# STATE OF NORTH CAROLINA DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO. B - 4 0 4 1

# PLAN FOR PROPOSED TRAFFIC CONTROL, MARKING & DELINEATION

# BURKE COUNTY

SHEET NO.

## ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" -ROADWAY DESIGN UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JANUARY 2002 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1135.01	CONES
1150.01	FLAGGERS
1160.01	TEMPORARY CRASH CUSHION
1170.01	PORTABLE CONCRETE BARRIER (NCHRP 230)
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

# INDEX OF SHEETS

TITLE

TCP - 1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, AND INDEX OF SHEETS
TCP-2	GENERAL NOTES AND PHASING
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TCP-4	PHASE II DETAILS
TCP-5	DRUMS
TCP-6	TYPE III BARRICADES
TCP-7	SHOULDER CLOSURES
TCP-8	ADVANCE WORK ZONE WARNING SIGNS
PM - 1	PAVEMENT MARKING PLAN

## LEGEND

### **GENERAL**

DIRECTION OF TRAFFIC FLOW

— PROPOSED PVMT. ----- EXIST. PVMT.

WORK AREA

REMOVAL OF EXISTING PAVEMENT

### TRAFFIC CONTROL DEVICES

T TYPE I BARRICADE

TYPE III BARRICADE

CONE

FLASHING ARROW PANEL (TYPE C)

TYPE 'B' WARNING LIGHT

— STATIONARY SIGN

PORTABLE SIGN

● STATIONARY OR PORTABLE SIGN

WARNING FLAGS

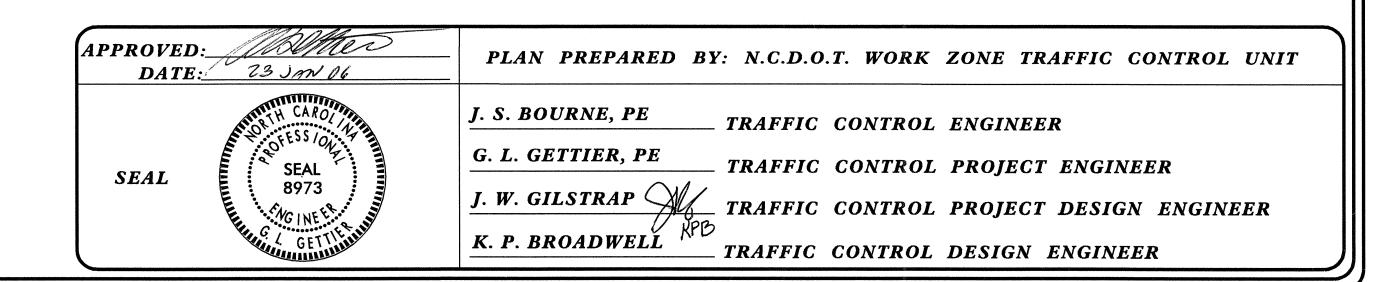
---- CRASH CUSHION

CHANGEABLE MESSAGE SIGN

TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)

POLICE

**FLAGGER** 



B-4041 TCP-2

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

A) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

#### LANE AND SHOULDER CLOSURE REQUIREMENTS

- B) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- C) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING DETAIL ON SHEET TCP-7 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- F) DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.
- G) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.
- H) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

### PAVEMENT EDGE DROP OFF REQUIREMENTS

I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER. AT NO EXPENSE TO THE DEPARTMENT.

J) DO NOT EXCEED A DIFFERENCE OF 1.5 INCHES IN ELEVATION
BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN
LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE
EVERY MILE THROUGHOUT THE UNEVEN AREA.

#### TRAFFIC PATTERN ALTERATIONS

K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

#### SIGNING

L) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

- M) PROVIDE PERMANENT SIGNING.
- N) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

#### TRAFFIC BARRIER

O) INSTALL MOVABLE/PORTABLE CONCRETE BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE MOVABLE/PORTABLE CONCRETE BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

ONCE MOVABLE/PORTABLE CONCRETE BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE MOVABLE/PORTABLE CONCRETE BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET MOVABLE/PORTABLE CONCRETE BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

P) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

> POSTED SPEED LIMIT LESS THAN 50 MPH 50 MPH or HIGHER

MINIMUM OFFSET 20 FT 30 FT

INSTALL MOVABLE/PORTABLE CONCRETE BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE MOVABLE/PORTABLE CONCRETE BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP CLOSED THE SECTION OF THE ROADWAY UNTIL THE BARRIER CAN BE PLACED OR AFTER BARRIER IS REMOVED.

#### TRAFFIC CONTROL DEVICES

- Q) WHEN USING ROADWAY STANDARD NO. 1101.02, DRUMS MAY BE USED IN LIEU OF CONES ON ALL ROADS.
- R) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.
- S) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

### PAVEMENT MARKINGS AND MARKERS

(i) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME

MARKING

MARKER

ALL ROADS

PAINT

NONE

) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME

ALL ROADS

MARKING

<u>MARKER</u>

TEMPORARY RAISED

V) PLACE AT LEAST TWO APPLICATIONS OF PAINT PAVEMENT MARKINGS ON THE FINAL WEARING SURFACE ON NEW ASPHALT PAVEMENT. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

PAINT

- TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- K) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.
- Y) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

# **PHASING**

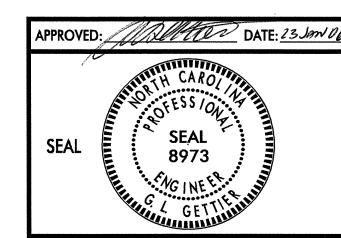
#### PHASE I

- STEP 1: PRIOR TO BEGINNING ANY WORK THE CONTRACTOR SHALL INSTALL ADVANCE WORK ZONE WARNING SIGNS AS SHOWN ON SHEET TCP-8.
- STEP 2: USING RSD 1101.02 SHEET 1 OF 7 INSTALL TEMPORARY GUARDRAIL AS SHOWN ON THE CONSTRUCTION PLANS.
- STEP 3: CONTRACTOR SHALL CONSTRUCT THE RIGHT SIDE OF THE PROPOSED -L-ROADWAY AND STAGE I OF THE PROPOSED CULVERT AS SHOWN ON SHEET TCP-3 AND AS DESCRIBED BELOW. SEE CONSTRUCTION PLANS.
  - -- USING RSD 1101.02 SHEET 1 OF 7 CONSTRUCT UP TO THE EDGE AND ELEVATION OF THE EXISTING PAVEMENT FROM STA. 10+00 -L- TO STA. 11+65+/- -L- AND FROM STA. 14+70+/- -L- TO STA. 16+81.78 -L-.
  - NOTE:
    INSTALL DRUMS AND TYPE III BARRICADES TO KEEP THE PROPOSED ROAD CLOSED TO TRAFFIC.
  - -- USING RSD 1101.02 SHEET 1 OF 7 INSTALL TEMPORARY SHORING FROM STA. 12+70+/- -L-, CONSTRUCT UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. 11+65+/- -L- TO STA. 14+70+/- -L-, INCLUDING STAGE I OF THE PROPOSED CULVERT, UTILIZING 2:1 SLOPES FROM STA. 11+65 +/- -L- TO STA. 12+50+/- -L-, AND CONSTRUCT THE SOIL FABRIC WALL FROM STA. 12+55+/- -L- TO STA. 13+05+/- -L-.
  - -- INSTALL PCB AND CRASH CUSHIONS FROM STA. 12+65+/- -L- TO STA. 13+75+/- -L-.

#### PHASE II

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK OF PHASE II, STEP 1 IN ONE WORK PERIOD.

- STEP 1: CONTRACTOR SHALL, USING RSD 1101.02 SHEET 1 OF 7, CONSTRUCT (SEE CONSTRUCTION PLANS AND SHEET TCP-4):
  - -- TIE-INS FROM STA. 10+00 -L- TO STA. 12+40+/- -L-, STA. 14+70+/- -L- TO STA. 16+81.78 -L- AND TEMPORARY TIE-IN TO
  - -- PLACE TEMPOARARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FROM 10+00-L- TO STA. 16+81.78 -L- AND OPEN TO TRAFFIC IN A TWO LANE, TWO WAY PATTERN.
  - -- PLACE DRUMS AND TYPE III BARRICADES TO CLOSE EXISTING TO TRAFFIC.
- STEP 2: USING RSD 1101.02 SHEET 1 OF 7 CONSTRUCT STAGE II OF THE PROPOSED CULVERT, THE LEFT SIDE OF -L- FROM STA. 10+00 -L- TO STA. 16+81.78 -L- AND THE REMAINDER OF -Y- (SEE CONSTRUCTION PLANS AND SHEET TCP-4).
- STEP 3: USING RSD 1101.02 SHEET 1 OF 7 REMOVE THE PORTABLE CONCRETE BARRIER, PLACE THE FINAL LAYER OF SURFACE COURSE AND THE FINAL PAVEMENT MARKINGS (PAINT) AND OPEN -L- AND -Y- TO THE FINAL TRAFFIC PATTERN (SEE CONSTRUCTION PLANS AND SHEET PM-1).
- STEP 4: REMOVE TRAFFIC CONTROL DEVICES.



# GENERAL NOTES & PHASING

DATE: 1/06

DWG. BY: KPB

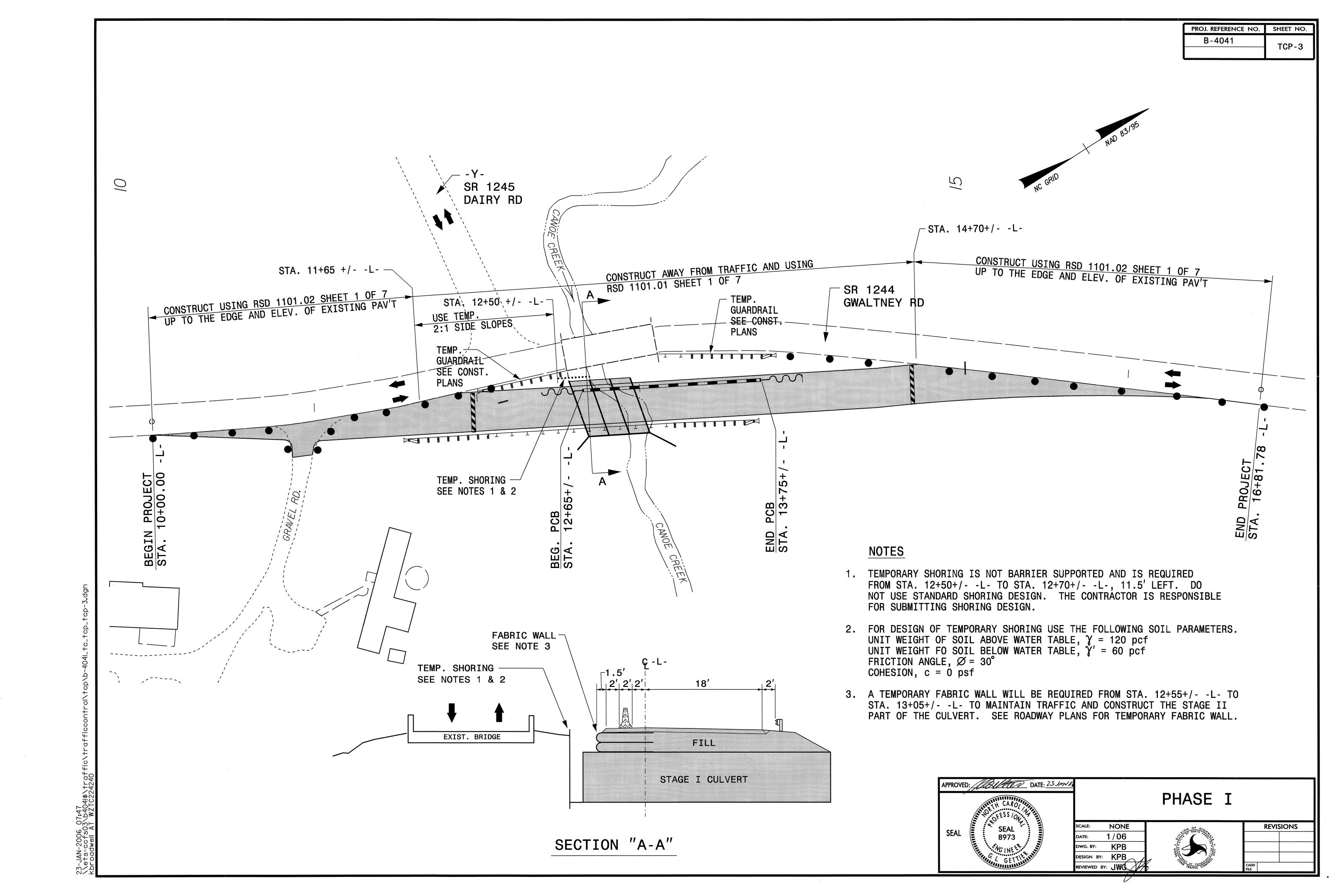
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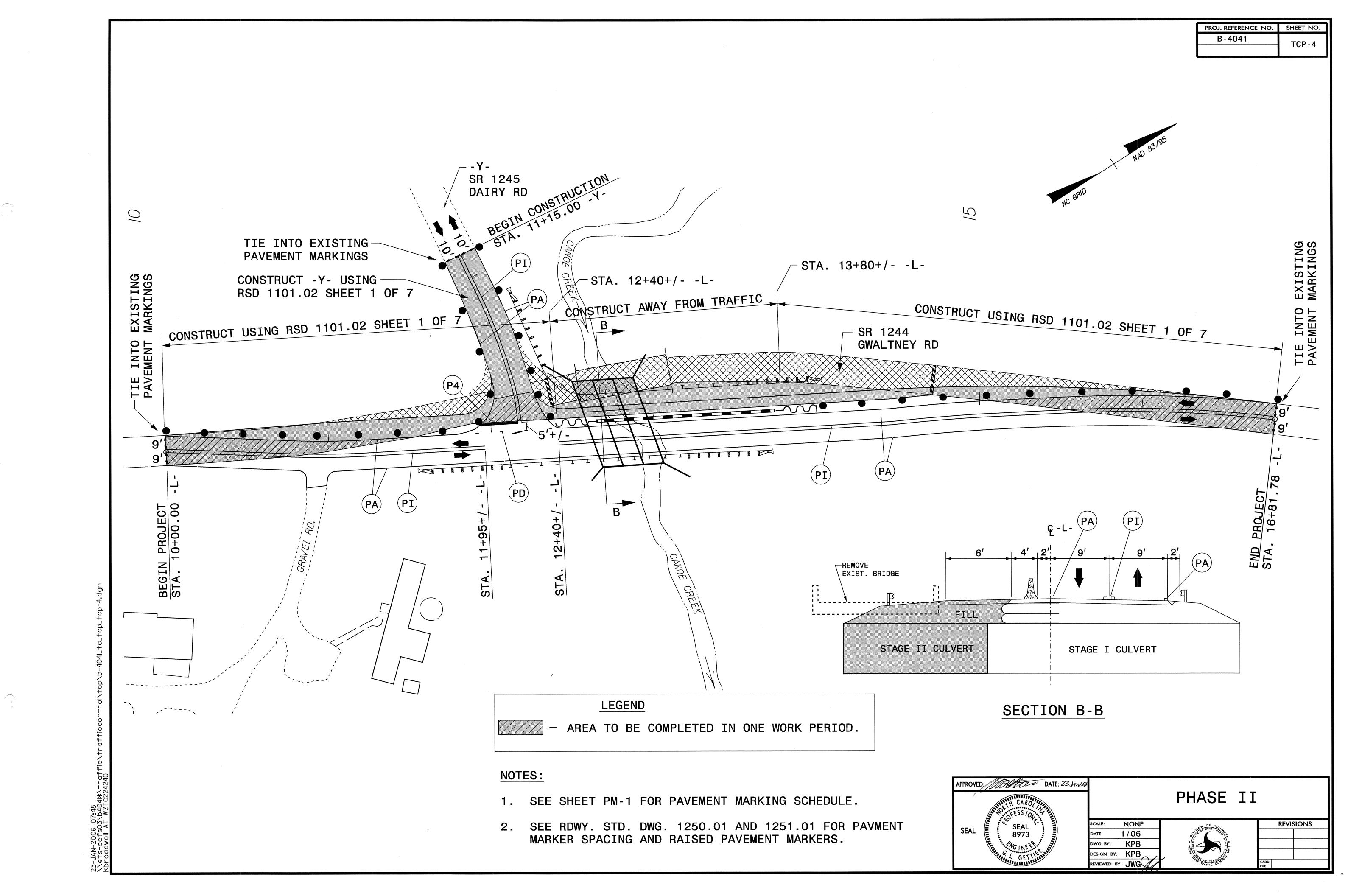
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REVISIONS

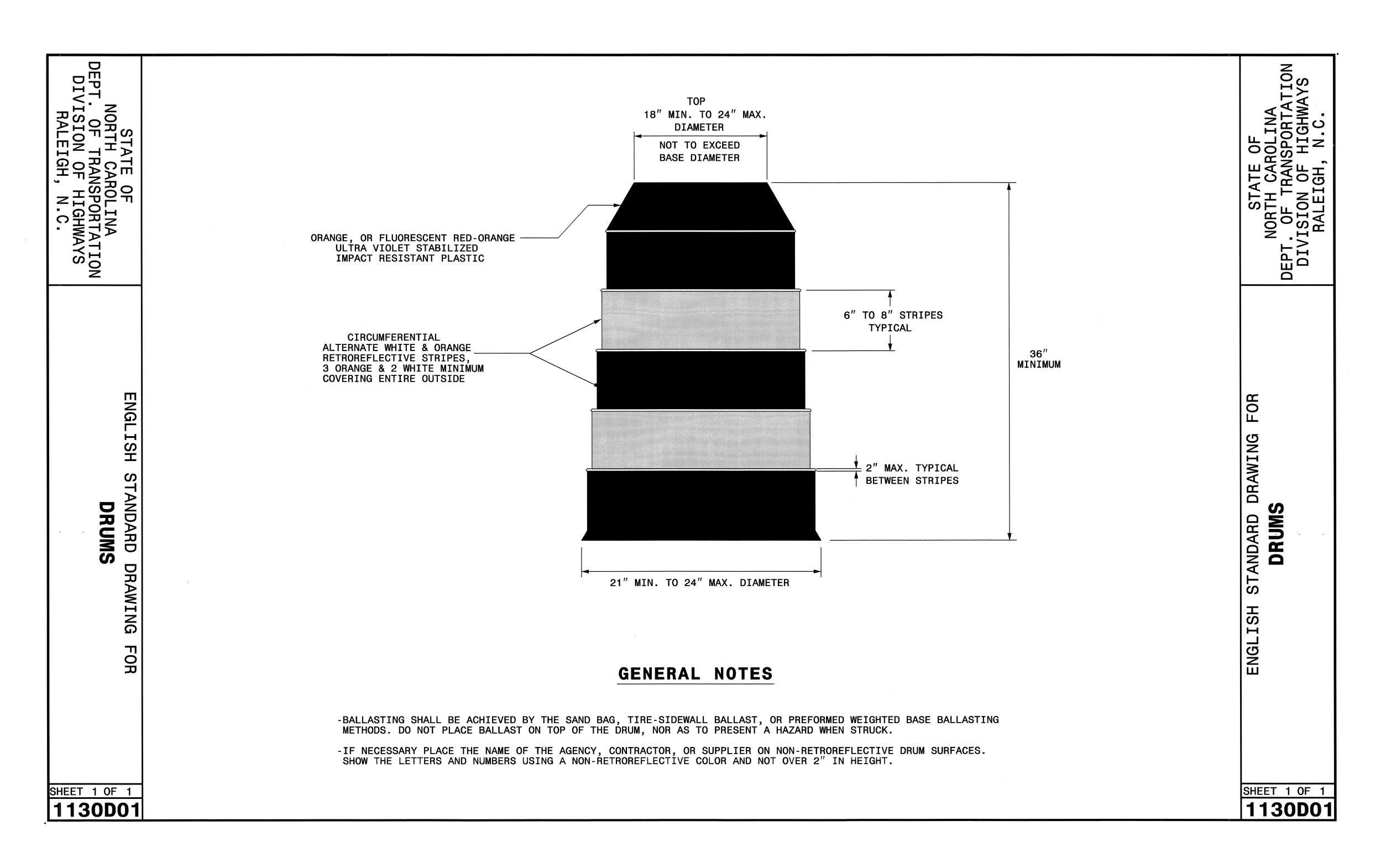
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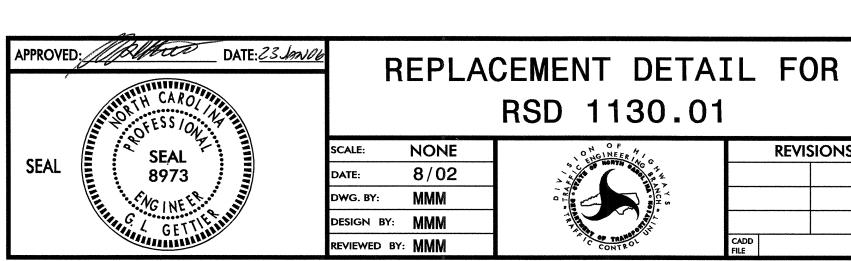




PROJ. REFERENCE NO. SHEET NO. B-4041 TCP-5

**REVISIONS** 

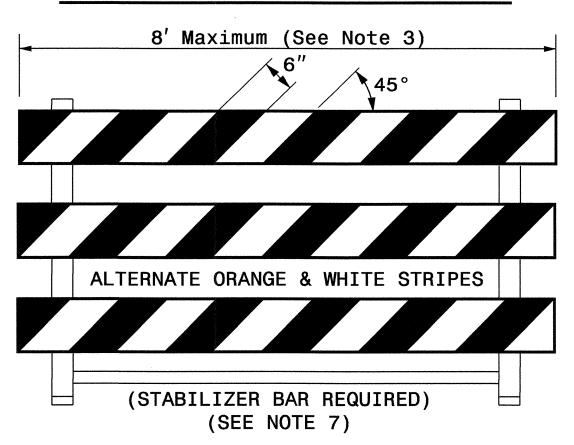




PROJ. REFERENCE NO. B-4041 TCP-6

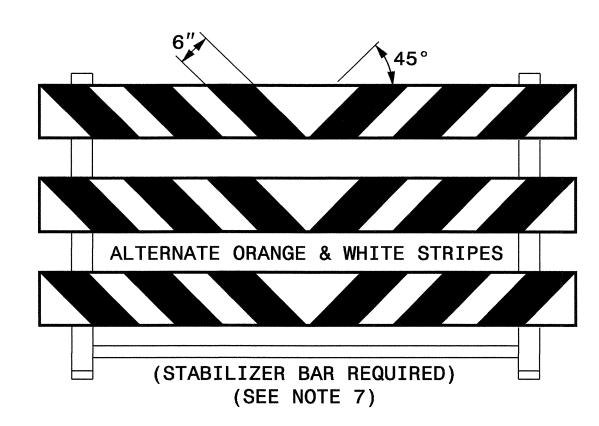
RTATION HWAYS

TYPE III BARRICADE



TYPE III BARRICADE

**END-OF-ROADWAY APPLICATIONS** 



**GENERAL NOTES** 

- 1) HORIZONTAL RAILS FOR TYPE-III BARRICADES MAY BE HOLLOW/CORRUGATED EXTRUDED RIGID POLYOLEFIN, HIGH DENSITY POLYETHYLENE, OR OTHER NCDOT APPROVED RAILS. BARRICADE RAILS OF FRANGIBLE PLASTICS SUCH AS PVC MAY NOT BE USED. IF APPROVED PLASTIC TYPE RAILS ARE USED, THEY MUST BE FLAME TREATED BY THE MANUFACTURER SO THAT REFLECTIVE SHEETING MAY ADHERE PROPERLY.
- 2) BARRICADES AND BARRICADE RAILS ARE APPROVED AS A SINGLE UNIT.
- 3) BARRICADE SHALL BE LIMITED TO A MAXIMUM LENGTH OF 8 FT UNLESS NCHRP 350 CRASH TESTED AND NCDOT APPROVED.
- 4) ONLY NCDOT APPROVED COMPOSITE AND ROLL-UP SIGNS MAY BE MOUNTED ON THE BARRICADE.
- 5) SIGNS MOUNTED ON BARRICADES SHOULD NOT COVER MORE THAN 50 PERCENT OF THE TOP TWO RAILS OR 33 PERCENT OF THE TOTAL AREA OF THE THREE RAILS.
- 6) USE TYPE VII, VIII OR IX SHEETING ON BOTH SIDES OF THE RAILS.
- 7) BARRICADE MUST BE NCHRP 350 AND NCDOT APPROVED WITH STABILIZER BAR OR ADEQUATE LATERAL BRACING.
- 8) ASSEMBLY OF THE GENERIC BARRICADES MUST BE SELF CERTIFIED BY THE ASSEMBLER.
- 9) BARRICADES USED TO CLOSE A ROADWAY SHALL EXTEND ACROSS THE ENTIRE ROADWAY. WHERE LOCAL TRAFFIC MUST BE MAINTAINED, THEY MAY BE PLACED IN A STAGGERED PATTERN.
- 10) STRIPES ON WORK ZONE BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE RETROREFLECTIVE STRIPES, SLOPED DOWNWARD TOWARDS THE SIDE WHICH TRAFFIC IS TO PASS OR TURN IN DETOURING. WHERE NO TURNS ARE INTENDED, THE STRIPES SHOULD SLOPE DOWNWARD TOWARD THE CENTER OF THE BARRICADE OR BARRICADES. USE RED AND WHITE STRIPES FOR PERMANENT BARRICADES.
- 11) SEE APPROVED PRODUCTS LIST FOR MANUFACTURERS OF APPROVED BARRICADES.
- 12) PLACE MANUFACTURER'S NAME AND FEDERAL HIGHWAY ADMINISTRATION'S NCHRP 350 APPROVAL LETTER NUMBER ON BARRICADE.
- 13) USE SANDBAGS PLACED ON THE LOWER PART OF THE FRAME FOR BALLASTING. DO NOT PLACE SANDBAGS ON TOP OF A STRIPED RAIL. DO NOT BALLAST BARRICADES BY HEAVY OBJECTS SUCH AS ROCKS, CHUNKS OF CONCRETE OR OTHER ITEMS THAT WOULD CAUSE DAMAGE IF THE BARRICADE IS STRUCK BY A VEHICLE.

**ADES** 

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SHEET 1 OF 1 1145D01

SHEET 1 OF 1 1145D01

III ADI

APPROVED: BATE: 13 Jan 100

REPLACEMENT DETAIL FOR RSD 1145.01

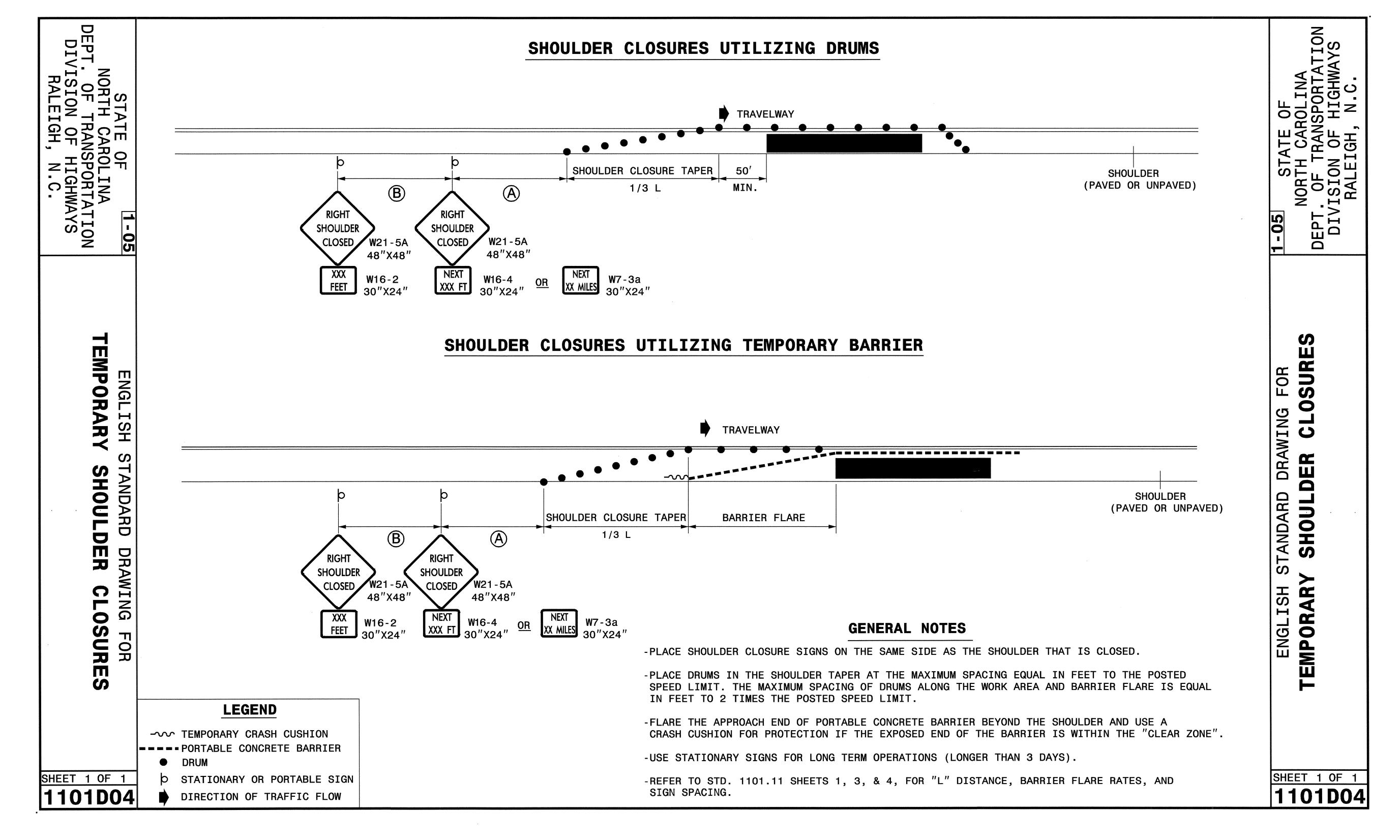
NONE 11/04 DWG. BY: MMM REVIEWED BY: MMM

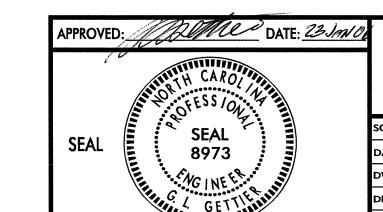


**REVISIONS** 

PROJ. REFERENCE NO. SHEET NO.

B-4041
TCP-7

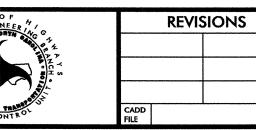


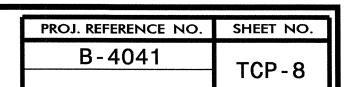


REPLACEMENT DETAIL FOR RSD 1101.04

SCALE:	NONE
DATE:	11/04
DWG. BY:	MMM
DESIGN BY:	MMM
REVIEWED BY:	MMM







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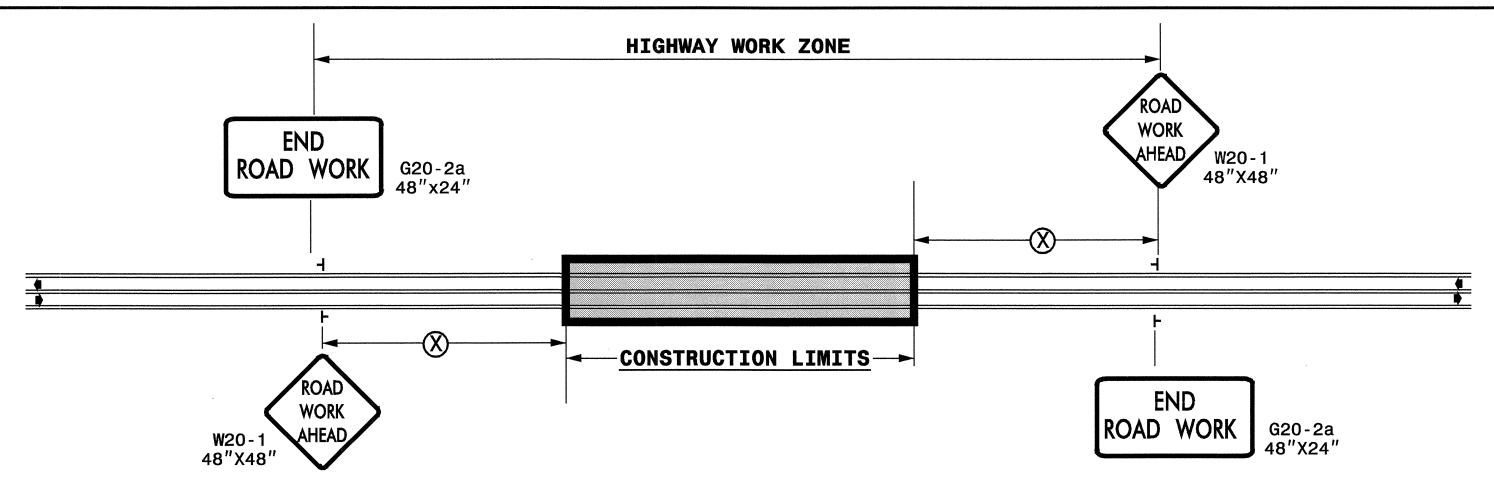
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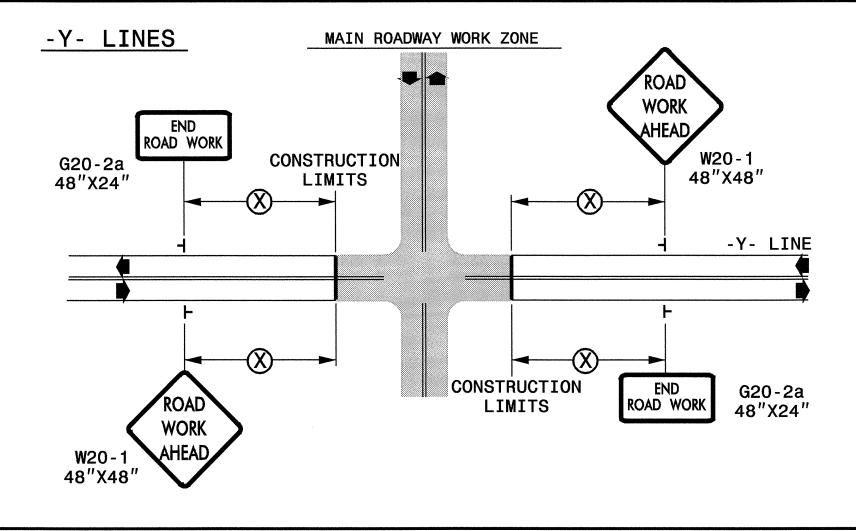




	RECOMMENDED MINIMUM SIGN SPACING
POSTED SPEED LIMIT (M.P.H.)	$\otimes$
≤ 50	500′
≥ 55	1000′

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS

### ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



### **GENERAL NOTES**

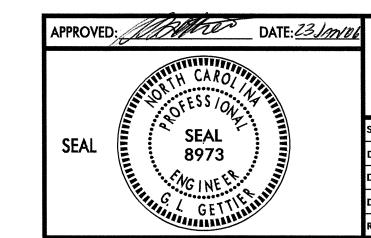
- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION. "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

### **LEGEND**

├ STATIONARY SIGN

♠ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1



DETAIL	. DRAV	WING	FOR	TWO-W	<b>I</b> AY
UNDIVID	ED A	ND U	RBAN	FREE	WAYS
ADVANCED	WORK	ZONI	E WAR	NING	SIGNS

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