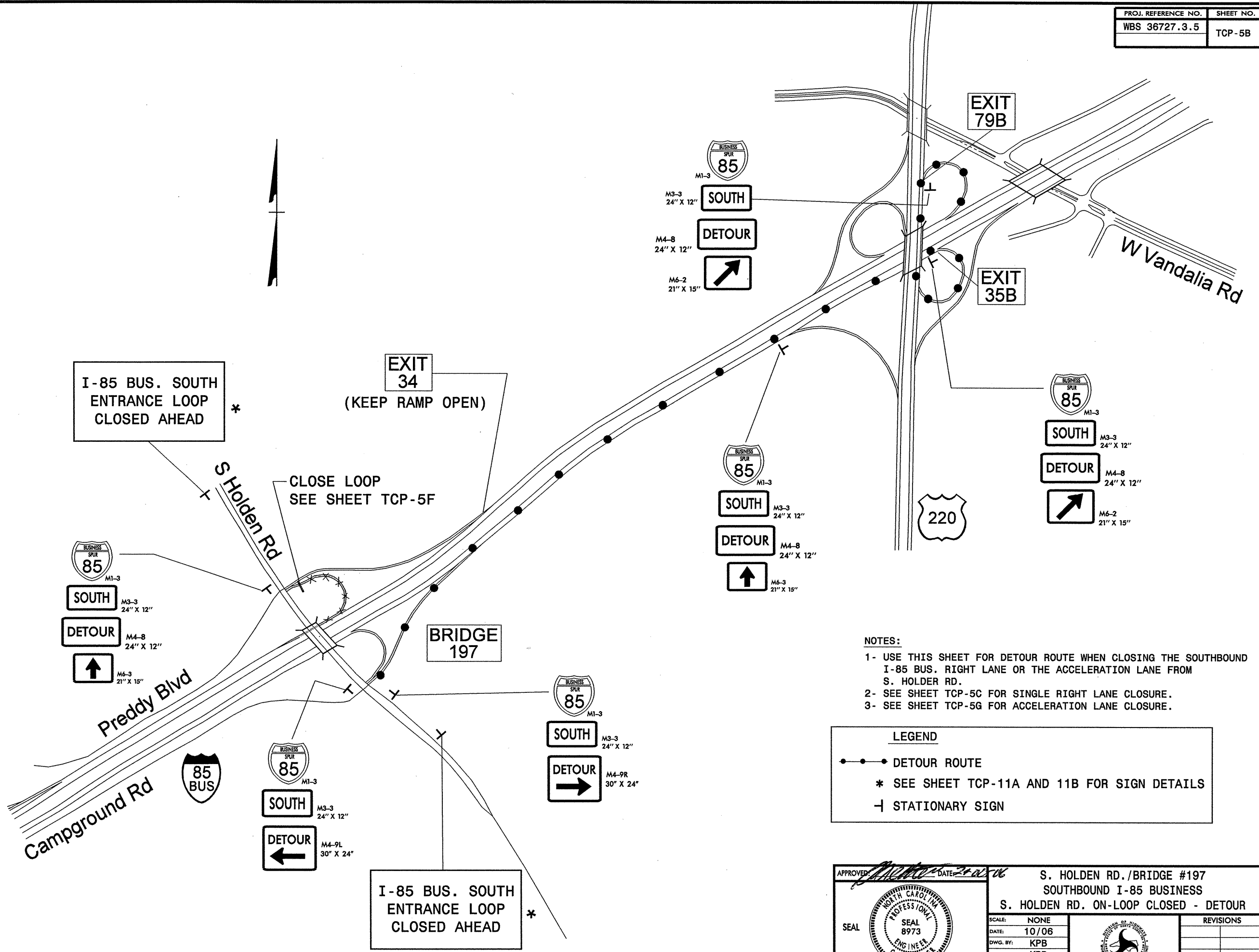
- DETOUR ROUTE
- * SEE SHEET TCP-11A AND 11B FOR SIGN DETAILS
- ↑ STATIONARY SIGN
- ▷ CHANGEABLE MESSAGE SIGN

| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| S HOLDEN ROAD EXIT 34 | CLOSED USE EXIT 35B |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1 MILE IN ADVANCE OF EXIT 34 OR AS DIRECTED BY THE ENGINEER.

| | | | |
|-----------|------------------|---|-----------|
| APPROVED: | DATE: 10/06 | S. HOLDEN RD./BRIDGE #197 NORTHBOUND I-85 BUSINESS S. HOLDEN RD. OFF-LOOP CLOSED - DETOUR | |
| SEAL | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 14:10 \\dot\dfsroot\dot\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-l-tcp5a.dgn kbrodwell AT W21C224240



I-85 BUS. SOUTH ENTRANCE LOOP CLOSED AHEAD *

EXIT 34 (KEEP RAMP OPEN)

CLOSE LOOP SEE SHEET TCP-5F

BRIDGE 197

I-85 BUS. SOUTH ENTRANCE LOOP CLOSED AHEAD *

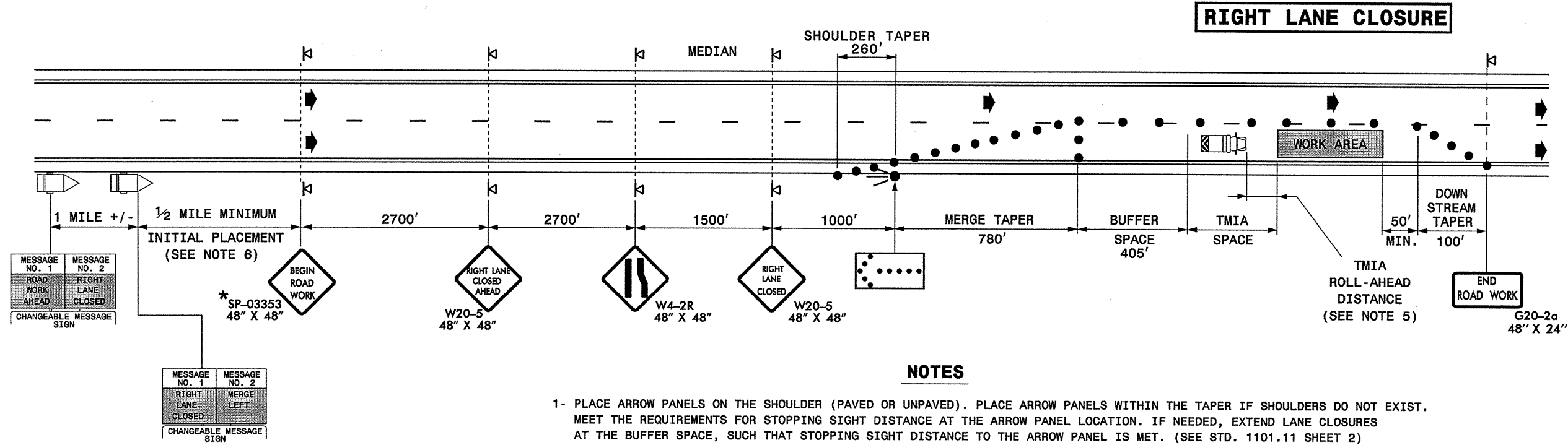
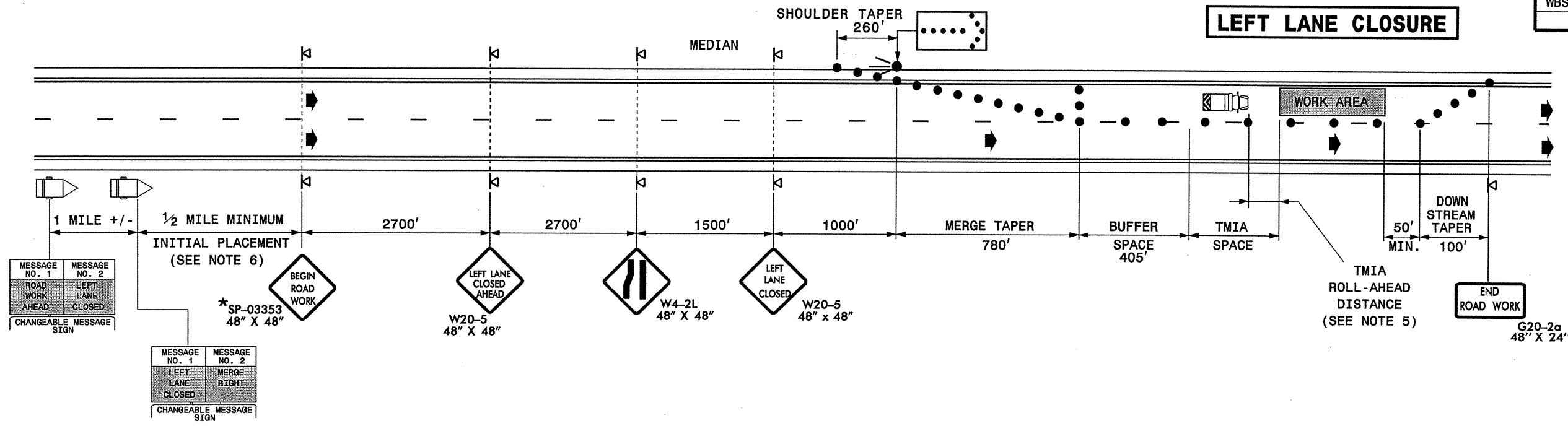
- NOTES:**
- 1- USE THIS SHEET FOR DETOUR ROUTE WHEN CLOSING THE SOUTHBOUND I-85 BUS. RIGHT LANE OR THE ACCELERATION LANE FROM S. HOLDER RD.
 - 2- SEE SHEET TCP-5C FOR SINGLE RIGHT LANE CLOSURE.
 - 3- SEE SHEET TCP-5G FOR ACCELERATION LANE CLOSURE.

LEGEND

- DETOUR ROUTE
- * SEE SHEET TCP-11A AND 11B FOR SIGN DETAILS
- ⊥ STATIONARY SIGN

| | | |
|--|--|--|
| APPROVED: <i>[Signature]</i> DATE: 10/06 | S. HOLDEN RD./BRIDGE #197 SOUTHBOUND I-85 BUSINESS S. HOLDEN RD. ON-LOOP CLOSED - DETOUR | |
| | SCALE: NONE | |
| | DATE: 10/06 | |
| | DWG. BY: KPB | |
| | DESIGN BY: KPB | |
| REVIEWED BY: JWG | REVISIONS | |

24-OCT-2006 14:10 \\dot\dfsroot\on\sp\proj\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp5b.dgn
 kbroadwell AT WZTC224240



NOTES

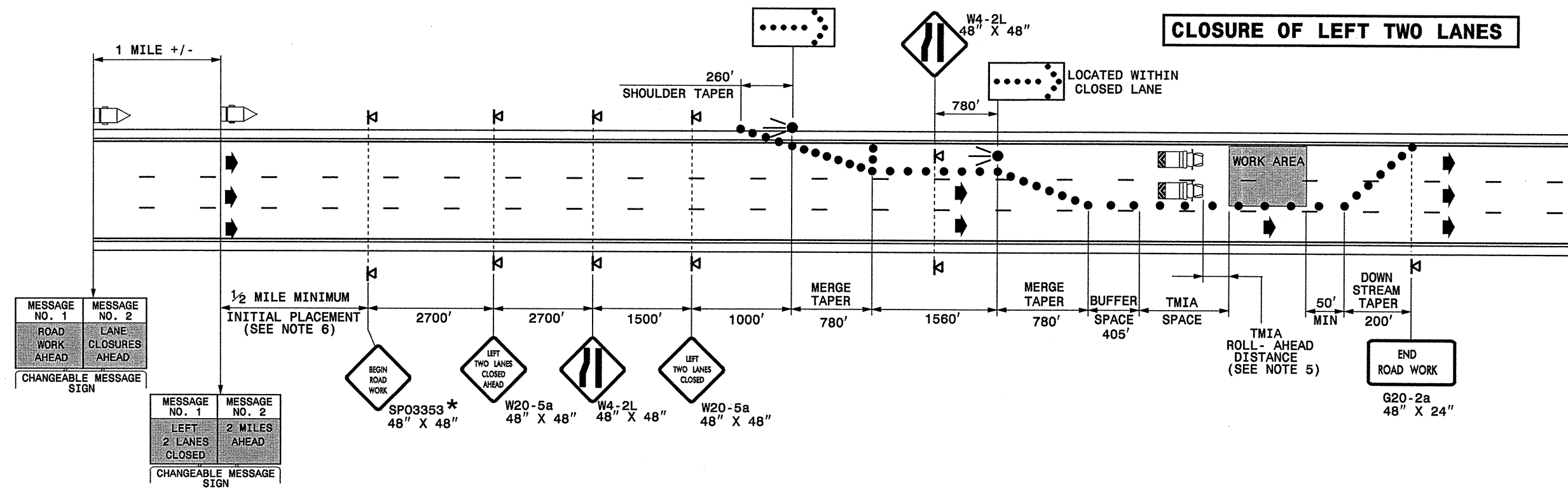
- 1- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 2- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- SEE SHEET TCP-5E FOR TREATMENT OF LANE CLOSURES AT EXIT RAMP.
- 4- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 5- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 6- PLACE CHANGEABLE MESSAGE SIGNS (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. IF TRAFFIC BACKS UP TO WHERE THE CMS IS LOCATED THEN RELOCATE THE CMS 1/2 MILE FROM THE ANTICIPATED BACK UP.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ☐ TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- ☐ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- ⚡ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

| APPROVED: <i>[Signature]</i> DATE: 10/06 | S. HOLDEN RD./BRIDGE #197 TEMPORARY LANE CLOSURE INSIDE OR OUTSIDE LANE CLOSED | | | | | | |
|---|--|-----------|--|--|--|---|-----------|
| | SCALE: NONE | | | | | | |
| | DATE: 10/06 | | | | | | |
| | DESIGN BY: KPB | | | | | | |
| | REVIEWED BY: JWJG | | | | | | |
| <table border="1"> <thead> <tr> <th>REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> </tbody> </table> | | REVISIONS | | | | <table border="1"> <tr> <td>CADD FILE</td> </tr> </table> | CADD FILE |
| REVISIONS | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| CADD FILE | | | | | | | |

24-OCT-2006 14:41 \\dot\dfsroot\01\sp\proj\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-tcp5c.dgn
 kbroadwell AT WZTC224240



| | |
|-----------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | LANE CLOSURES AHEAD |

CHANGEABLE MESSAGE SIGN

| | |
|---------------------|---------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| LEFT 2 LANES CLOSED | 2 MILES AHEAD |

CHANGEABLE MESSAGE SIGN

GENERAL NOTES

1-REFER TO NOTES ON SHEET TCP-5C.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

APPROVED: *[Signature]* DATE: 10/06

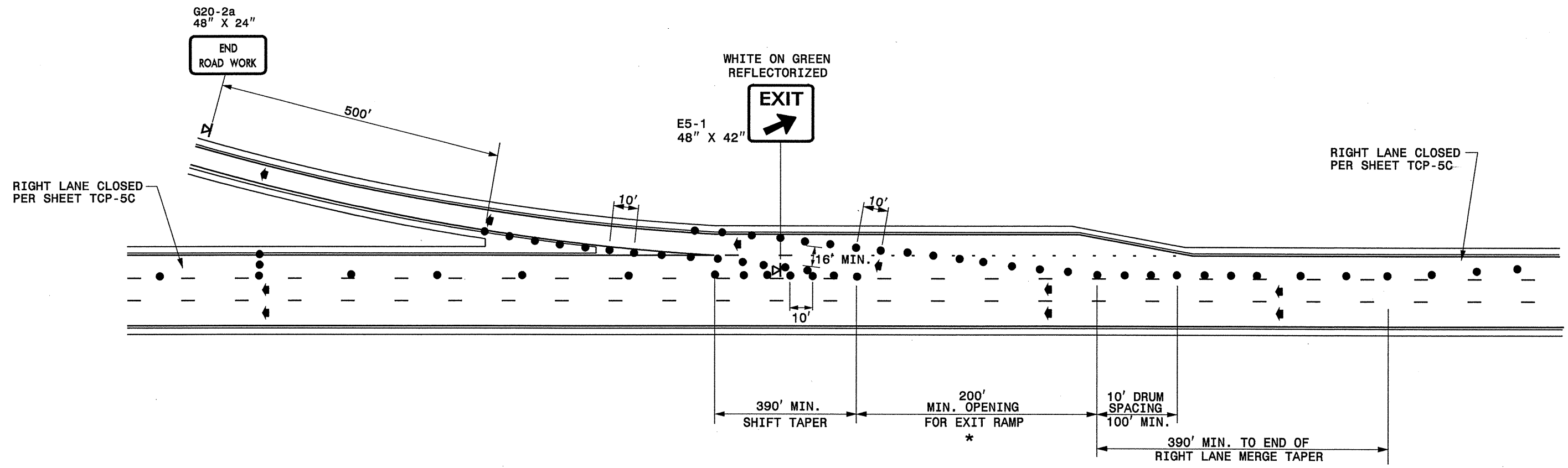
**S. HOLDEN RD./BRIDGE #197
TEMPORARY LANE CLOSURES
INSIDE TWO LANES CLOSED**

SCALE: NONE
DATE: 10/06
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

SEAL:

| REVISIONS |
|-----------|
| |
| |
| |

24-OCT-2006 14:11 \\dot\dfsroot\01\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp5d.dgn KBRoodwell AT WZTC224240



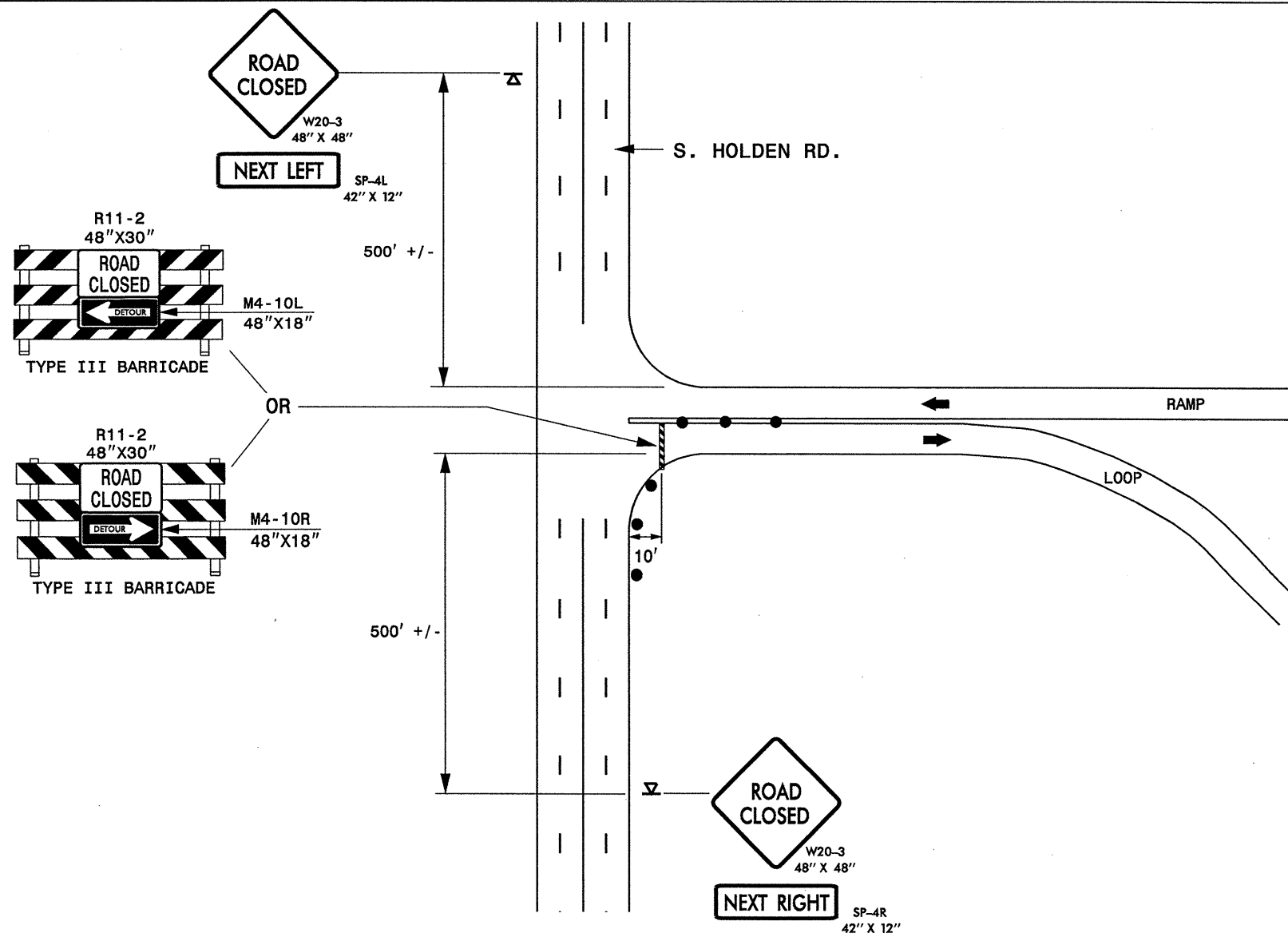
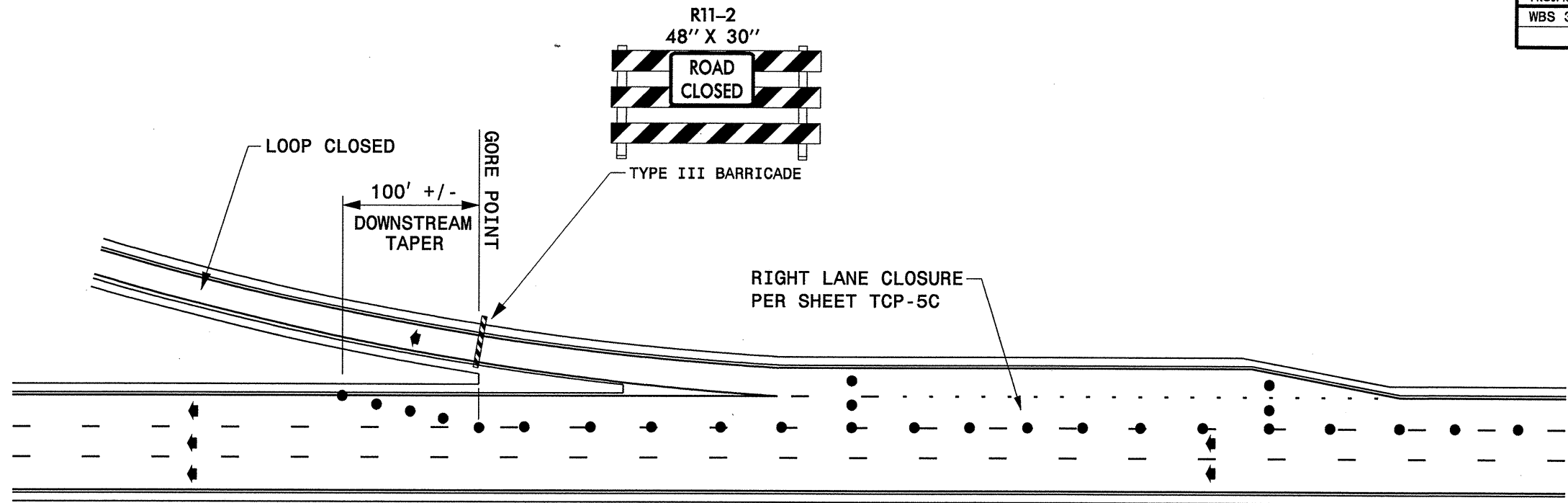
*** NOTE:**
 1-USE EXISTING RAMP OPENING LENGTH, BUT NO LESS THAN 1/2 ORIGINAL LENGTH.
 2-CONSIDER CLOSING RAMP IF LESS THAN 1/2 ORIGINAL LENGTH CANNOT BE OBTAINED.

| LEGEND | |
|--------|---------------------------|
| ● | DRUM |
| ⊠ | PORTABLE SIGN |
| ↔ | DIRECTION OF TRAFFIC FLOW |

GENERAL NOTES
 1-MOUNT EXIT SIGNS A MINIMUM OF 7 FEET ABOVE THE PAVEMENT ELEVATION.

| | | | |
|---|---|--|-----------|
| APPROVED: <i>[Signature]</i> DATE: 10/06/06 | S. HOLDEN RD./BRIDGE #197 TEMPORARY LANE CLOSURES THRU EXIT RAMPS | | |
| | SCALE: NONE | | |
| | DATE: 10/06 | | REVISIONS |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |




24-OCT-2006 14:11
 \\dot\dfs\root\01\nsproj\special\36727.3\Traffic\36727.3.5\greensbor\bp-40-2006-l-tcp5e.dgn
 Kbrodwell AT WZ TC24240



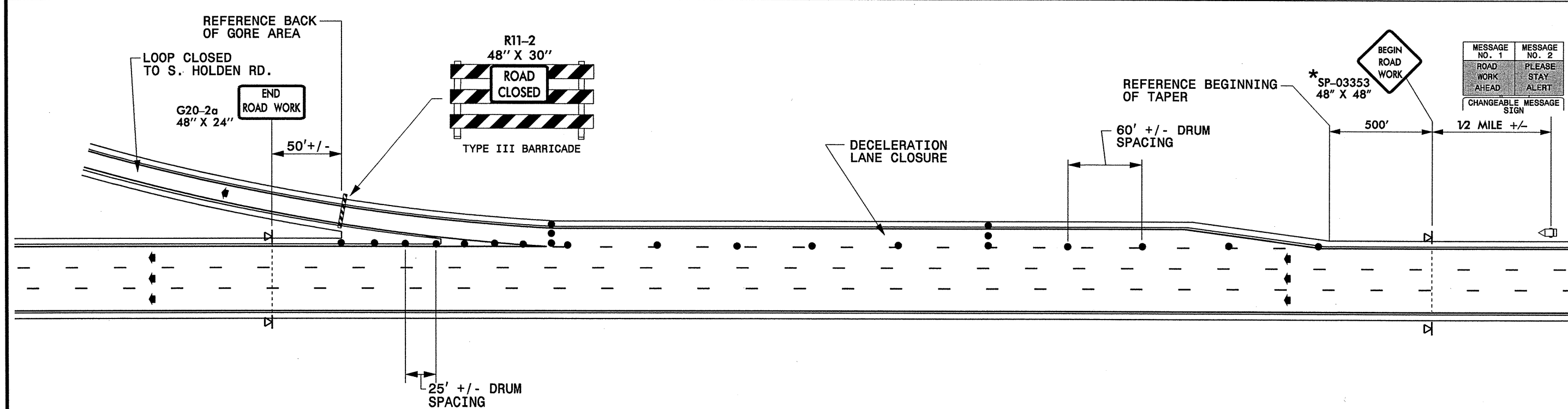
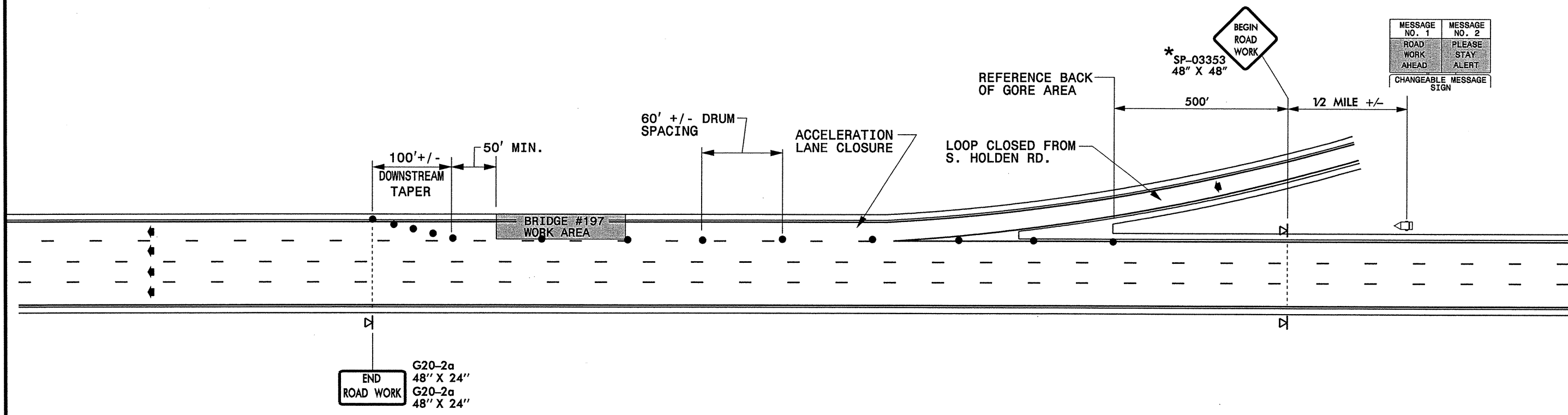
NOTES

- 1- PROVIDE A 21 CALENDAR DAY NOTICE TO THE ENGINEER PRIOR TO A ROAD BEING CLOSED SUCH THAT NECESSARY PROVISIONS CAN BE MADE TO INFORM LOCAL EMERGENCY AND LAW ENFORCEMENT PERSONNEL, OR ANY OTHER PARTIES AFFECTED BY THE ROAD CLOSURE.
- 2- INSTALL SIGNS BEFORE BARRICADES WHEN CLOSING THE ROADWAY TO TRAFFIC. REMOVE BARRICADES BEFORE SIGNS WHEN OPENING THE ROADWAY TO TRAFFIC.
3. POSITION BARRICADES SUCH THAT THE STRIPES ARE SLOPED DOWNWARD IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN IN DETOURING.

| LEGEND | |
|--------|---------------------------|
| ● | DRUM |
| ◀ | PORTABLE SIGN |
| ↔ | DIRECTION OF TRAFFIC FLOW |

| APPROVED:  DATE: 2/20/06 | <p align="center">S. HOLDEN RD./BRIDGE #197 ROAD CLOSURE AT LOOPS</p> | | | | | | |
|---|--|---|-----------|--|--|--|--|
|  | | | | | | | |
| SCALE: NONE | DATE: 10/06 |  | | | | | |
| DWG. BY: KPB | DESIGN BY: KPB | | | | | | |
| REVIEWED BY: JWG | | | | | | | |
| <table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table> | | | REVISIONS | | | | |
| REVISIONS | | | | | | | |
| | | | | | | | |
| | | | | | | | |

24-OCT-2006 14:11
 \\dot\dfsroot\inspr\special\36727.3\Traffic\36727.3\greensboro\bp-40-2006-l-tcp5f.dgn
 kbroadwell AT WZTC224240



LEGEND

- DRUM
- ◄ DIRECTION OF TRAFFIC FLOW
- ⊘ PORTABLE SIGN
- ◻ CHANGEABLE MESSAGE SIGN
- * SEE SHEETS TCP-11A TCP-11B FOR SIGN DETAILS

APPROVED: *[Signature]* DATE: 24/10/06

**S. HOLDEN RD./BRIDGE #197
TEMPORARY LANE CLOSURES
AT ACCEL. & DECEL. LANES**

| | | |
|----------------|--|-----------|
| SCALE: NONE | | REVISIONS |
| DATE: 10/06 | | |
| DWG. BY: KPB | | |
| DESIGN BY: KPB | | |

REVIEWED BY: JWG

24-OCT-2006 14:11
 \\dot\dfsroot\onnspro\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp5g.dgn
 kbrodwell AT WZTC224240

PHASING

PHASE 1

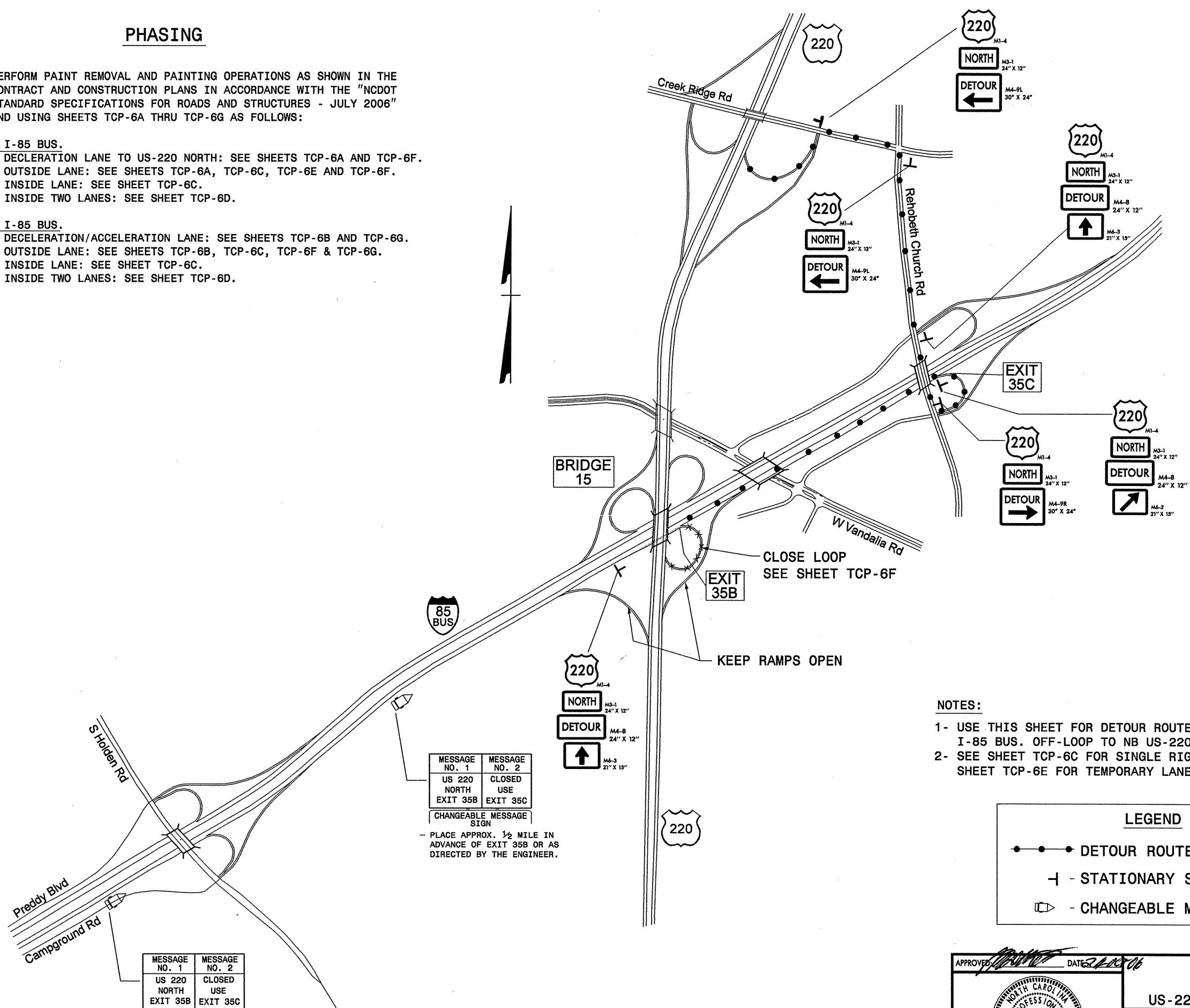
STEP 1: PERFORM PAINT REMOVAL AND PAINTING OPERATIONS AS SHOWN IN THE CONTRACT AND CONSTRUCTION PLANS IN ACCORDANCE WITH THE "NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES - JULY 2006" AND USING SHEETS TCP-6A THRU TCP-6G AS FOLLOWS:

NORTHBOUND I-85 BUS.

- CLOSE NB DECLARATION LANE TO US-220 NORTH: SEE SHEETS TCP-6A AND TCP-6F.
- CLOSE NB OUTSIDE LANE: SEE SHEETS TCP-6A, TCP-6C, TCP-6E AND TCP-6F.
- CLOSE NB INSIDE LANE: SEE SHEET TCP-6C.
- CLOSE NB INSIDE TWO LANES: SEE SHEET TCP-6D.

SOUTHBOUND I-85 BUS.

- CLOSE SB DECELERATION/ACCELERATION LANE: SEE SHEETS TCP-6B AND TCP-6G.
- CLOSE SB OUTSIDE LANE: SEE SHEETS TCP-6B, TCP-6C, TCP-6F & TCP-6G.
- CLOSE SB INSIDE LANE: SEE SHEET TCP-6C.
- CLOSE SB INSIDE TWO LANES: SEE SHEET TCP-6D.



| | |
|-----------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| US 220 NORTH EXIT 35B | CLOSED USE EXIT 35C |

CHANGEABLE MESSAGE SIGN
- PLACE APPROX. 1/2 MILE IN ADVANCE OF EXIT 35B OR AS DIRECTED BY THE ENGINEER.

| | |
|-----------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| US 220 NORTH EXIT 35B | CLOSED USE EXIT 35C |

CHANGEABLE MESSAGE SIGN
- PLACE APPROX. 1 MILE IN ADVANCE OF EXIT 35B OR AS DIRECTED BY THE ENGINEER.

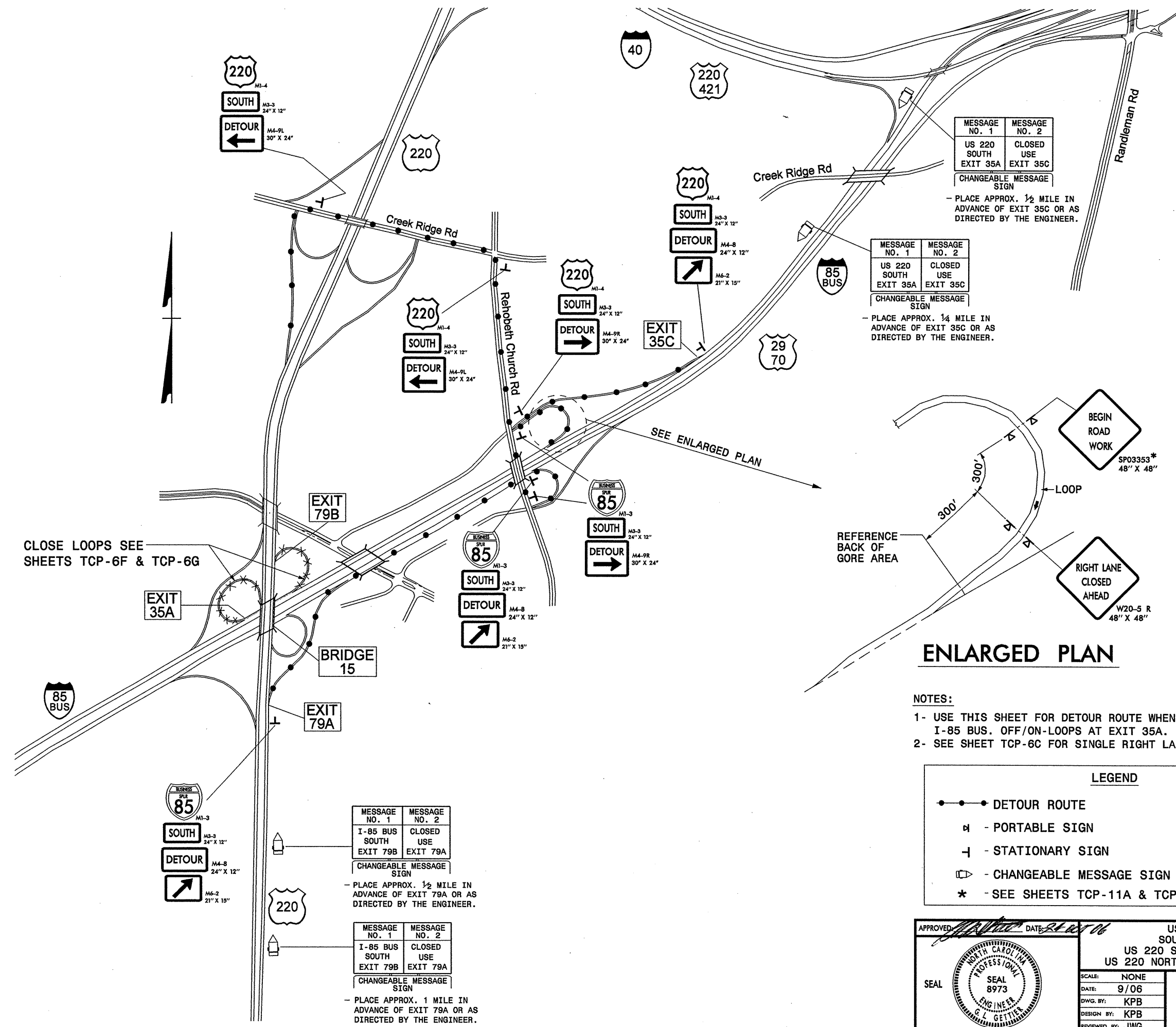
NOTES:

- 1- USE THIS SHEET FOR DETOUR ROUTE WHEN CLOSING THE NORTHBOUND I-85 BUS. OFF-LOOP TO NB US-220.
- 2- SEE SHEET TCP-6C FOR SINGLE RIGHT LANE LANE CLOSURE AND SHEET TCP-6E FOR TEMPORARY LANE CLOSURE AT EXIT RAMP.

| LEGEND | |
|--------|-------------------------|
| ●—●—● | DETOUR ROUTE |
| + | STATIONARY SIGN |
| ◁ ▷ | CHANGEABLE MESSAGE SIGN |

| | | | |
|-----------|------------------|--|-----------|
| APPROVED: | DATE: 2/2/06 | US-220/BRIDGE #15 NORTHBOUND I-85 BUS. US-220 OFF-LOOP CLOSED - DETOUR | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 14:11 \\dot\dfsroot\01\sp\proj\36727.3\N.Traffic\36727.3.5\gr\eesboro\bp-40-2006-1-tcp6a.dgn
 kbr\oadwell AT W21C224240

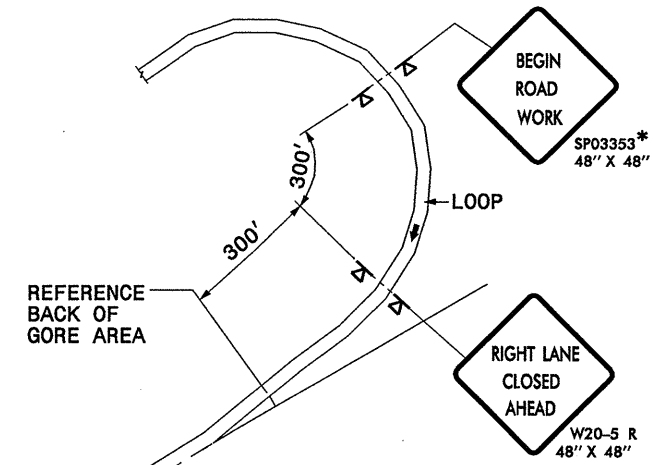


| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| US 220 SOUTH EXIT 35A | CLOSED USE EXIT 35C |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1/2 MILE IN ADVANCE OF EXIT 35C OR AS DIRECTED BY THE ENGINEER.

| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| US 220 SOUTH EXIT 35A | CLOSED USE EXIT 35C |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1/4 MILE IN ADVANCE OF EXIT 35C OR AS DIRECTED BY THE ENGINEER.



ENLARGED PLAN

NOTES:

- 1- USE THIS SHEET FOR DETOUR ROUTE WHEN CLOSING THE SOUTHBOUND I-85 BUS. OFF/ON-LOOPS AT EXIT 35A.
- 2- SEE SHEET TCP-6C FOR SINGLE RIGHT LANE CLOSURE.

| LEGEND | |
|--------|---|
| | DETOUR ROUTE |
| | PORTABLE SIGN |
| | STATIONARY SIGN |
| | CHANGEABLE MESSAGE SIGN |
| * | SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS |

| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| I-85 BUS SOUTH EXIT 79B | CLOSED USE EXIT 79A |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1/2 MILE IN ADVANCE OF EXIT 79A OR AS DIRECTED BY THE ENGINEER.

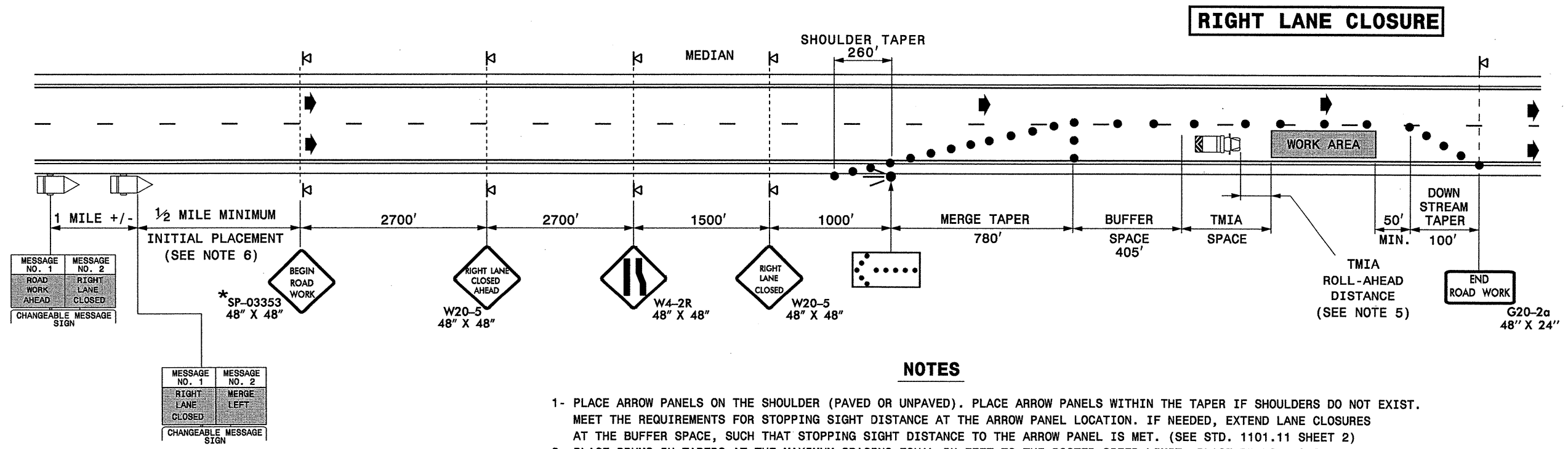
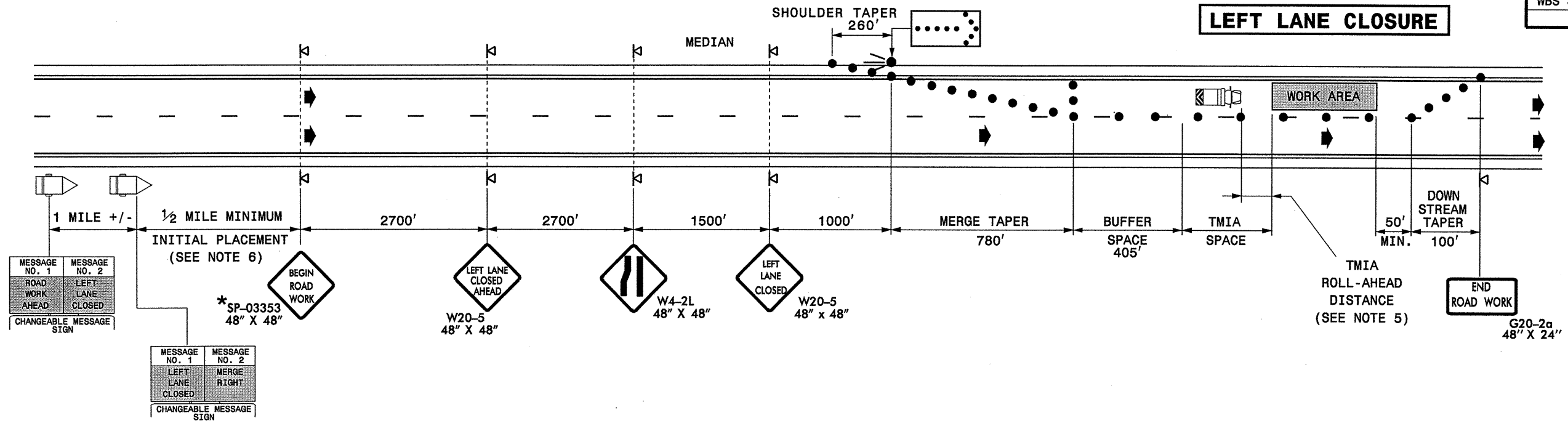
| | |
|-------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| I-85 BUS SOUTH EXIT 79B | CLOSED USE EXIT 79A |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1 MILE IN ADVANCE OF EXIT 79A OR AS DIRECTED BY THE ENGINEER.

CLOSE LOOPS SEE SHEETS TCP-6F & TCP-6G

| | | | |
|-----------|------------------|--|-----------|
| APPROVED: | DATE: 9/06 | US-220/BRIDGE #15 SOUTHBOUND I-85 BUS. US 220 SOUTH OFF-LOOP CLOSED & US 220 NORTH ON-LOOP CLOSED - DETOUR | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 9/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 14:11 \\dot\dfsroot\p\special\36727.3.5\greensboro\bp-40-2006-l-tcp6b.dgn
 kbroodwell AT WZTC224240



NOTES

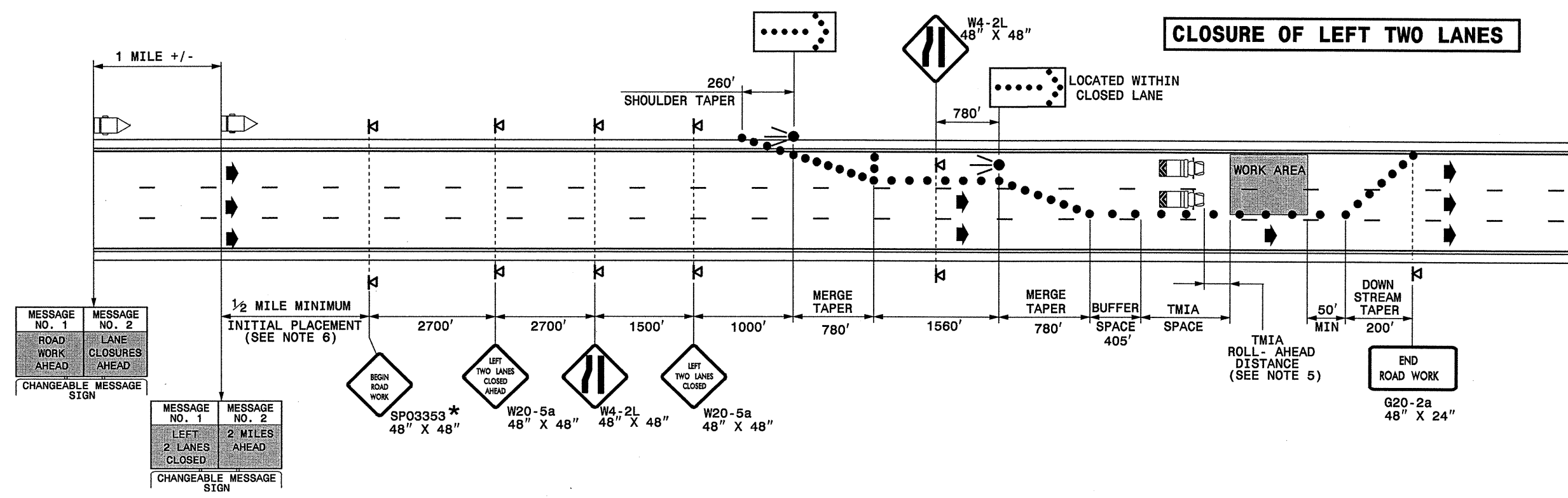
- 1- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 2- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- SEE SHEET TCP-6E FOR TREATMENT OF LANE CLOSURES AT EXIT RAMP.
- 4- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 5- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 6- PLACE CHANGEABLE MESSAGE SIGNS (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. IF TRAFFIC BACKS UP TO WHERE THE CMS IS LOCATED THEN RELOCATE THE CMS 1/2 MILE FROM THE ANTICIPATED BACK UP.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ☐ TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- ☐ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- ⚡ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

| | | |
|---|---|--|
| APPROVED: <i>[Signature]</i> DATE: 2-2-06 | US-220/BRIDGE #15 TEMPORARY LANE CLOSURE INSIDE OR OUTSIDE LANE CLOSED | |
| | SCALE: NONE | |
| | DATE: 10/06 | |
| | DWG. BY: KPB | |
| | DESIGN BY: KPB | |
| REVIEWED BY: JWG | REVISIONS | |

24-OCT-2006 14:2
 \\dot\dfsroot\01\nsproj\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-1tcp6c.dgn
 kbrodwell AT W21222420



| | |
|-----------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | LANE CLOSURES AHEAD |

CHANGEABLE MESSAGE SIGN

| | |
|---------------------|---------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| LEFT 2 LANES CLOSED | 2 MILES AHEAD |

CHANGEABLE MESSAGE SIGN

GENERAL NOTES

1-REFER TO NOTES ON SHEET TCP-6C

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

APPROVED: DATE: 10/27/06

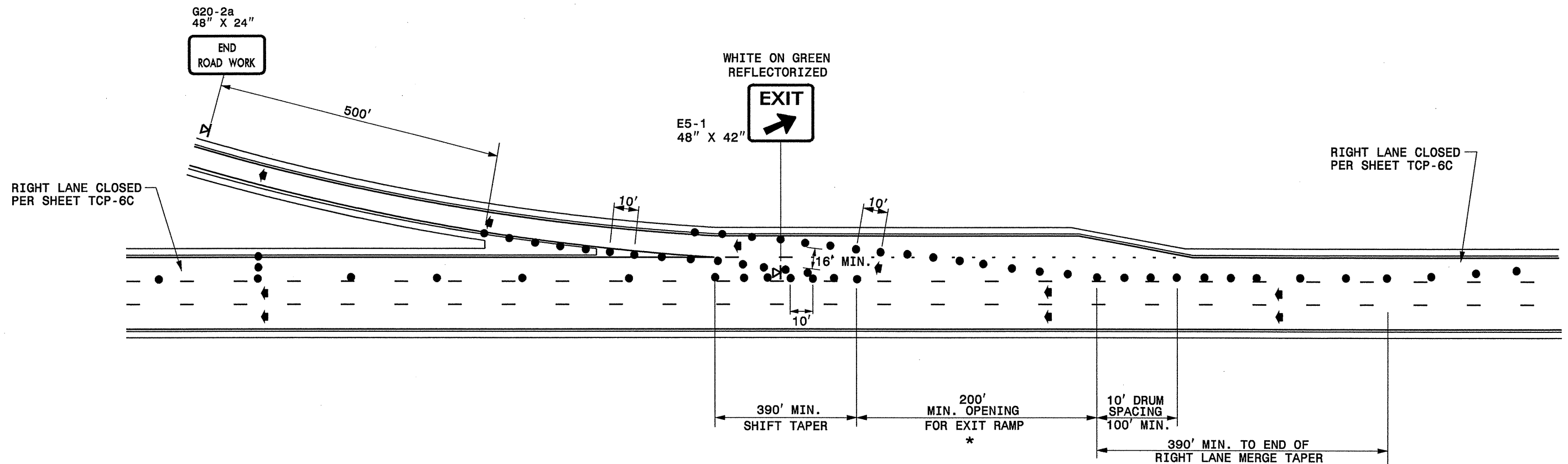
SEAL

US-220/BRIDGE #15
TEMPORARY LANE CLOSURES
INSIDE TWO LANES CLOSED

| | |
|--------------|-------|
| SCALE: | NONE |
| DATE: | 10/06 |
| DWG. BY: | KPB |
| DESIGN BY: | KPB |
| REVIEWED BY: | JWG |

| REVISIONS |
|-----------|
| |
| |
| |

24-OCT-2006 14:42 \\dot\dfsroot\inspr\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-11-tcp6d.dgn kbroadwell AT WZTC224240



*** NOTE:**
 1-USE EXISTING RAMP OPENING LENGTH, BUT NO LESS THAN 1/2 ORIGINAL LENGTH.
 2-CONSIDER CLOSING RAMP IF LESS THAN 1/2 ORIGINAL LENGTH CANNOT BE OBTAINED.

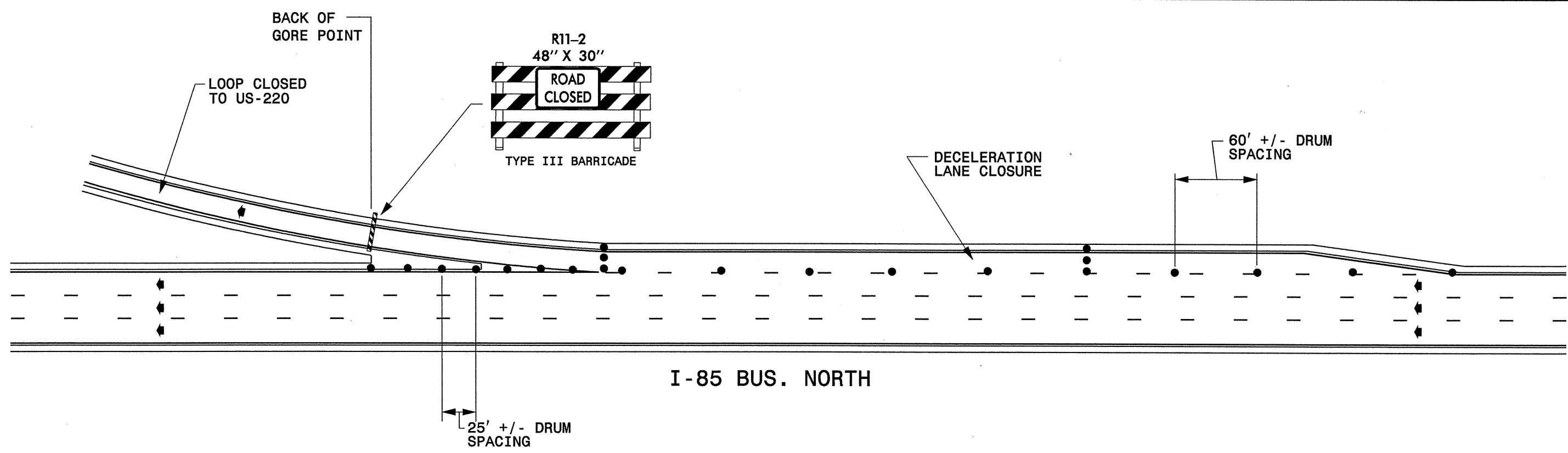
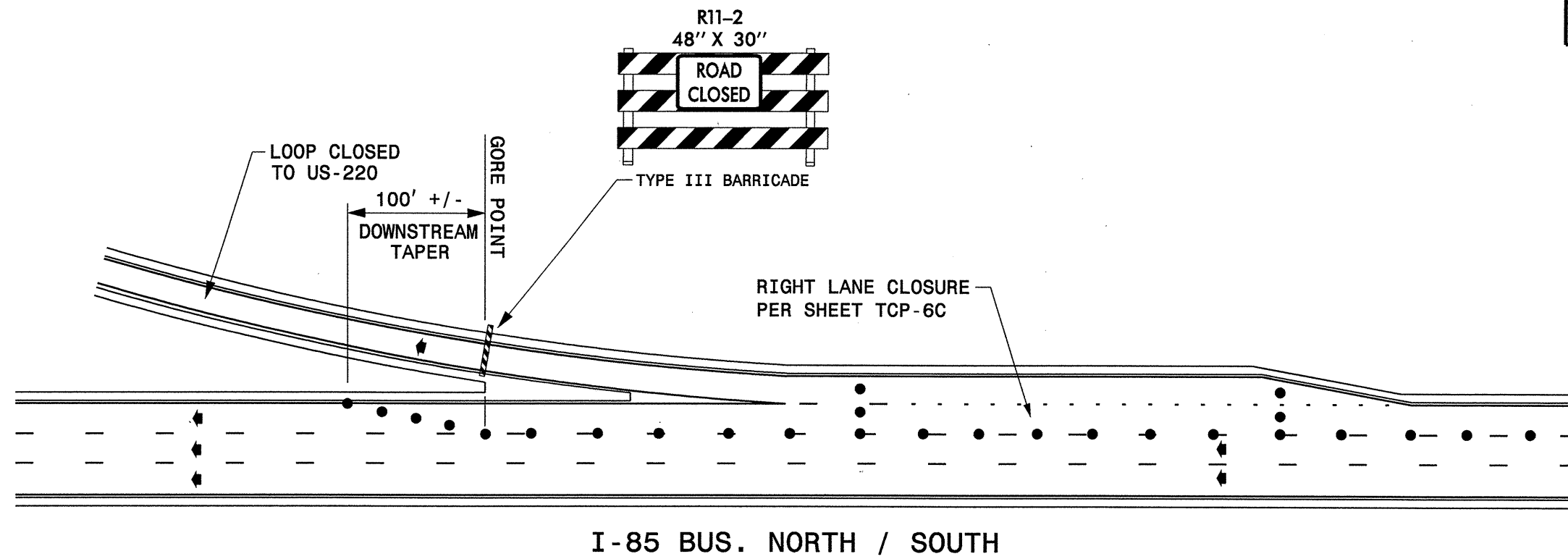
| LEGEND | |
|--------|---------------------------|
| ● | DRUM |
| ⚡ | PORTABLE SIGN |
| ↔ | DIRECTION OF TRAFFIC FLOW |

GENERAL NOTES

1-MOUNT EXIT SIGNS A MINIMUM OF 7 FEET ABOVE THE PAVEMENT ELEVATION.

| APPROVED: <i>[Signature]</i> DATE: 10/06 | US-220/BRIDGE #15 TEMPORARY LANE CLOSURES THRU EXIT RAMP | | | | | | | |
|--|---|---|-----------|--|--|--|--|--|
| | SCALE: NONE | | | | | | | |
| | DATE: 10/06 | | | | | | | |
| | DWG. BY: KPB | | | | | | | |
| | REVIEWED BY: JWG | | | | | | | |
| | | <table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table> | REVISIONS | | | | | |
| REVISIONS | | | | | | | | |
| | | | | | | | | |
| | | | | | | | | |

24-OCT-2006 14:12
 \\dot\dfsroot\0\nsproj\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-ttcp6e.dgn
 kbroadwell AT WZTC224240



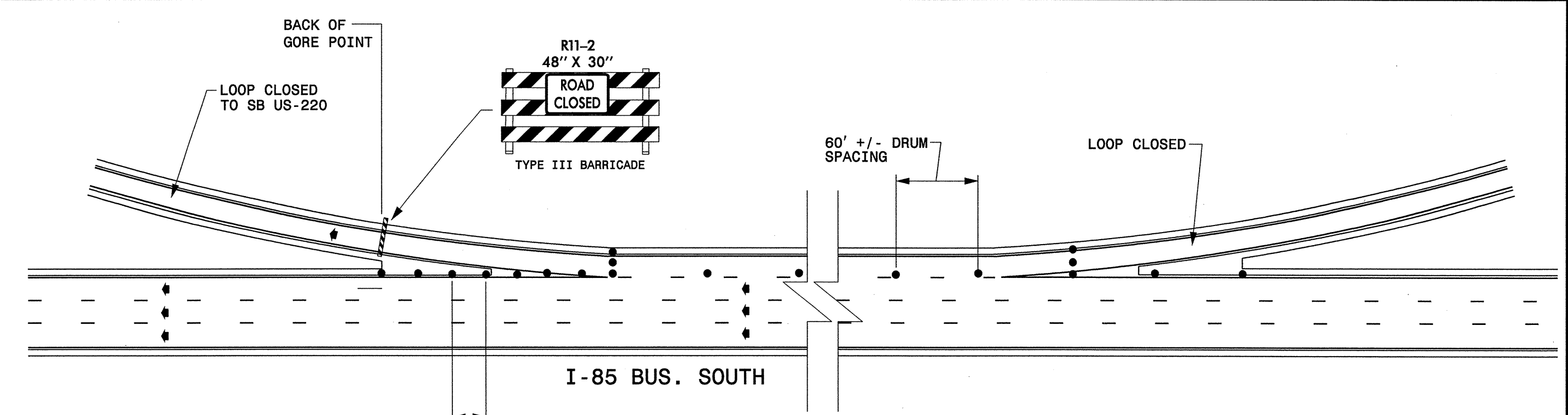
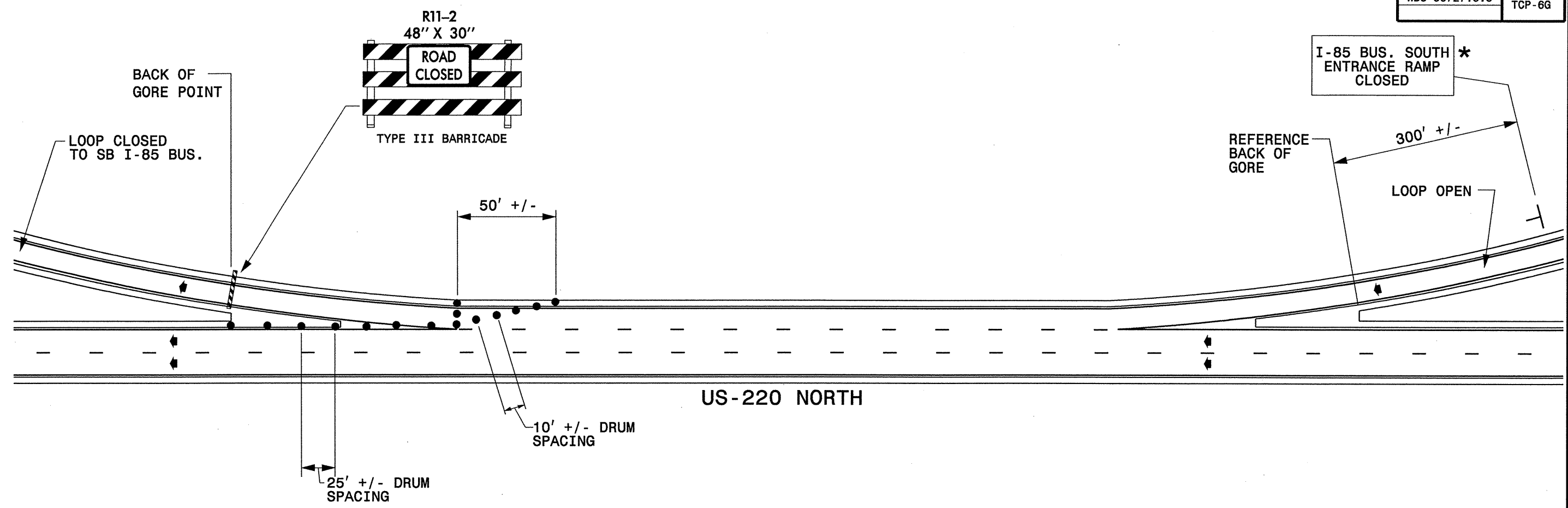
NOTES

- 1- PROVIDE A 21 CALENDAR DAY NOTICE TO THE ENGINEER PRIOR TO A ROAD BEING CLOSED SUCH THAT NECESSARY PROVISIONS CAN BE MADE TO INFORM LOCAL EMERGENCY AND LAW ENFORCEMENT PERSONNEL, OR ANY OTHER PARTIES AFFECTED BY THE ROAD CLOSURE.
- 2- INSTALL SIGNS BEFORE BARRICADES WHEN CLOSING THE ROADWAY TO TRAFFIC. REMOVE BARRICADES BEFORE SIGNS WHEN OPENING THE ROADWAY TO TRAFFIC.
3. POSITION BARRICADES SUCH THAT THE STRIPES ARE SLOPED DOWNWARD IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN IN DETOURING.

| LEGEND | |
|--------|---------------------------|
| ● | DRUM |
| ◀ | PORTABLE SIGN |
| ↔ | DIRECTION OF TRAFFIC FLOW |

| | | |
|--|--|--|
| APPROVED: <i>[Signature]</i> DATE: 10/06 | US-220/BRIDGE #15 ROAD CLOSURE AT LOOPS | |
| | SCALE: NONE | |
| | DATE: 10/06 | |
| | DWG. BY: KPB | |
| | DESIGN BY: KPB | |
| REVIEWED BY: JWG | REVISIONS | |

24-OCT-2006 14:12
 \\dot\dfsroot\01\inspro\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp6f.dgn
 kbrodwell AT W2TC224240



LEGEND

- DRUM
- | STATIONARY SIGN
- ◄ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

| | | | |
|------------------|-----------------|--|-----------|
| APPROVED: | DATE: 22 OCT 06 | US-220/BRIDGE #15 LOOP CLOSURE AT ACCEL./DECEL. LANE | |
| | | | |
| SCALE: NONE | | | REVISIONS |
| DATE: 10/06 | | | |
| DWG. BY: KPB | | | |
| DESIGN BY: KPB | | | |
| REVIEWED BY: JWG | | | CARD FILE |

PHASE 1

PHASING

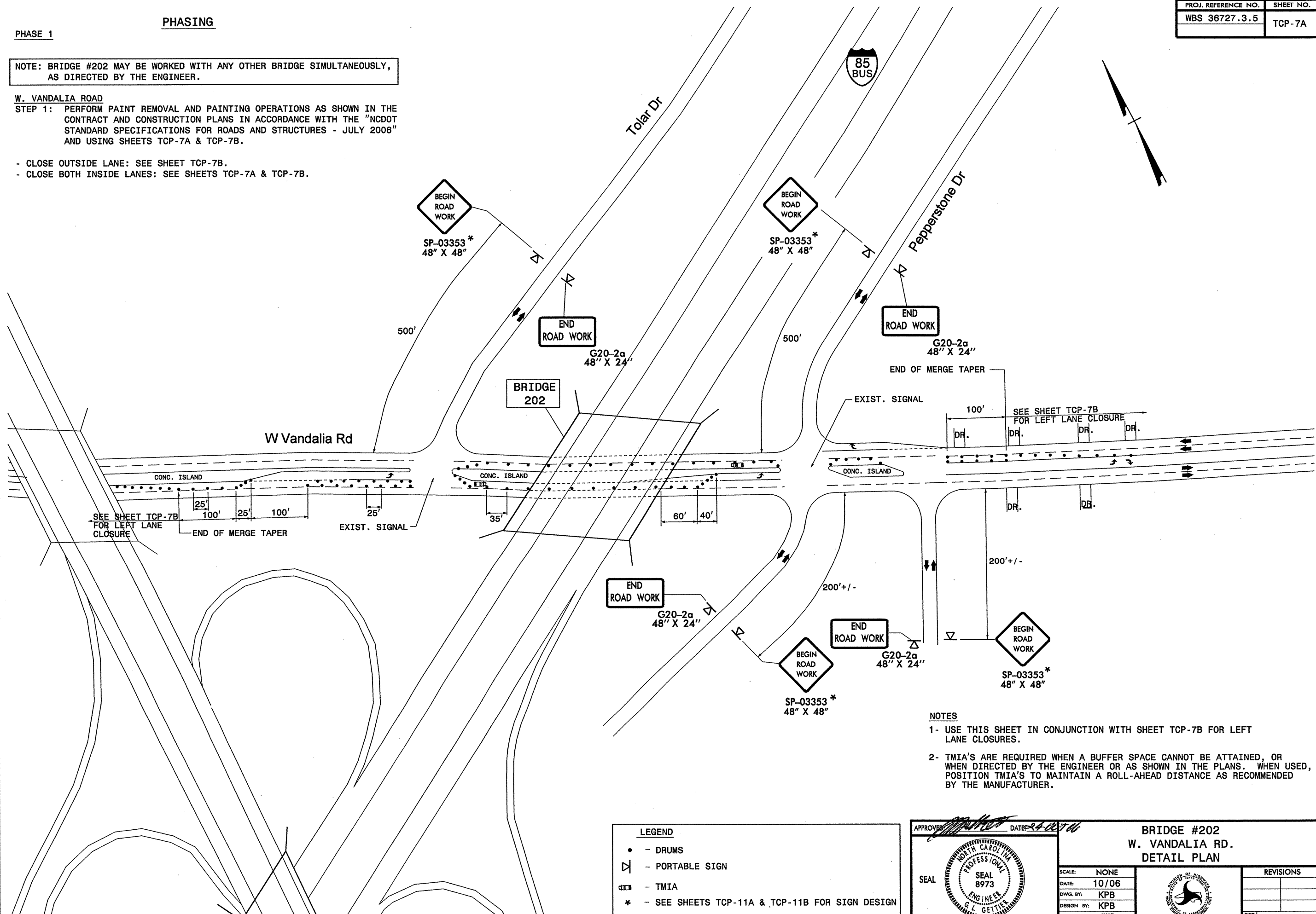
NOTE: BRIDGE #202 MAY BE WORKED WITH ANY OTHER BRIDGE SIMULTANEOUSLY, AS DIRECTED BY THE ENGINEER.

W. VANDALIA ROAD

STEP 1: PERFORM PAINT REMOVAL AND PAINTING OPERATIONS AS SHOWN IN THE CONTRACT AND CONSTRUCTION PLANS IN ACCORDANCE WITH THE "NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES - JULY 2006" AND USING SHEETS TCP-7A & TCP-7B.

- CLOSE OUTSIDE LANE: SEE SHEET TCP-7B.
- CLOSE BOTH INSIDE LANES: SEE SHEETS TCP-7A & TCP-7B.

24-OCT-2006 14:15
 \\dot\dfs\root\inspro\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-r-top7a.dgn
 kbradwell AT W7TC224240



- NOTES**
- 1- USE THIS SHEET IN CONJUNCTION WITH SHEET TCP-7B FOR LEFT LANE CLOSURES.
 - 2- TMIA'S ARE REQUIRED WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR AS SHOWN IN THE PLANS. WHEN USED, POSITION TMIA'S TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.

LEGEND

- - DRUMS
- ▽ - PORTABLE SIGN
- Ⓜ - TMIA
- * - SEE SHEETS TCP-11A & TCP-11B FOR SIGN DESIGN

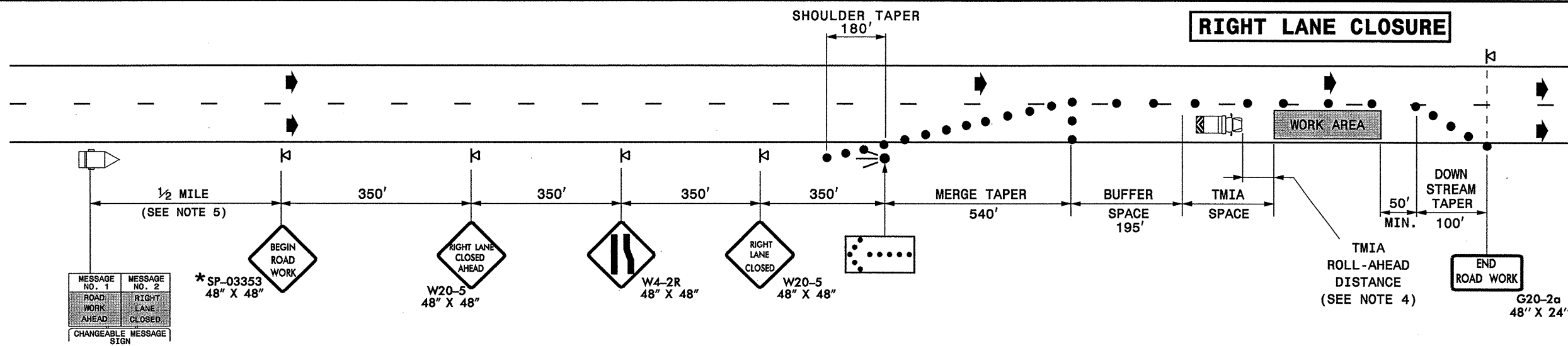
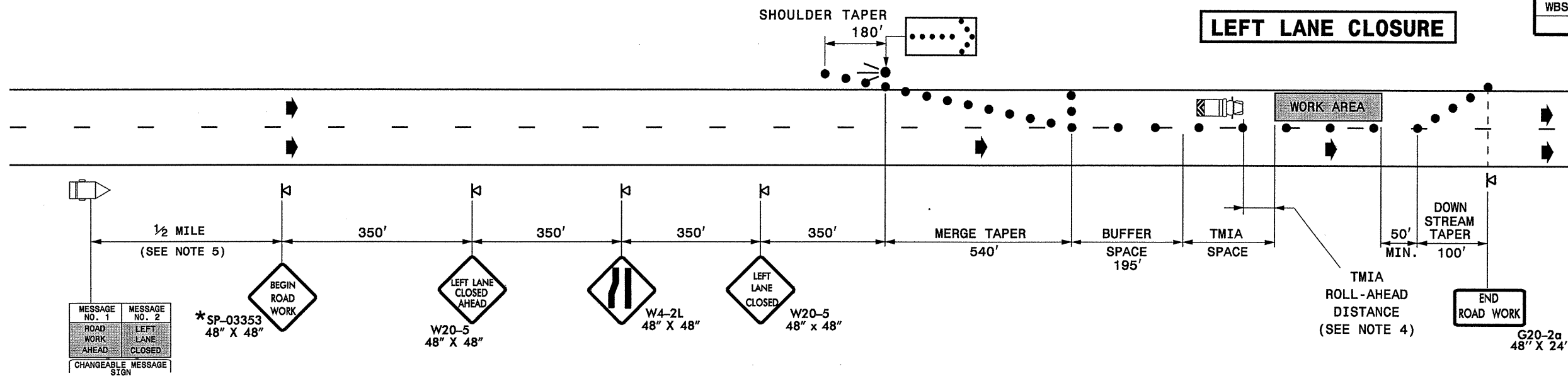
APPROVED: *[Signature]* DATE: 10/06/06

SEAL

**BRIDGE #202
W. VANDALIA RD.
DETAIL PLAN**

| | |
|--------------|-------|
| SCALE: | NONE |
| DATE: | 10/06 |
| DWG. BY: | KPB |
| DESIGN BY: | KPB |
| REVIEWED BY: | JWG |

| REVISIONS |
|-----------|
| |
| |
| |
| |



NOTES

- 1- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11).
- 2- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 5- PLACE CHANGEABLE MESSAGE SIGNS (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. IF TRAFFIC BACKS UP TO WHERE THE CMS IS LOCATED THEN RELOCATE THE CMS 1/2 MILE FROM THE ANTICIPATED BACK UP.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS

| | | | |
|-----------|------------------|--|-----------|
| APPROVED: | DATE: 10/06 | W. VANDALIA RD./BRIDGE #202 TEMPORARY LANE CLOSURE ONE LANE CLOSED | |
| SEAL | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWJ | | |

24-OCT-2006 14:31
 \\dot\dfsroot\01\nsproj\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-fcp7b.dgn
 kbr\codwell AT WZTC224240

PHASING

PHASE 1

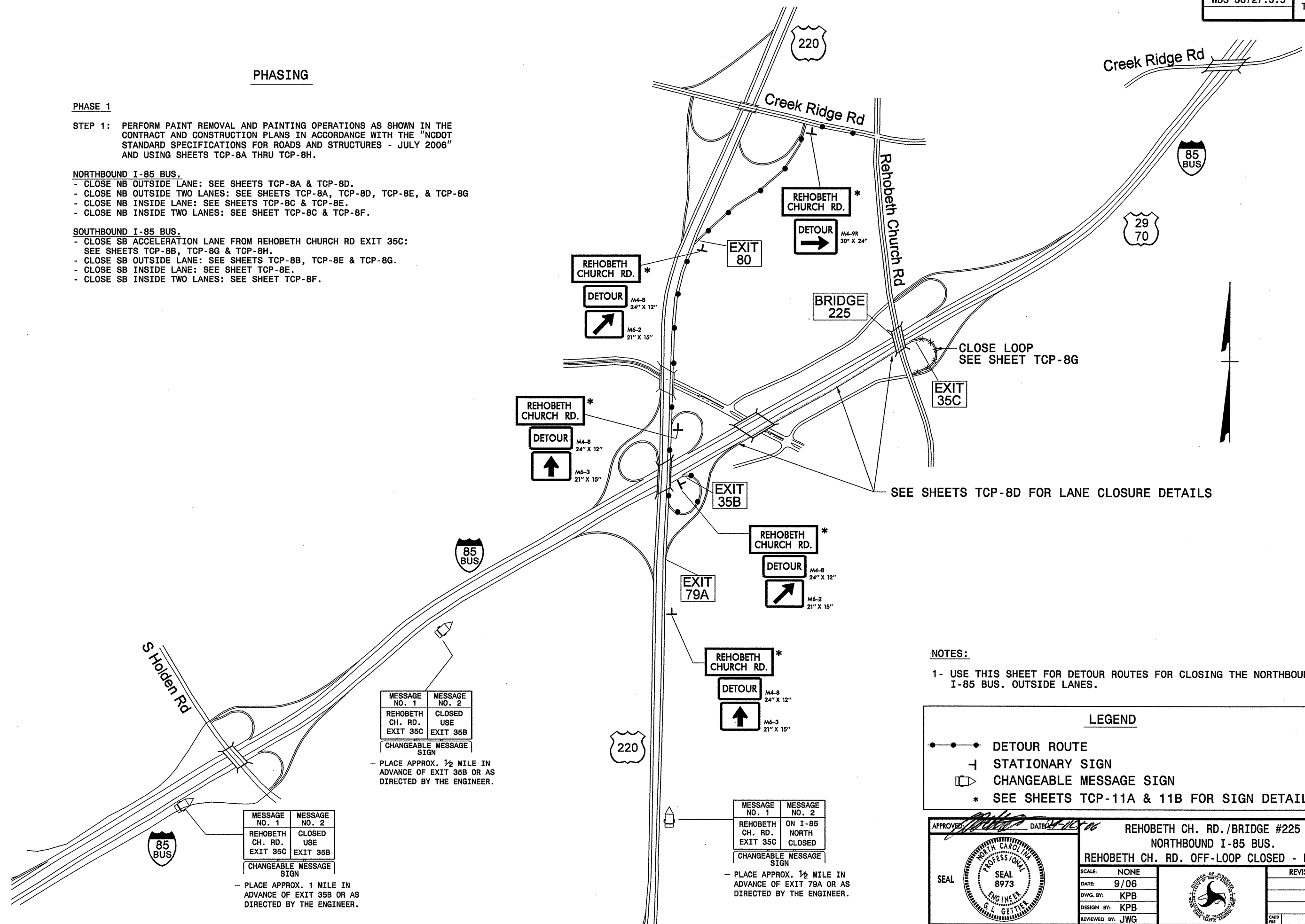
STEP 1: PERFORM PAINT REMOVAL AND PAINTING OPERATIONS AS SHOWN IN THE CONTRACT AND CONSTRUCTION PLANS IN ACCORDANCE WITH THE "NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES - JULY 2006" AND USING SHEETS TCP-8A THRU TCP-8H.

NORTHBOUND I-85 BUS.

- CLOSE NB OUTSIDE LANE: SEE SHEETS TCP-8A & TCP-8D.
- CLOSE NB OUTSIDE TWO LANES: SEE SHEETS TCP-8A, TCP-8D, TCP-8E, & TCP-8G
- CLOSE NB INSIDE LANE: SEE SHEETS TCP-8C & TCP-8E.
- CLOSE NB INSIDE TWO LANES: SEE SHEET TCP-8C & TCP-8F.

SOUTHBOUND I-85 BUS.

- CLOSE SB ACCELERATION LANE FROM REHOBETH CHURCH RD EXIT 35C: SEE SHEETS TCP-8B, TCP-8G & TCP-8H.
- CLOSE SB OUTSIDE LANE: SEE SHEETS TCP-8B, TCP-8E & TCP-8G.
- CLOSE SB INSIDE LANE: SEE SHEET TCP-8E.
- CLOSE SB INSIDE TWO LANES: SEE SHEET TCP-8F.



SEE SHEETS TCP-8D FOR LANE CLOSURE DETAILS

NOTES:

- 1- USE THIS SHEET FOR DETOUR ROUTES FOR CLOSING THE NORTHBOUND I-85 BUS. OUTSIDE LANES.

| LEGEND | |
|--------|---|
| | DETOUR ROUTE |
| | STATIONARY SIGN |
| | CHANGEABLE MESSAGE SIGN |
| * | SEE SHEETS TCP-11A & 11B FOR SIGN DETAILS |

| | |
|---------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| REHOBETH CH. RD. EXIT 35C | CLOSED USE EXIT 35B |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1/2 MILE IN ADVANCE OF EXIT 35B OR AS DIRECTED BY THE ENGINEER.

| | |
|---------------------------|---------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| REHOBETH CH. RD. EXIT 35C | CLOSED USE EXIT 35B |
| CHANGEABLE MESSAGE SIGN | |

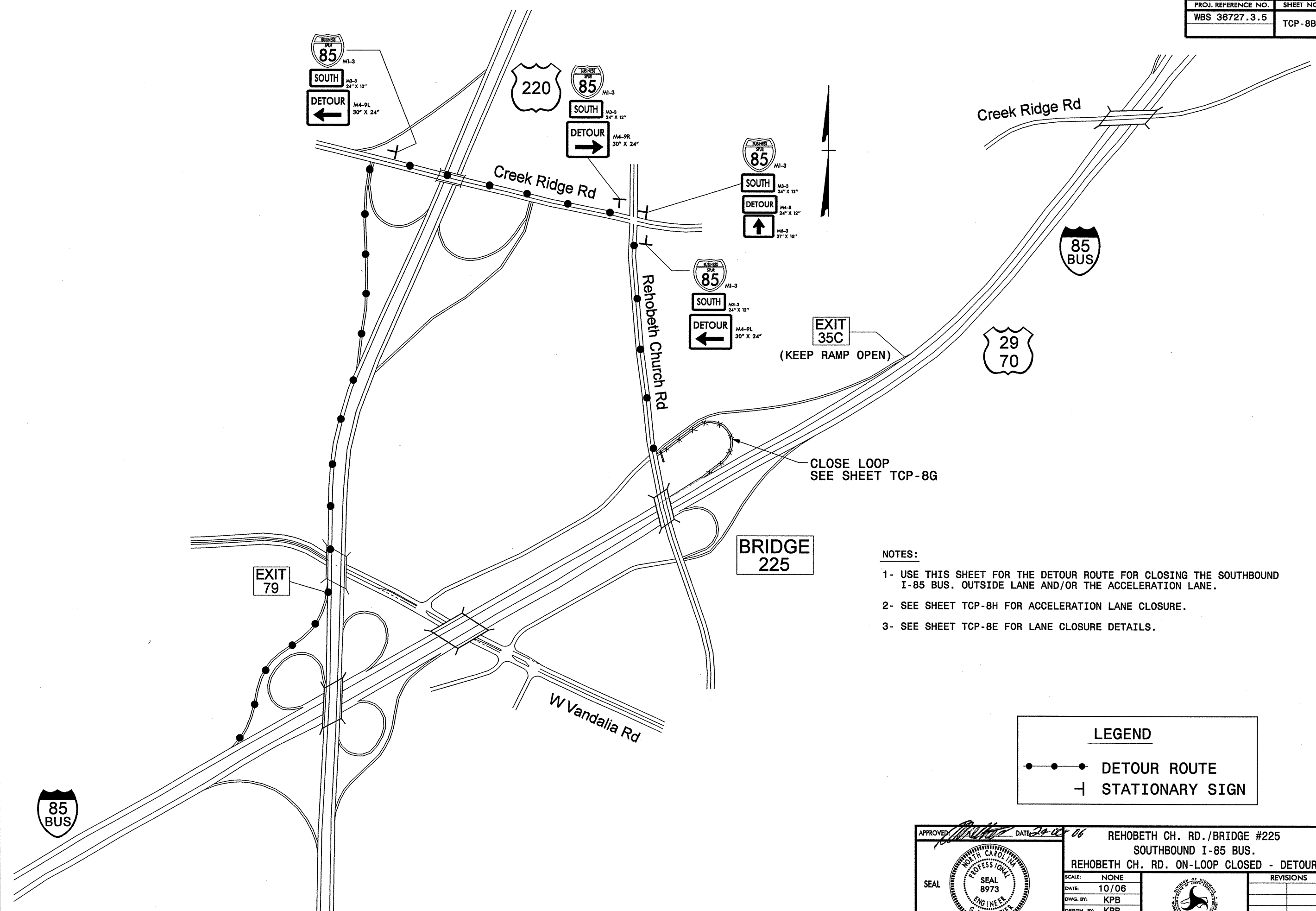
- PLACE APPROX. 1 MILE IN ADVANCE OF EXIT 35B OR AS DIRECTED BY THE ENGINEER.

| | |
|---------------------------|----------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| REHOBETH CH. RD. EXIT 35C | ON I-85 NORTH CLOSED |
| CHANGEABLE MESSAGE SIGN | |

- PLACE APPROX. 1/2 MILE IN ADVANCE OF EXIT 79A OR AS DIRECTED BY THE ENGINEER.

| | | | |
|-----------|---|---|------------------|
| APPROVED: | DATE: 9/15/06 | REHOBETH CH. RD./BRIDGE #225 NORTHBOUND I-85 BUS. | |
| SEAL | REHOBETH CH. RD. OFF-LOOP CLOSED - DETOUR | | |
| | SCALE: NONE | DATE: 9/06 | REVISIONS |
| | DWG. BY: KPB | DESIGN BY: KPB | REVIEWED BY: JWG |
| | | | |

24-OCT-2006 14:37
 \\dot\dfsroot\proj\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-tcp8a.dgn
 kbr\oadwell AT WZTC224240



EXIT 35C
(KEEP RAMP OPEN)

CLOSE LOOP
SEE SHEET TCP-8G

BRIDGE 225

EXIT 79

NOTES:

- 1- USE THIS SHEET FOR THE DETOUR ROUTE FOR CLOSING THE SOUTHBOUND I-85 BUS. OUTSIDE LANE AND/OR THE ACCELERATION LANE.
- 2- SEE SHEET TCP-8H FOR ACCELERATION LANE CLOSURE.
- 3- SEE SHEET TCP-8E FOR LANE CLOSURE DETAILS.

LEGEND

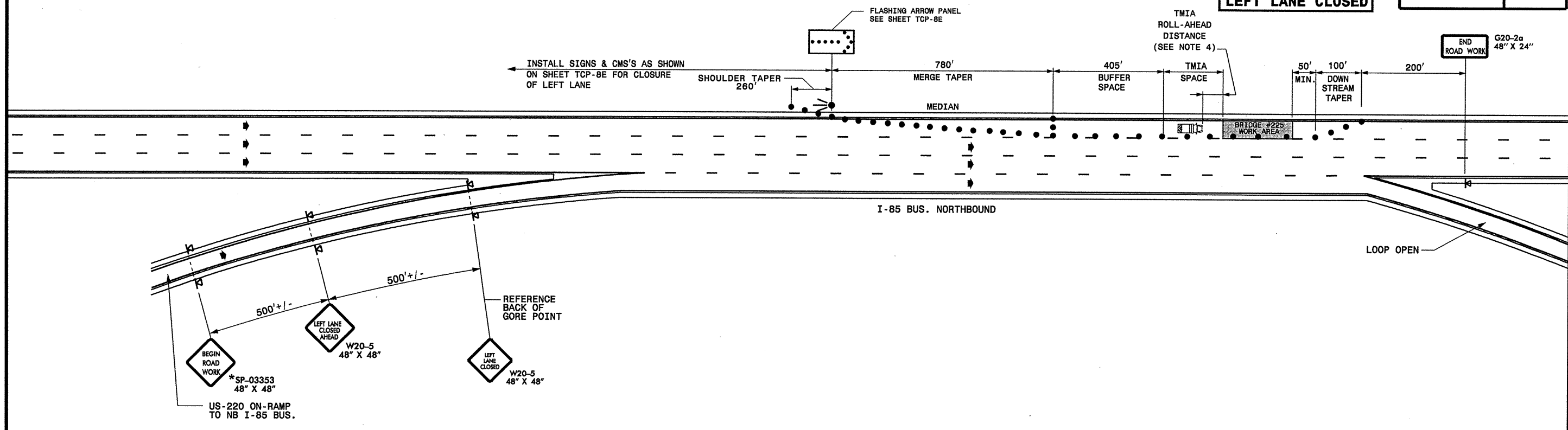
●—●—●—● DETOUR ROUTE

⊥ STATIONARY SIGN

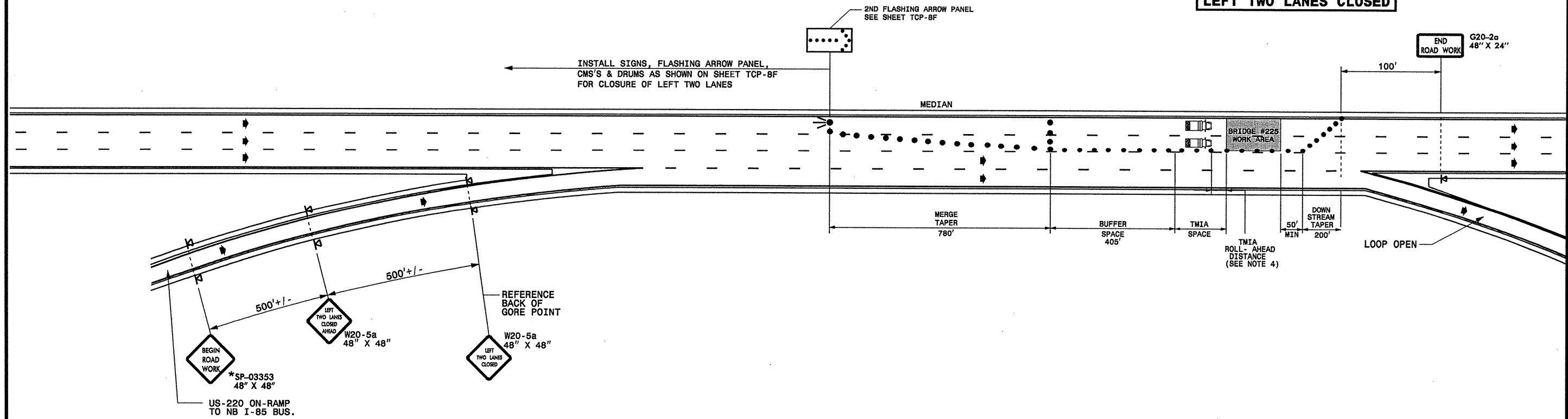
24-OCT-2006 14:37
 \\dot\dfsroot01\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-1-top8b.dgn
 kbroodwell AT W2TC224240

| | | | |
|-----------|------------------|--|-----------|
| APPROVED: | DATE: 29 OCT 06 | REHOBETH CH. RD./BRIDGE #225 SOUTHBOUND I-85 BUS. REHOBETH CH. RD. ON-LOOP CLOSED - DETOUR | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KP | | |
| | DESIGN BY: KP | | |
| | REVIEWED BY: JWG | | |

LEFT LANE CLOSED



LEFT TWO LANES CLOSED



- NOTES**
- REFER TO NOTES ON SHEET TCP-8E.
 - MOUNT SIGNS SHOWN A MINIMUM OF 5 FEET ABOVE THE PAVEMENT ELEVATION.

LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DESIGN

APPROVED: *[Signature]* DATE: 2-20-06

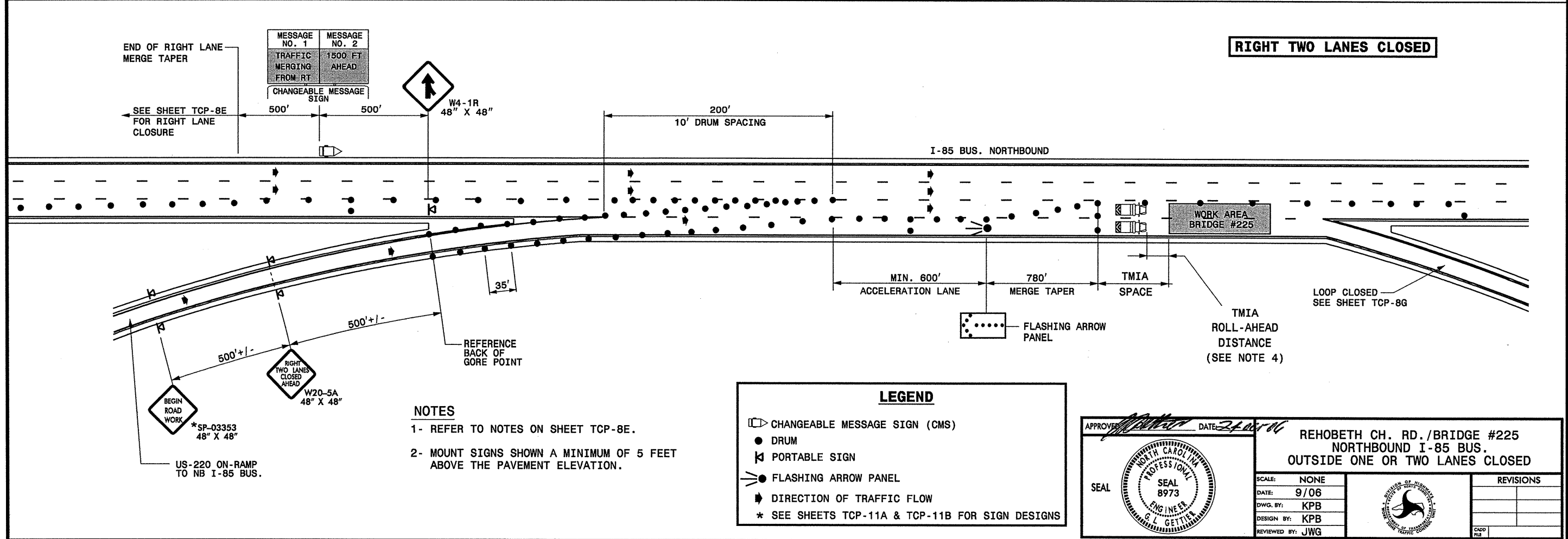
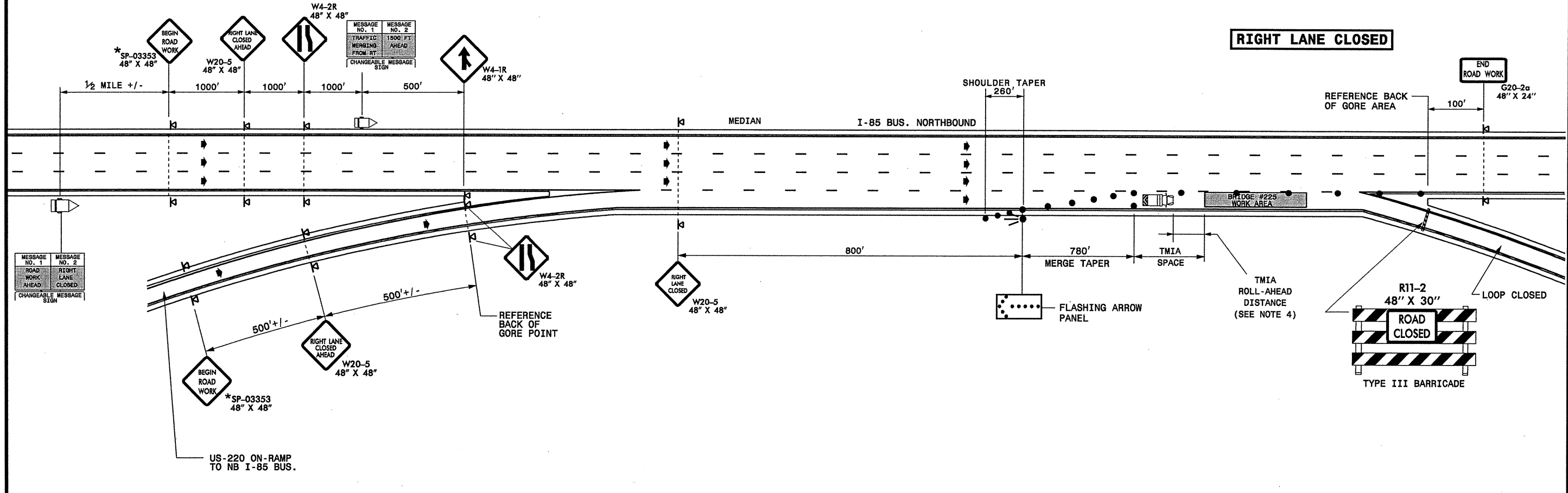
SEAL

**REHOBETH CH. RD./BRIDGE #225
NORTHBOUND I-85 BUS.
INSIDE ONE OR TWO LANE CLOSED**

| | | |
|----------------|--|-----------|
| SCALE: NONE | | REVISIONS |
| DATE: 10/06 | | |
| DWG. BY: KPB | | |
| DESIGN BY: KPB | | |

REVIEWED BY: JWG

24-OCT-2006 14:37
 \\dot\dfsroot\0\insproj\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-i-tcp8c.dgn
 kbroadwell AT WZTC24240



- NOTES**
- REFER TO NOTES ON SHEET TCP-8E.
 - MOUNT SIGNS SHOWN A MINIMUM OF 5 FEET ABOVE THE PAVEMENT ELEVATION.

LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- FLASHING ARROW PANEL
- DIRECTION OF TRAFFIC FLOW
- SEE SHEETS TCP-11A & TCP-11B FOR SIGN DESIGNS

APPROVED: [Signature] DATE: 2/2/06

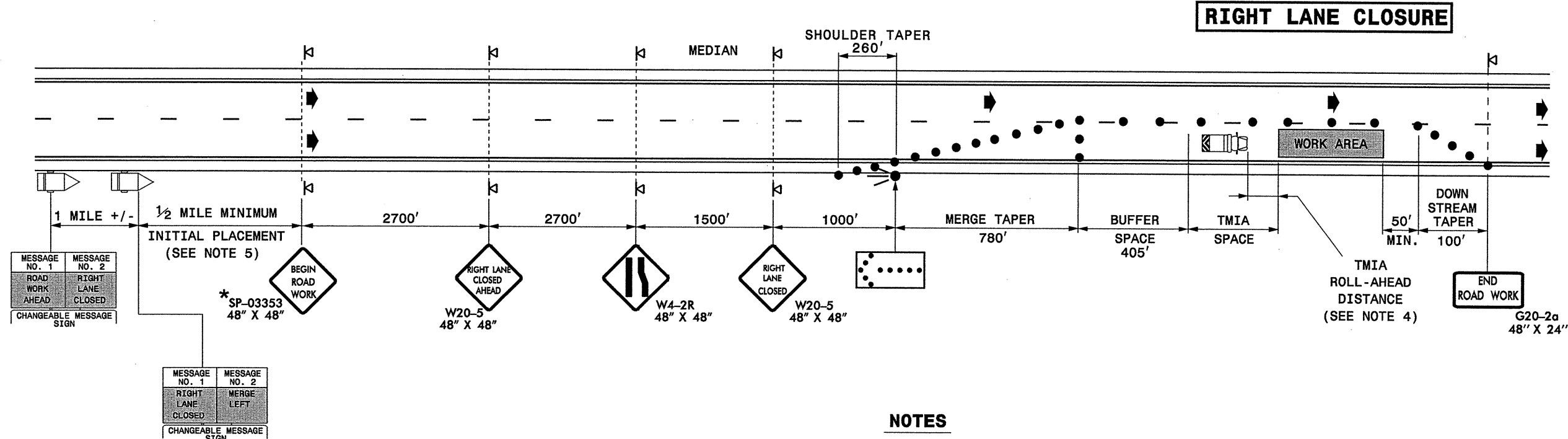
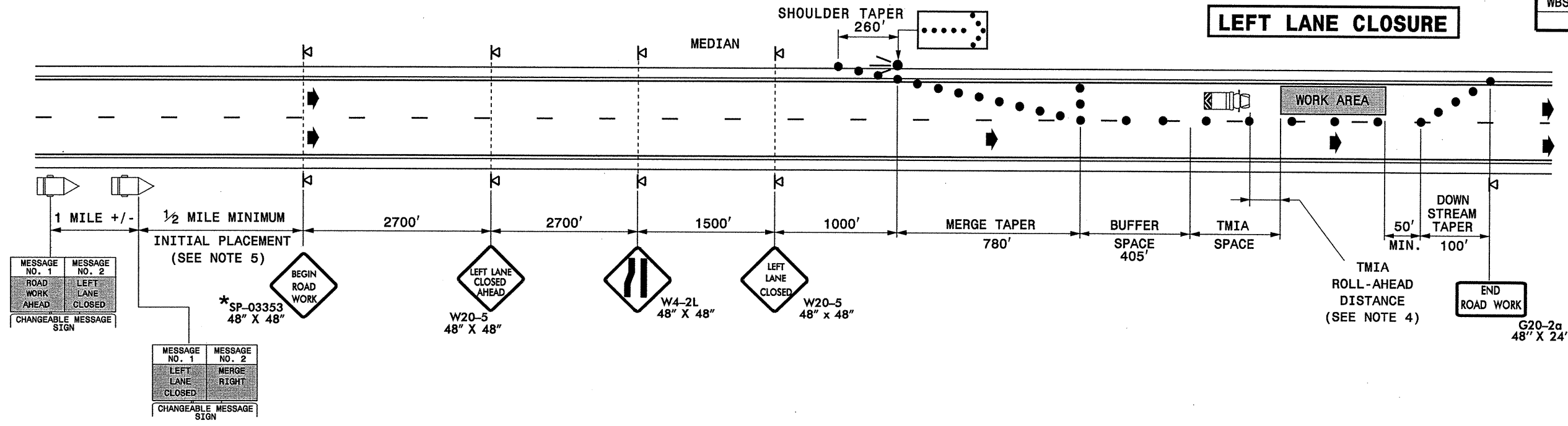
REHOBETH CH. RD./BRIDGE #225
NORTHBOUND I-85 BUS.
OUTSIDE ONE OR TWO LANES CLOSED

SCALE: NONE
DATE: 9/06
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

SEAL: NORTH CAROLINA PROFESSIONAL ENGINEER SEAL 8973 G. L. GETTNER

| REVISIONS |
|-----------|
| |
| |
| |

24-OCT-2006 14:37 \\dot\dfsroot\0\unsp\o\special\36727.3\N\Traffic\36727.3.5\greensboro\bp-40-2006-i-tcp8d.dgn kbr-oadwell AT WZTC224240



NOTES

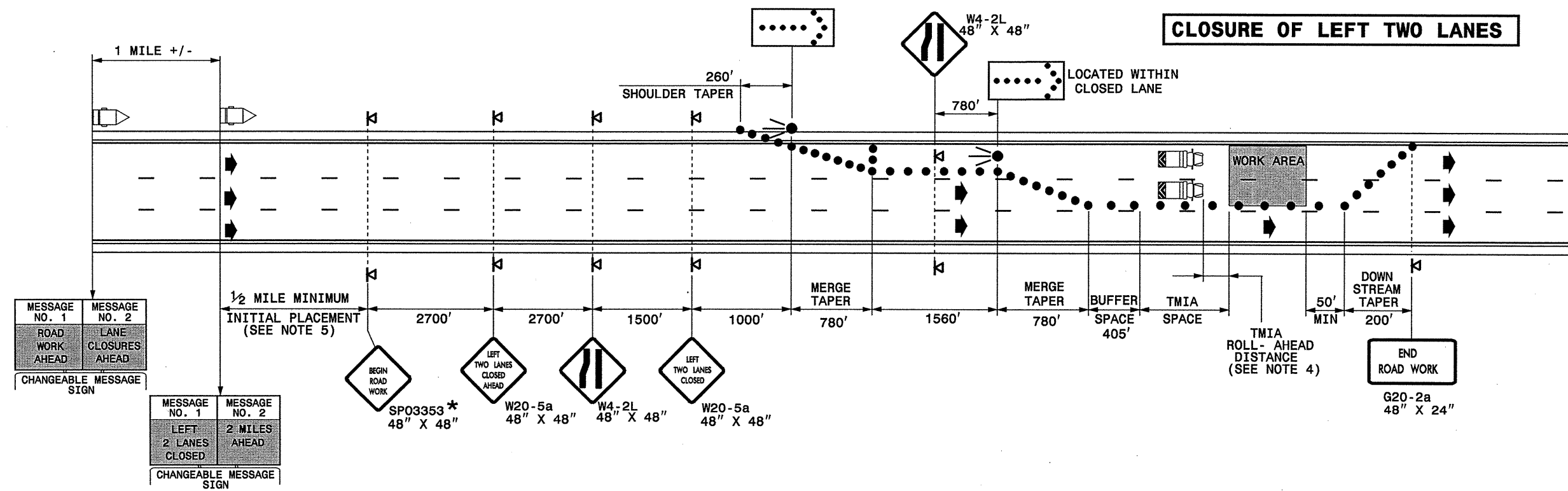
- 1- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 2- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 5- PLACE CHANGEABLE MESSAGE SIGNS (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. IF TRAFFIC BACKS UP TO WHERE THE CMS IS LOCATED THEN RELOCATE THE CMS 1/2 MILE FROM THE ANTICIPATED BACK UP.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ☐ TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- ☐ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- ⊥ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

| | | | |
|------------------|----------------|---|-----------|
| APPROVED: | DATE: 10/10/06 | REHOBETH CH. RD./BRIDGE #225 TEMPORARY LANE CLOSURE ONE LANE CLOSED | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| REVIEWED BY: JWG | | | |

24-OCT-2006 14:37
 \\dot\dfsroot\01\aspr\special\36727.3\1\traffic\36727.3.5\greensboro\bp-40-2006-l-tcp8e.dgn
 kbroodwell AT WZTC224240



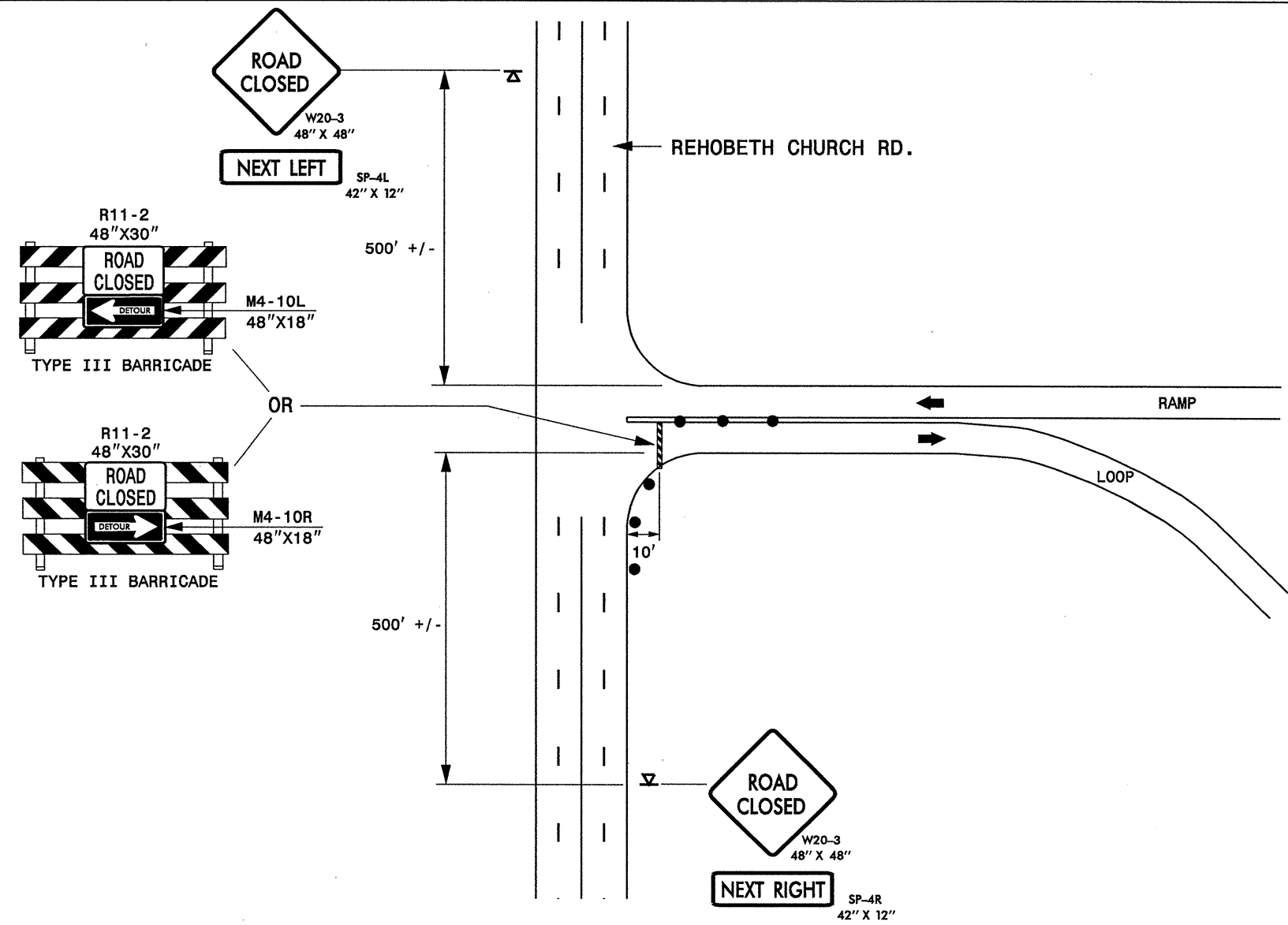
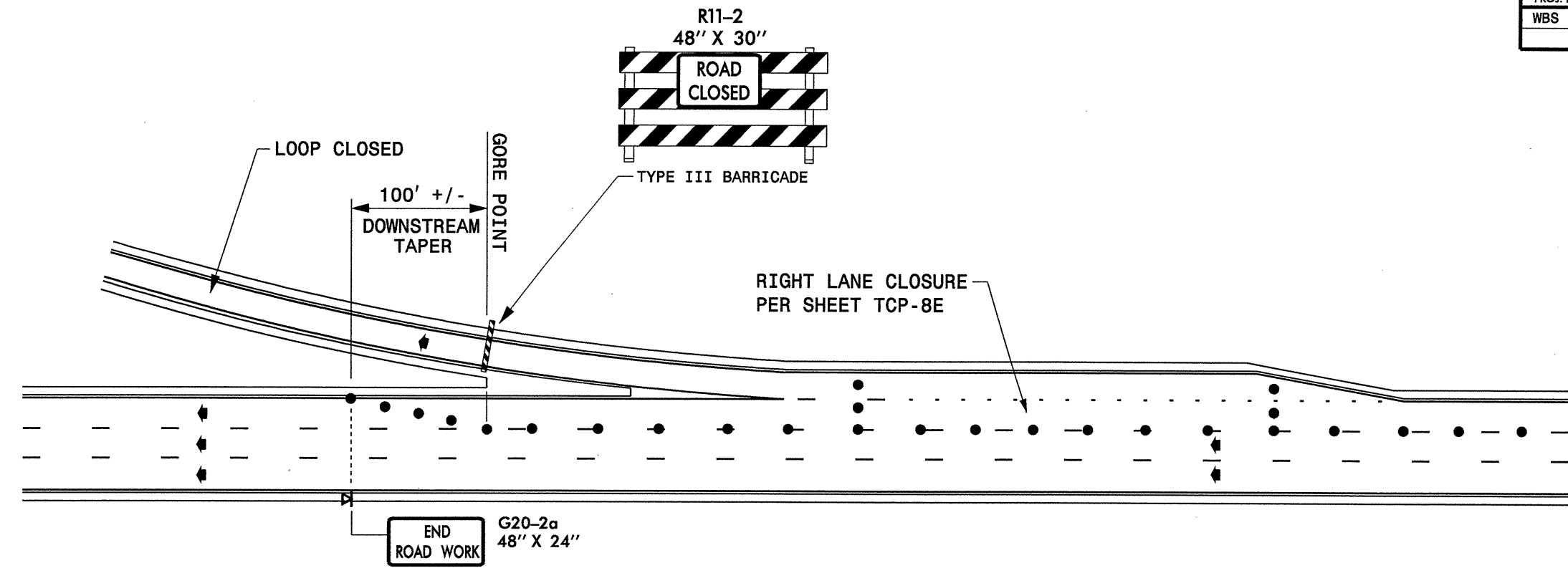
NOTES
1-REFER TO NOTES ON SHEET TCP-8E.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

| APPROVED: | DATE: 10/06 | REHOBETH CH. RD./BRIDGE #225 TEMPORARY LANE CLOSURES INSIDE TWO LANES CLOSED | | | | | | | |
|------------------|----------------|---|--|-----------|--|--|--|--|--|
| | SCALE: NONE | | | | | | | | |
| | DATE: 10/06 | | | | | | | | |
| | DWG. BY: KPB | | | | | | | | |
| | DESIGN BY: KPB | | | | | | | | |
| REVIEWED BY: JWG | | <table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table> | | REVISIONS | | | | | |
| REVISIONS | | | | | | | | | |
| | | | | | | | | | |
| | | | | | | | | | |

24-OCT-2006 14:37 \\dot\dfsroot\insproj\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-1-top8f.dgn kbroodwell AT WZTC224240



NOTES

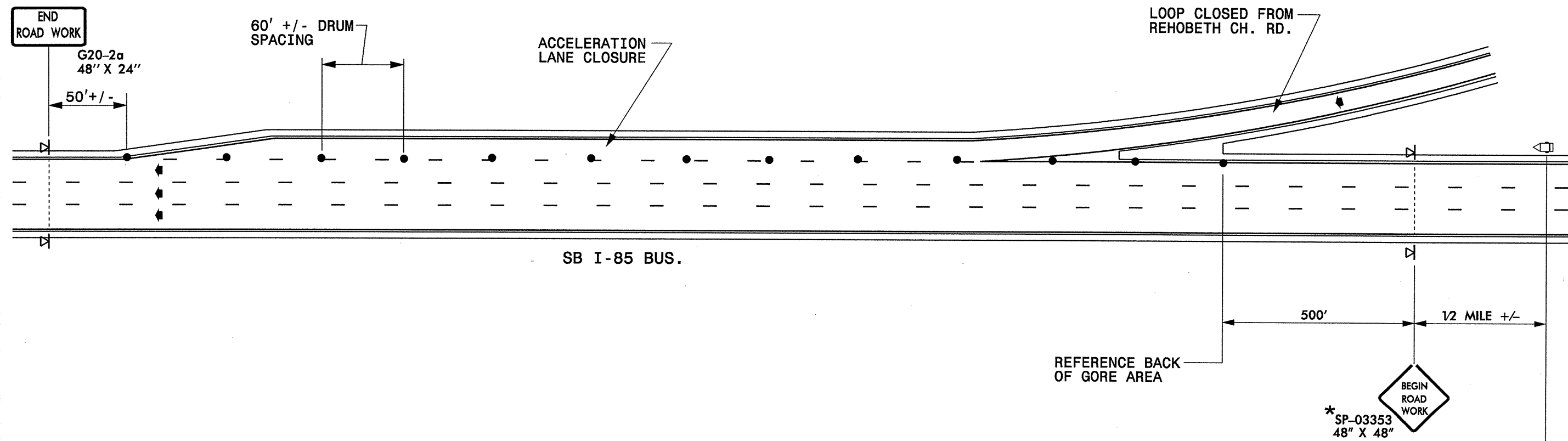
- 1- PROVIDE A 21 CALENDAR DAY NOTICE TO THE ENGINEER PRIOR TO A ROAD BEING CLOSED SUCH THAT NECESSARY PROVISIONS CAN BE MADE TO INFORM LOCAL EMERGENCY AND LAW ENFORCEMENT PERSONNEL, OR ANY OTHER PARTIES AFFECTED BY THE ROAD CLOSURE.
- 2- INSTALL SIGNS BEFORE BARRICADES WHEN CLOSING THE ROADWAY TO TRAFFIC. REMOVE BARRICADES BEFORE SIGNS WHEN OPENING THE ROADWAY TO TRAFFIC.
3. POSITION BARRICADES SUCH THAT THE STRIPES ARE SLOPED DOWNWARD IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN IN DETOURING.

LEGEND

- DRUM
- ◀ PORTABLE SIGN
- ◀ DIRECTION OF TRAFFIC FLOW

| APPROVED: <i>[Signature]</i> DATE: 24 Oct 06 | REHOBETH CH. RD./BRIDGE #225 ROAD CLOSURE AT LOOPS | | | | | | | | | |
|--|---|--|-----------|--|--|--|--|--|--|--|
| | | | | | | | | | | |
| SCALE: NONE | DATE: 10/06 | <table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table> | REVISIONS | | | | | | | |
| REVISIONS | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| DWG. BY: KP | DESIGN BY: KP | REVIEWED BY: JWG | | | | | | | | |

24-OCT-2006 14:38
 \\dot\dfsroot01\inspro\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-l-top8g.dgn
 kbr\codwell AT W2TC224240



SB I-85 BUS.

REFERENCE BACK OF GORE AREA

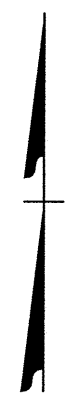
* SP-03353
48" X 48"

| | |
|-------------------------|-------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | PLEASE STAY ALERT |
| CHANGEABLE MESSAGE SIGN | |

| LEGEND | |
|---|---------------------------|
| ● | DRUM |
| ◆ | DIRECTION OF TRAFFIC FLOW |
| ▽ | PORTABLE SIGN |
| ◁ | CHANGEABLE MESSAGE SIGN |
| * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS | |

| | | | |
|------------------|-------------|------------------------------------|-----------|
| APPROVED: | DATE: 10/06 | REHOBETH CH. RD./BRIDGE #225 | |
| | | TEMPORARY ACCELERATION LANE CLOSED | |
| | | SCALE: NONE | REVISIONS |
| DWG. BY: KPB | | | |
| DESIGN BY: KPB | | | |
| REVIEWED BY: JWG | | | |

24-OCT-2006 14:38
 \\dot\dfsroot\01\nsproj\special\36727.3\Traffic\36727.3.5\greensboro\bp-40-2006-l-top8h.dgn
 kbr\oadwell_AT_WZTC224240



Creek Ridge Rd

BRIDGE
H261

CLOSE LEFT OR RIGHT LANE USING SHEET TCP-9H.
CLOSE LEFT TWO OR RIGHT TWO LANES USING SHEET TCP-9I.

PHASING

PHASE 1

STEP 1: PERFORM PAINT REMOVAL AND PAINTING OPERATIONS AS SHOWN IN THE CONTRACT AND CONSTRUCTION PLANS IN ACCORDANCE WITH THE "NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES - JULY 2006" AND USING SHEETS TCP-9A THRU TCP-9I.

NORTHBOUND I-85 BUS.




- CLOSE NB INSIDE/OUTSIDE LANE: SEE SHEETS TCP-9A & TCP-9H.
- CLOSE NB INSIDE/OUTSIDE TWO LANES: SEE SHEETS TCP-9A & TCP-9I.

SOUTHBOUND I-85 BUS.

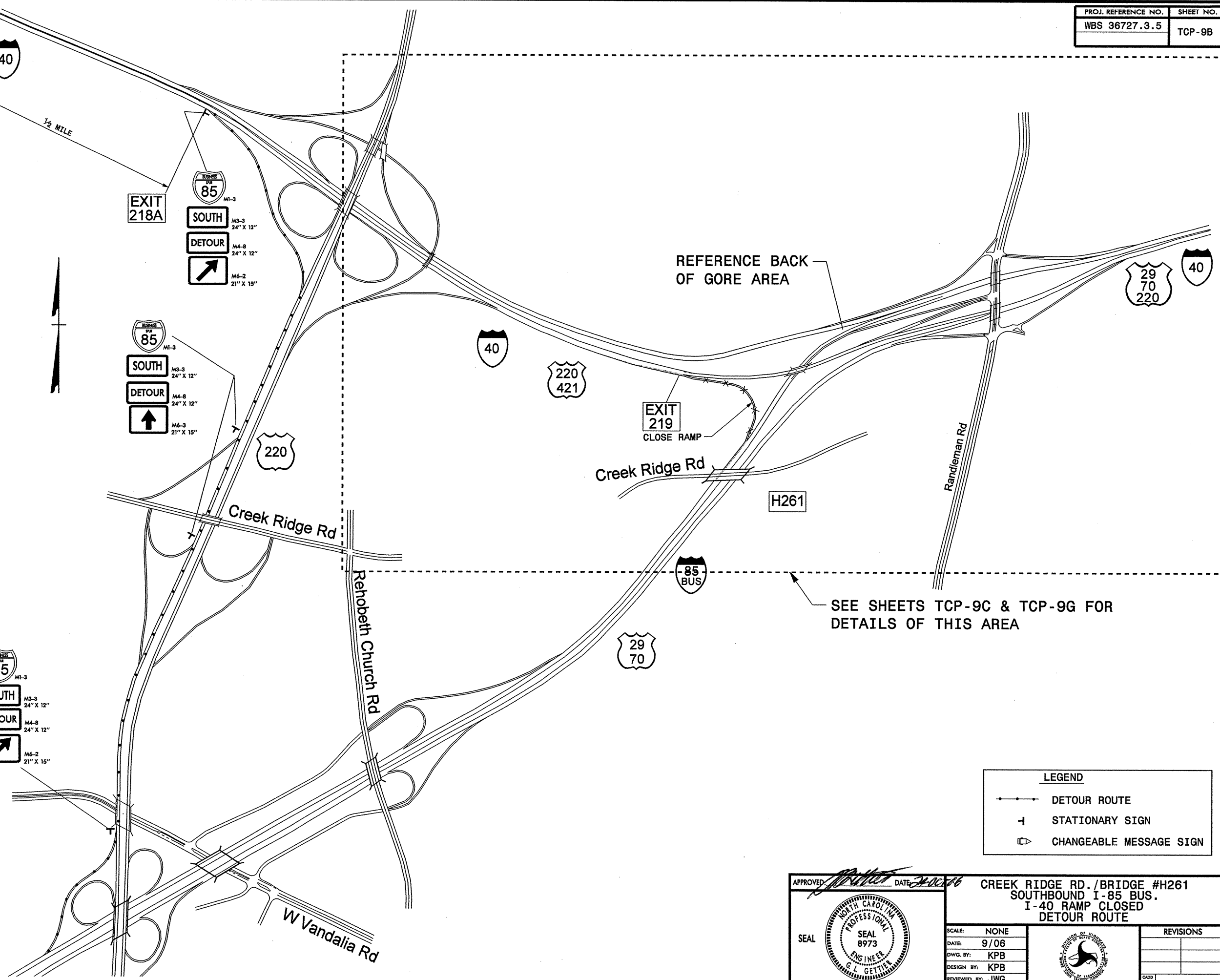
- CLOSE SB ON-RAMP ACCELERATION LANE FROM I-40 EAST: SEE SHEETS TCP-9B, TCP-9C & TCP-9G.
- CLOSE SB OUTSIDE LANE: SEE SHEETS TCP-9B, TCP-9C & TCP-9H.
- CLOSE SB INSIDE LANE: SEE SHEETS TCP-9D & TCP-9E.
- CLOSE SB INSIDE TWO LANES: SEE SHEETS TCP-9D, TCP-9F & TCP-9H.



24-OCT-2006 14:38
 \\dot\dfsroot\01\sp\proj\special\36727.3.5\greensboro\bp-40-2006-1-tcp9a.dgn
 kbroadwell AT WZTC24240

| | | | |
|---|-----------------|---|-----------|
| APPROVED:  | DATE: 24 OCT 06 | CREEK RIDGE RD./BRIDGE #H261 NORTHBOUND I-85 BUS. DETAIL PLAN | |
|  | SCALE: NONE |  | REVISIONS |
| | DATE: 9/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| REVIEWED BY: JWG | | | |

| | |
|-------------------------|----------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| I-85 BUS SOUTH TRAFFIC | TAKE NEXT EXIT |
| CHANGEABLE MESSAGE SIGN | |



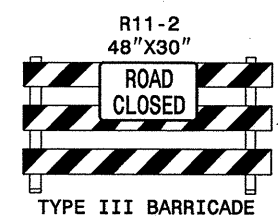
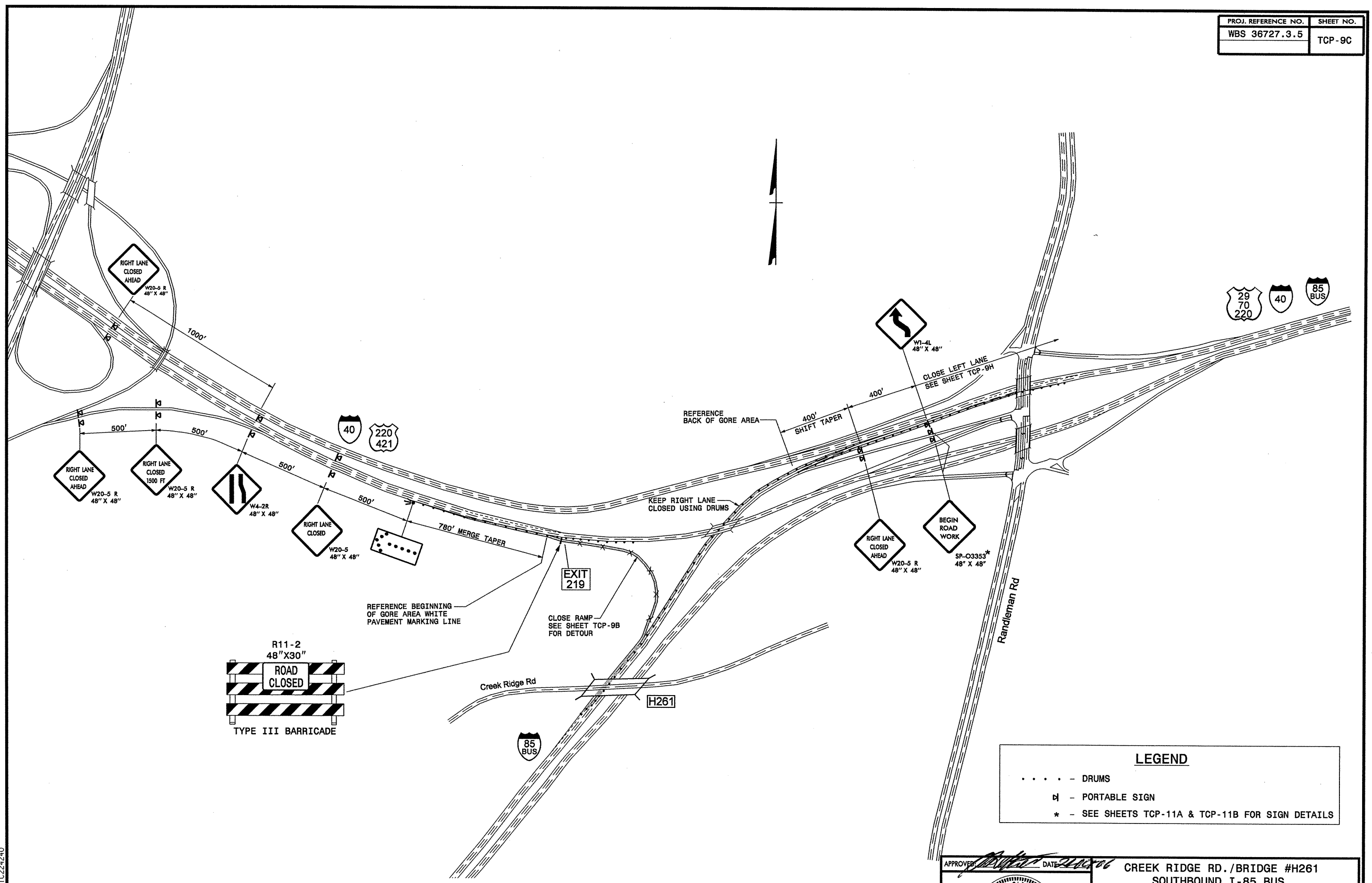
SEE SHEETS TCP-9C & TCP-9G FOR DETAILS OF THIS AREA

| LEGEND | |
|--------|-------------------------|
| | DETOUR ROUTE |
| | STATIONARY SIGN |
| | CHANGEABLE MESSAGE SIGN |

| APPROVED: | DATE: 9/06/06 | CREEK RIDGE RD./BRIDGE #H261 SOUTHBOUND I-85 BUS. I-40 RAMP CLOSED DETOUR ROUTE | | | | | | | | |
|--------------|----------------|--|-----------|--|--|--|--|--|--|--|
| | | | | | | | | | | |
| SCALE: NONE | DATE: 9/06 | <table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table> | REVISIONS | | | | | | | |
| REVISIONS | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| DWG. BY: KPB | DESIGN BY: KPB | REVIEWED BY: JWG | | | | | | | | |

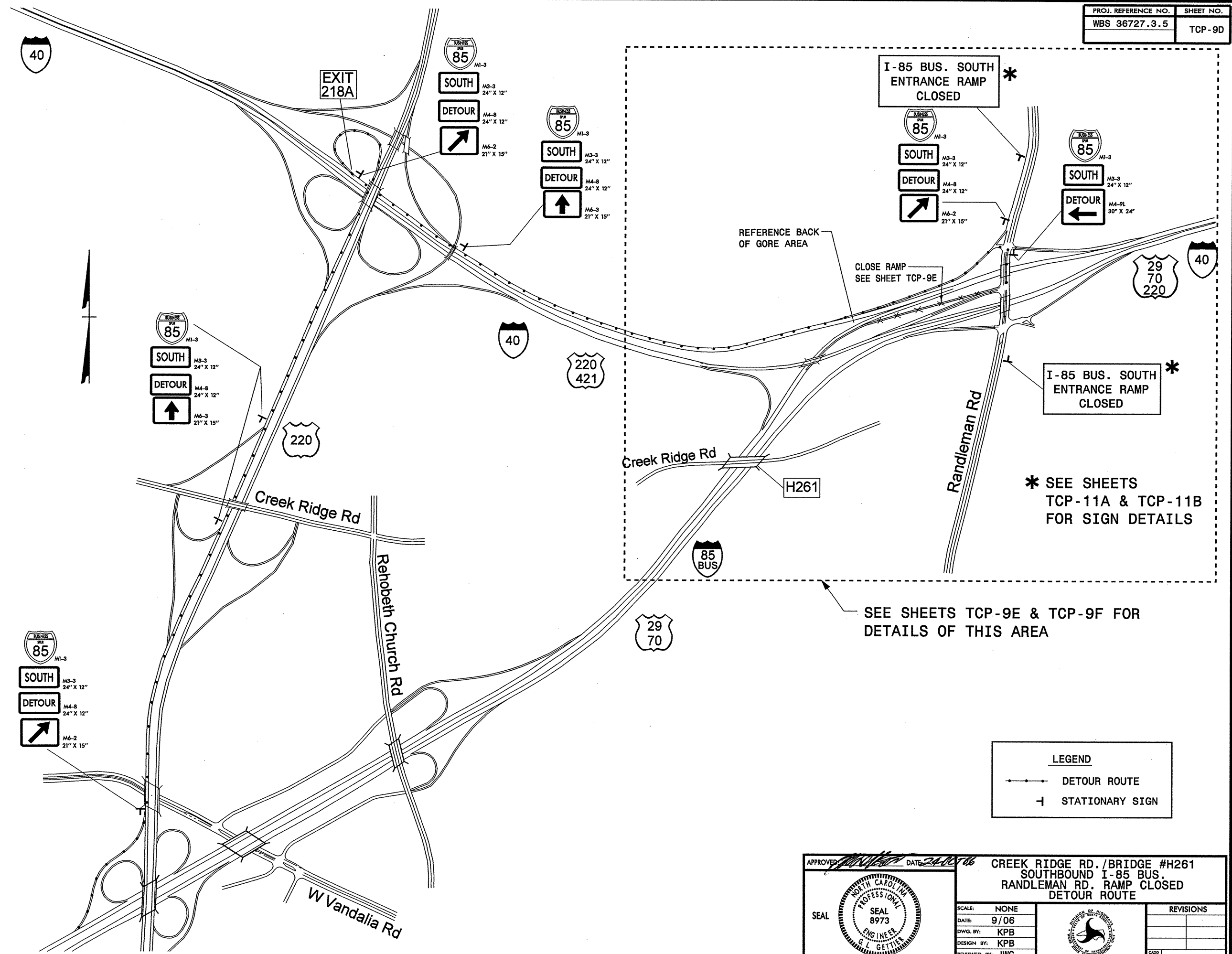
24-OCT-2006 14:38
 \\dot\dfsroot\inspro\special\36727.3\traffic\36727.3.5\greensbor\bp-40-2006-1-top9b.dgn
 kbr\oadwell_AT_W2TC224240

24-OCT-2006 14:39
 \\dot\dfsroot01\mspro\special\36727.3.1\traffic\36727.3.5\greensboro\bp-40-2006-l-tcp9c.dgn
 Kbr\oodwell AT WZTC224240



| LEGEND | |
|--------|---|
| | DRUMS |
| ⊠ | PORTABLE SIGN |
| * | SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS |

| APPROVED: | DATE: 9/20/06 | CREEK RIDGE RD./BRIDGE #H261 SOUTHBOUND I-85 BUS. RIGHT LANE & I-40 RAMP CLOSED | | | | | | | | | |
|--------------|--|--|-----------|------|-------|------|----------|-----|------------|-----|--------------|
| | <table border="1"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>9/06</td> </tr> <tr> <td>DWG. BY:</td> <td>KPB</td> </tr> <tr> <td>DESIGN BY:</td> <td>KPB</td> </tr> <tr> <td>REVIEWED BY:</td> <td>JWG</td> </tr> </table> | | SCALE: | NONE | DATE: | 9/06 | DWG. BY: | KPB | DESIGN BY: | KPB | REVIEWED BY: |
| SCALE: | NONE | | | | | | | | | | |
| DATE: | 9/06 | | | | | | | | | | |
| DWG. BY: | KPB | | | | | | | | | | |
| DESIGN BY: | KPB | | | | | | | | | | |
| REVIEWED BY: | JWG | | | | | | | | | | |
| | | <table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | REVISIONS | | | | | | | | |
| REVISIONS | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |



MI-3
 M3-3 24" X 12"
 M4-8 24" X 12"
 M6-2 21" X 15"

MI-3
 M3-3 24" X 12"
 M4-8 24" X 12"
 M6-3 21" X 15"

MI-3
 M3-3 24" X 12"
 M4-8 24" X 12"
 M6-2 21" X 15"

MI-3
 M3-3 24" X 12"
 M4-8 24" X 12"
 M6-3 21" X 15"

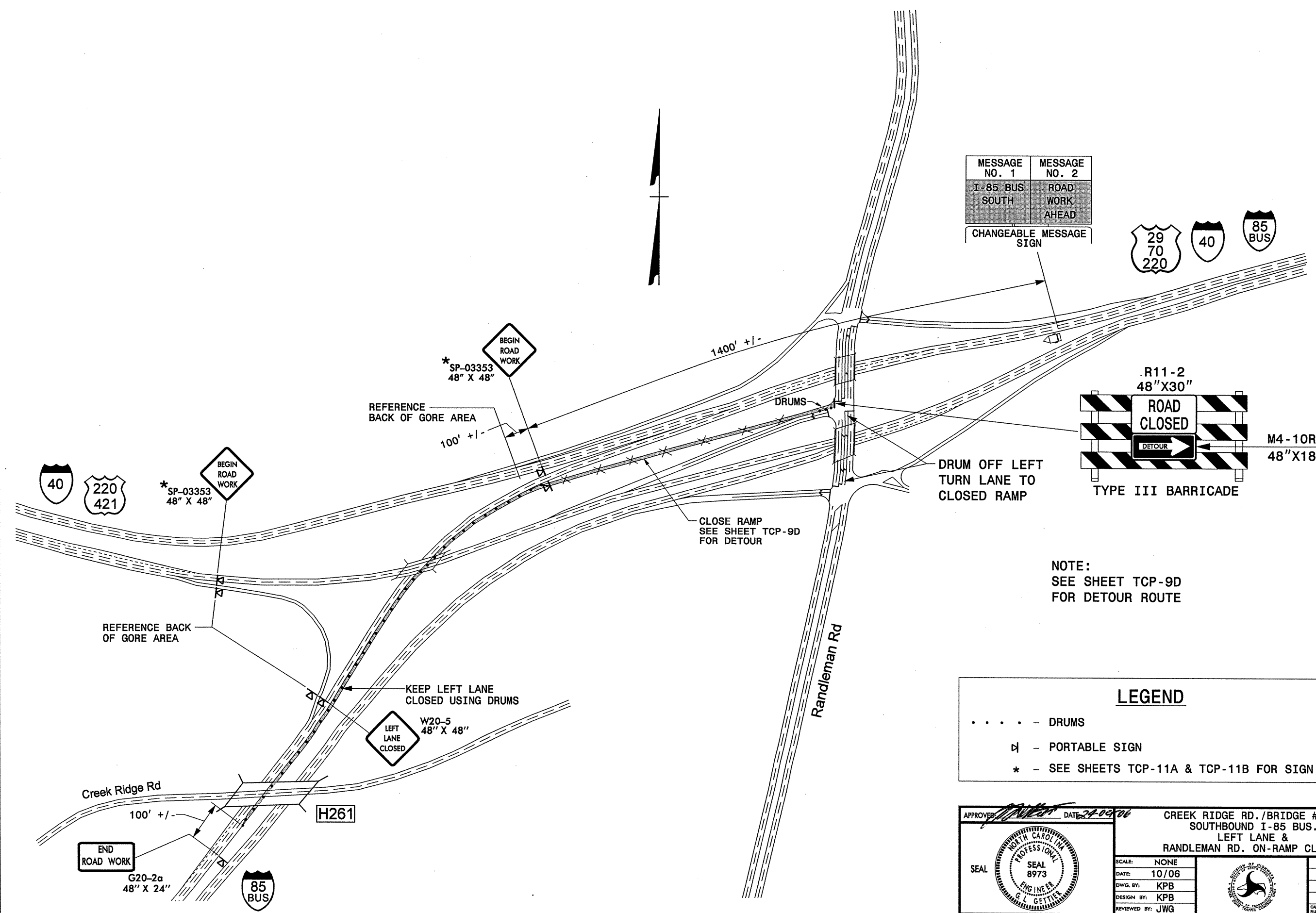
MI-3
 M3-3 24" X 12"
 M4-8 24" X 12"
 M6-2 21" X 15"

MI-3
 M3-3 24" X 12"
 M4-9L 30" X 24"

| LEGEND | |
|--------|-----------------|
| | DETOUR ROUTE |
| | STATIONARY SIGN |

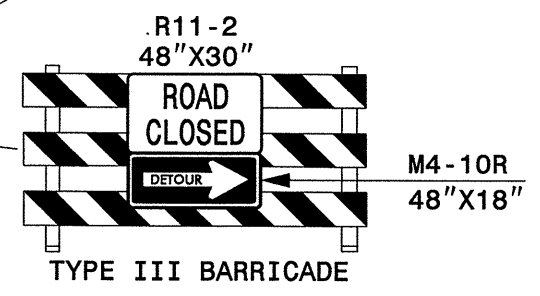
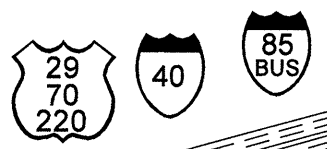
| | | | | | | | |
|------------------|----------------|---|--|--|--|--|--|
| APPROVED: | DATE: 24/06/06 | CREEK RIDGE RD./BRIDGE #H261 SOUTHBOUND I-85 BUS. RANDLEMAN RD. RAMP CLOSED DETOUR ROUTE | | | | | |
| | SCALE: NONE | | | | | | |
| | DATE: 9/06 | | | | | | |
| | DWG. BY: KPB | | | | | | |
| | DESIGN BY: KPB | | | | | | |
| REVIEWED BY: JWG | REVISIONS | <table border="1"> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | | | | | |
| | | | | | | | |
| | | | | | | | |

24-OCT-2006 14:39
 \\dot\dfsroot\insproj\special\36727.3.5\greensboro\bp-40-2006-1-tcp9d.dgn
 kbr\oodwell AT WZTC224240



| | |
|----------------|-----------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| I-85 BUS SOUTH | ROAD WORK AHEAD |

CHANGEABLE MESSAGE SIGN

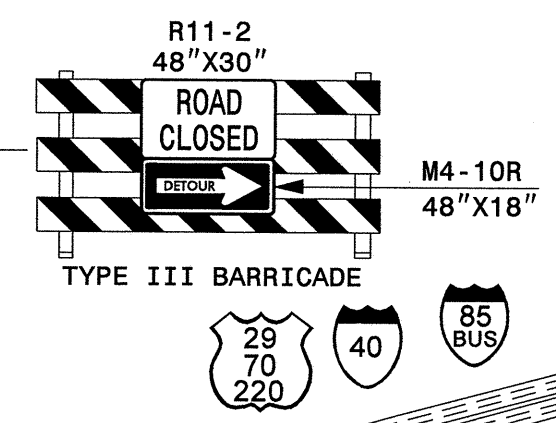
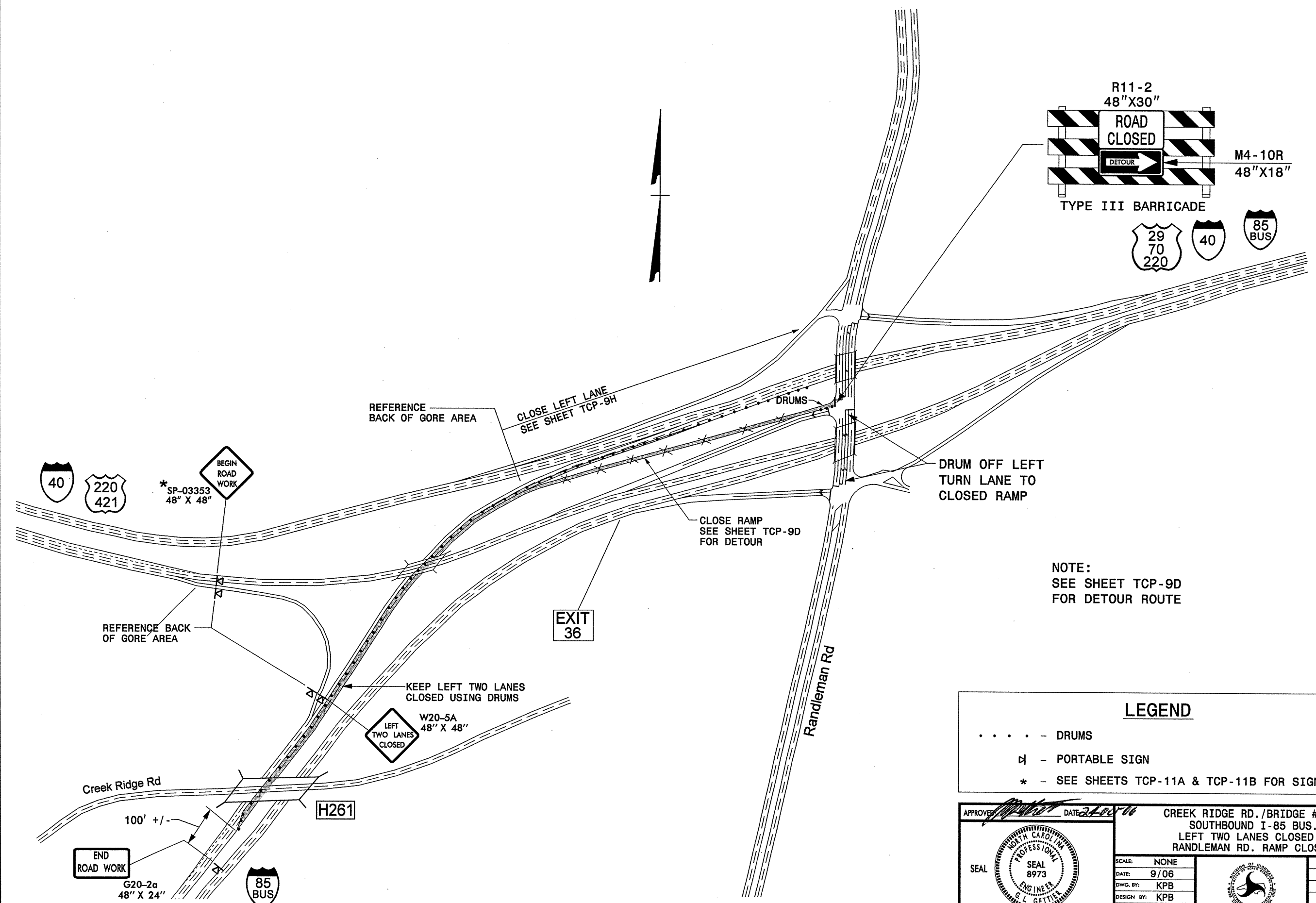


NOTE:
SEE SHEET TCP-9D
FOR DETOUR ROUTE

| LEGEND | |
|--------|---|
| | DRUMS |
| ▤ | PORTABLE SIGN |
| * | SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS |

| APPROVED: <i>[Signature]</i> DATE: 10/06/06 | CREEK RIDGE RD./BRIDGE #H261 SOUTHBOUND I-85 BUS. LEFT LANE & RANDLEMAN RD. ON-RAMP CLOSED | | | | | | | | | | |
|---|---|-----------|------|-------|-------|----------|-----|------------|-----|--------------|-----|
| | <table border="1"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>10/06</td> </tr> <tr> <td>DWG. BY:</td> <td>KPB</td> </tr> <tr> <td>DESIGN BY:</td> <td>KPB</td> </tr> <tr> <td>REVIEWED BY:</td> <td>JWG</td> </tr> </table> | SCALE: | NONE | DATE: | 10/06 | DWG. BY: | KPB | DESIGN BY: | KPB | REVIEWED BY: | JWG |
| SCALE: | NONE | | | | | | | | | | |
| DATE: | 10/06 | | | | | | | | | | |
| DWG. BY: | KPB | | | | | | | | | | |
| DESIGN BY: | KPB | | | | | | | | | | |
| REVIEWED BY: | JWG | | | | | | | | | | |
| | <table border="1"> <tr> <th>REVISIONS</th> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> </table> | REVISIONS | | | | | | | | | |
| REVISIONS | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |

24-OCT-2006 14:39
 \\dot\dfsroot\inspr\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006+tcp9e.dgn
 kbrodwell AT WZTC24240



REFERENCE BACK OF GORE AREA
CLOSE LEFT LANE
SEE SHEET TCP-9H

CLOSE RAMP
SEE SHEET TCP-9D
FOR DETOUR

DRUM OFF LEFT
TURN LANE TO
CLOSED RAMP

NOTE:
SEE SHEET TCP-9D
FOR DETOUR ROUTE

REFERENCE BACK
OF GORE AREA

KEEP LEFT TWO LANES
CLOSED USING DRUMS

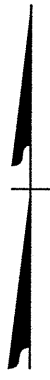
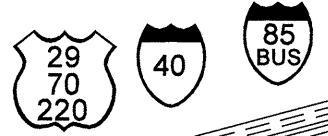
Creek Ridge Rd

END
ROAD WORK

| LEGEND | |
|--------|---|
| | DRUMS |
| ⏏ | PORTABLE SIGN |
| * | SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS |

| APPROVED: | DATE: 2/20/06 | CREEK RIDGE RD./BRIDGE #H261 SOUTHBOUND I-85 BUS. LEFT TWO LANES CLOSED & RANDLEMAN RD. RAMP CLOSED | | | | | | | | | |
|--------------|--|---|-----------|------|-------|------|----------|-----|------------|-----|--------------|
| | <table border="1"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>9/06</td> </tr> <tr> <td>DWG. BY:</td> <td>KPB</td> </tr> <tr> <td>DESIGN BY:</td> <td>KPB</td> </tr> <tr> <td>REVIEWED BY:</td> <td>JWG</td> </tr> </table> | | SCALE: | NONE | DATE: | 9/06 | DWG. BY: | KPB | DESIGN BY: | KPB | REVIEWED BY: |
| SCALE: | NONE | | | | | | | | | | |
| DATE: | 9/06 | | | | | | | | | | |
| DWG. BY: | KPB | | | | | | | | | | |
| DESIGN BY: | KPB | | | | | | | | | | |
| REVIEWED BY: | JWG | | | | | | | | | | |
| | | <table border="1"> <tr> <th>REVISIONS</th> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> </table> | REVISIONS | | | | | | | | |
| REVISIONS | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |

24-OCT-2006 14:39
 \\dot.dfsr\root\01\ans\proj\36727.3.5\traffic\36727.3.5\greensboro\bp-40-2006-tcp9f.dgn
 kbr\cadwell AT WZTC224240



FOR RAMP CLOSURE
SEE SHEET TCP-9C

CLOSE RAMP
SEE SHEET TCP-9B
FOR DETOUR

* SP-03353
48" X 48"
BEGIN
ROAD
WORK

| | |
|----------------------------|-----------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| I-85 BUS SOUTH | ROAD WORK AHEAD |
| CHANGEABLE MESSAGE SIGN | |

NOTE:
POSITION CMS SO THAT
IT IS VISIBLE FROM BOTH
ON-RAMPS

REFERENCE
BACK OF GORE AREA

Creek Ridge Rd

60'+/- DRUM
SPACING

H261

100' +/-

END
ROAD WORK

G20-2a
48" X 24"

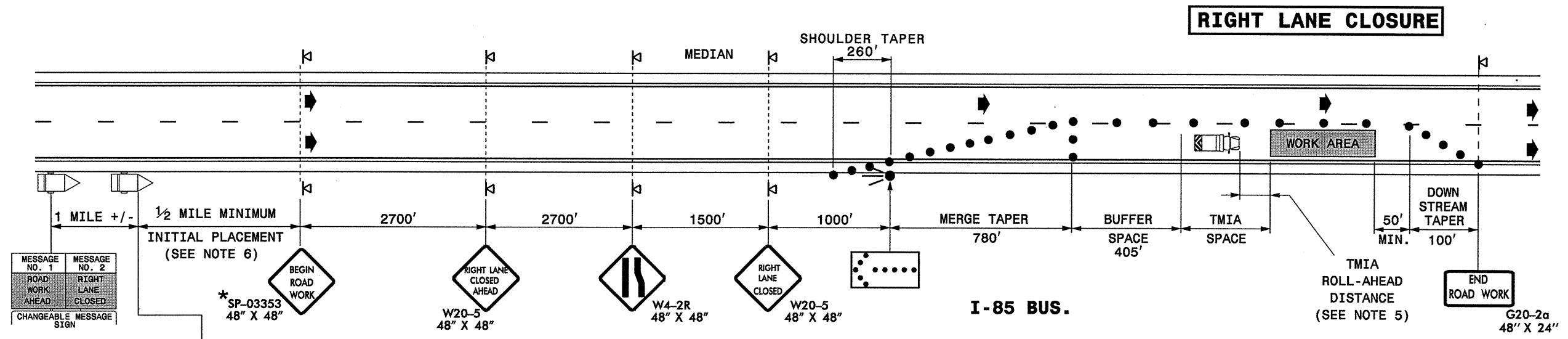
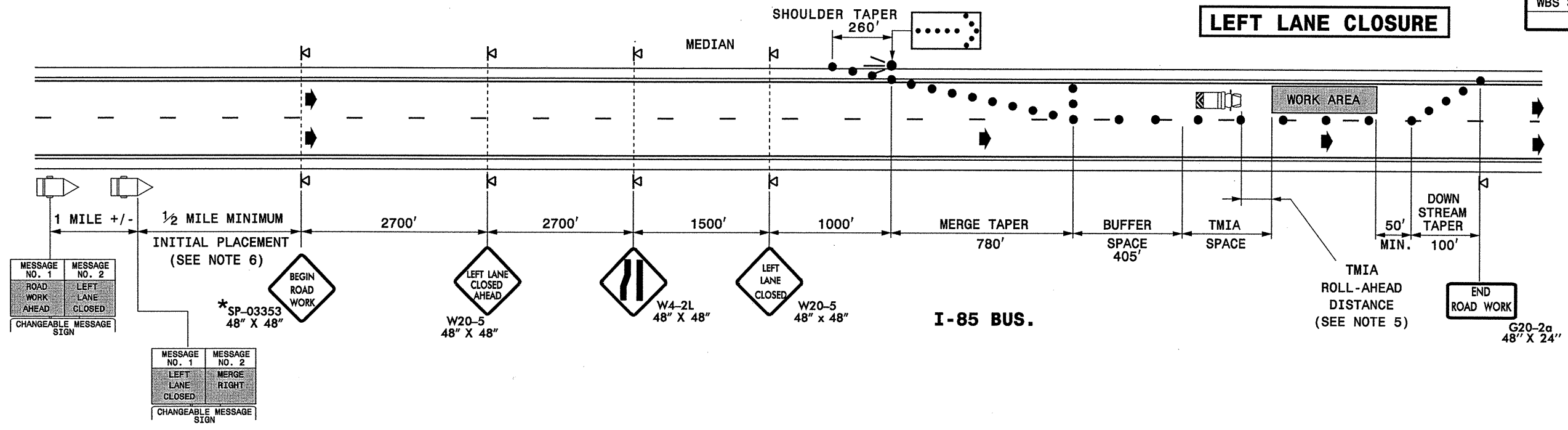


Randleman Rd

| LEGEND | |
|--------|---|
| | DRUMS |
| ▤ | PORTABLE SIGN |
| * | SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS |

| | | | |
|-----------|------------------|---|-----------|
| APPROVED: | DATE: 10/06 | CREEK RIDGE RD./BRIDGE #H261 SOUTHBOUND I-85 BUS. I-40 EAST ON-RAMP ACCELERATION LANE CLOSED | |
| | SCALE: NONE | | REVISIONS |
| | DATE: 10/06 | | |
| | DWG. BY: KPB | | |
| | DESIGN BY: KPB | | |
| | REVIEWED BY: JWG | | |

24-OCT-2006 14:39
 \\dot\dfsroot\dot\special\36727.3\Traffic\36727.3.5\greensbor-o\bp-40-2006-1-tcp9g.dgn
 kbroodwell AT WZTC224240



NOTES

- 1- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 2- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- OMITTED.
- 4- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 5- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 6- PLACE CHANGEABLE MESSAGE SIGNS (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. IF TRAFFIC BACKS UP TO WHERE THE CMS IS LOCATED THEN RELOCATE THE CMS 1/2 MILE FROM THE ANTICIPATED BACK UP.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ☐ TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- ☐ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- ⏏ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

APPROVED: *[Signature]* DATE: 10/06

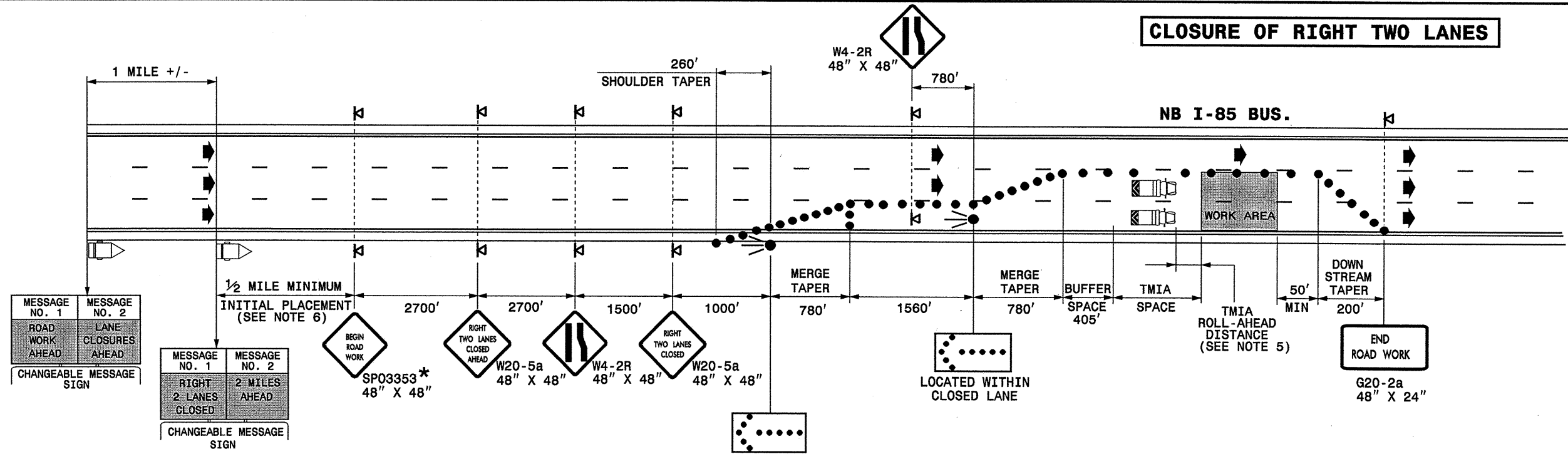
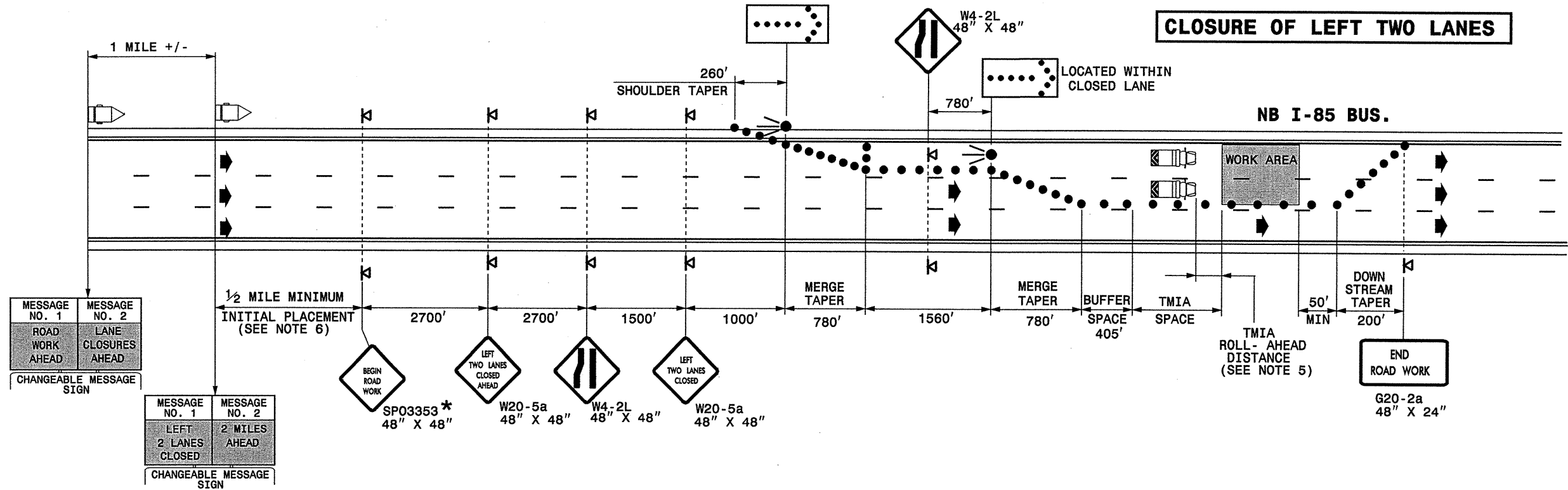
CREEK RIDGE RD./BRIDGE #H261
TEMPORARY LANE CLOSURE
ONE LANE CLOSED

SCALE: NONE
DATE: 10/06
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

SEAL: NORTH CAROLINA PROFESSIONAL ENGINEER SEAL 8973 G. L. GETTIE

| REVISIONS | |
|-----------|--|
| | |
| | |

24-OCT-2006 14:40 \\dot\dfsroot\0\nsproj\special\36727.3.1\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp9h.dgn kbroadwell AT WZTC224240



GENERAL NOTES

1-REFER TO NOTES ON SHEET TCP-9H.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DETAILS

APPROVED: DATE: 2-10-06

**CREEK RIDGE RD./BRIDGE #H261
TEMPORARY LANE CLOSURES
TWO LANES CLOSED**

SCALE: NONE

DATE: 10/06

DWG. BY: KPB

DESIGN BY: KPB

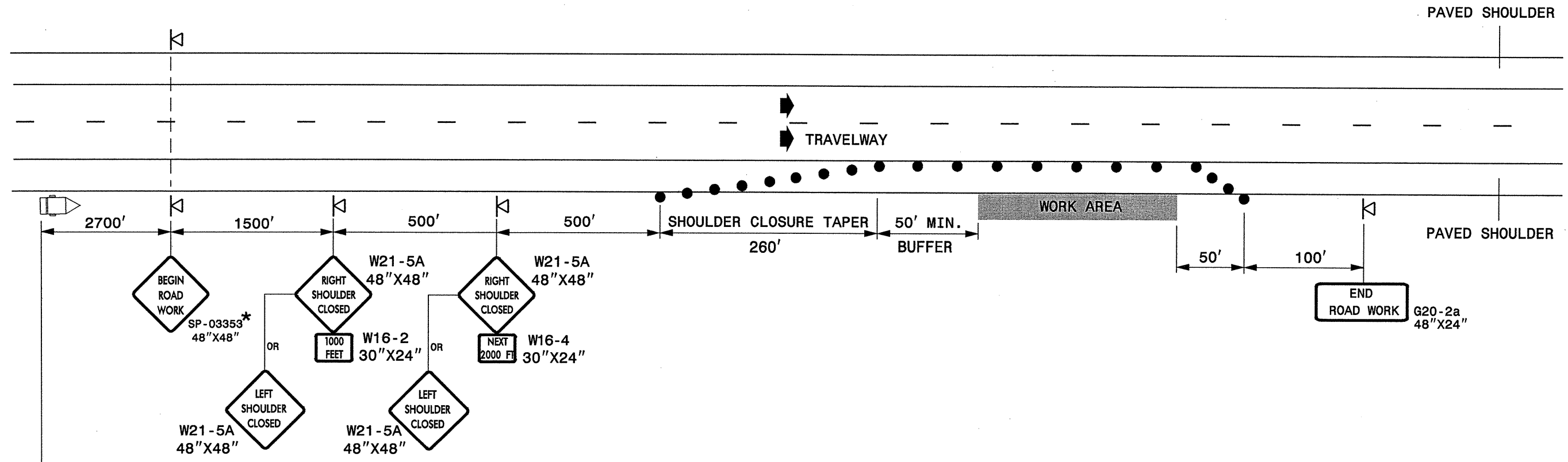
REVIEWED BY: JWG

SEAL

| REVISIONS |
|-----------|
| |
| |
| |

24-OCT-2006 14:40
 \\dot\dfs\oc\of\p\proj\special\36727.3\traffic\36727.3.5\greensboro\bp-40-2006-1-tcp9i.dgn
 kbroadwell AT WZTC224240

SHOULDER CLOSURE ON I-85/I-85 BUS.



| | |
|-----------------|----------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | LEFT SHOULDER CLOSED |

CHANGEABLE MESSAGE SIGN

| | |
|-----------------|-----------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD WORK AHEAD | RIGHT SHOULDER CLOSED |

CHANGEABLE MESSAGE SIGN

OR

GENERAL NOTES

- 1- PLACE SHOULDER CLOSURE SIGNS ON THE SAME SIDE AS THE SHOULDER THAT IS CLOSED.
- 2- PLACE DRUMS IN THE SHOULDER TAPER AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. THE MAXIMUM SPACING OF DRUMS ALONG THE WORK AREA IS EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- DO NOT CLOSE THE SHOULDERS ON RIGHT SIDE AND MEDIAN SIDE OF THE ROADWAY AT THE SAME TIME.

LEGEND

- DRUM
- ◁ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW
- * SEE SHEETS TCP-11A & TCP-11B FOR SIGN DESIGNS

| APPROVED: DATE: 2/20/06 | TEMPORARY SHOULDER CLOSURE | | | | | | | | | | |
|--------------------------|--|-----------|------|-------|-------|----------|-----|------------|-----|--------------|-----|
| | <table border="1" style="width: 100%;"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>10/06</td> </tr> <tr> <td>DWG. BY:</td> <td>KPB</td> </tr> <tr> <td>DESIGN BY:</td> <td>KPB</td> </tr> <tr> <td>REVIEWED BY:</td> <td>JWG</td> </tr> </table> | SCALE: | NONE | DATE: | 10/06 | DWG. BY: | KPB | DESIGN BY: | KPB | REVIEWED BY: | JWG |
| SCALE: | NONE | | | | | | | | | | |
| DATE: | 10/06 | | | | | | | | | | |
| DWG. BY: | KPB | | | | | | | | | | |
| DESIGN BY: | KPB | | | | | | | | | | |
| REVIEWED BY: | JWG | | | | | | | | | | |
| | <table border="1" style="width: 100%;"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | REVISIONS | | | | | | | | | |
| REVISIONS | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |

24-OCT-2006 14:40
 \\dot1\dfs\proj\01\special\36727.3.5\traffic\36727.3.5\greensboroc\bp-40-2006-1-top10.dgn
 kbroadwell AT WZT224240

SIGN NUMBER: SP06417 BACKG COLOR: Fluorescent Orange
 TYPE: PORTABLE COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 4'-0"
 HEIGHT: 4'-0"
 TOTAL AREA: 16 Sq. Ft.

BORDER TYPE: RECESSED
 RECESS: 0.75"
 WIDTH: 1.25"
 RADII: 3"

NO. Z BARS: MAT'L: COMPOSITE
 LENGTH: OR ROLL-UP SUBSTRATES

DESIGN BY: M. TRACEY CHECKED BY: *Sam P. King*
 PROJECT ID: 36727.3.5 DIV: 7 DATE: Oct 11, 2006

BORDER
 R=3"
 TH=1.25"
 IN=0.75"

LETTER POSITIONS

Letter spacings are to start of next letter

| | L | E | F | T | | | | | | Series/Size Text Length |
|--|------|-----|-----|-----|-----|------|-----|-----|------|----------------------------|
| | 22.7 | 5.2 | 5.4 | 4.6 | 4.1 | 22.8 | | | | C 2000 |
| | 3 | | | L | A | N | E | S | | C 2000 |
| | 12.9 | 5 | 7.5 | 4.6 | 6.2 | 6.2 | 5 | 4.5 | 12.9 | 39 |
| | | C | L | O | S | E | D | | | C 2000 |
| | 16 | 6.1 | 5 | 5.9 | 5.8 | 5.4 | 4.5 | 16 | | 32.8 |

Spacing Factor is 1 unless specified otherwise

SIGN NUMBER: SP06418 BACKG COLOR: Fluorescent Orange
 TYPE: PORTABLE COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 4'-0"
 HEIGHT: 4'-0"
 TOTAL AREA: 16 Sq. Ft.

BORDER TYPE: RECESSED
 RECESS: 0.75"
 WIDTH: 1.25"
 RADII: 3"

NO. Z BARS: MAT'L: COMPOSITE
 LENGTH: OR ROLL-UP SUBSTRATES

DESIGN BY: M. TRACEY CHECKED BY: *Sam P. King*
 PROJECT ID: 36727.3.5 DIV: 7 DATE: Oct 11, 2006

BORDER
 R=3"
 TH=1.25"
 IN=0.75"

LETTER POSITIONS

Letter spacings are to start of next letter

| | L | E | F | T | | | | | | Series/Size Text Length |
|--|------|-----|-----|-----|-----|------|------|------|------|----------------------------|
| | 25.2 | 3.9 | 4.1 | 3.4 | 3.1 | 25.2 | | | | C 2000 |
| | 3 | | | L | A | N | E | S | | C 2000 |
| | 17.7 | 3.7 | 5.6 | 3.4 | 4.7 | 4.7 | 3.8 | 3.4 | 17.7 | 29.3 |
| | | C | L | O | S | E | D | | | C 2000 |
| | 20.1 | 4.6 | 3.8 | 4.4 | 4.4 | 4.1 | 3.4 | 20.1 | | 24.6 |
| | | A | H | E | A | D | | | | C 2000 |
| | 21.9 | 4.7 | 4.7 | 3.6 | 4.7 | 3.4 | 21.9 | | | 21 |

Spacing Factor is 1 unless specified otherwise

SIGN NUMBER: SP-03353 BACKG COLOR: Fluorescent Orange
 TYPE: PORTABLE COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 4'-0"
 HEIGHT: 4'-0"
 TOTAL AREA: 16.0 Sq. Ft.

BORDER TYPE: FLUSH
 RECESS: 0.59"
 WIDTH: 0.75"
 RADII: 1.38"

NO. Z BARS: N/A MAT'L: composite
 LENGTH: N/A OR substrate roll-up.

DESIGN BY: CL DOWNEY CHECKED BY: CHECKED STD #: W20-1
 PROJECT ID: ALL PROJECTS DIV: DIV DATE: Aug 20, 2003

BORDER
 R=1.38"
 TH=0.75"
 IN=0.59"

LETTER POSITIONS

Letter spacings are to start of next letter

| | B | E | G | I | N | | | | | Series/Size Text Length |
|--|------|-----|-----|-----|-----|------|------|--|--|----------------------------|
| | 22.4 | 5.3 | 4.6 | 5.4 | 2.5 | 3.8 | 22.4 | | | C7 |
| | | R | O | A | D | | | | | C7 |
| | 23.4 | 5 | 5.2 | 5.6 | 3.8 | 23.4 | | | | 19.6 |
| | | W | O | R | K | | | | | C7 |
| | 22.8 | 6.4 | 5.6 | 5.2 | 4 | 22.8 | | | | 21.2 |

Spacing Factor is 1 unless specified otherwise

APPROVED: *[Signature]* DATE: 2/10/06

SEAL

SPECIAL SIGN DETAILS

SCALE: NONE
 DATE: 8/06
 DWG. BY: KPB
 DESIGN BY: KPB
 REVIEWED BY: JWG

REVISIONS

SEAL 8973
 G. L. GETTNER
 ENGINEER

STATE OF NORTH CAROLINA
 PROFESSIONAL ENGINEER

24-OCT-2006 14:40
 \\dot\dfsroot\0\inspr\special\36727.3.5\traffic\36727.3.5\greensboro\bp-40-2006-l-tcp11.dgn
 kbrodwell AT WZTC224240

SIGN NUMBER: SP06413 BACKG COLOR: Flourescent Orange
 TYPE: STATIONARY COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 7'-6"
 HEIGHT: 3'-0"
 TOTAL AREA: 22.5 Sq.Ft.

BORDER TYPE: RECESSED
 RECESS: 0.63"
 WIDTH: 0.88"
 RADII: 2.25"

NO. Z BARS: MAT'L: 0.125" ALUMINUM OR COMPOSITE
 LENGTH:

DESIGN BY: M. TRACEY CHECKED BY: *San P. King* DATE: Oct 11, 2006
 PROJECT ID: 36727.3.5 DIV: 7

NOTES
 1. Legend and border shall be direct applied non-reflective sheeting.
 2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

| Letter spacings are to start of next letter | | | | | | | | | | | Series/Size |
|---|-----|-----|------|-----|------|-----|-----|-----|-----|--|-------------|
| | | | | | | | | | | | Text Length |
| S | H | O | L | D | E | N | | | | | E 2000 |
| 8.4 | 8.6 | 9.5 | 9.7 | 10 | 8.7 | 9.7 | 8.9 | 8.1 | 8.4 | | 73.2 |
| R | O | A | D | | | | | | | | E 2000 |
| 25.9 | 9.3 | 9.4 | 11.4 | 8.1 | 25.9 | | | | | | 38.2 |

Spacing Factor is 0.7

SIGN NUMBER: SP06414 BACKG COLOR: Flourescent Orange
 TYPE: STATIONARY COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 8'-0"
 HEIGHT: 3'-0"
 TOTAL AREA: 24.0 Sq.Ft.

BORDER TYPE: RECESSED
 RECESS: 0.63"
 WIDTH: 0.88"
 RADII: 2.25"

NO. Z BARS: MAT'L: 0.125" ALUMINUM OR COMPOSITE
 LENGTH:

DESIGN BY: M. TRACEY CHECKED BY: *San P. King* DATE: Oct 11, 2006
 PROJECT ID: 36727.3.5 DIV: 7

NOTES
 1. Legend and border shall be direct applied non-reflective sheeting.
 2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

| Letter spacings are to start of next letter | | | | | | | | | | | Series/Size |
|---|-----|-----|-----|-----|-----|-----|-----|-----|------|------|-------------|
| | | | | | | | | | | | Text Length |
| R | E | H | O | B | E | T | H | | | | D 2000 |
| 16.2 | 8.3 | 7.7 | 8.8 | 9.1 | 8.3 | 6.9 | 7.6 | 6.8 | 16.2 | | 63.5 |
| C | H | U | R | C | H | R | D | | | | D 2000 |
| 10.2 | 8.8 | 9 | 9 | 8.2 | 8.8 | 7.9 | 8.9 | 8.3 | 6.8 | 10.2 | 75.6 |

Spacing Factor is 0.9

SIGN NUMBER: SP06415 BACKG COLOR: Flourescent Orange
 TYPE: STATIONARY COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 6'-6"
 HEIGHT: 3'-0"
 TOTAL AREA: 19.5 Sq.Ft.

BORDER TYPE: RECESSED
 RECESS: 0.63"
 WIDTH: 0.88"
 RADII: 2.25"

NO. Z BARS: MAT'L: 0.125" ALUMINUM OR COMPOSITE
 LENGTH:

DESIGN BY: M. TRACEY CHECKED BY: *San P. King* DATE: Sep 25, 2006
 PROJECT ID: 36727.3.5 DIV: 7

NOTES
 1. Legend and border shall be direct applied non-reflective sheeting.
 2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

| Letter spacings are to start of next letter | | | | | | | | | | | Series/Size | | | | | |
|---|-----|-----|-----|-----|-----|-----|------|-----|-----|---|-------------|--------|-----|-----|-----|--------|
| | | | | | | | | | | | Text Length | | | | | |
| I | - | 8 | 5 | B | U | S | S | O | U | T | H | D 2000 | | | | |
| 6.7 | 1.9 | 2.9 | 5.3 | 4.7 | 5.4 | 5.1 | 5.1 | 4.4 | 5.7 | 5 | 5.6 | 5 | 4.6 | 4.1 | 6.7 | 64.6 |
| E | N | T | R | A | N | C | E | R | A | M | P | | | | | D 2000 |
| 5.9 | 4.7 | 5 | 4.6 | 4.6 | 6 | 5.4 | 4 | 5.7 | 4.6 | 6 | 6.1 | 4.1 | 5.9 | | | 66.2 |
| C | L | O | S | E | D | | | | | | | | | | | D 2000 |
| 24.5 | 5.4 | 4.5 | 5.2 | 5.1 | 4.7 | 4.1 | 24.5 | | | | | | | | | 29 |

Spacing Factor is 1 unless specified otherwise

SIGN NUMBER: SP06416 BACKG COLOR: Flourescent Orange
 TYPE: STATIONARY COPY COLOR: Black
 QUANTITY: SEE PLANS

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

SIGN WIDTH: 6'-6"
 HEIGHT: 3'-0"
 TOTAL AREA: 19.5 Sq.Ft.

BORDER TYPE: RECESSED
 RECESS: 0.63"
 WIDTH: 0.88"
 RADII: 2.25"

NO. Z BARS: MAT'L: 0.125" ALUMINUM OR COMPOSITE
 LENGTH:

DESIGN BY: M. TRACEY CHECKED BY: *San P. King* DATE: Sep 25, 2006
 PROJECT ID: 36727.3.5 DIV: 7

NOTES
 1. Legend and border shall be direct applied non-reflective sheeting.
 2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

| Letter spacings are to start of next letter | | | | | | | | | | | Series/Size | | | | | |
|---|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-------------|--------|-----|-----|-----|--------|
| | | | | | | | | | | | Text Length | | | | | |
| I | - | 8 | 5 | B | U | S | S | O | U | T | H | D 2000 | | | | |
| 6.7 | 1.9 | 2.9 | 5.3 | 4.7 | 5.4 | 5.1 | 5.1 | 4.4 | 5.7 | 5 | 5.6 | 5 | 4.6 | 4.1 | 6.7 | 64.6 |
| E | N | T | R | A | N | C | E | L | O | O | P | | | | | D 2000 |
| 6.5 | 4.7 | 5 | 4.6 | 4.6 | 6 | 5.4 | 5.4 | 4 | 5.7 | 4.5 | 5.5 | 5.6 | 4.1 | 6.5 | | 65 |
| C | L | O | S | E | D | A | H | E | A | D | | | | | | D 2000 |
| 8.6 | 5.4 | 4.5 | 5.2 | 5.1 | 4.7 | 4.7 | 5.4 | 6 | 5.5 | 4.2 | 6 | 4.1 | 8.6 | | | 60.8 |

Spacing Factor is 1 unless specified otherwise

24-OCT-2006 14:40 \\dot\dfsroot0\special\36727.3.5\greensboro\bp-40-2006-1-tcp11.dgn

APPROVED: *[Signature]* DATE: 24 OCT 06

SEAL: SPECIAL SIGN DETAILS

| | |
|------------------|-----------|
| SCALE: NONE | REVISIONS |
| DATE: 8/06 | |
| DWG. BY: KPB | |
| DESIGN BY: KPB | |
| REVIEWED BY: JWG | |