

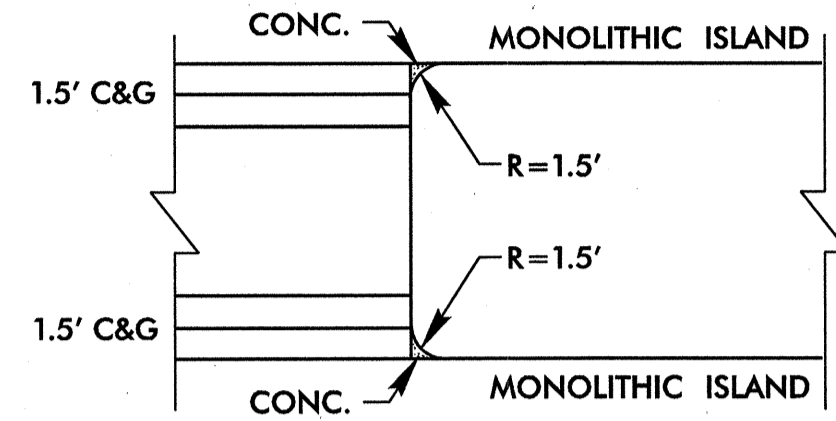
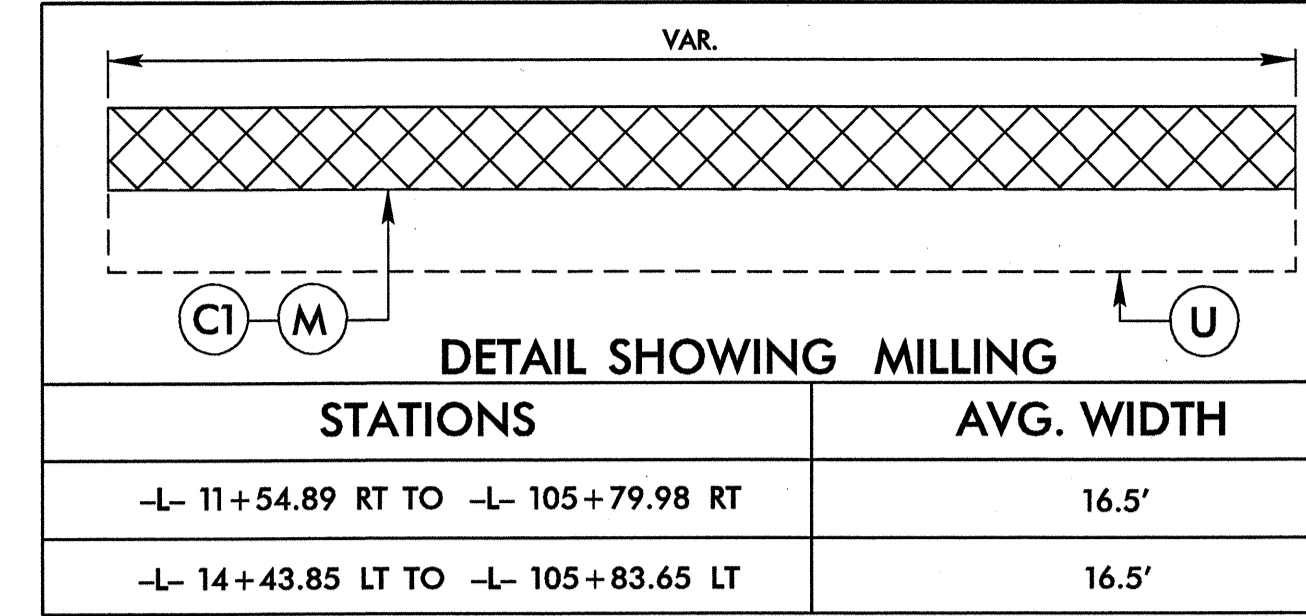
5/28/99

REVISIONS

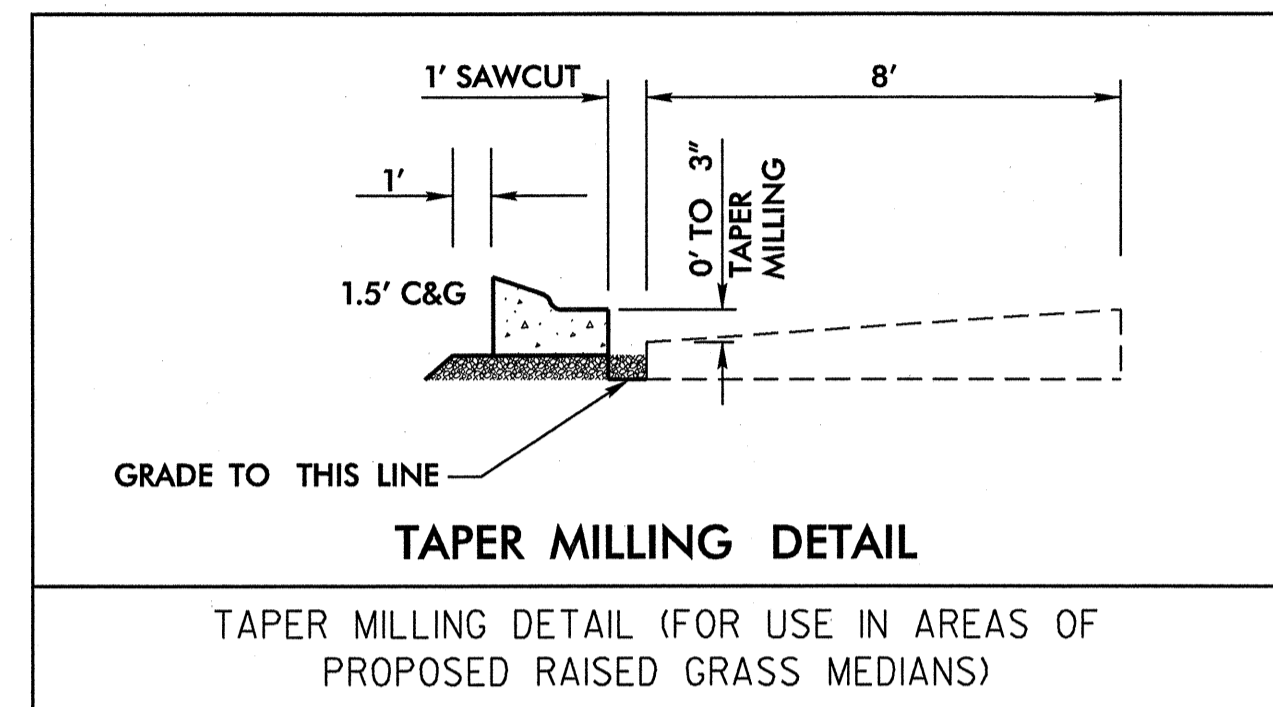
PAVEMENT SCHEDULE

C1	PROPOSED APPROXIMATELY 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS SQ. YARD.
D1	PROPOSED APPROXIMATELY 3" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 342 LBS SQ. YARD.
E1	PROPOSED APPROXIMATELY 5" ASPHALT CONCRETE BASE COURSE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS SQ. YARD.
E2	PROPOSED APPROXIMATELY 5 1/2" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS SQ. YARD.
J	5" AGGREGATE BASE COURSE
R1	2' - 6" CONCRETE CURB AND GUTTER
R2	1' - 6" CONCRETE CURB AND GUTTER
R3	5" MONOLITHIC CONCRETE ISLAND (SURFACE MOUNTED)
T	EARTH MATERIAL
M	MILLING BITUMINOUS PAVEMENT. 1 1/2" DEPTH.
U	EXISTING PAVEMENT.
W	WEDGING DETAIL (SHEET 2A)

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE



USE THESE DETAILS FOR TRANSITION BETWEEN RAISED CONCRETE MONOLITHIC ISLAND AND 1.5' C&G



USE TYPICAL SECTION No. 1 FOR THE FOLLOWING

- L- STA. 47+02.79 TO STA. 47+90.00
- L- STA. 57+95 TO STA. 71+50

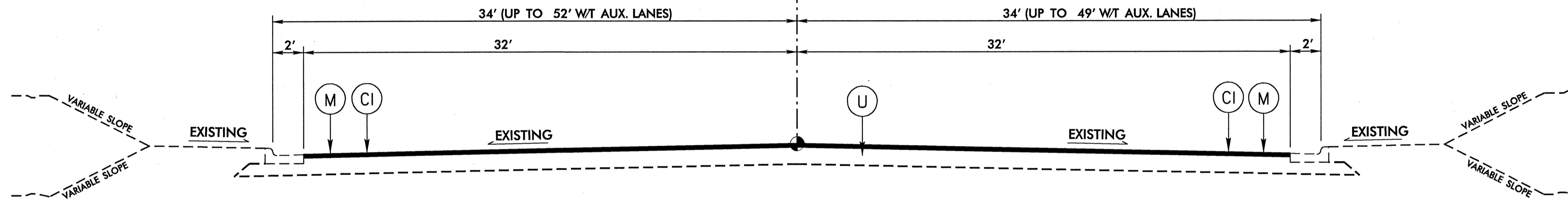
USE TYPICAL SECTION No. 2 FOR THE FOLLOWING

- L- STA. 11+54.89 RT TO STA. 16+43.67 RT
- L- STA. 13+44.37 LT TO STA. 16+43.67 LT
- L- STA. 20+36 TO STA. 23+20.35 (TRANSITION TO TYP # 3)
- L- STA. 26+88.79 TO STA. 28+61.21 (TRANSITION TO TYP # 3)
- L- STA. 32+71 TO STA. 36+88.67 (TRANSITION TO TYP # 3)
- L- STA. 40+96 TO STA. 43+40 (TRANSITION TO TYP # 3)
- L- STA. 51+4.98 TO STA. 54+38.93 (TRANSITION TO TYP # 3)
- L- STA. 71+50 TO STA. 73+73.92 (TRANSITION TO TYP # 3)
- L- STA. 77+71.06 TO STA. 82+83.81 (TRANSITION TO TYP # 3)
- L- STA. 90+01 TO STA. 101+38.92 (TRANSITION TO TYP # 3)

NOTE: LEFT ONLY TURN LANE ISLANDS; LEFT AND RIGHT OVERLAP BETWEEN TYPICALS 3 & 4.

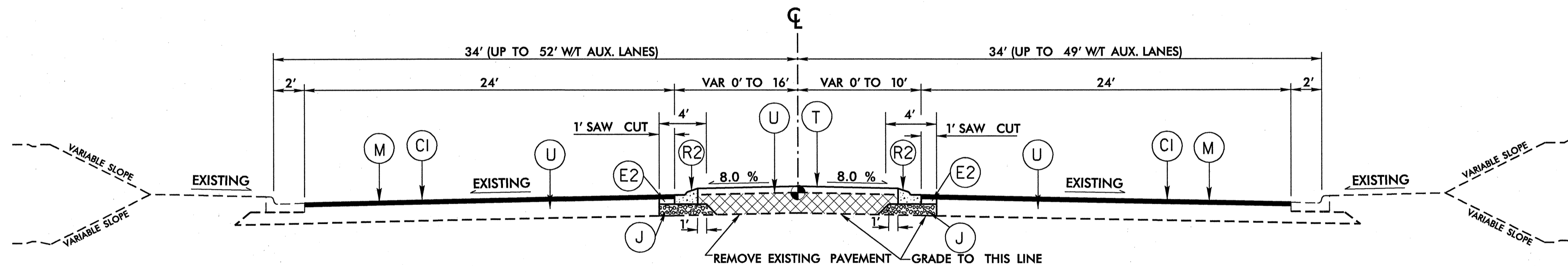
USE TYPICAL SECTION No. 3 FOR THE FOLLOWING

- L- STA. 16+43.67 TO STA. 17+55.03
- L- STA. 23+20.35 TO STA. 26+15
- L- STA. 28+61.21 TO STA. 29+80.00
- L- STA. 36+88.67 TO STA. 38+05.00
- L- STA. 43+40.00 TO STA. 47+02.79
- L- STA. 54+38.93 TO STA. 57+95.00
- L- STA. 73+73.92 TO STA. 74+65.00
- L- STA. 82+83.81 TO STA. 85+40.00
- L- STA. 85+77.78 TO STA. 87+10.00
- L- STA. 101+38.92 TO STA. 105+79.98



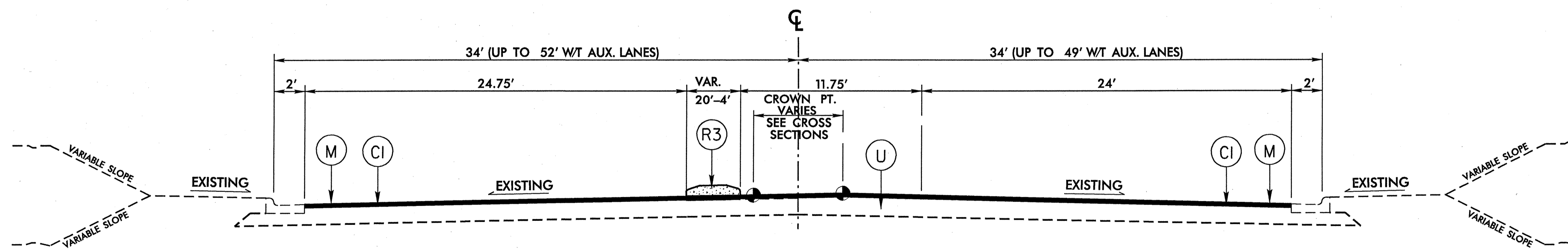
TYPICAL SECTION No. 1

(-L- LINE BRAGG BOULEVARD)



TYPICAL SECTION No. 2

(-L- LINE BRAGG BOULEVARD)



TYPICAL SECTION No. 3

(-L- LINE BRAGG BOULEVARD)

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PROJECT REFERENCE NO. U-4719 SHEET NO. 2

RAW SHEET NO.

ROADWAY DESIGN ENGINEER: JAMES M. MORRISON, PROFESSIONAL ENGINEER, SEAL 022091

PAVEMENT DESIGN ENGINEER: W. BURNS, PROFESSIONAL ENGINEER, SEAL 17247

PB PARSONS BRINCKERHOFF