

GENERAL NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- A) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- B) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT (1.5m) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- C) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- D) DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- E) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:
 - BACKFILL DROP-OFFS THAT EXCEED 2 INCHES (50mm) ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
 - BACKFILL DROP-OFFS THAT EXCEED 3 INCHES (75mm) ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
 - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- F) DO NOT EXCEED A DIFFERENCE OF 1.5 inches (40mm) IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT (150m) IN ADVANCE OF THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- G) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- H) STATE FORCES WILL BE RESPONSIBLE FOR PERMANENT SIGNING.
- I) PROVIDE ALL SIGNING WITHIN THE PROJECT LIMITS.
- J) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

- K) WHEN USING ROADWAY STANDARD NO. 1101.02, DRUMS MAY BE USED IN LIEU OF CONES ON SR 1212 (OLD HOMESTEAD RD).
- L) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT (3m) ON-CENTER IN RADII, AND 3 FT (1m) OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.
- M) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.
- N) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT (150m) CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

PAVEMENT MARKINGS AND MARKERS

- O) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. SR 1212 (OLD HOMESTEAD RD)	PAINT	NONE
- P) PLACE AT LEAST TWO APPLICATIONS OF PAINT PAVEMENT MARKINGS ON THE FINAL WEARING SURFACE ON NEW ASPHALT PAVEMENT. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.
- Q) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- R) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.
- S) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

PHASING

PHASE I

STEP 1)
INSTALL THE ADVANCE WORK ZONE WARNING SIGNS ON SR 1212. IF WORK IS NOT PURSUED WITHIN ONE WEEK OF SIGN INSTALLATION, THE SIGNS SHALL BE COVERED OR REMOVED IN A METHOD APPROVED BY THE ENGINEER ACCORDING TO STANDARD SPEC SECTION 1110, AT NO COST TO THE DEPARTMENT.

STEP 2)
CONSTRUCT THE DETOUR CULVERT AND ALIGNMENT FROM STA. 10+42 +/- -DET- TO STA. 16+33 +/- -DET- (SEE ROADWAY STANDARD DRAWING (RSD) 1101.02, SHEET 1 OF 7 AS NEEDED, AND TCP-2 LOCAL NOTE 1). INSTALL THE 30" CMP AT STA. 15+00 -L- AND INSTALL SIGNS AND MARKINGS (SEE TCP-3).

STEP 3)
USING RSD 1101.02, SHEET 1 OF 7, REMOVE EXISTING MARKINGS ON EXISTING FROM STA. 10+42 +/- -L- TO STA. 11+35 -L- AND FROM STA. 14+50 -L- TO STA. 16+44 -L-. SHIFT TRAFFIC ONTO -DET- AND INSTALL THE REMAINING TEMPORARY PAVEMENT MARKINGS ON EXISTING AND -DET- FROM STA. 10+42 +/- -DET- STA. 11+04 -DET- AND FROM STA. 15+36 -DET- TO STA. 16+70 -DET- (SEE TCP-3).

STEP 4)
USING RSD 1101.03, SHEET 3 OF 9, CLOSE EXISTING SR 1212.

- MAINTAIN ACCESS TO DRIVEWAYS AT ALL TIMES WITH INCIDENTAL STONE.

PHASE II

STEP 1)
AWAY FROM TRAFFIC CONSTRUCT -L- INCLUDING THE BRIDGE UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE, FINAL PAVEMENT MARKINGS, AND FINAL PAVEMENT MARKERS (SEE TCP-3, AND TCP-2 LOCAL NOTE 1).

STEP 2)
USING RSD 1101.02, SHEET 1 OF 7, REMOVE AND REPLACE TEMPORARY PAVEMENT MARKINGS FROM STA. 10+42 +/- -L- TO STA. 16+44 -L-, THE TEMPORARY RAILROAD CROSSING PAVEMENT MARKINGS AT STA. 15+31 -L-, AND SHIFT TRAFFIC ONTO -L-.

STEP 3)
AWAY FROM TRAFFIC AND USING RSD 1101.02, SHEET 1 OF 7 AS NEEDED REMOVE -DET-.

STEP 4)
REMOVE DETOUR SIGNS AND USING RSD 1101.02, SHEET 1 OF 7, COMPLETE THE FOLLOWING:

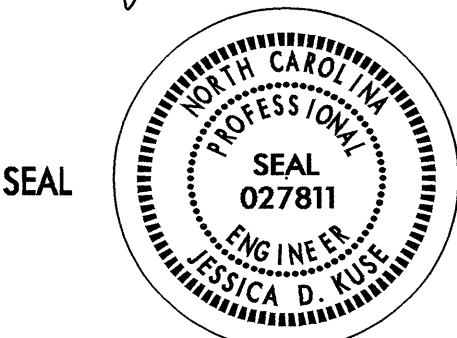
- INSTALL THE FINAL LAYER OF SURFACE COURSE
- INSTALL THE FINAL PAVEMENT MARKINGS FROM STA. 10+42 +/- -L- TO STA. 16+44 -L-
- INSTALL THE FINAL RAILROAD CROSSING PAVEMENT MARKINGS AT STA. 15+31 -L-
- RE-STRIPE THE EXISTING RAILROAD CROSSING MARKINGS AT STA. 9+85 -L-

STEP 5)
REMOVE REMAINING SIGNS AND TRAFFIC CONTROL DEVICES ON PROJECT LIMITS.

LOCAL NOTES

- 1) COORDINATE WITH THE RAILROAD DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER.

31-JAN-2005 15:27
\\nets-ccf\50N\B3663\TCP\B3663.TC.TCP2.dgn
chayes AT TET09272

APPROVED: <i>Jessica D. Kuss</i> DATE: 9/10/05	PROJECT NOTES AND PHASING	
	SCALE: NONE	REVISIONS
	DATE: 09/04	
	DWG. BY: DAH	
	DESIGN BY: DAH	
REVIEWED BY: JDK		