

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
B-3406	TCP-1

**PLAN FOR PROPOSED  
TRAFFIC CONTROL, MARKING & DELINEATION  
AVERY COUNTY**

**B-3406**

**ROADWAY STANDARD DRAWINGS**

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.12	PAVEMENT MARKINGS - BRIDGES
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

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PM-1	FINAL PAVEMENT MARKING SCHEDULE
PM-2	FINAL PAVEMENT MARKING PLAN

**LEGEND**

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
  - NORTH ARROW
  - PROPOSED PVMT. EXIST. PVMT.
  - WORK AREA
  - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
  - TYPE II BARRICADE
  - TYPE III BARRICADE
  - CONE
  - DRUM SKINNY DRUM
  - FLASHING ARROW PANEL (TYPE C)
  - STATIONARY SIGN
  - PORTABLE SIGN
  - STATIONARY OR PORTABLE SIGN
  - CRASH CUSHION
  - CHANGEABLE MESSAGE SIGN
  - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
  - POLICE
  - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
  - YELLOW/YELLOW PAVEMENT MARKER
  - CRYSTAL/RED PAVEMENT MARKER
  - PAVEMENT MARKING SYMBOLS

**TEMPORARY PAVEMENT MARKING SCHEDULE**

SYMBOL	DESCRIPTION	PAY ITEM	PAY ITEM QUANTITY BREAKDOWN	TOTAL QUANTITY
		PAVEMENT MARKING LINES PAINT (4")		
PA	WHITE EDGELINE (2X)		2,000 LF	
PI	YELLOW DOUBLE CENTER (2X)		2,000 LF	
				4,000 LF

**TIP PROJECT:**

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C:\Users\jrd\Documents\TrafficControl\TCP\B3406\_tcp\_tsh\_01.dgn  
jrd

APPROVED: DATE: 8/15/06	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT  S. BOURNE, P.E. TRAFFIC CONTROL ENGINEER J. ISHAK, P.E. TRAFFIC CONTROL PROJECT ENGINEER J. PORTANOVA, P.E. TRAFFIC CONTROL PROJECT DESIGN ENGINEER E. GROUNDWATER TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN	Prepared in the Office of: <b>EarthTech</b> <small>A Tyco International Ltd. Company</small> 701 Corporate Center Drive, Suite #475; Raleigh, NC 27607 Phone: (919) 854-6200 Fax: (919) 854-6259  NEIL J. DEAN, P.E. PROJECT ENGINEER
SEAL 		

# PROJECT NOTES

## GENERAL NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

### TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME  
SR 1321 (CURTIS CREEK ROAD)

WEDNESDAY FROM 6:00 PM TO 8:00 PM  
SUNDAYS FROM 12:01 AM TO 11:59 PM

B) DO NOT STOP TRAFFIC FOR MORE THAN 15 MINUTES AS FOLLOWS:

ROAD NAME	OPERATION
SR 1321 (CURTIS CREEK ROAD)	WATERLINE CROSSING REPLACEMENT

### LANE AND SHOULDER CLOSURE REQUIREMENTS

C) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.

D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

G) DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.

H) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.

I) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

### PAVEMENT EDGE DROP OFF REQUIREMENTS

J) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

K) DO NOT EXCEED A DIFFERENCE OF 1.5 inches IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING \*UNEVEN LANES\* SIGNS (WB-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

### TRAFFIC PATTERN ALTERATIONS

L) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

### SIGNING

M) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

N) PROVIDE PERMANENT SIGNING

O) PROVIDE DETOUR SIGNING WITHIN AND OFF THE PROJECT LIMITS.

P) THE CONTRACTOR WILL COVER OR REMOVE ALL DETOUR SIGNS WITHIN AND OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION

Q) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

### TRAFFIC CONTROL DEVICES

R) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.

S) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

T) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

### PAVEMENT MARKINGS AND MARKERS

U) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
SR 1321 (CURTIS CREEK RD)	PAINT	NONE

V) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
SR 1321 (CURTIS CREEK RD)	PAINT	NONE

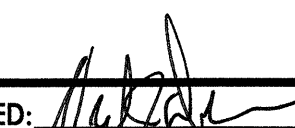
W) PLACE AT LEAST TWO APPLICATIONS OF PAINT PAVEMENT MARKINGS ON THE FINAL WEARING SURFACE ON NEW ASPHALT PAVEMENT. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

X) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

Y) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.

Z) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

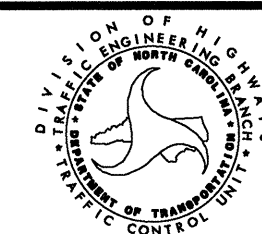
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APPROVED:  DATE: 8/21/06



## PROJECT NOTES

SCALE: NONE  
DATE: AUGUST, 2006  
DWG. BY: BRP  
DESIGN BY: BRP  
REVIEWED BY: NJD



REVISIONS	

Prepared in the Office of:  
  
A Tyco International Ltd. Company  
701 Corporate Center Drive, Suite #475; Raleigh, NC 27607  
Phone: (919) 854-6200 Fax: (919) 854-6259



# PHASING

NOTE: IN ALL STEPS AND IN ALL PHASES, RETURN TRAFFIC TO TWO LANE-TWO WAY OPERATION BY THE END OF EACH WORKDAY

## PHASE I

PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY, INSTALL WORK ZONE ADVANCED WARNING SIGNS AS SHOWN IN THE STANDARD DETAILS, TCP-7.

STEP 1 MAINTAIN THE EXISTING TRAFFIC PATTERN ON CURTIS CREEK ROAD (-L-) AS SHOWN ON TCP-4. THIS INCLUDES MAINTAINING INGRESS AND EGRESS TO THE PRIVATE ENTRANCES ALONG CURTIS CREEK RD (-L-) AT ALL TIMES.

STEP 2 CONSTRUCT THE PROPOSED DETOUR AWAY FROM TRAFFIC FROM STA 11+15 TO STA 16+10, INCLUDING THE CONSTRUCTION OF THE TEMPORARY BRIDGE OVER CURTIS CREEK. PLACE TEMPORARY MARKINGS (PAINT) ON THE NEWLY CONSTRUCTED DETOUR AS SHOWN ON TCP-5.

WHEN THE CONSTRUCTION IS ADJACENT TO THE EXISTING EASTBOUND TRAVEL LANE OF CURTIS CREEK RD (-L-), USE RDWY STD 1101.02 SHT 1 OF 9 FOR LANE CLOSURES. REMOVE THE EASTBOUND LANE CLOSURE AND RETURN TRAFFIC TO THE EXISTING PATTERN BY THE END OF EACH WORKDAY.

STEP 3 USING RDWY STD 1101.02 SHT 1 OF 9 FOR LANE CLOSURES AS NEEDED, SHIFT TRAFFIC FROM EXISTING CURTIS CREEK RD (-L-) TO THE DETOUR AS FOLLOWS:

WORKING IN A CONTINUOUS MANNER, STOP EASTBOUND TRAFFIC AND RELOCATE TRAFFIC CONTROL DEVICES AS NEEDED AT EITHER END OF DETOUR TIE-IN LOCATIONS. SHIFT THE EASTBOUND TRAFFIC ONTO THE DETOUR. PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AT THE TIE-IN LOCATIONS AS NEEDED AND OBLITERATE ANY CONFLICTING PAVEMENT MARKINGS. SHIFT THE WESTBOUND TRAFFIC ONTO THE FINAL DETOUR PATTERN.

USING RDWY STD DWG 1101.03 SHEET 3 OF 9 FOR ROAD CLOSURES, INSTALL TRAFFIC CONTROL DEVICES TO CLOSE EXISTING CURTIS CREEK ROAD (-L-).

## PHASE II

STEP 1 MAINTAIN TRAFFIC ON THE CURTIS CREEK RD DETOUR (-DETOUR-) AND MAINTAIN INGRESS AND EGRESS TO THE PRIVATE DRIVES ALONG CURTIS CREEK RD (-L-).

CONSTRUCT THE PROPOSED CURTIS CREEK RD (-L-) AWAY FROM TRAFFIC UP TO AND INCLUDING THE FINAL LAYER OF SURFACE COURSE AS SHOWN ON TCP-5. THIS INCLUDES THE CONSTRUCTION OF THE PROPOSED BRIDGE STRUCTURE OVER CURTIS CREEK AND THE INSTALLATION OF THE PROPOSED GUARDRAIL AND IMPACT ATTENUATORS.

WHEN THE CONSTRUCTION IS ADJACENT TO THE WESTBOUND TRAVEL LANE OF THE DETOUR, USE RDWY STD 1101.02 SHT 1 OF 9 FOR LANE CLOSURES. REMOVE THE LANE CLOSURE AND RETURN TO THE EXISTING TRAFFIC PATTERN BY THE END OF EACH WORKDAY.

STEP 2 PLACE THE FINAL PAVEMENT MARKINGS (PAINT) ON THE NEWLY CONSTRUCTED ROADWAY AND BRIDGE FOR THE FINAL TRAFFIC PATTERN.

## PHASE III

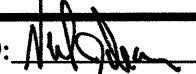

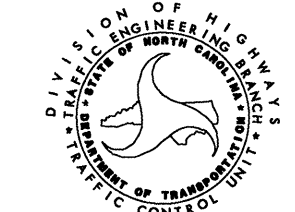
COMPLETE CONSTRUCTION OF CURTIS CREEK RD (-L-) PREVIOUSLY BEGUN.

STEP 1 USING RDWY STD 1101.02 SHT 1 OF 9 FOR LANE CLOSURES AS NEEDED, SHIFT TRAFFIC FROM THE CURTIS CREEK RD DETOUR TO THE PROPOSED CURTIS CREEK RD (-L-) AS FOLLOWS:

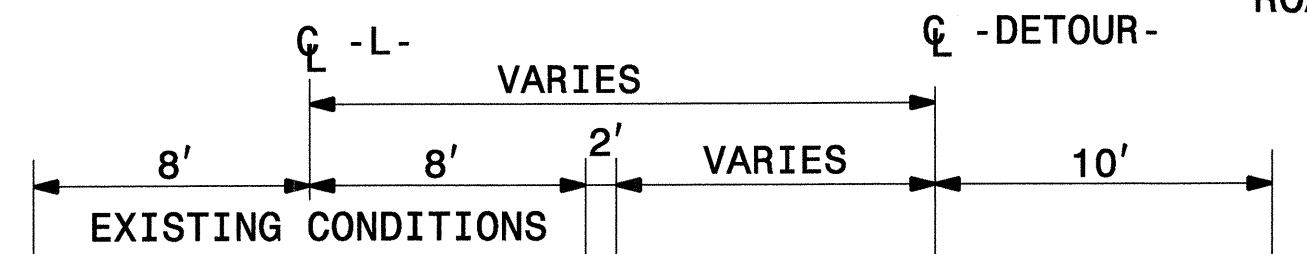
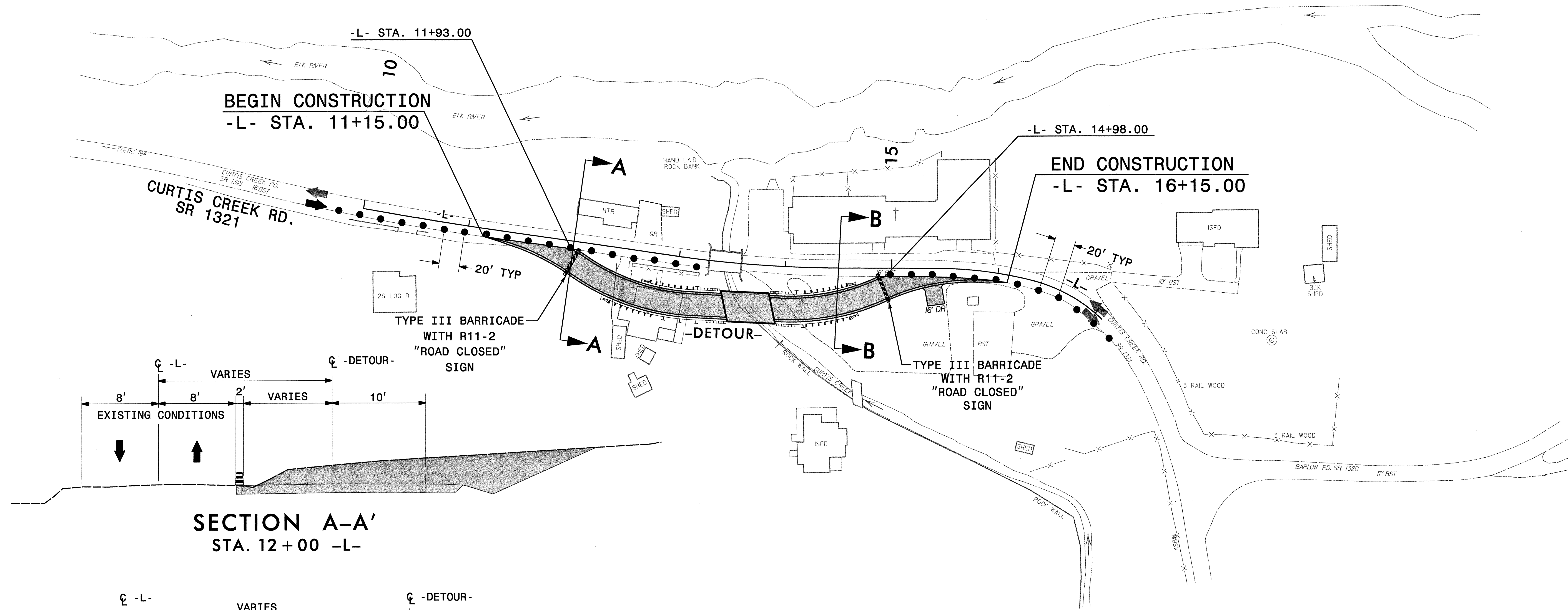
WORKING IN A CONTINUOUS MANNER, STOP WESTBOUND TRAFFIC AND RELOCATE TRAFFIC CONTROL DEVICES AS NEEDED AT EITHER END OF DETOUR TIE-IN LOCATIONS. SHIFT THE WESTBOUND TRAFFIC ONTO THE NEWLY CONSTRUCTED ROADWAY. PLACE PAVEMENT MARKINGS (PAINT) AT THE TIE-IN LOCATIONS AS NEEDED. OBLITERATE ANY CONFLICTING PAVEMENT MARKINGS. SHIFT THE EASTBOUND TRAFFIC ONTO THE NEW ROADWAY IN THE FINAL PATTERN.

STEP 2 OPEN CURTIS CREEK RD (-L-) TO TRAFFIC IN THE FINAL TRAFFIC PATTERN. OBLITERATE THE CURTIS CREEK RD DETOUR (-DETOUR-).

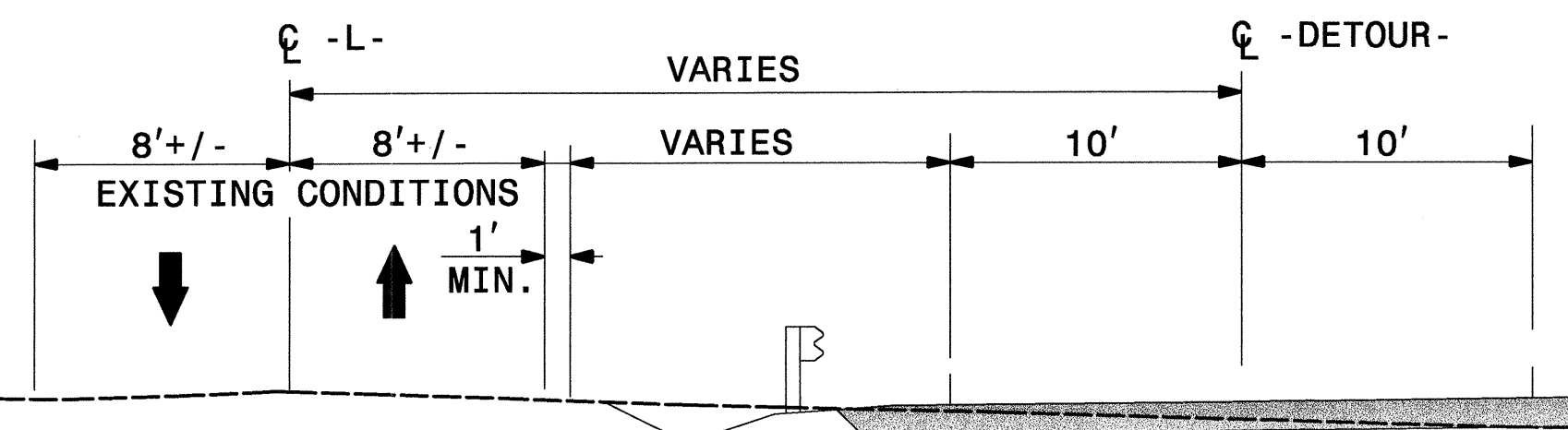
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APPROVED:  DATE: 8/15/06	<b>PHASING</b>	
	SCALE: NONE	
	DATE: AUGUST, 2006	
	DWG. BY: BRP	
	DESIGN BY: BRP	
REVIEWED BY: NJD	REVISIONS	

Prepared in the Office of: **EarthTech**  
 A Tyco International Ltd. Company  
 701 Corporate Center Drive, Suite #475; Raleigh, NC 27607  
 Phone: (919) 854-6200 Fax: (919) 854-6259



**SECTION A-A'**  
STA. 12+00 -L-



**SECTION B-B'**  
STA. 14+50 -L-

APPROVED: *[Signature]* DATE: 9/29/06

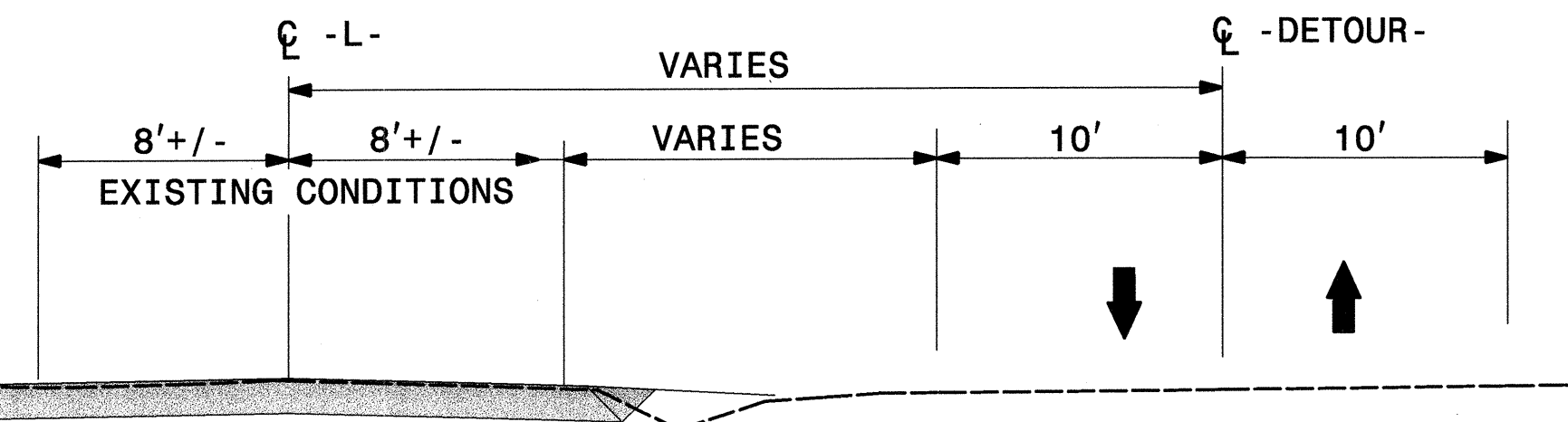
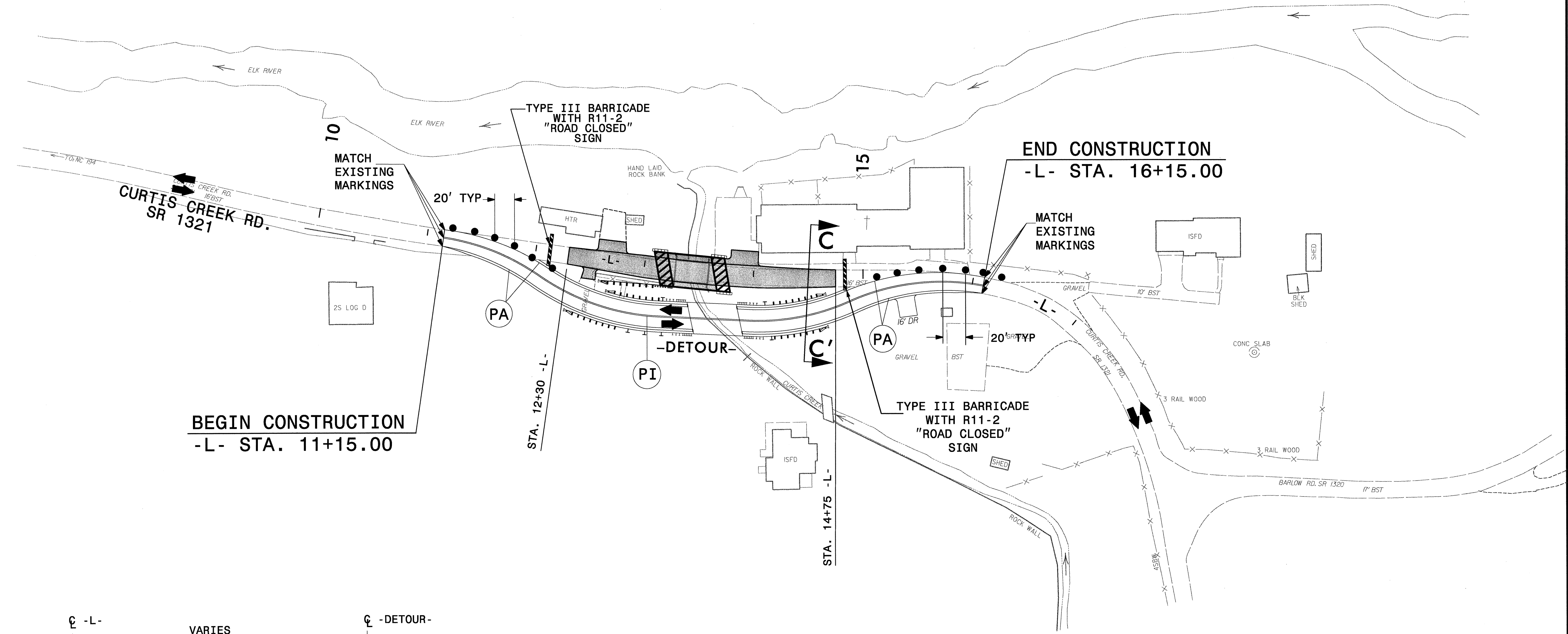


**PHASE I DETAIL**

Prepared in the Office of:  
**EarthTech**  
A Tyco International Ltd. Company  
701 Corporate Center Drive, Suite #475, Raleigh, NC 27607  
Phone: (919) 854-6200 Fax: (919) 854-6259

SCALE: NONE		REVISIONS
DATE: AUGUST, 2006		
DWG. BY: BRP		
DESIGN BY: BRP		
REVIEWED BY: NJD		

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**SECTION C-C'**  
STA. 14+50 -L-

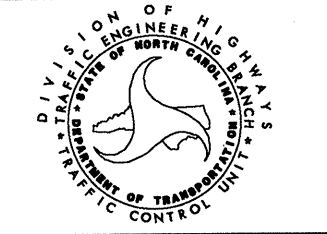
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**PHASE II DETAIL**

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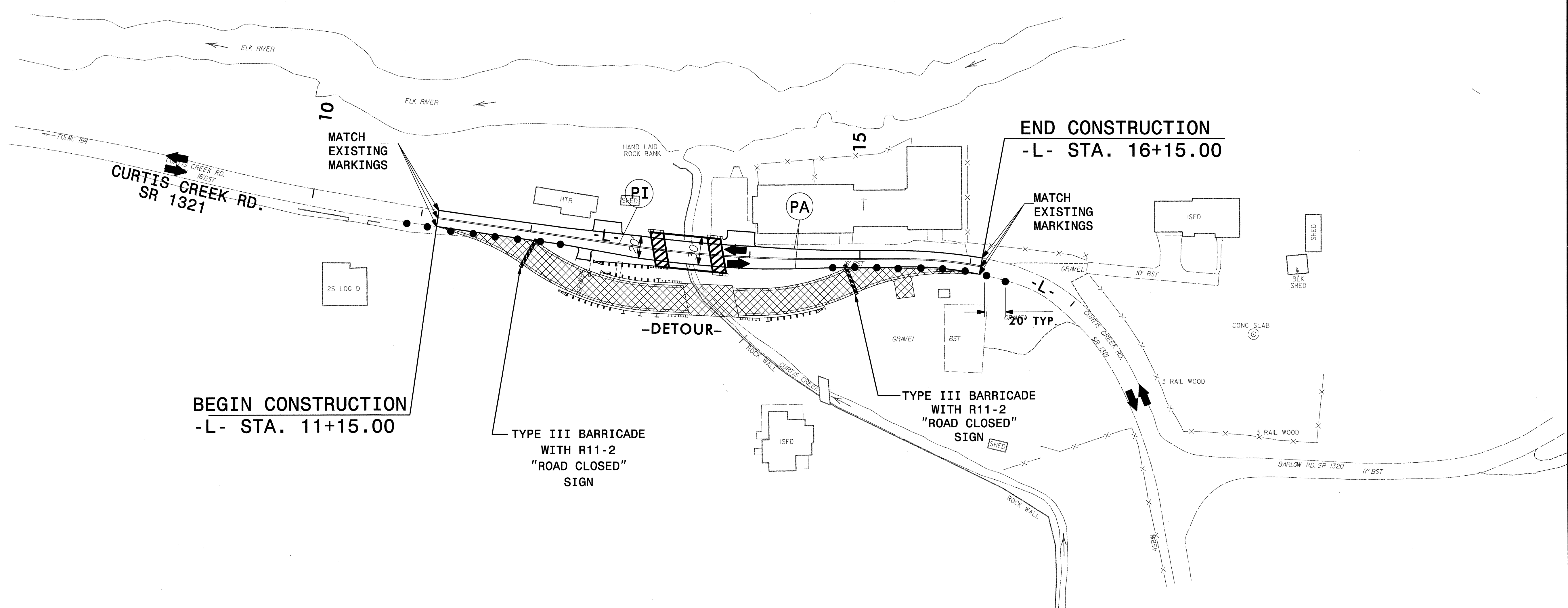
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DWG. BY: BRP  
DESIGN BY: BRP  
REVIEWED BY: NJD



REVISIONS	

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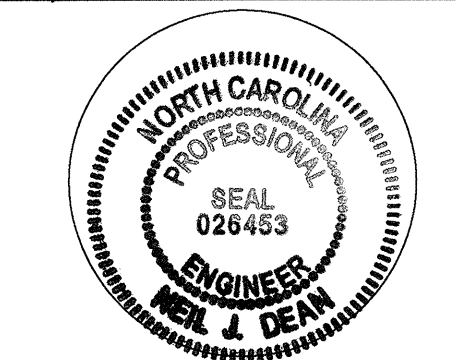
BEGIN CONSTRUCTION  
-L- STA. 11+15.00

END CONSTRUCTION  
-L- STA. 16+15.00

TYPE III BARRICADE  
WITH R11-2  
"ROAD CLOSED"  
SIGN

TYPE III BARRICADE  
WITH R11-2  
"ROAD CLOSED"  
SIGN

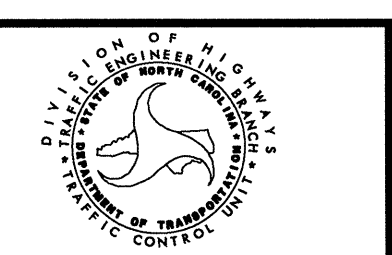
APPROVED: [Signature] DATE: 8/19/06



### PHASE III DETAIL

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A Tyco International Ltd. Company  
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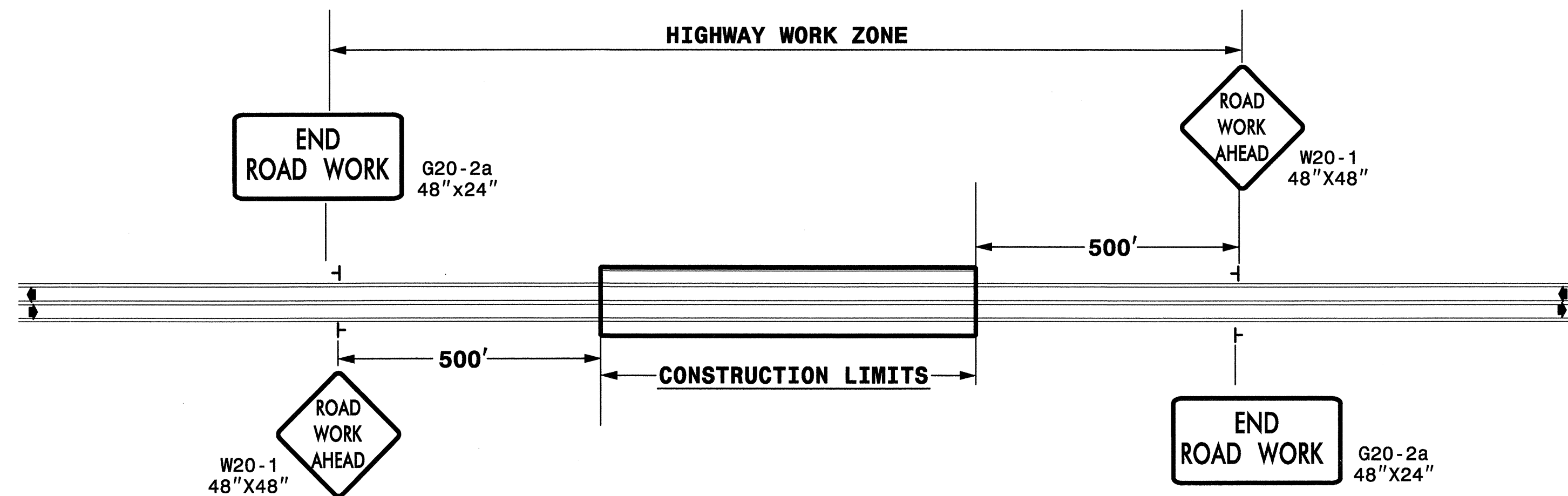
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REVIEWED BY: NJD



REVISIONS	

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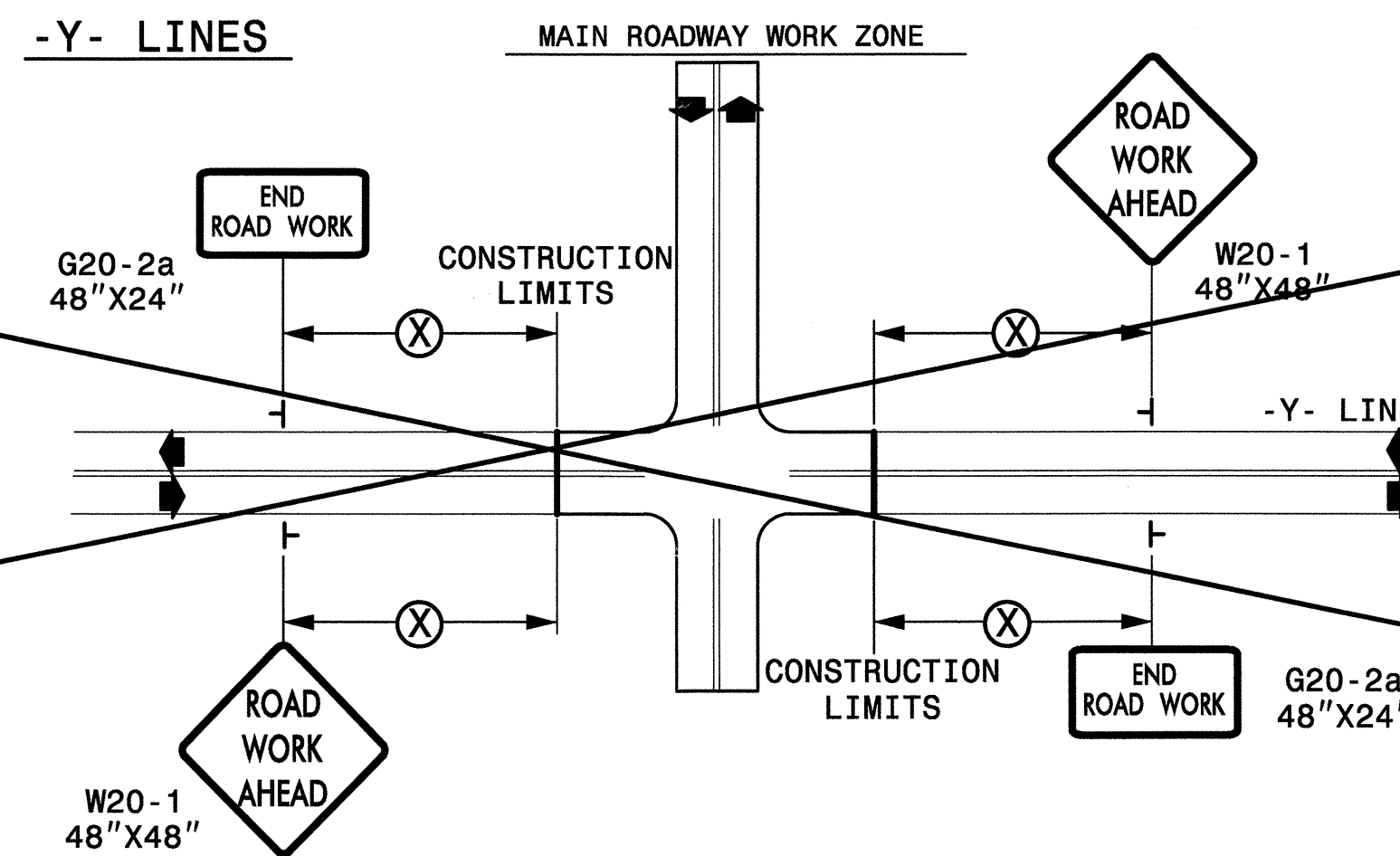
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



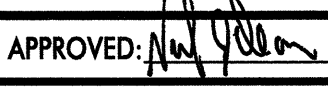

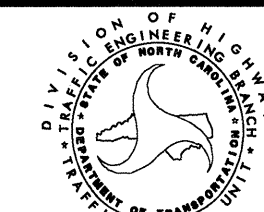
**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND	
⊥	STATIONARY SIGN
◀	DIRECTION OF TRAFFIC FLOW

DETAIL DRAWING FOR  
TWO-WAY UNDIVIDED  
WORK ZONE WARNING SIGNS

SHEET 1 OF 1

APPROVED: 	DATE: 01/01/04	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS		
	SCALE: NONE			
	DATE: 10-98			REVISIONS
	DESIGN BY: 03/04			7-98 10/01
	REVIEWED BY: 11/04			01/01 11/04

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